



**BY-LAW NO. 2023-0089**

A By-law to adopt Amendment No. 50 to the Official Plan of the Town of Halton Hills – Premier Gateway Employment Area Phase 2B Secondary Plan.

**WHEREAS** Council of the Corporation of the Town of Halton Hills is empowered to enact this By-law by virtue of the provisions of the Planning Act, R.S.O. 1990, c. P.13, as amended;

**AND WHEREAS** the Regional Municipality of Halton, as the approval authority, has exempt this Official Plan Amendment from approval;

**AND WHEREAS** on October 10, 2023, Council for the Town of Halton Hills adopted Report No. PD-2023-0049, dated September 07, 2023, in which certain recommendations were made relating to the Premier Gateway Phase 2B Employment Area Secondary Plan.

**NOW, THEREFORE, BE IT RESOLVED THAT THE COUNCIL OF THE CORPORATION OF THE TOWN OF HALTON HILLS ENACTS AS FOLLOWS:**

1. That Amendment No. 50 to the Official Plan of the Town of Halton Hills, being the attached text and schedules, is hereby adopted.
2. That the Town Clerk is hereby authorized and directed to make an application to the Region of Halton for the approval of Amendment No. 50 to the Official Plan of the Town of Halton Hills.

**BY-LAW** read and passed by the Council for the Town of Halton Hills this 10<sup>th</sup> day of October, 2023.

Originally Signed By:

\_\_\_\_\_  
MAYOR – ANN LAWLOR

Originally Signed By:

\_\_\_\_\_  
TOWN CLERK – VALERIE PETRYNIAK

**AMENDMENT NO. 50**  
**TO THE OFFICIAL PLAN**  
**FOR THE TOWN OF HALTON HILLS**

**Premier Gateway Employment Area**

AMENDMENT NO. 50 TO THE OFFICIAL PLAN  
OF THE TOWN OF HALTON HILLS

The attached text and schedules constitute Amendment No. 50 to the Official Plan of the Town of Halton Hills, which was adopted by the Council of the Town of Halton Hills by By-law 2023- 0089 in accordance with the provisions of the Planning Act, 1990, R.S.O., c.P. 13, as amended:

THE CORPORATION OF THE TOWN OF HALTON HILLS.

Originally Signed By:

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MAYOR – A. Lawlor

Originally Signed By:

\_\_\_\_\_  
CLERK – V. Petryniak

AMENDMENT NO. 50

TO THE OFFICIAL PLAN FOR THE TOWN OF HALTON HILLS

PART A – THE PREAMBLE does not constitute part of the Amendment.

PART B - THE AMENDMENT, consisting of the following text and schedules, constitutes Amendment No. 50 to the Official Plan for the Town of Halton Hills.

PART C – THE APPENDICES, does not constitute part of the Amendment, but is included for information purposes only.

## **Part A – The Preamble**

### **1. Purpose of the Amendment**

The purpose of this Amendment is to establish a Secondary Plan for the Premier Gateway Phase 2B Employment lands with appropriate land use designations and policies to facilitate the development of these lands for employment purposes.

### **2. Location**

The lands affected by this Amendment generally include lands east of Eighth Line, west of Winston Churchill Boulevard, north of Steeles Avenue to a distance 0.6 km north of and parallel to Steeles Avenue. The lands are identified as Lot 1, Part of Concession 7 and Concession 8.

### **3. Basis of the Amendment**

It is an objective of the Halton Hills Official Plan to ensure that there are sufficient lands available for the creation of diverse employment opportunities and increased tax assessment at strategic locations throughout the Town to maintain and enhance the economic vitality of the Town. The Premier Gateway Employment Area Phase 2B Secondary Plan is a required step in the process of ensuring that there are appropriate lands available for employment growth within the Town during the 2031 planning horizon and that the lands have the appropriate planning documents in place to support their development.

Council approved the Terms of Reference for the Premier Gateway Phase 2B Employment Area Integrated Planning Project whose purpose is to develop a comprehensive Secondary Plan in support of the general policy framework provided by the Official Plan. The more detailed policies will guide new employment uses as well as address the interface between those new uses and the existing residential uses, the natural heritage system, cultural heritage resources and the agricultural system.

This study was undertaken as an integrated planning project, with two main components, the land use planning study (or Secondary Plan) and a Scoped Subwatershed Study. The study also fulfilled Phases 1 and 2 of the Environmental Assessment process for transportation and water/wastewater services planning in the area. The Secondary Plan incorporates the recommendations of the Scoped Subwatershed Study including the provision of updated mapping of the natural heritage system.

The Secondary Plan Study provided for a public consultation process that included three public open houses, on-line questionnaires, multiple discussions with various companies and individuals as well as the statutory public meeting to provide information and obtain public feedback. The public open houses included presentations, poll questions and opportunities for questions and answers. The project website on Let's Talk Halton Hills was kept up to date throughout the project in order to communicate with stakeholders, provide copies of the various reports and presentations, and undertake the on-line surveys.

The study process also included a Technical Advisory Committee and Steering Committee which met throughout the study process. The Technical Advisory Committee included staff from relevant Town and Regional departments, Conservation Authorities and agencies, and the Steering Committee included members of Council, residents of the study area and developer/landowner interests.

Official Plan Amendment 50 finalizes the Study process. Updated Urban Design Guidelines will provide further guidance for development within the area.

## **Part B – The Amendment**

All of this part of the document entitled PART B – THE AMENDMENT, consisting of the following text and schedules, constitutes Amendment No. 50 to the Official Plan for the Town of Halton Hills.

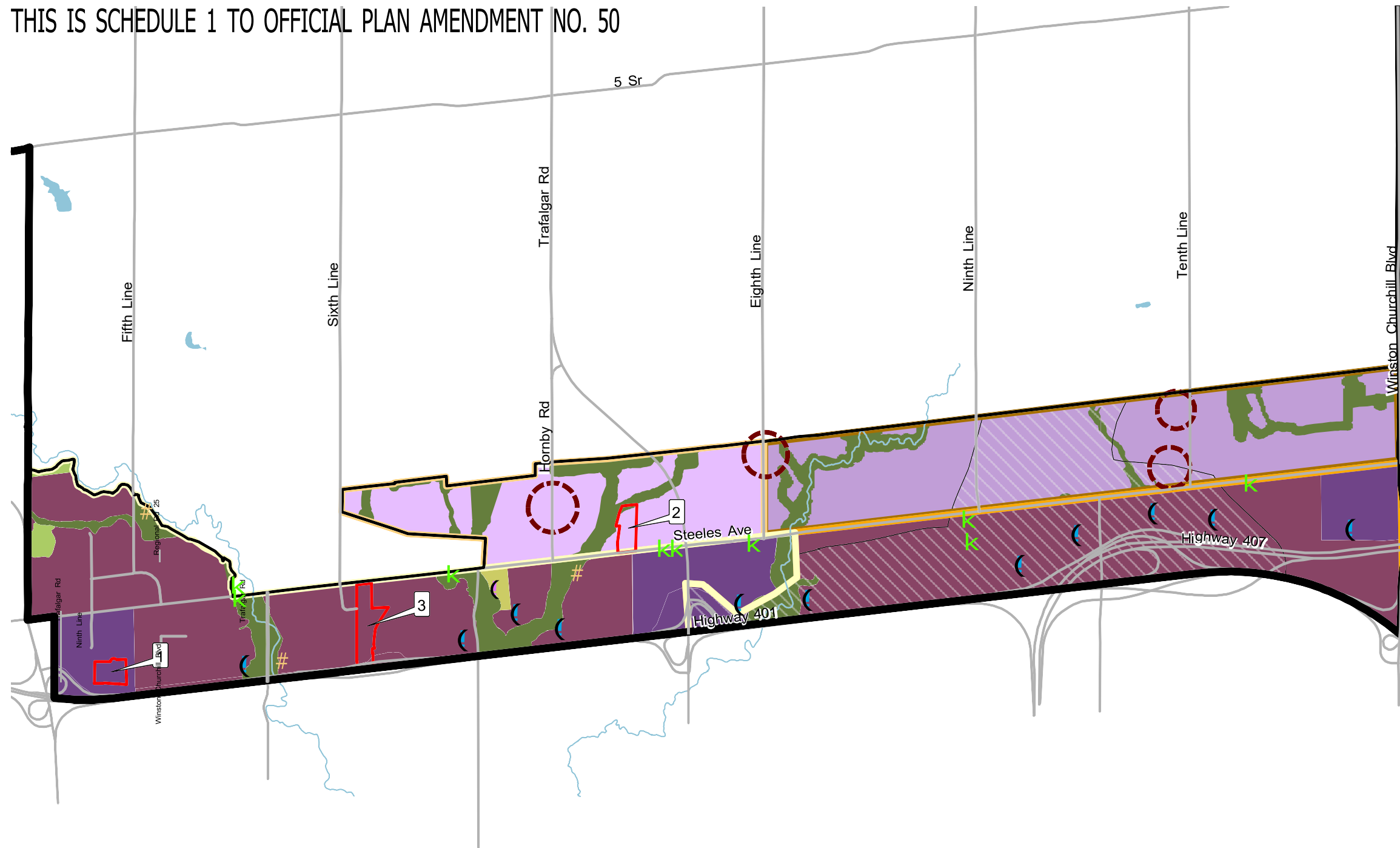
### **Details of the Amendment**

1. That Schedule A8 – Premier Gateway Land Use Plan is amended as shown on Schedule 1 attached to and forming part of this Amendment No. 50, by revising the HPBATS/GTA West Corridor Protection Area to the Highway 413 Corridor Protection Area and reducing the area covered by it to reflect the area approved by Regional Official Plan Amendment 49.
2. That Section H1 be amended by the addition of the following bullet point at the end of the existing section.
  - “Premier Gateway Employment Area Phase 2B Secondary Plan”.
3. That Section H, Secondary Plans is amended by adding the following new section, “H8, Premier Gateway Phase 2B Secondary Plan”, to the Official Plan.

### **Implementation and Interpretation**

This Official Plan Amendment shall be implemented and interpreted in accordance with the implementation and interpretation provisions set out in the Amendment and the relevant sections of the Official Plan.

THIS IS SCHEDULE 1 TO OFFICIAL PLAN AMENDMENT NO. 50



-  Phase 1B Employment Area
-  Phase 2B Employment Area
-  Prestige Industrial Area
-  Gateway Area
-  Major Parks and Open Space Area
-  Private Open Space Area
-  Greenlands
-  Employment Phase 1A
-  Employment Phase 1B
-  Employment Phase 2A (Regional Phasing 2021-2031)
-  Employment Phase 2B (Regional Phasing 2021-2031)
-  Existing Rural Residential Concentration
-  Special Policy Area
-  HPBATS/GTA West Corridor Protection Area
-  Town of Halton Hills Boundary
-  Urban Boundary
-  Waterbody
-  Watercourse
-  Community Park
-  Building with Historic Significance
-  Stormwater Management Pond
-  Potentially Unstable Slopes

SCHEDULE A8  
PREMIER GATEWAY EMPLOYMENT  
AREA LAND USE PLAN

DECEMBER 31, 2020

CONSOLIDATION

NOTE:  
This schedule forms part of the Official Plan and must be read together with the text.

Parts of this Schedule are currently pending approval. Please Refer to Figure 1 in the Preamble.



# Town of Halton Hills

## Premier Gateway Employment Area Phase 2B Secondary Plan



September 2023

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# PREMIER GATEWAY EMPLOYMENT AREA

## PHASE 2B SECONDARY PLAN

### H8.1 PURPOSE

The purpose of the Premier Gateway Employment Area Phase 2B Secondary Plan is to develop land use designations and policies for a portion of the Premier Gateway Employment Area needed to accommodate employment lands needs for the 2031 planning horizon. The Secondary Plan will establish a more comprehensive planning framework for the area in support of the general policy framework provided by the Official Plan. The policies will guide new employment uses as well as address the interface between new uses and the existing uses and *Natural Heritage System*. The Plan incorporates the recommendations of the Phase 2B Scoped Subwatershed Study including the provision of updated *Natural Heritage System* mapping. It integrates transportation system updates, servicing policies, and land use compatibility directions, and addresses cultural heritage resources, surrounding agricultural uses and urban design. It is also intended to help ensure the long-term financial sustainability of the Town.

### H8.2 STRUCTURE OF THE SECONDARY PLAN

All of this part of the document, consisting of the following text and Schedule H8 constitutes Amendment No. 50 to the Official Plan for the Town of Halton Hills. The appendices contain additional information that supports the policies of this Plan but do not form an operative part of the Plan.

### H8.3 LOCATION

The Secondary Plan affects all lands as shown on Schedule H8, which is generally bounded by:

- a) Steeles Avenue to the south
- b) Winston Churchill Boulevard to the east
- c) Eighth Line to the west
- d) A line parallel to, and approximately 0.6 kilometers north of, Steeles Avenue to the north.

The Secondary Plan area contains approximately 251 hectares (620 acres) of land.

## H8.4 VISION STATEMENT

The Secondary Plan Area will be a high quality, comprehensively planned, prestige employment area that forms a key component of the Premier Gateway Employment Area. The area will provide the majority of the Town’s future employment growth. It will diversify and enhance the Town's economy, increase assessment, and expand local employment opportunities. Development will leverage Halton Hills’ strategic location in close proximity to major transportation networks and shipping hubs.

The area will provide quality prestige employment uses that align with the Town’s strategic objectives. It will be a leading and thriving Green Economy Innovation and Employment Hub that focuses on advanced manufacturing, food and beverage processing, clean technology and renewable energy, research and development, office, major hospitality and tourism, and related employment uses. The permitted uses will reinforce a more intensive economic base that will amplify synergies between the Town’s economic development, land use planning and climate change objectives.

Visually attractive and sustainable development that reflects the Town’s leadership in climate change and its 2030 Net-Zero target will improve resiliency. The refined and enhanced *Natural Heritage System* will further support the Town’s climate change and sustainability programs.

Access and connectivity will be improved, and attractive and comfortable streetscapes will encourage active transportation and support future public transit. Development should proceed on full municipal services that meet the needs of businesses and existing residents.

## H8.5 GUIDING PRINCIPLES

- H8.5.1 Prioritize the development of prestige employment uses that diversify and enhance the Town’s non-residential assessment base, generate higher density employment, support economic resiliency, raise the Town’s economic competitiveness, and provide expanded local employment opportunities, aligned with the strategic objectives of the Town’s Economic Development and Tourism Strategy, Foreign Direct Investment (FDI) Attraction Strategy and Business Concierge program, as updated.
- H8.5.2 Attract and serve as the primary location for the Town’s target sectors of advanced manufacturing, food and beverage processing, clean technology and renewable energy, and agri-business as well as higher density employment uses such as, research and development, office, major hospitality and tourism related uses.
- H8.5.3 Encourage eco-industrial activities including, but not limited to:

- a. Mutually supportive business relationships between industries through resource sharing;
  - b. Re-use of industrial by-products;
  - c. Sharing of services and facilities, including transportation and demand management strategies;
  - d. Sustainable and energy efficient net-zero climate resilient building infrastructure that focuses on renewable energy and co-generation opportunities including district energy, and supports an integrated energy distribution system;
  - e. Integration of climate change mitigation and adaptation techniques; and
  - f. Leveraging partnerships between public and private organizations that support the area emerging as a major regional Green Economy Innovation and Employment Hub.
- H8.5.4 Attract a post-secondary education institution to locate in the area to support, generate and drive innovation, research and workforce development in order to support a leading and thriving Green Economy Innovation and Employment Hub.
- H8.5.5 Encourage development of multi-tenant industrial malls that can accommodate tenants of various sizes in order to support the growth of emerging and innovative uses.
- H8.5.6 Support accessory retail and service commercial uses that assist existing and future businesses and reinforce a diversified economic base.
- H8.5.7 Facilitate the creation of high quality development and an attractive public realm through the use of urban design guidelines and other measures, including zoning regulations. Ensure development is consistent with the other phases of the Premier Gateway Employment Area in order to provide a cohesive and coordinated employment area. Allow individual areas to develop their own specific character while ensuring they fit with the general vision for the Employment Area.
- H8.5.8 Encourage high standards of built form, site design and landscape treatments with a strong visual identity in a sustainable environment that supports the area's gateway location. Ensure a consistent level of quality on both public and private lands with the highest quality of development at key locations.
- H8.5.9 Collaborate with the Regions of Halton and Peel to ensure that the Winston Churchill Boulevard and Steeles Avenue intersection reflects its gateway role by promoting a distinct visual presence and sense of arrival as well as providing a comfortable and engaging environment.

- H8.5.10 Strengthen the *Natural Heritage System*, located within both the Sixteen Mile Creek and Credit River watersheds, by protecting, enhancing and where possible restoring the natural heritage features and functions, and by connecting it with the *Natural Heritage System* within the Town.
- H8.5.11 Respect the existing low density residential and institutional uses within and adjacent to the Secondary Plan area, recognize their right to continue to exist and allow office and home-based businesses. Prohibit new residential uses and restrict institutional uses in order to create a stable operating environment for businesses. Provide appropriate buffering to minimize impacts on sensitive land uses from adjacent employment uses, while recognizing that the primary and long term use of the area is for employment.
- H8.5.12 Collaborate with the Region of Halton, Halton Hills Community Energy Corporation and other private and public stakeholders to ensure the expeditious provision of soft and hard infrastructure including a leading edge telecommunication service that is required to attract and retain innovative employment uses in the Green Economy Innovation and Employment Hub and to supply municipal services to existing residential uses in advance of, or in conjunction with, new development.
- H8.5.13 Recognize that the proposed Highway 413 which traverses through the Secondary Plan area may enhance exposure and visibility but may also impact internal connectivity and existing sensitive land uses. Ensure transportation, servicing and land use frameworks do not preclude or negatively affect the planning and/or implementation of the proposed Highway 413 while providing alternative options should it not proceed.
- H8.5.14 Create a safe, efficient and integrated transportation network that provides public road access to all parcels, meets the needs of all modes of travel and promotes active transportation. Facilitate connections to major transportation corridors as well as urban development areas within the Town to maximize connectivity to key markets and major areas of population.
- H8.5.15 Ensure appropriate interfaces between new employment uses and adjacent existing agricultural areas to allow existing farming operations to continue to operate effectively and efficiently.
- H8.5.16 Integrate significant existing cultural heritage resources into future land use development through retention of heritage attributes that express the resource's cultural heritage value.
- H8.5.17 Collaborate with the Region of Halton to monitor and phase development in an orderly manner in order to address the cost-effective and timely supply of major

capital projects including municipal services and transportation network upgrades as the area develops.

- H8.5.18 Promote sustainable site and net-zero climate resilient building design and construction techniques to reduce energy and water consumption, encourage alternative modes of transportation, protect and enhance the natural environment, protect air and water quality, and improve waste management.

## H8.6 GENERAL POLICIES

The general policies of the Official Plan will apply to the Secondary Plan area, particularly:

- a) General Development Policies, particularly Subdivision of Land, Urban Design, Cultural Heritage Resources, Transportation, Public Parkland, Community Facilities and Services, and Development Phasing Strategies; and,
- b) Plan Implementation and Administration, particularly Secondary Plans and More Detailed Plans, Site Plan Control, and Pre-Consultation and Complete Application Requirements, and Interpretation
- c) The development of employment land uses in this Plan is intended to occur in a timely and orderly manner. Build out of the Phase 2B lands is anticipated to occur over a ten to fifteen year period although the policies address a twenty year time frame.

### H8.6.1 Employment Targets

- H8.6.1.1 The employment target for the Secondary Plan area at full build out is 3570 jobs assuming a density of 25 jobs per net hectare.
- H8.6.1.2 In order to achieve this target, compact development within the *Prestige Industrial* and *Business Commercial Areas* will be encouraged. It will also be important to ensure that critical linkages such as access to markets, service providers, and available labour pools are maintained.
- H8.6.1.3 All of the lands within this Secondary Plan are identified as *provincially significant employment zone* under the Growth Plan for the Greater Golden Horseshoe and therefore a site-specific Official Plan or zoning by-law amendment to convert lands to other non-employment uses including *major retail* uses is prohibited .

- H8.6.1.4 It is the policy of this Plan to prohibit new residential uses and other non-employment uses, including *major retail* uses, in the *Prestige Industrial Area* and *Business Commercial Area* designations, except:
- a) to recognize existing uses within the *Prestige Industrial Area* and *Business Commercial Area* as shown on Schedule H8 of this Plan;
  - b) for institutional uses which have provided a detailed study that sets limits on, and establishes criteria for, such uses based on the following principles:
    - i) the use is small scale, and collectively such uses within the *Prestige Industrial Area* and/or *Business Commercial Area* designations do not change the character of that designation;
    - ii) the location and design of the use addresses land use compatibility in a manner based upon Regional Guidelines and the policies in section H8.13;
    - iii) the use is located near the periphery of the Premier Gateway Employment Area; and,
    - iv) such uses do not collectively displace employment from the *Prestige Industrial Area* and *Business Commercial Area* designations to an extent that the supply of land within the *Prestige Industrial Area* and *Business Commercial Area* designations are insufficient to meet the employment target contained in Tables A1A (Population and Employment Targets) and F10 (Regional Phasing) of the Official Plan.
  - c) Notwithstanding H8.6.1.2 b), post-secondary education facilities will be permitted provided they undertake a land use compatibility study that has been approved by the Town in consultation with the Region, which indicates that they will not adversely affect the overall viability of the employment area.
- H8.6.1.5 It is the policy of this Plan to permit supportive commercial and secondary uses and services that enhance the Premier Gateway as set out in Business Commercial and the Prestige Industrial Area designations. Such uses are particularly encouraged to locate in the Business Commercial Area designation. The policy framework for these secondary uses was informed by a Supportive Commercial Needs Assessment study which provided estimates of the amount of supportive commercial floor space and land area which might be required. As the Secondary Plan area develops, the Supportive Commercial Needs Assessment study should be used as a guide in evaluating individual applications that include permitted secondary uses.

## H8.7 LAND USE DESIGNATIONS

### H8.7.1 Prestige Industrial Area

H8.7.1.1 The Secondary Plan area is part of a larger Premier Gateway Employment Area that is located in close proximity to Highways 401 and 407 and the proposed Highway 413 where other lands are also designated *Prestige Industrial Area* and are intended to form an economically competitive, high quality employment area.

H8.7.1.2 Within the Secondary Plan, where lands are identified as *Prestige Industrial Area* on Schedule H8 to this Plan, the predominant use of the land will be for employment uses. Advanced manufacturing, food and beverage processing, clean technology and renewable energy, research and development, office, major hospitality and tourism will be encouraged.

H8.7.1.3 Development will be designed to reflect and enhance the prominent location of the area through the creation of well-designed and sustainable buildings, structures and landscaping in a visually attractive environment.

H8.7.1.4 The main permitted uses within the *Prestige Industrial Area* will be limited to:

- a) business and professional offices in free-standing buildings or as part of an industrial mall;
- b) industrial uses, such as manufacturing, assembling, processing, fabricating, warehousing, wholesaling and distribution facilities located primarily within wholly enclosed buildings. Outdoor storage will not be permitted. For the purposes of this Secondary Plan, trailer waiting spaces are not considered outdoor storage;
- c) computer, electronics and data processing facilities;
- d) clean technology and renewable energy;
- e) research and development facilities;
- f) post-secondary education facilities excluding residential accommodation subject to policy H8.6.1.4 c);
- g) printing and associated service establishments; and,
- h) industrial malls, containing one or more of the main uses permitted in this designation.

New residential uses are prohibited. No use will be permitted that causes, or is likely to cause air pollution, offensive odours, ground or surface water pollution, or noise in excess of Provincial regulations or guidelines.

H8.7.1.5 Permitted Secondary Uses

The following secondary uses, which are intended to be predominately ancillary to and supportive of the main permitted uses, may be permitted in the *Prestige Industrial Area*:

- a) administrative office uses accessory to, and located on the same lot as a permitted industrial use;
- b) limited commercial uses which are ancillary to the main permitted uses, such as service commercial uses excluding personal service uses, take-out or full-service restaurants, financial institutions, convenience stores, and commercial fitness centres. *Major retail* uses will not be permitted;
- c) commercial or trade school facilities;
- d) public parks and recreational facilities, in accordance with Section F7 (Parkland) of this Plan;
- e) limited private parks and recreational facilities located on lands adjacent to Arterial or Collector roads;
- f) limited retail sales of goods and materials manufactured, or substantially assembled, on the premises;
- g) limited institutional in accordance with the requirements of Part H8.6.1.4;
- h) motor vehicle service stations; and,
- i) uses that legally existed at the date of adoption of this Plan and expansions or alterations thereto which conform to the intent of this Plan and the implementing Zoning By-law.

#### H8.7.1.6 Development Policy for Secondary Commercial Uses

The development of new secondary commercial uses will be limited to those uses that serve the *Prestige Industrial Area* and as noted in H8.7.2.1, these uses will be encouraged to locate in the *Business Commercial Area*. Secondary commercial uses are subject to the following:

- a) Individual retail and service commercial uses permitted in section H8.7.1.5 b) will not exceed 400 sq m in total floor area within the *Prestige Industrial Area* unless they are located within a freestanding office building where they will be located on the ground floor and will not exceed 750 sq m in total floor area. The total retail and service commercial space on individual sites designated *Prestige Industrial Area* will not exceed 2000 sq m in total floor area.
- b) Full-service restaurants, commercial fitness centres and financial institutions may be integrated into industrial malls and/or free-standing office buildings, or may be developed as free-standing buildings provided that the building design is consistent and compatible with the overall design of the adjacent development.
- c) Take-out restaurants and convenience stores will be integrated into industrial malls or free-standing office buildings.

- d) Two or more full-service restaurants may be developed in a “campus” development at the intersection of arterial roads with arterial or collector roads, subject to a comprehensive site development plan in order to achieve well-designed and integrated development, including:
  - i) integrated internal circulation and parking areas;
  - ii) coordinated access points, in order to minimize the number of access points to abutting roads;
  - iii) compatible building design and location;
  - iv) complementary landscaping; and,
  - v) consistent signage and lighting facilities.
- e) A high quality of building and landscaping design will be required for all commercial uses to ensure compatibility with adjacent development.

H8.7.1.7 The maximum height of all buildings or structures within the *Prestige Industrial Area* will not exceed six storeys although this policy does not apply to signs, utility towers or other non-habitable structures. The greatest heights will be encouraged to locate adjacent to Steeles Avenue and Winston Churchill Boulevard. Where heights in excess of 3 storeys are proposed adjacent to institutional and existing residential uses, appropriate transitions and buffering will be provided in accordance with the Premier Gateway Employment Area Urban Design Guidelines.

H8.7.1.8 To fulfill the vision and guiding principles of the Secondary Plan, which encourage a variety of higher density employment uses, prior to considering any application, Council shall be satisfied that:

- a) the development provides a land use which contributes to achieving the required density targets as prescribed by Provincial and Regional Policy;
- b) the development is planned to encourage shared use of land, secondary uses such as office spaces, efficient use of multi-storey buildings (full or partial), and to maximize the space to encourage higher employment densities.
- c) elements of the public realm which will serve both employees and the community alike will be improved as a condition of development/redevelopment; and
- d) low density development such as warehousing/storage does not preclude intensification in the future. Therefore, a plan must be submitted that demonstrates how the site and buildings could feasibly transition to higher density employment uses in accordance with market demands/trends and in keeping with the Town’s objective to pursue investment that generates the greatest community benefits, including a higher number of jobs per sq. ft. of gross floor area.

## H8.7.2 Business Commercial Area

H8.7.2.1. The purpose of the *Business Commercial Area* is to provide services that are important to support the primary employment function of the *Prestige Industrial Area* and help to promote a gateway function. These include such uses such as hotels, business and professional offices and limited service commercial uses. Hotels are only permitted in the *Business Commercial Area* and while ancillary service commercial uses are also permitted within the *Prestige Industrial Area* designation, they will be encouraged to locate in the *Business Commercial Area*. Development will be designed to reflect and enhance the visible location of the land within the *Business Commercial Area* through the creation of well-designed and sustainable buildings, structures and landscaping in a visually attractive environment. The designation of new *Business Commercial Area* sites will only be permitted as part of a Municipal Comprehensive Review undertaken by the Region of Halton.

H8.7.2.2. Permitted Uses within *Business Commercial Area* are limited to:

- a) hotels including full service hotels with conference, exhibition, banquet and amenity facilities;
- b) business and professional offices in free-standing buildings or as part of an industrial mall;
- c) research and development facilities;
- d) limited retail and commercial uses which are ancillary to and serve the main permitted uses such as banks and financial institutions and services; take-out restaurants subject to the policies in section H8.7.2.3 c), full service restaurants, convenience stores and commercial fitness centres. Limited retail and commercial uses do not include major retail or supermarkets, specialty food stores, department stores, general merchandise stores, fashion retailers, and personal service uses;
- e) motor vehicle service stations;
- f) printing and associated service establishments;
- g) industrial malls, containing one or more of the uses permitted in this designation;
- h) uses that legally existed at the date of adoption of this Plan and expansions or alterations thereto, which conform to the implementing Zoning By-law.

H8.7.2.3 Development within the *Business Commercial Area* permitted by H8.7.2.2 is subject to the following:

- a) All individual retail and service commercial uses permitted in Section H8.7.2.2 d) will not exceed 750 sq m in total floor area within the *Business Commercial Area* designation. The total retail and service commercial space

- on individual sites designated *Business Commercial Area* will not exceed 2500 sq m in total floor area.
- b) Full-service restaurants, financial institutions and commercial fitness centres may be integrated into industrial malls and/or free-standing office buildings, or may be developed as free-standing buildings provided that the building design is consistent and compatible with the overall design of the adjacent development.
  - c) Take-out restaurants and convenience stores will be integrated into industrial malls or free-standing office buildings.
  - d) Two or more full-service restaurants may be developed in a “campus” development, subject to a comprehensive site development plan in order to achieve well-designed and integrated development, including:
    - i) integrated internal circulation and parking areas;
    - ii) coordinated access points, in order to minimize the number of access points to abutting roads;
    - iii) compatible building design and location;
    - iv) complementary landscaping; and,
    - v) consistent signage and lighting facilities.
  - e) Building and landscaping design will be required to meet the objectives of the Urban Design Guidelines to ensure appropriate design considerations along the Regional Major Arterial, Local Minor Arterial and Local Collector Road frontages.

H8.7.2.4 The maximum height of any buildings or structures within the *Business Commercial Area* will not exceed six storeys except for hotels and free-standing office buildings which may be permitted up to ten storeys. This policy does not apply to signs, utility towers or other non-habitable structures. Where heights in excess of 3 storeys are proposed adjacent to institutional and existing residential uses, appropriate transitions and buffering will be provided in accordance with the Urban Design Guidelines.

### **H8.7.3 Residential Special Policy Area**

H8.7.3.1 While it is the intent of the Secondary Plan that over time, existing uses will be replaced by uses contemplated in accordance with the land use designations and provisions of the Secondary Plan, the Residential Special Policy Area identifies locations where existing residential uses might not redevelop in the short term. The Residential Special Policy Area is an overlay to the *Prestige Industrial Area* designation and new development in the Residential Special Policy Area on Schedule H8 will be in accordance with the underlying land use designation. To recognize this situation, the following special policies will apply to those lands:

- a) The existing residential uses may be recognized in the Implementing Zoning By-law. Any new development will require rezoning which will be reviewed by the Town in the context of the Secondary Plan policies.
- b) In addition to the uses permitted in the underlying land use designation on Schedule H8, within the Residential Special Policy Area, the Town may rezone lands to permit the use of the existing residential dwelling, including additions to such dwellings, for office uses, home occupations or other uses which are compatible with the adjacent residential uses and uses permitted by the underlying employment land use designation.
- c) Where development is proposed on lands abutting a building being used for residential purposes in the Residential Special Policy Area, the Town may require the provision of landscaping and other buffering on the boundary of the residential use in accordance with the provisions in section H8.13.

### **H8.7.4 Natural Heritage System**

H8.7.4.1 The goal of the *Natural Heritage System* is:

- a) to protect areas which have been identified as having environmental significance or which contain natural hazards such as valleylands, wetlands, watercourses and other hazardous lands, based on the functional recommendations of the Phase 2B Scoped Subwatershed Study and applicable Conservation Authority Regulations, and
- b) to ensure that the *Natural Heritage System* will be resilient to the impacts of the adjacent urban development and climate change by achieving enhanced ecological and hydrologic features and functions.

H8.7.4.2 The *Natural Heritage System* reflects the systems approach taken to ensure the protection, preservation and enhancement of the natural heritage features, and to maintain biological diversity and ecological functions of the area. The *Natural Heritage System* is a critical component of a healthy community and helps to define the character of the area.

H8.7.4.3 Lands within the *Natural Heritage System* will be protected and enhanced over time, and the following uses may be permitted in accordance with the objectives of the Secondary Plan and subject to, other policies of this Plan, applicable Zoning By-laws, and Conservation Authority Regulations:

- a) existing agricultural operations;
- b) single detached dwellings on existing lots;
- c) passive non-motorized recreational uses, such as nature viewing and pedestrian trail activities, only on publicly owned lands;
- d) forest management, *wildlife management* and *fisheries management*;
- e) archaeological activities;

- f) *essential* transportation and *utility* facilities; and,
- g) *essential watershed management* and flood and erosion control projects carried out or supervised by a *public authority*.

H8.7.4.4 Where any land within the *Natural Heritage System* designation is held under private ownership, this Plan will not be construed as implying that such areas are free and open to the general public. The Town will seek to obtain dedication of these lands through the development review process.

H8.7.4.5 Where new development is proposed on a site, part of which is designated *Natural Heritage System*, such lands shall not be considered acceptable as part of the dedication for park purposes as required by the *Planning Act*.

H8.7.4.6 The land in the *Natural Heritage System* shown on Schedule H8 is based on a systems approach to protecting and enhancing natural features and functions and consists of the following components:

- 1) *Key Features*, which include:
  - a) habitat of endangered and threatened species,
  - b) *significant wetlands*,
  - c) significant woodlands,
  - d) *significant valleylands*,
  - e) *significant* wildlife habitat,
  - f) *significant* areas of natural and scientific interest,
  - g) *fish habitat*,
- 2) *Enhancements to the Key Features*,
- 3) *Linkages*,
- 4) *Buffers*,
- 5) *Watercourses* that are within Conservation Halton or Credit Valley Conservation Regulation Limits or drainage features that provide a *linkage* to a wetland or a significant woodland,
- 6) Wetlands other than those considered significant, and
- 7) Natural Hazards which include flooding hazards, erosion hazards and/or hazardous lands as determined, mapped and refined from time to time by Conservation Halton or Credit Valley Conservation.

*Key Features* that have been identified are included in the *Natural Heritage System* shown on Schedule H8. The boundaries and/or characterization of these features may be refined through the Subwatershed Impact Study identified in Section H8.8. The Subwatershed Impact Study may also identify other *Key Features* that are not within the *Natural Heritage System* shown on Schedule H8 and if found, these unmapped *Key Features* will be protected through appropriate development approvals in accordance with Section H8.7.4.7.

- H8.7.4.7 A systems based approach to implementing the *Natural Heritage System* will be applied by:
- a) Prohibiting development and site alteration within significant wetlands, significant habitat of endangered and threatened species, and fish habitat except in accordance with Provincial and Federal legislation or regulations;
  - b) Not permitting the alteration of any components of the *Natural Heritage System* unless it has been demonstrated that there will be no negative impacts on the natural features or their ecological functions, and hydrologic functions if applicable;
  - c) Refining the boundaries of the *Natural Heritage System*, including additions, deletions, and/or boundary adjustments, during or outside of site specific applications through a Subwatershed Impact Study in accordance with Section H8.8 accepted by the Town and applicable Conservation Authority in consultation with the Region, an individual Environmental Impact Assessment (EIA) accepted by the Town in consultation with the Region, or similar studies based on terms of reference accepted by the Town in consultation with the Region. Such refinements may occur without an amendment to the Secondary Plan on a site-specific basis;
  - d) Restoring and enhancing, where feasible, natural features and areas within the *Natural Heritage System*. Where areas are not in a natural state, they should be naturalized and planted with native, non-invasive species.
  - e) Requiring the proponent of any development or site alteration, including public works, that are located within the *Natural Heritage System* or on adjacent lands within 120 m of the Natural Heritage System to carry out an Environmental Impact Assessment (EIA), unless:
    - i) the proponent demonstrates to the satisfaction of the Town and applicable Conservation Authority in consultation with the Region that the proposal is minor in scale and/or nature and does not warrant an EIA; or
    - ii) the proponent demonstrates to the satisfaction of the Town and applicable Conservation Authority in consultation with the Region that a Subwatershed Impact Study, prepared in accordance with Section H8.8, adequately addresses the purpose of an EIA.
  - f) The purpose of an EIA is to demonstrate that the proposed development or site alteration will result in no negative impacts to that portion of the *Natural Heritage System* or unmapped *Key Features* affected by the development or site alteration by identifying components of the *Natural Heritage System* as listed in Section H8.7.4.6 and their associated ecological functions, and by assessing the potential environmental impacts, requirements for impact avoidance and mitigation measures, and opportunities for enhancement. The EIA, will identify any *Key Features* on or near the subject site that are not mapped on Schedule H8.

H8.7.4.8 The *Natural Heritage System* may include additional lands beyond identified natural heritage features and their *buffers*, which are identified through an approved Subwatershed Impact Study as being a *Key Feature* and/or providing additional enhancement to the system. These enhancements provide for increased ecological services such as wildlife habitat and movement opportunities, increased buffering to natural heritage features, and restoration opportunities including additional vegetation plantings.

H8.7.4.9 The *Natural Heritage System* in the Secondary Plan area connects with the *natural heritage system* within the Town. This connectivity is important to support natural processes and provide corridors that allow species to move and adapt, thereby ensuring a healthy and resilient ecosystem that protects biodiversity for the long term.

H8.7.4.10 *Buffers* for natural heritage features have been included within the *Natural Heritage System* illustrated on Schedule H8, so additional *buffers* should not be required except where unmapped *Key Features* have been identified through an approved Subwatershed Impact Study and/or requirements to address any Endangered or Threatened species may be applicable. The following *buffer* widths are provided for general guidance but the final *buffer* widths, which may be smaller or larger, will be determined in the Subwatershed Impact Study based on an analysis of the feature sensitivity and proposed adjacent land uses.

a) Watercourse Corridors:

Generally greater of 15 metres from the greatest hazard (Regional Storm flood plain, meander belt or stable top of bank) or 30 metres from the bankfull channel of cold, cool and warm water sportfish watercourses. The hazard limits shall be determined as per policy H8.7.4.18 Watercourses and Natural Hazards. Where a trail is planned, it may be located within the *buffer* subject to section H8.7.4.20.

b) Woodlands:

Generally 30 metres from the drip line for significant woodlands and 10 metres for other woodlands. Where a trail is planned, it may be located within the *buffer* subject to section H8.7.4.17.

c) Wetlands:

Generally 30 metres from the wetlands limits. A 15 m setback is currently shown on Schedule H8 for the wetland within the Mullet Creek watershed. The size and significance of the wetlands are subject to wetland delineation and the Ontario Wetland Evaluation System as part of the Subwatershed Impact Study.

d) Significant Valleylands:

The East Sixteen Mile Creek valleyland is considered significant and is included in the *Natural Heritage System*. The valleyland boundaries will be defined by the long term stable top of slope and natural heritage features that overlap with the top of slope boundary. The overlapping surveyed feature boundaries and appropriate *buffers* will determine the Significant Valleyland boundary as part of the Subwatershed Impact Study.

- e) Significant wildlife habitat  
*Buffers* will be determined through the permitting process with the Ministry of Northern Development, Mines, Natural Resources and Forestry in accordance with provincial requirements under the Endangered Species Act, 2007.
- f) Significant habitat of endangered or threatened species  
Generally 30 metres from the boundary of the habitat unit.
- g) Fish Habitat  
Generally 30 metres from the boundary of the habitat unit.
- h) *Linkages*  
Generally 60 metres when associated with a wetland area

H8.7.4.11 *Natural features* and areas including *Linkages* within the *Natural Heritage System*, should be restored and enhanced, where feasible, and where areas are not in a natural state, they should be naturalized and planted with native, non-invasive species. The requisite planting and seeding plans will be established at the time of subdivision or site plan approval.

H8.7.4.12 Through the Subwatershed Impact Study or detailed design stage, areas not suitable for development that are created through the irregular *Natural Heritage System* boundary, should be included in the *Natural Heritage System* and naturalized to provide an enhancement to the proposed system. Where the provision of enhancement measures as well as open space uses such as stormwater management and Low Impact Development are to be provided, they will be encouraged to locate on lands adjacent to the *Natural Heritage System*.

#### **H8.7.4.13 Headwater Drainage Features**

H8.7.4.13.1 The location of Headwater Drainage Features identified through the Phase 2B Scoped Subwatershed Study for protection or conservation, and the width of the associated *buffers* will be determined as part of the Subwatershed Impact Study.

H8.7.4.13.2 The Phase 2B Scoped Subwatershed Study management recommendations dealing with drainage features indicates that some headwater drainage features may be suitable for relocation or compensation if they are classified as “Mitigation” or “Conservation”, are not regulated by a Conservation Authority, and are outside of the *Natural Heritage System*. The hazard limits shall be determined as per policy H8.7.5 Watercourses and Natural Hazards. Where relocation occurs, the feature should be moved to an area where its form and function can be replicated and where it is contiguous with other natural features where possible, in order to provide overall benefit to the natural heritage feature and the wildlife that use and inhabit them. Final determination as to whether the relocation is appropriate for headwater drainage features will be determined through the Subwatershed Impact Study submitted in accordance with H8.8 as part of a development application and may be further supported by a design report, or inclusion within a Natural Channel Design Brief for the subject lands at the detailed design stage which may include:

- a) Details related to the natural channel design principles applied to the design of the feature;
- b) Fluvial geomorphological analysis of the proposed feature design;
- c) Rationale for selection of plantings within the riparian zone and floodplain;
- d) Details regarding any enhancements proposed within the adjacent feature;
- e) Detailed hydrologic and hydraulic analyses of proposed feature and hydraulic structures to demonstrate impacts to floodplains, and freeboard under proposed conditions, maintenance of riparian storage post-development;
- f) Detailed groundwater analysis to demonstrate that the groundwater seepage function can be replicated in proposed condition;
- g) Detailed assessment of impacts of proposed feature to aquatic habitat and fish species; and
- h) Detailed design of the drainage for proposed feature and corridor.

H8.7.4.13.3 As noted in H8.7.4.15.2, a headwater drainage feature located within the Mullet Creek watershed is associated with a wetland. It is included in the *Natural Heritage System* and as such, is not intended to be removed. The Subwatershed Impact Study prepared as part of the development application will be required to further assess this wetland in accordance with H8.7.4.6 and the requirements of the management recommendations of the Phase 2B Scoped Subwatershed Study to determine the exact configuration and width of the headwater drainage feature and to address the opportunity to also utilize the feature for flood protection purposes.

#### H8.7.4.14 Woodlands

- H8.7.4.14.1 The Woodlands associated with the Eastern Sixteen Mile Creek valley were considered significant at the time of the Phase 2B Scoped Subwatershed Study and included in the *Natural Heritage System*. Retention of other woodlands, hedgerows and individual trees will be encouraged where possible. Tree removal must take into consideration the Migratory Birds Convention Act and the Endangered Species Act with regards to breeding birds and Species at Risk bats.
- H8.7.4.14.2 The Subwatershed Impact Study will identify or refine the boundaries of woodlands and implement measures to protect and enhance significant woodlands including refining the width of *buffer*.

#### H8.7.4.15 Wetlands

- H8.7.4.15.1 Wetlands within the Secondary Plan area may require further evaluation as part of the Subwatershed Impact Studies as per the Ontario Wetland Evaluation System. Where wetlands exist, they have been incorporated into the *Natural Heritage System*.
- H8.7.4.15.2 An unevaluated wetland was identified to the east of Tenth Line and north of the proposed collector road. This wetland is regulated by Credit Valley Conservation and will require a development setback based on delineation and evaluation in the Subwatershed Impact Study. In accordance with H8.8.3, the Subwatershed Impact Study prepared as part of the development application will further assess this feature in conjunction with the headwater drainage feature that will connect the wetland to the Mullet Creek.
- H8.7.4.15.3 Within the lands located east of Ninth Line, north of Steeles Avenue that are part of the Lisgar drainage area, the Scoped Subwatershed Study has identified two vegetation communities that require further assessment during the Subwatershed Impact Study. The general locations of these two areas are shown on Schedule H8. They have not been included in the *Natural Heritage System* based on their lack of provincial or regional significance or their connection to other natural heritage features or areas as determined by the Phase 2B Scoped Subwatershed Study. If either of these areas are determined to be wetlands, the potential for them to be removed and replicated elsewhere may be considered through the Subwatershed Impact Study to the satisfaction of the Town and Conservation Halton. The Corridor Protection Area of the proposed Highway 413 traverses this area and these potential wetlands will also be evaluated through that Environmental Assessment process.

#### **H8.7.4.16 Significant Wildlife Habitat**

H8.7.4.16.1 Significant Wildlife Habitat has been identified within the Secondary Plan area and included in the *Natural Heritage System*. Further assessment and mitigation requirements for Significant Wildlife Habitat are required in the Subwatershed Impact Study. Any additional Significant Wildlife Habitat identified as part of a Phase 2B Subwatershed Impact Study should be considered for incorporation into the *Natural Heritage System* where appropriate along with appropriately sized *buffers* to ensure its protection and enhancement.

#### **H8.7.4.17 Species at Risk**

H8.7.4.17.1 Species at Risk were observed within the Secondary Plan area. Further assessment and mitigation requirements for Species at Risk habitat will be addressed through the Subwatershed Impact Study. Boundaries of any habitat for endangered and threatened species identified as part of the Subwatershed Impact Study should be delineated in consultation with the Ministry of Environment, conservation and Parks in accordance with provincial requirements under the ESA, 2007. The boundaries of this habitat and any *buffers* deemed necessary through the Subwatershed Impact Study should be incorporated into the *Natural Heritage System* where appropriate, to ensure its protection and enhancement.

#### **H8.7.4.18 Watercourses and Natural Hazards**

H8.7.4.18.1 Watercourses and natural hazard lands within the Secondary Plan area are part of the *Natural Heritage System*.

H8.7.4.18.2 The delineation and regulation of watercourses and natural hazard lands is administered by either Conservation Halton or Credit Valley Conservation. The approximate limits of these lands are included in the *Natural Heritage System* as shown on Schedule H8 but may be subject to change until such time as the limits are delineated through a Subwatershed Impact Study (SIS). The studies and resulting delineation of the watercourses, natural hazards and their associated regulatory allowances and *buffers* will be to the satisfaction of the Town and the applicable Conservation Authority and new development will be located outside these limits.

H8.7.4.18.3 As part of the development approval process, watercourses, natural hazards and their associated regulatory allowances and *buffers* will be zoned to an appropriate natural areas zoning category.

#### H8.7.4.19 Road Crossings and Alignments

H8.7.4.19.1 New roads, are intended to be located outside of the *Natural Heritage System* including its *buffers* in order to reduce impacts on the natural heritage features and only *essential* road infrastructure may be permitted after all other alternatives have been considered through an appropriate study. The approach to be taken in addressing road locations will be to first avoid impacts if possible, and then minimize, mitigate and finally compensate for any resulting impacts. Crossings and alignments within or in close proximity to the *Natural Heritage System* will need to be assessed through an Environmental Assessment, Subwatershed Impact Study, or an equivalent comprehensive study to address negative impacts. Best management practices to facilitate fish and wildlife passage across road crossings and appropriate *buffers* from alignments, will be addressed as part of the detailed design.

#### H8.7.4.20 Trails

H8.7.4.20.1 A trail system should be established along the edge of the *Natural Heritage System*, including *linkages*, where lands are being put into public ownership to allow public use and access. The trails and associated activities will not negatively impact ecologically sensitive areas. Where possible, the trail system should be constructed at the start of development, to provide immediate access and connection with the natural environment, while discouraging the establishment of footpaths and ensuring that proper regard is given to avoiding trespassing on private properties. For all trails proposed within *Natural Heritage System buffers* and enhancement areas, trail siting should be located outside the Conservation Authority Regulated Area where possible and close to the development side of the *buffer* to provide as much area as possible for naturalization plantings between the development and key natural heritage features. Adjacent landowners potentially affected by the trails will be consulted.

H8.7.4.20.2 Fencing should be considered along the edge of the *Natural Heritage System* irrespective of ownership in order to restrict access from people and pets. Public access to the *Key Features* should be discouraged and trails should avoid ecologically sensitive features.

### H8.8 SUBWATERSHED STUDY

H8.8.1 The Phase 2B Scoped Subwatershed Study was undertaken in conjunction with preparation of this Secondary Plan in order to characterize the terrestrial and aquatic ecology, stream systems and surface water and groundwater resources, refine the *Natural Heritage System*, achieve a greater level of detail for natural

hazards, assist with development of the land use plan, determine the expected impacts from development and address the associated requirements and opportunities for mitigation and management. All new development in the Secondary Plan area will generally comply with the recommendations of Premier Gateway Phase 2B Scoped Subwatershed Study except for the land at 8029 Eighth Line identified on Schedule H8 as subject to settlement decision D09OPA22.001 at the Ontario Land Tribunal under file PL180499 which is subject to a separate Scoped Subwatershed Study. No amendments to the Secondary Plan will be required to implement the recommendations of the Phase 2B Scoped Subwatershed Study.

- H8.8.2 A Subwatershed Impact Study will be required in support of a complete development application unless it has been demonstrated to the satisfaction of the Town and applicable Conservation Authority in consultation with the Region that either the proposal is minor in nature and does not warrant a study, or the issues have been or will be addressed through other studies such as an Environmental Impact Study and that matters related to the *Natural Heritage System* are addressed in accordance with H8.7.4.7.
- H8.8.3 The goal of the Subwatershed Impact Study will be to achieve a greater level of detail in the integration of land use, servicing, stormwater management and the *Natural Heritage System*. The study area for the Subwatershed Impact Study should be based on subcatchment areas but could be modified or consolidated subject to the approval of the Town, and the applicable Conservation Authority in consultation with the Region of Halton.
- H8.8.4 Greater detail regarding the matters to be addressed by the Subwatershed Impact Studies is outlined in Appendix A and should be incorporated into the Terms of Reference for the Subwatershed Impact Study. The Terms of Reference will be refined on an individual basis and subject to the approval of the Town, in consultation with the applicable Conservation Authority and the Region of Halton.

## H8.9 TRANSPORTATION

The Secondary Plan area will provide a safe, integrated, holistic, multi-modal transportation network that is well connected to other lands within the Premier Gateway Employment area, and provides easy access to major population areas within the Town as well as key transportation corridors. As a result of this work, a road network as well as a number of transportation infrastructure improvements were identified as required to support the development of the Secondary Plan area. All future development will be tied to the provision of these required infrastructure improvements. The Transportation policies of Section F6 of this Plan are applicable to the Secondary Plan area and in addition, the following policies:

### **H8.9.1 Highway 413 and the Northwest GTA Transmission Corridor**

- H8.9.1.1 The Highway 413 Focused Analysis Area and the Northwest GTA Transmission Corridor Narrowed Area of Interest are to be protected as planned corridors for the purpose(s) for which they are identified.
- H8.9.1.2 Development within the Highway 413 Focus Analysis Area and Northwest GTA Transmission Corridor Narrowed Area of Interest shown on Schedule H8 of this Plan will not preclude or negatively affect the planning and/or implementation of the planned corridors for the purpose(s) for which they are identified.
- H8.9.1.3 Lands shown on Schedule H8 as being within the Highway 413 Corridor Protection Area, have an underlying Prestige Industrial Area designation. When the final location of Highway 413 and Northwest GTA Transmission Corridor has been determined by the Province, all lands not required for Highway 413 or the Northwest GTA Transmission Corridor may develop in accordance with the Prestige Industrial Area designation and policies of this Plan without an amendment to this Secondary Plan.

### **H8.9.2 Road Network**

- H8.9.2.1 The road network will provide for the safe and efficient movement of people and goods. Road right of way widths shown on Schedule B2 to this Plan and Map 4 of the Region of Halton Official Plan, will be provided in accordance with Table F6 (Function of Transportation Facilities) of this Plan and in accordance with Policy 173 (5) of the Region of Halton Official Plan.

The location and general alignment of the new collector roads as shown on Schedule H8 are conceptual and approximate, and subject to study as may be required by the Town and/or the Region but are necessary to ensure appropriate overall functioning of a transportation system that can support development of the area. Where the east-west collector road crosses Highway 413 and the Northwest GTA Transmission Corridor, the final alignment will be subject to approval by the Province and may be modified to reflect the locational needs of the Highway and Transitway station. Any proposed additions or deletions to collector roads identified will require an amendment to this plan.

- H8.9.2.2 All roadway and driveway spacings will conform to standard roadway engineering practices and will be approved by the respective roadway jurisdiction. The provision of additional local roads where deemed necessary, may be determined as part of the development approval process. The road network will be designed to augment the existing system, improve connectivity within the Secondary Plan

area and to adjacent areas, provide opportunities to relieve pressure on key intersections, facilitate access to individual properties, and reduce driveway accesses onto arterial roads.

Access to Regional Roads shall be in accordance with the most current Halton Region Access Management Guidelines and by-laws.

As part of the Staging and Monitoring Plan for the Secondary Plan area, an Access Management Strategy must be developed to ensure interim and ultimate access during implementation is achieved through landowner coordination and in conformity with the Town's and Region's requirements (for example, Access Management Guideline and By-law NO. 32-17).

- H8.9.2.3 The Town will work with the Region of Halton to ensure that Steeles Avenue functions efficiently as a major route into and through the Secondary Plan area. Given the future projected traffic volumes, improvements will be required at all Steeles Avenue intersections within the Secondary Plan in order to address the capacity issues. The Region of Halton will be undertaking a Municipal Class Environmental Assessment for improvements to Steeles Avenue from Trafalgar Road to Winston Churchill Boulevard. A range of multi-modal options for corridor improvements will be considered, such as widening of the roadway, cross-sectional requirements, active transportation, transit infrastructure improvements, intersection improvements, and overall traffic operations.
- H8.9.2.4 As part of any development application, a Traffic Impact Study may be required in order to demonstrate
- a) that adequate network capacity exists to accommodate the proposed development;
  - b) the need for new collector road segments;
  - c) the need for road and/or intersection improvements (such as traffic signals, turn lanes, access construction, illumination etc.) and the timing for such improvements;
  - d) the development supports the overall function of the road network for the Phase 2B Secondary Plan area;
  - e) any additional requirements as determined by the Town and/or Region.

The Traffic Impact Study shall be completed in accordance with the Town and Region's Transportation Impact Study Guidelines.

- H8.9.2.5 The Winston Churchill Boulevard and Steeles Avenue intersection should be designed to reflect its role as major gateway to Halton Hills. As such, the intersection should include enhanced streetscape design, landscaping, signage and lighting in accordance with all jurisdictional guidelines, as applicable

- H8.9.2.6 Future access to the Regional Road Network will be restricted and/or controlled, should be limited to the connections with the road network identified for the Secondary Plan area, and will be based on a Traffic Impact Study. Access to Regional roads will be subject to Regional Access Management Guidelines, and By-Law 32-17, a By-Law to Prohibit, Restrict, and Regulate Access to the Regional Road System.
- H8.9.2.7 A new east-west collector road with several new north-south collector road connections is shown through the Secondary Plan area on Schedule H8. The east-west collector road will not extend to Eighth Line given the significance of the *Natural Heritage System* near Eighth Line. Where the east-west collector road connects with Steeles Ave east of Eighth Line, consideration may be given to moving to the road to the east only if such a request is supportable, assessed and justified by a Transportation Impact Study and approved by the Town and Region. A scope of work for the Transportation Impact Study must be completed in accordance with the Town's and Region's most up to date guidelines and policies. The location of the road must be coordinated with the Wastewater Servicing Feasibility Study for the Secondary Plan area.
- H8.9.2.8 A shift north to the location of the east-west segment of the collector road could be considered so that it follows Sixteen Mile Creek as it turns east only if such a request is assessed and justified by a Transportation Impact Study and approved by the Town and Region. The location of the new east-west collector road must be coordinated with the Region as it relates to the water and wastewater services. A terms of reference for a Transportation Impact Study is required and must be approved by the Town and the Region in advance if undertaking any analysis. All studies must be completed in accordance with the Town's and Region's most up to date guidelines and policies.
- H8.9.2.9 The new east-west collector road is intended to cross Tenth Line in a relatively central location between Steeles Avenue and the northern boundary of the Secondary Plan. The location of the new east-west collector road will be subject to further review at the time of development applications taking into consideration the existing residential houses on the west side of Tenth Line and the final location of the *Natural Heritage System* east of Tenth Line.

### **H8.9.3 Active Transportation**

- H8.9.3.1 Development will be based on facilitating the use of walking, cycling and lightly motorized modes of travel within the Secondary Plan area in order to assist the Town in achieving its environmental stewardship goals and to encourage increased activity levels regardless of age, fitness, or ability as a component of healthy living by those who live and work within the Town.

- H8.9.3.2 The transportation system will be designed to maximize connectivity, safety and comfort for pedestrians, cyclists, those with accessibility issues and those using lightly motorized modes of travel both within the Secondary Plan area and between the Secondary Plan area and other activity areas within the Town.
- H8.9.3.3 The Town’s Active Transportation Master Plan, which supports and promotes an increased level of cycling within the Town, has identified a two-tiered cycling network that provides for both on-road opportunities such as bike lanes and off-road opportunities such as trails and multi-use paths.
- H8.9.3.4 The Halton Region Active Transportation Master Plan also identifies regional cycling and walking networks for Regional Roads. Detailed facility types will be confirmed through a future Municipal Class Environmental Assessment.
- H8.9.3.5 In addition to the infrastructure identified, Council will also:
- a) require the provision of sidewalks as new roads are created or existing roads are redeveloped in the Secondary Plan area;
  - b) ensure the provision of safe and convenient on-site pedestrian connections and adequate bicycle storage facilities in the review of development applications;
  - c) require barrier-free design of all new pedestrian facilities; and
  - d) ensure that lands for bicycle, pedestrian and lightly motorized facilities are included with the land requirements for roads.

#### **H8.9.4 Transit**

- H8.9.4.1 Although the Town does not have a full transit system, all development within the Secondary Plan area will be designed to promote transit supportive land use in order to facilitate its use when full services are provided. Future transit stops should be designed to capture the majority of development within a 10-minute walk.
- H8.9.4.2 The Region of Halton has identified Steeles Avenue as a Transit Priority Corridor in their Mobility Management Strategy for Halton and the Defining Major transit Requirements for Halton. As the Region undertakes further work, the form and function of the corridor may be updated.

#### **H8.9.5 Transportation Demand Management**

- H8.9.5.1 In support of the creation of a multi-modal transportation system, the Town will actively work with developers and businesses to develop and implement

Transportation Demand Management measures that aim to reduce motorized vehicular trips and that promote and enable the use of active transportation modes, car-sharing and/or carpooling, use of existing Steeles Avenue public transit and other transit routes when they become available, and travel during off-peak hours.

## **H8.9.6 Parking**

H8.9.6.1 Surface parking will be minimized in order to reduce large expanses of hard surfaces and to encourage the use of alternative means of transportation. Where surface parking is provided, it should be appropriately landscaped and screened from the street in accordance with F.2.2.5, with clearly defined pedestrian connections between the parking area and the building entrances. Consideration will also be given to addressing the Premier Gateway Employment Area Urban Design Guidelines and the Town's Green Development Standards.

## **H8.10 SERVICING**

### **H8.10.1 Functional Servicing Study**

H8.10.1.1 A Functional Servicing Study will be prepared as part of the subdivision or site plan approval process, which will generally include:

- a) Location (including depth) and preliminary sizing of wastewater sewers;
- b) Location and preliminary sizing of storm sewers;
- c) Location and preliminary sizing of watermains;
- d) Preliminary site grading plan;
- e) Preliminary analysis of where infrastructure will be constructed below the water table and identification of mitigation measures to be used for dewatering during construction and post construction;
- f) Location and preliminary sizing of stormwater management facilities;
- g) Location where Low Impact Development Best Management Practices may be considered;
- h) Location and preliminary sizing of hydraulic structures (i.e. bridges and culverts);
- i) Preliminary channel grading plans and supporting analyses; and
- j) Assessment of riparian storage for existing channel and preliminary channel designs.

The report will be prepared in accordance with the approved Area Servicing Plan to the satisfaction of the Town, Region and applicable Conservation Authority in accordance with the policies in Section C8 and G12 of this Plan.

## **H8.10.2 Municipal Water and Wastewater Services**

- H8.10.2.1 The Region is responsible for the development of water treatment and distribution as well as wastewater collection and treatment infrastructure. All water treatment, water distribution, wastewater collection and wastewater treatment infrastructure servicing the Secondary Plan area shall be developed in accordance with the Water and Wastewater Master Plan and Development Charges Background Study, as further refined by the Area Servicing Plan.
- H8.10.2.2 All new development will occur on the basis of full municipal services. Extension of the Region of Halton's water distribution and wastewater collection systems will be required to support development within the Secondary Plan area in accordance with the approved Area Servicing Plan. The Region of Halton is preparing an Area Servicing Plan which is intended to include the proposed timing and sequencing of the provision of services. Notwithstanding the requirement for development to proceed on the basis of full municipal services, due to the significant economic benefits associated with development in the Phase 2B Secondary Plan Area and in lieu of the Region's water and wastewater services being available, temporary private water and wastewater services may be considered on a limited basis subject to the requirements of the Region's Urban Services Guidelines and other considerations.
- H8.10.2.3 New servicing to the Secondary Plan area will generally be developed from west to east to allow for an efficient and cost-effective progression of services. Private front-end construction of water and waste water services and/or the construction of interim servicing facilities to accommodate development west of the GTA West Corridor ahead of the implementation of the Region's planned infrastructure may be considered on a case-by-case basis. The availability of water flows and adequate pressure and wastewater system capacity must be confirmed with the Region. The design of front-ended infrastructure must be to the satisfaction of the Region.
- H8.10.2.4 The extension of water and waste water services will need to cross the proposed Highway 413 and the construction of a looped watermain system will be required.
- H8.10.2.5 Existing houses in the area are currently serviced by individual water wells. Houses within the Secondary Plan area will be eligible for connection to full municipal services as part of the provision of municipal services to the employment area in accordance with the Region's Urban Services Guidelines. The use of Best Management Practices to protect local aquifer and water supply wells will be required for all development within the Secondary Plan area.
- H8.10.2.6 The extension of municipal services on the lands north of the East Sixteen Mile Creek may not be permitted to cross the main branch of the East Sixteen Mile

Creek given the significance of the *Natural Heritage System* in that area. This could delay development of the lands north of the East Sixteen Mile Creek until servicing is available to be extended from lands to the north which are outside of the Secondary Plan area.

### H8.10.3 Stormwater Management

- H8.10.3.1 The Stormwater Management Facilities shown on Schedule H8 represent the general locations for stormwater management facilities within the Secondary Plan Area and relocation of these facilities or refinement of the number of facilities will not require an amendment to the Secondary Plan. These locations are intended to maintain the supply of runoff to receiving systems and to minimize requirements for operation and maintenance by the Town. Stormwater Management Facilities will accommodate the ultimate drainage from arterial roads. Notwithstanding the location of these designations, such facilities will be permitted in all designations except the *Natural Heritage System* where only those components of such facilities that are *essential* for conveying stormwater to the receiving watercourse may be permitted. Stormwater Management Facilities will be located and designed to the satisfaction of the Town and the applicable Conservation Authority in consultation with the Region, and should be integrated with the open space system. Final details such as location and design criteria for stormwater management facilities, including Low Impact Development are to be determined through the Subwatershed Impact Study and Functional Servicing Study taking into account any proposed watercourse and/or headwater drainage feature alterations.
- H8.10.3.2 All new development will be supported by a Stormwater Management (SWM) report unless waived by the Town through a pre-consultation process. The purpose of the report is to demonstrate capability and compliance with the principles and criteria in the Phase 2B Scoped Subwatershed Study as well as the Subwatershed Impact Study. It will identify the specific stormwater management infrastructure to be implemented, and the details related to its design, maintenance and subsequent monitoring requirements.
- H8.10.3.3 In preparing stormwater management plans, a treatment train approach is recommended in order to address requirements for water quality, erosion control, flood control, thermal mitigation and water budget. Low Impact Development Best Management Practices such as bioswales/biofilters with underdrains, infiltration trenches, rain gardens and perforated pipes will be encouraged in addition to wet end-of-pipe facilities. Consideration will be given to minimizing the size of storm water management facilities through the use of such measures as roof top storage, parking lot storage, infiltration galleries, modified pond design

standards, and other appropriate measures to the satisfaction of the Town in consultation with the applicable Conservation Authority.

- H8.10.3.4 Where regulatory storm controls are required, and the Town has determined that a publicly owned facility is not feasible, the Town and the applicable Conservation Authority will evaluate the use of privately owned regulatory storm control facilities on a case-by-case basis. If the Town and the applicable Conservation Authority determine that a privately owned regulatory storm control pond or other appropriate stormwater management system may be permitted, the owner will be required to address the terms for its construction and maintenance through the development application process.
- H8.10.3.5 The stormwater management plans for land within the proposed Highway 413 corridor will not be integrated with the rest of the lands within the Secondary Plan area and will be determined through the Environmental Assessment and detailed design of the highway.
- H8.10.3.6 Stormwater management facilities for the lands within the area covered by Settlement Decision D09OPA22.001 at the Ontario Land Tribunal under file PL180499 are subject to a separate Scoped Subwatershed Study and will be addressed through a site specific development application process.

#### **H8.10.4 Utilities**

- H8.10.4.1 The Town will confirm with all utility providers that adequate servicing networks are, or will be established to serve the anticipated and existing development, and that these networks can be phased in a manner that is cost-effective and efficient. The Town will encourage the establishment of high speed internet services to the area.

#### **H8.11 AGRICULTURE**

- H8.11.1 The full range of existing and new agricultural, agriculture-related, and secondary uses/ on-farm diversified uses are permitted within the Secondary Plan Area until lands are developed in accordance with the policies in the Secondary Plan.
- H8.11.2 Nothing in this Plan will limit the ability of farmers to carry out normal and reasonable farm practices in accordance with the Farming and Food Production Protection Act. All new development, including development on existing lots of record, will be sensitive to existing agricultural uses, including minimizing potential

impacts and will be required to address Minimum Distance Separation One formula, as amended, with respect to setbacks from agricultural operations.

## **H8.12 CULTURAL HERITAGE**

H8.12.1 The Secondary Plan area was historically an agricultural area and the cultural heritage resources reflect that history.

H8.12.2 It is an objective of the Town to conserve significant cultural heritage resources and to ensure that all new development and any site alteration conserves significant cultural heritage resources. They shall be maintained and integrated into new development, where appropriate and feasible. The Town will exercise the powers and apply the tools provided by legislation, including the Ontario Heritage Act, in implementing and enforcing the cultural heritage policies of the Town.

H8.12.3 The following properties within the Secondary Plan area are currently listed on the Town's Municipal Heritage Register and shown on Schedule H8 as locations of Cultural Heritage Value:

- Hornby Presbyterian Cemetery at 8021 Eighth Line (corner of Steeles Avenue);
- Theodore Brain House at 15145 Steeles Avenue

H8.12.4 Another cultural heritage resource has been identified as a candidate for conservation at 15625 Steeles Avenue and is shown on Schedule H8 as a location of Cultural Heritage Value. This cultural heritage resource retains historical and associative, design and architectural and/or contextual value associated with the late-nineteenth-century farmstead with a farmhouse, two barns and agricultural fields. The property should be included on the Town's Municipal Heritage Register and should be prioritized for research and evaluation for potential designation under the Ontario Heritage Act.

H8.12.5 In evaluating development applications, the Town:

- a) Will encourage the use or adaptive reuse of cultural heritage resources, or key components of such resources, whenever possible as part of the new development in situ, or on an alternate site; or
- b) May, where resources which are not designated and are not to be conserved, request the documentation of such resources in a cultural heritage evaluation report with a detailed property history, architectural description and photographic recording.

The Town may impose, as a condition of any development approvals, the implementation of appropriate measures to ensure the conservation of any affected cultural heritage resources, and where appropriate, their integration into new development.

- H8.12.6 The Town will require a Cultural Heritage Impact Assessment be prepared in accordance with section F5.1.2 of this Plan to determine the resource's specific heritage significance and to establish appropriate conservation plans and/or mitigation measures be prepared where development or redevelopment is proposed:
- a) on, within, adjacent to, or in the immediate vicinity of any designated cultural heritage resource; or
  - b) on a property listed on the Town's Heritage Register.

The Town may require a Cultural Heritage Impact Assessment be prepared in accordance with section F5.1.2 of this Plan to determine the resource's specific heritage significance and to establish appropriate conservation plans and/or mitigation measures for any development or redevelopment proposal that affects any of the cultural heritage resources mentioned above in policies H8.12.3 and H8.12.4.

- H8.12.7 The conservation of cultural heritage resources should be integrated with the conservation strategies for natural heritage features and environmentally sensitive areas where feasible.
- H8.12.8 New development should be compatible with, legible from and subordinate to cultural heritage resources in their context (both those within and those adjacent to the Secondary Plan area) through consideration of such matters as, but not limited to, building height, massing, scale, setbacks, stepbacks, roof line and profile and architectural character and expression.
- H8.12.9 Development that impacts areas identified for potential archaeological significance will require assessment in accordance with Provincial requirements and the Regional Archaeological Management Plan to determine the presence of archaeological resources and, if required, provide recommendations for mitigation and conservation. Development and site alteration may be permitted after the significant archaeological resources have been conserved by removal and documentation, or preservation on site. Where significant archaeological resources must be preserved on site, only development and site alteration, which maintains the heritage integrity of the site, will be permitted.

## **H8.13 LAND USE COMPATIBILITY**

The Official Plan recognizes that residential and institutional uses are sensitive to noise, vibration, odour, dust or other emissions, particulates or contaminants from industrial, transportation and utility sources. Schedule H8 has identified that the need for buffering

and/or mitigation must be addressed adjacent to existing residential uses. Where new facilities are proposed in proximity to existing sensitive uses, a proponent will be required to undertake the necessary impact analysis and implement, as a condition of approval, the appropriate abatement measures, in accordance with Ministry of Environment, Conservation and Parks guidelines and the Halton Region Land Use Compatibility Guidelines. The potential for impacts (Air and Noise) associated with new facilities will be addressed by provincial permitting and review tools such as Environmental Compliance Approvals, Environmental Activity and Sector Registry, or Environmental Assessments.

### **H8.13.1 Noise and Vibration**

H8.13.1.1 Noise can be expected to vary significantly within the Secondary Plan area and will occur from both stationary sources (the employment and commercial land uses) and transportation sources within and in the vicinity of the Secondary Plan area.

H8.13.1.2 A Noise and/or Vibration Impact Study will be required in accordance with Ministry of Environment, Conservation and Parks guidelines and the Halton Region Land Use Compatibility Guidelines to the satisfaction of the Town in consultation with the Region and will address Environmental Protection Act requirements with respect to existing sensitive land uses. The Noise Impact and/or Vibration Study will consider potential impacts and address mitigation measures through building design, setbacks and/or other measures such as berms and fencing. The Study will be provided early in the site plan approval process to assist in determining the most efficient way to mitigate noise emissions. Changes such as building orientation can significantly affect noise impacts at nearby sensitive land uses and address applicable Ministry of Environment, Conservation and Parks noise criteria without the need for other costly or operationally restrictive mitigation measures.

H8.13.1.3 Design measures to avoid queuing or traffic congestion such as strategic siting of entrances and exits and a reasonable setback from sensitive land uses may be required as part of site plan approval in order to limit nuisance effects associated with the truck traffic.

### **H8.13.2 Air Quality**

H8.13.2.1 Air quality studies will be required in support of Class II industrial development in accordance with Ministry of Environment, Conservation and Parks guidelines and the Halton Region Land Use Compatibility Guidelines unless it can be demonstrated using a screening that no potential air quality effects are likely.

Odour may be caused by discharges from a variety of sources including stationary sources such as buildings, outdoor sources, or fugitive sources such as equipment leaks. As a result, odour assessment, and odour management plans and control measures that are specific to the facility that is being proposed should be required at the Site Plan application stage to avoid odour release and off-site effects.

- H8.13.2.2 Effective fugitive dust management will be required in support of Class II industrial development (including warehouse/distribution facilities) in accordance with Ministry of Environment, Conservation and Parks guidelines. A Best Management Plan outlining procedures and practices to prevent nuisance effects and deposition of fugitive dust should be prepared in support of development applications.

### H8.13.3 Other Compatibility Issues

- H8.13.3.1 All development applications will be required to identify means to minimize and mitigate light pollution and should consider International Dark Sky guidelines. The Town may consider developing a strategic lighting master plan that addresses both private lighting of facilities and municipal lighting of roadways and supporting facilities such as future transit stops in order to address:
- Light intrusion or light trespass of unwanted light onto adjacent properties and the *Natural Heritage System*;
  - Timing of lighting; and
  - Light intensity, spectrum, clutter and glare.
- H8.13.3.2 Construction activities can be a source of emissions and should be managed to control effects from these emissions, with consideration of scheduling, monitoring and mitigation.
- H8.13.3.3 Effective communication with residents during planning and construction phases will be encouraged and the Town may consider establishing a public liaison committee to encourage resident participation.
- H8.13.3.4 New or expanding sensitive land uses, if proposed within the Secondary Plan area, will require the completion of a land use compatibility assessment to ensure that the use is appropriate and the location and design of the use can minimize and mitigate any adverse impacts on industrial, manufacturing or other uses that are particularly vulnerable to encroachment. Land use compatibility requirements in the Provincial Policy Statement must be achieved for development in proximity to major facilities.

## H8.14 URBAN DESIGN

### H8.14.1 Urban Design Policies

H8.14.1. The Secondary Plan area is intended to reflect its role as a high profile gateway into the Town and to ensure that future development reflects high quality architectural and urban design standards that support the corporate image envisaged for it. All development within the Secondary Plan area will address the Urban Design objectives and policies in section F2 of this Plan, which are intended to ensure an attractive public realm, a comfortable and engaging environment and a consistent level of quality on both public and private lands. The highest quality of development will occur at key locations, particularly at the Winston Churchill Boulevard and Steeles Avenue intersection, in order to promote a distinct visual presence and sense of arrival.

- a) The view of the Town from the proposed Highway 413 will be significant in presenting an image of the community for both residents and visitors. In order to enhance that image, the following policies apply to lands that abut the proposed Highway 413:
  - i) The Town will expect a high level of architectural design and finish on the facades of buildings that are visible from the highway;
  - ii) Properties that abut the highway must be designed to have two “faces”, one to the internal street network, and one to the highway;
  - iii) High quality, well maintained landscaping, will be required along all lot lines that abut the highway; and
  - iv) Parking and loading facilities will be permitted between the highway right of way and the building, but these must be limited in nature, and screened from view of the highway.

H8.14.1.2 The zoning by-law and site plan approval process will assist in controlling development along Winston Churchill Boulevard and Steeles Avenue to ensure a high quality of site design. In particular:

- a) buildings will be designed to front on these roads;
- b) parking including the parking of transport trucks and trailers and delivery trucks regularly used in the business operation will be at least partially screened from these streets by berms and landscaping;
- c) the majority of service and loading facilities will be screened from these roads; and,
- d) safe and functional vehicular and pedestrian access will be provided.

H8.14.1.3 Development standards, including zoning provisions and urban design guidelines, will be coordinated with the other phases of the Premier Gateway Employment Area in order to provide a cohesive built environment and an overall unified employment area.

H8.14.1.4 Development will support sustainable site and building design and construction practices.

### **H8.14.2. Urban Design Guidelines**

H8.14.2.1 The Premier Gateway Employment Area Urban Design Guidelines have been prepared to establish a planning framework that will help the Town achieve its goal to create a dynamic, accessible, and economically competitive employment area that includes sustainable development practices and have been updated to include the Phase 2B lands. The Premier Gateway Employment Area Urban Design Guidelines provide guidance on the creation of high quality streetscape design and urban design which is supportive of transit and active transportation.

H8.14.2.2 The Premier Gateway Employment Area Urban Design Guidelines are based on the following principles:

- a) Enhancing Natural Features and mitigating impacts;
- b) Achieving a Strong Visual Identity for Trafalgar Road, Steeles Avenue and Winston Churchill Boulevard;
- c) Developing Nodes at three Gateway Locations;
- d) Ensuring a High Quality of Built Form and Landscape Development;
- e) Providing Flexible Development to Accommodate Evolving Needs;
- f) Encouraging the Protection and Adaptive Reuse of Significant Cultural Heritage Resources; and
- g) Encouraging the Construction of Sustainable Buildings.

H8.14.2.3 The Premier Gateway Employment Area Urban Design Guidelines address matters related to site plan design, built form design, and environment and open space.

## **H8.15 SUSTAINABLE DEVELOPMENT**

H8.15.1 This Plan is intended to facilitate development by addressing such matters as: energy and water; ecology; air quality; resiliency; transportation; and innovation. References are made throughout this Secondary Plan to where sustainable development practices may be considered or encouraged. Further guidance is provided in the Premier Gateway Urban Design Guidelines and the Town's Green Development Standards.

H8.15.2 The Green Development Standards Section in the Official Plan addresses how development will have been deemed to have met the goals for sustainable or green development.

## **H8.16 PHASING**

H8.16.1 Development should occur in a logical and orderly manner to ensure that major capital projects including municipal services and transportation network upgrades are provided in a cost-effective and timely manner as the area develops. Development is generally expected to occur from west to east as municipal servicing becomes available.

H8.16.2 Development on full municipal services will only be permitted to proceed if servicing capacity is available. Until commissioning of the proposed Eighth Line/Trafalgar Trunk Wastewater Sewer occurs, there are downstream capacity constraints that may impact servicing allocation to proposed development within the Premier Gateway Employment Area lands.

H8.16.3 Prior to the development of land east of the proposed Highway 413, construction of the Steeles Avenue trunk watermain east of the proposed Highway 413 will be required, as well as a looped watermain system that includes the crossing of the proposed Highway 413 in association with the planned east-west collector road. If the timing of the Steeles Avenue trunk watermain is delayed, the feasibility of an alternative water supply may be considered.

H8.16.4 Lands located east of the proposed Highway 413 will require the ultimate waste water servicing solution to be constructed by the Region to discharge into local sewers, the component of Steeles Avenue trunk sewer constructed to west of the proposed Highway 413 or directly into the Eighth Line Trunk Sewer.

H8.16.5 Updated Municipal Class Environmental Assessments or Amendments to the existing approved Municipal Class Environmental Assessments may be required for projects impacted by the proposed Highway 413. Future Municipal Class Environmental Assessments may be completed through Integration with Planning Act Applications.

H8.16.6 A more detailed implementation plan for servicing and transportation infrastructure improvements should be prepared once the Region has completed the updates to the Water and Waste Water Master Servicing Plan and Transportation Master Plan in order to articulate the timing of the required improvements. The availability of water and wastewater services may also be impacted by decision related to the proposed Highway 413.

H8.16.7 Private front-end construction of water and waste water services to accommodate development ahead of the Region of Halton planned implementation may be considered as part of development applications.

H8.16.8 Landowners within the Secondary Plan Area may be required to enter into a private cost sharing agreement or agreements amongst themselves to address the distribution of costs of development for the provision of servicing or transportation infrastructure. Cost sharing agreements should consider best management practices.

## H8.17 IMPLEMENTATION

H8.17.1 The Secondary Plan will be implemented and interpreted in accordance with the provisions in the Official Plan pertaining to the administration and implementation of the Plan.

H8.17.2 The Town, at its discretion, may request a peer review of any of the reports, plans and/or studies required in support of complete applications.

### H8.17.3 **Glossary**

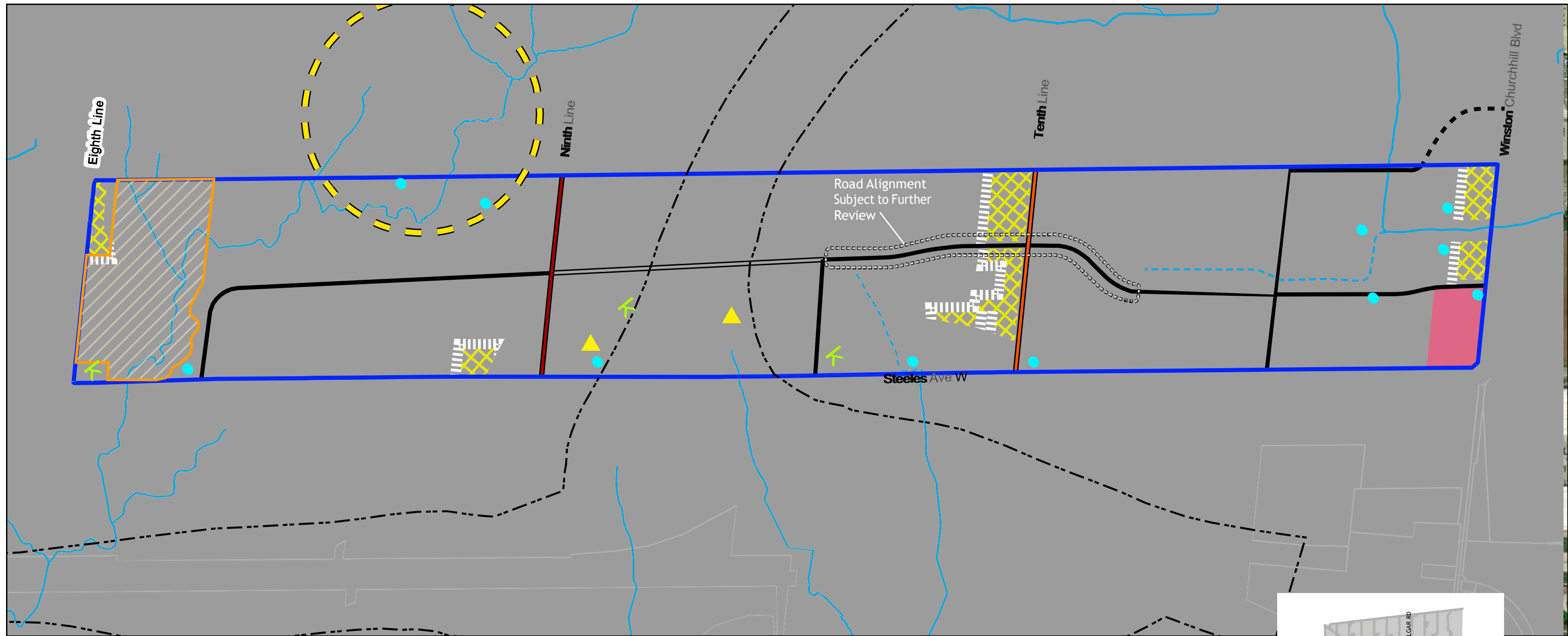
The following terms are defined to assist in understanding the intent of this Plan.

**Major Retail means a large scale or large format stand alone retail stores or retail centres that have the primary purpose of commercial activities.**

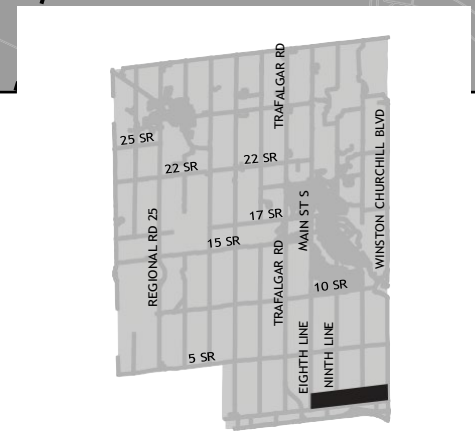
**Trailer Waiting Spaces means an on-site designated commercial vehicle waiting spaces that are restricted in scope and/or extent in the Zoning By-law. Trailer Waiting Spaces do not include loading spaces. No other commercial vehicle parking is permitted, as it would be considered outdoor storage.**

**Regulatory allowance means the distance from a hazard prescribed in the Conservation Authority's Regulation.**

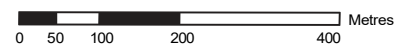
H8.17.4 For the purposes of the Plan, any italicized terms in this Plan reference defined terms in the Region of Halton Official Plan.



- |   |   |   |                                   |
|---|---|---|-----------------------------------|
| Secondary Plan Area   | Residential Special Policy Area         | Collector Road                                  | Cultural Heritage Resource        |
| Natural Heritage System   | Business Commercial Area                | Potential Future Collector Road Alignment       | Minimum Distance Separation (MDS) |
| Prestige Industrial Area  | Subject to D09OPA22.001 As Per PL180499 | Collector Road subject to GTA West confirmation | Headwater Drainage Feature (HDF)  |
| Highway 413 Preferred Route and Interchange Locations   | Buffer for existing residential uses    | Minor Arterial Road                             | Watercourse                       |
| Highway 413 Focused Analysis Area & Northwest GTA Transmission Corridor Narrowed Area of Interest | Refer to Policy H8.7.4.15.3             | Major Arterial Road                             | Storm Water Management Facility   |



September 2023



**SCHEDULE H8 - Land Use**  
**Premier Gateway Employment Area**  
**Phase 2B Secondary Plan**

**APPENDIX A**  
**Subwatershed Impact Study**  
**Matters to Be Addressed**


The Subwatershed Impact Study will:

- a) Update the characterization of features that are recommended to be integrated in the *Natural Heritage System*;
- b) Refine the natural heritage and natural hazard limits reflecting the *Natural Heritage System* objectives and other intentions of the subwatershed study (i.e. final staking of *Natural Heritage System* features and buffers, calculation of riparian storage volumes, etc.);
- c) Delineate the location of key *Natural Heritage System* features, and Conservation Halton and Credit Valley Conservation regulated areas based on site specific topographic survey information and detailed technical studies regarding watercourses, flooding hazards (eg. floodplain analysis), erosion hazards (eg. physical top of bank staking, geotechnical slope stability and/or meander belt erosion assessments) and wetlands (wetland limit staking and Ontario Wetland Evaluation System). This includes addressing policy H8.7.4.15.3 where applicable;
- d) Conduct feature-based wetland and pond water balance assessments for those features that have been identified as Significant Wildlife Habitat and/or associated with watercourses or any “protection” or “conservation” rated headwater drainage features;
- e) Confirm presence of turtle nesting and wintering habitat, snake hibernacula, and amphibian breeding habitat in areas not surveyed in the Phase 2B Scoped Subwatershed Study;
- f) Conduct Species at Risk surveys and follow Endangered Species Act regulations in consultation with the Ministry of Environment, Conservation and Parks, where required;
- g) Confirm the watercourse constraint ranking and Headwater Drainage Feature classification and which may include a Natural Channel Design Brief if relocation is being proposed;
- h) Determine the preferred servicing plan;
- i) Determine the road layout;
- j) Verify and refine, as appropriate, the location and sizing criteria for stormwater management facilities;
- k) Define phasing in areas of multiple ownership;
- l) Establish and fulfill terrestrial and aquatic natural heritage and surface water and groundwater monitoring requirements including duration, methodology and purpose for the components being monitored for pre, during and post development and define cost sharing for monitoring programs;
- m) Refine the meander belt width delineation, hazard setbacks and regulatory setbacks;
- n) Refine the limits of the *Natural Heritage System* with respect to updated watercourse corridor widths;
- o) Develop further characterization of the groundwater resources associated protection measures, and establish mitigation techniques to ensure pre to post water balance is provided to maintain hydrological form and function long term;

- p) Verify the appropriate management of the water budget based on the proposed development and stormwater management plan;
- q) Determine the detailed road alignment and configuration of watercourse and valley crossings, determine the potential impacts on these features and establish the appropriate mitigation measures to ensure the form and function is maintained;
- r) Prescribe site specific standards and preliminary design for landscaping, implementation and the management of corridors, wetland creation areas, buffers, and restoration areas;
- s) Develop strategies to enable construction phasing while allowing rescue of biota from small isolated habitats, and maintenance of the *Natural Heritage System* resources and functioning through the construction period;
- t) Develop the use of Low Impact Development measures;
- u) Undertake a preliminary analysis of the stream corridor dimensions required to maintain the pre-development riparian strategy; and
- v) Determine the location and design of any trail system.

As stated in section H8.8.3, the applicant will consult with the Town, the applicable Conservation Authority and the Region of Halton in order to finalize the Terms of Reference before undertaking a Subwatershed Impact Study (SIS).

It is noted that within Sixteen Mile Creek, the flood hazard limits will be based on the Subwatershed Study modelling, refined as necessary through a SIS or other subsequent detailed study, until such a time that the CH Sixteen Mile Creek Flood Hazard Mapping (FHM) Study is substantially completed. Flood hazard delineations may be required to use the updated modelling from the Sixteen Mile FHM study, once it becomes available, and the stormwater design strategy will use the criteria within the Town's Subwatershed Study, refined as necessary, by the SIS. Once the Sixteen Mile Creek FHM is substantially complete, applicants may be required to use the updated modelling from this study to demonstrate the proposed stormwater management strategy will have no unacceptable impacts to flood hazards.



The Town of Halton Hills

# Premier Gateway Employment Area Urban Design Guidelines

Brook McIlroy/

May, 2023



*Steeles Avenue and Hornby Road, Aug 2015*

This document reflects an updated version of the Halton Hills Premier Gateway Employment Area Urban Design Guidelines. Originally released in 2017, it was updated in 2020 to include the Phase 1B lands. This draft, dated 2022, expands the Guidelines to include Phase 2B lands.

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# 1.0 Introduction



The Halton Hills Premier Gateway Employment Area Urban Design Guidelines (2023) reflect the results of a comprehensive review of the applicable zoning standards in Zoning By-Law 00-138 and the 401 Corridor Integrated Planning Project Design Guidelines (2000). The Premier Gateway Employment Area is located between Highway 401 and Steeles Avenue, as well as along the north side of Steeles Avenue, and between the Town of Milton in the vicinity of James Snow Parkway, and the City of Brampton at Winston Churchill Boulevard.

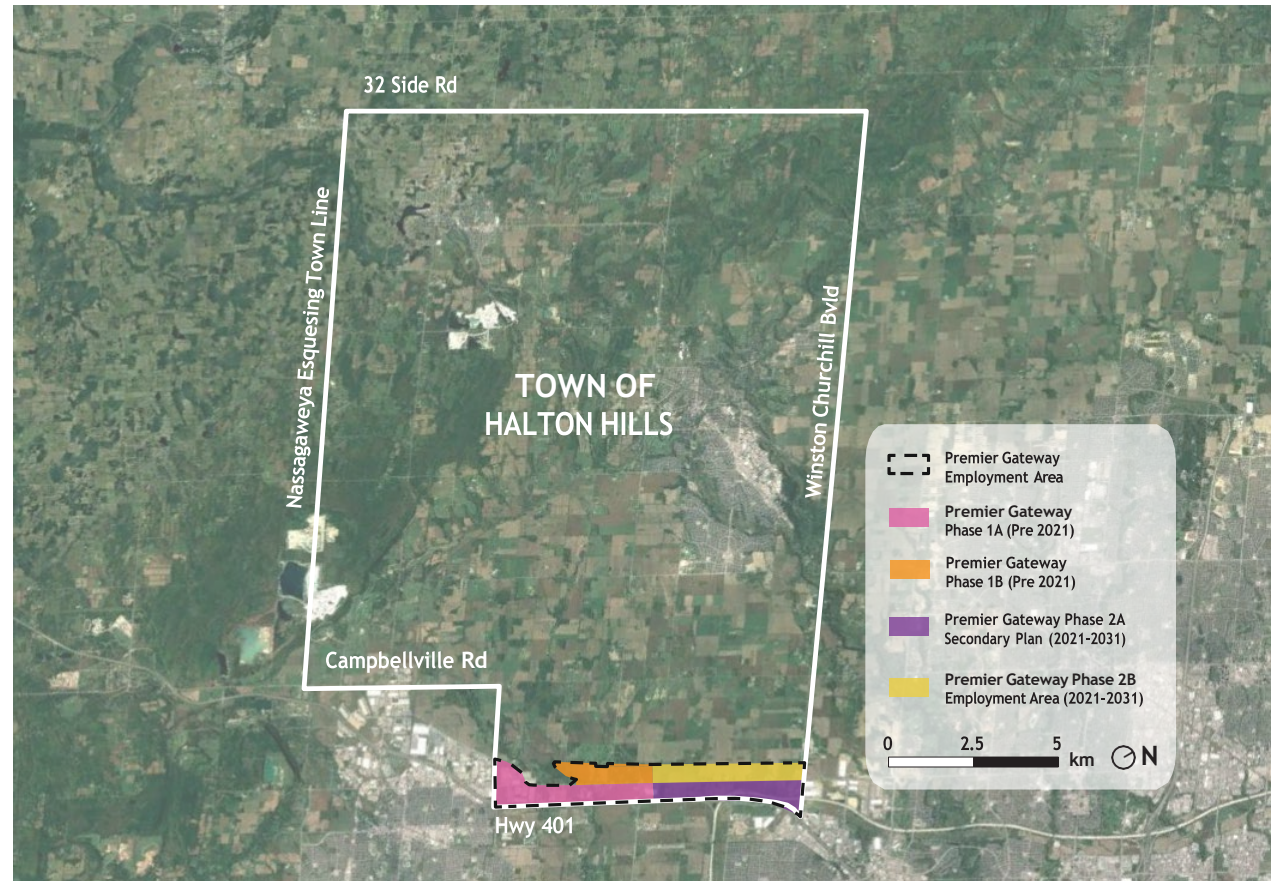
The guidelines are applicable to the Phase 1A, 1B, 2A and 2B areas.

These urban design guidelines support the vision and objectives in the Halton Region Official Plan, and the Halton Hills Official Plan, in particular the policies related to the Premier Gateway Employment Area, as well as the regulations of the Premier Gateway Zoning By-Law. The urban design guidelines aim to fulfill the Town's vision and objectives for the Premier Gateway Employment Area, in order to ensure high quality development

with a strong visual identity. The intent of the guidelines is to establish a planning framework that will help the area thrive as a dynamic, accessible, and economically competitive employment area that reflects high-quality urban design aesthetics and sustainable development practices appropriate for this emerging regional Green Economy Innovation and Employment Hub.

## 1.1 The Premier Gateway Employment Area

The Premier Gateway Employment Area is identified as the Town's prestige employment area where large scale employment growth is directed. As the "gateway" to the Town, a high standard of design is required for new office, warehousing, manufacturing and commercial development. The Premier Gateway Employment Area is a competitive employment area based on its locational advantages, including access to Highways 401 and 407 and the proposed Highway 413, rail, and the airport.



Map of the Premier Gateway Employment Area

## General Policy and Regulatory Direction

### Halton Region Official Plan (ROPA 38 and 49)

The Halton Region Official Plan presents a vision for Halton Region's growth and development to the year 2031. The document includes policies that enforce the Region's desire for economic competitiveness, protection and preservation of natural and agricultural areas, sustainable development, and the creation of healthy communities.

The Official Plan primarily designates the Premier Gateway Employment Area as "Urban Area" with an "Employment Area" overlay designation. The key objective of the Employment Area designation is to provide "opportunities for a fully-diversified economic base, including maintaining a range and choice of suitable sites for employment uses which support a wide range of economic activities and ancillary uses, and take into account the needs of existing and future businesses"(Section 77.1(2)). In addition, the three main tributaries of the Sixteen Mile Creek that cross the area are designated as "Regional Natural Heritage System".

ROPA 49 was approved with amendments by the Province on November 4, 2022. The document expands the regional urban boundary to include additional employment lands north of the Premier Gateway Employment Area lands to accommodate growth and development to the year 2051.

### Halton Hills Official Plan

The Halton Hills Official Plan, as amended by Official Plan Amendment Nos. 10, 21, and 30 and 31 A & B<sup>1</sup> establish a vision for the land use and development of the Town, for the planning horizon of 2031. The Official Plan provides direction on managing growth and development in the Town of Halton Hills, and states that development should demonstrate high-quality built form, site design, and landscape design. The document identifies the Premier Gateway Employment Area as the only employment area that can accommodate large-scale employment growth (Section D3.3). Sections D3.5 and D6.4 and Schedule A8 of the Official Plan provide detailed policy direction and designations with respect to the Premier Gateway Employment Area.

### Premier Gateway Phase 1B Secondary Plan

The Premier Gateway Phase 1B Employment Area Secondary Plan is a comprehensive secondary plan that provides land use designations and policies for certain lands on the north side of Steeles Avenue. The Secondary Plan area includes Lot 1, bounded by Steeles Avenue, Sixth Line, Eighth Line,

<sup>1</sup>Official Plan Amendment Nos. 10, 21, 30 and 31 A & B have been adopted by the Town of Halton Hills. As of November 2020, OPAs 21, 30 and 31 A & B have not yet been approved by the Region of Halton. OPA 21 is held in abeyance by the Region awaiting resolution of the ongoing ROPA 43 appeal.

and a line parallel to, and approximately 1.2 kilometres north of, Steeles Avenue. North of this, the Plan identifies an additional Lot 2 which includes approximately 75 hectares of land for employment uses that form part of the Premier Gateway Employment Area.

### Premier Gateway Zoning By-Law Review

The Zoning By-Law 00-138 for the Premier Gateway Employment Area was created in 2000 as a standalone by-law to implement the direction in the Official Plan regarding the design of new development, and to ensure consistency with the area's urban design guidelines. The 2022 update of this document updates several standards related to setbacks, employee parking, commercial vehicle waiting spaces, loading docks, and minimum landscaping requirements, and will integrate these policies into a comprehensive zoning by-law.

### Premier Gateway Phase 2B Secondary Plan

The Premier Gateway Phase 2B Employment Area Secondary Plan is a comprehensive secondary plan that provides land use designations and policies for lands on the north side of Steeles Avenue, between Eighth Line and Winston Churchill Boulevard. The Phase 2B lands have an area of approximately 252 hectares.

## 1.2 Purpose of the Guidelines

The Premier Gateway Employment Area Urban Design Guidelines establish the physical design framework for the area, to ensure that new built form demonstrates sustainability and high-quality design that reflects building practices that are appropriate for the area. These guidelines contain an Urban Design Vision, a detailed set of Guiding Principles, and guidelines that will establish urban design parameters for the Premier Gateway Employment Area. The guidelines have been prepared to provide guidance to developers, Town staff, and the public on the desired physical development.

## 1.3 How to Use the Guidelines

The Premier Gateway Employment Area Urban Design Guidelines provide guidance for consideration by developers and land owners designing developments within the Premier Gateway Employment Area. The Guidelines will also be used by Town staff to evaluate development applications. The objective of the guidelines is to provide direction related to site plan design, built form design, sustainability, Greenlands, Major Parks & Open Space, and to provide strategies for implementation. These aspects are interrelated and should be referenced in their entirety in the design and review of all projects.

The document contains general guidelines that apply to Phase 1A, 1B, 2A, and 2B lands within the Premier Gateway Employment Area. Additional directions for the Phase 2B lands, as well as Steeles Avenue, Trafalgar Road and the Gateway Areas are found within relevant sections to provide further insight on the design of these key areas.



# 2.0 Vision and Guiding Principles



## 2.1 Land Use Plan

Phases 1A and 2A of the Premier Gateway Employment Area includes four land use designations: Prestige Industrial Area; Gateway Area; Greenlands; and Major Parks & Open Space. Phase 1B includes four land use designations: Prestige Industrial Area; Business Commercial Area; Residential Special Policy Area, and Natural Heritage System. Phase 2B includes four land use designations: Prestige Industrial Area; Business Commercial Area; Residential Special Policy Area, and Natural Heritage System. Phases 2A and 2B also include the Proposed GTA West Transportation Corridor (“potential Highway 413”) Preferred Route.

The Official Plan designates most of the Premier Gateway Employment Area as “Prestige Industrial Area”. The other major

designation is “Gateway Area”. Lands in this designation are found south of Steeles Avenue on the west side of Winston Churchill Boulevard, north of Highway 407; surrounding the intersection of Trafalgar Road and Steeles Avenue north of Highway 401; and on the east side of the Town boundary, east of James Snow Parkway, north of Highway 401.

Within Phase 1B, the other major designation is the “Business Commercial Area,” which is found north of Steeles Avenue surrounding its intersections with Hornby Road and Trafalgar Road. Phase 2B includes a “Supportive Commercial Area” at the northwest corner of Steeles Avenue and Winston Churchill Boulevard. Phases 1B and 2B include “Residential Special Policy Areas” which contain existing residential buildings.

The three main tributaries of the Sixteen Mile Creek and the Mullet Creek (part of the Credit River) cross the area in a north-south direction and are designated as “Greenlands” or “Natural Heritage System”. In addition, Hornby Park, a Community Park at Sixth Line and Steeles Avenue is designated “Major Parks & Open Space.”

A number of buildings along both sides of Steeles Avenue are identified as “Significant Cultural Heritage resources” as well as along Hornby Road. In addition, listed heritage cemeteries are located at the northwest and northeast corners of Steeles Avenue and Eighth Line.

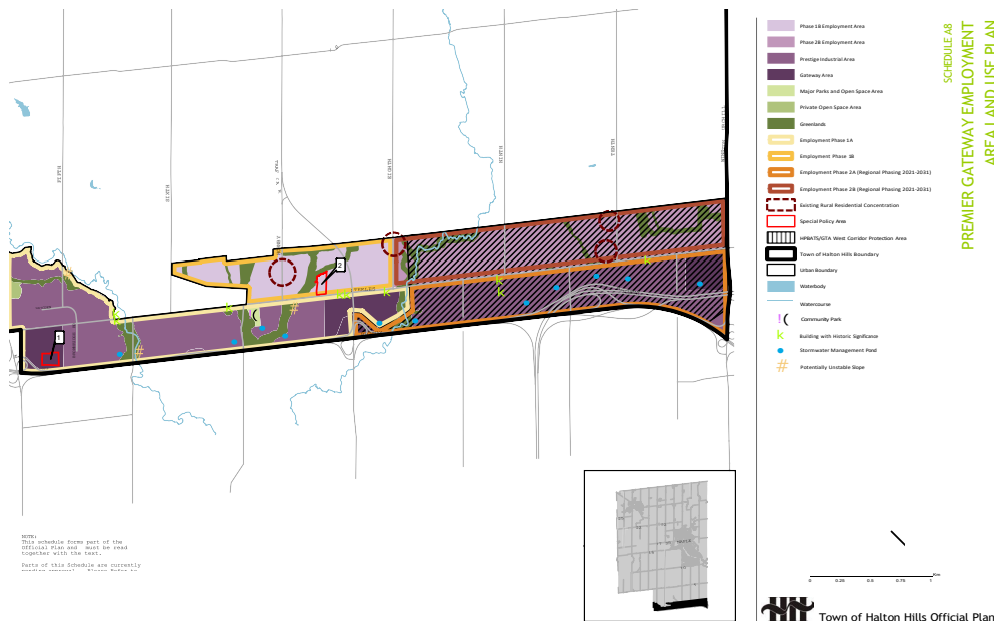
## 2.2 Urban Design Vision

The Premier Gateway Employment Area should reflect the vision and policy framework established in the Halton Hills Official Plan. The Urban Design Vision, as identified in Section D3.5 of the Official Plan is focused on accommodating “a range of industrial, office, commercial and institutional uses on full municipal services.” With respect to design, the development is to be “comprised of visually attractive buildings in aesthetically pleasing and sustainable environments....natural systems are a central element in helping to define the

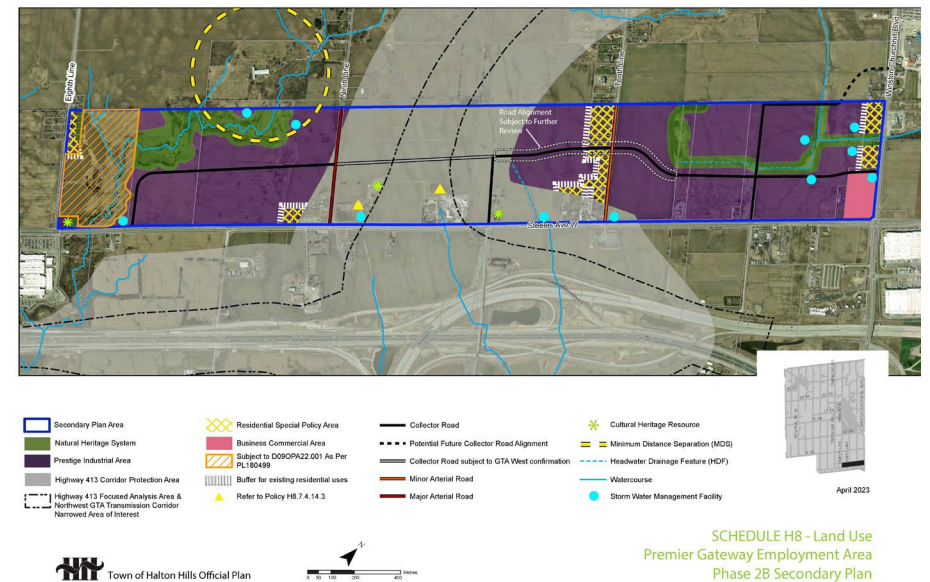
Corridor.” In addition, design of development is to be representative of the area’s location as one of the gateways into the Town of Halton Hills and is to be “sensitive to the character of the area.”(Section D3.5.1).

Building on the specific directions in the Official Plan, as well as Provincial and Regional policy, the Premier Gateway Employment Area should reflect an urban character, including animated and comfortable streetscapes, support for public transit and alternative transportation, and

buildings that address primary streets. New developments should aim to incorporate cultural and natural heritage, and should provide physical and visual connections to Headwaters, Greenlands and Major Parks & Open Space. Gateway Areas should be given special architectural and landscaping treatments to create aesthetically pleasing and prominent entrances into the Premier Gateway Employment Area and the Town as a whole.



Premier Gateway Employment Area Land Use Plan, As Amended by OPA 10 and 21



Draft Preferred Land Use Plan for Phase 2B Secondary Plan, Premier Gateway Employment Area

## 2.3 Guiding Principles

The Premier Gateway Employment Area Urban Design Guidelines are premised on the following Guiding Principles, which reflect the Urban Design Vision for the area:

### 1. Enhance Natural Features

- a. Preserve and enhance natural features through sensitive planning and design.
- b. Provide physical and visual connections to Greenlands/Natural Heritage Systems and Major Parks & Open Space.
- c. Integrate stormwater facilities into site design.
- d. Use Low Impact Development (LID) techniques in the site design process.

### 2. Achieve a Strong Visual Identity for Trafalgar Road, Steeles Avenue and Winston Churchill Boulevard

- a. Encourage a strong visual identity for Trafalgar Road, Steeles Avenue and Winston Churchill Boulevard, through urban design, site planning, building design and landscaping.
- b. Design buildings to address Trafalgar Road and Steeles Avenue, with a strong secondary elevation on 400-series highways (where appropriate).
- c. Design Trafalgar Road, Steeles Avenue and Winston Churchill Boulevard with a comfortable pedestrian-focused streetscape design that supports a multi-modal environment accommodating pedestrians, cyclists, and vehicles, with appropriate sidewalks or multi-use pathways, street furniture, lighting, and landscaping.

### 3. Develop Nodes at the Three Gateway Areas

- a. Focus the highest-quality development within the three Gateway Areas, located along Steeles Avenue where it intersects with Winston Churchill Boulevard, Trafalgar Road, and James Snow Parkway.
  - Enhanced softscape and hardscape treatments, including street trees, vegetation, and unique paving;
  - Coordinated street furniture such as benches, lighting, waste receptacles, and bicycle racks;
  - Wayfinding and branding elements including banners, flags, and signage to reinforce entrances into the Premier Gateway Employment Area; and
  - Public art in areas with the highest visibility.
- b. Enhance development within the three Gateway Areas through features such as:
  - Taller, articulated building elements including towers and other features to create a prominent architectural presence;



#### 4. Ensure a High Quality of Built Form and Landscape Development

- a. Encourage high-quality built form and landscape development that reflects sustainable building practices including:
  - Transit-supportive design that results in more efficient development, with access to transit and active transportation;
  - Streetscape design that facilitates goods movement while at the same time being pedestrian-oriented, which includes buildings that address main streets, reduced building setbacks, and active uses at-grade in Gateway Areas; and
  - Enhanced softscape and hardscape landscaping treatments along major streets.
- b. Provide for the provision of buffering and other measures to allow for a high quality environment for existing transitional uses.

#### 5. Provide Flexible Development to Accommodate Evolving Needs

- a. Provide a framework of roads, including private roads, and infrastructure that facilitates goods movement and allows for maximum flexibility of development lot sizes, thereby accommodating the evolving needs of employment developments.

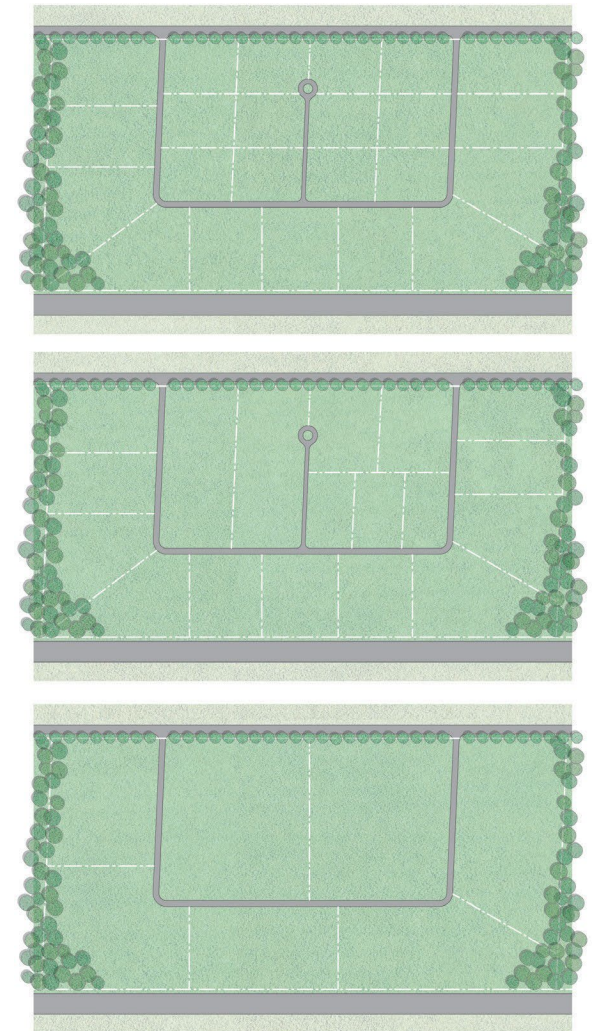
#### 6. Encourage the Protection and Adaptive Reuse of Heritage Elements

- a. Encourage the protection of identified Significant Cultural Heritage resources, where feasible.
- b. Provide for the interface of new development and heritage elements with appropriate buffers and other transitional elements between them.
- c. Encourage the appropriate adaptive re-use of heritage structures within new developments.

#### 7. Encourage the Construction of Sustainable Buildings

- a. Promote the integration of net-zero or net-zero ready buildings.
- b. Use LID techniques in the site design process and consider resilience of buildings to future impacts of climate change.
- c. Adhere to, and where possible exceed, the Town of Halton Hills' Green Development Standards.
- d. Integrate green or white roof practices into the design of built form.
- e. Consider the use of geothermal heating and, where available, connect to district heating and cooling systems.

*Flexibility of Lot Division*



# 3.0 Site Plan Design Guidelines



*The design and organization of sites within the Premier Gateway Employment Area should promote safe and efficient circulation for multimodal transportation, including vehicles, pedestrians, and cyclists. The design of front yards and boulevards should be high-quality and pedestrian-supportive, resulting in comfortable, accessible, and attractive streets.*

*Buildings and main entrances should be located to frame and address public streets and should provide access to public open spaces. Pedestrian pathways should be clearly demarcated through landscaping, lighting, and signage, and should facilitate safe movement.*

## 3.1 Block Layout and Street Design

### Special Streets

The Premier Gateway Employment Area contains several streets that should be given the highest design attention due to their role as major corridors and gateways. These include:

**Steeles Avenue:** The main (east-west) corridor within the Premier Gateway Employment Area.

**Trafalgar Road:** The major (north-south) corridor through the Premier Gateway Employment Area and the major Gateway Area to the Employment Area and the Town from Highway 401.

**Winston Churchill Boulevard:** The major (north-south) corridor bordering the Premier Gateway Employment Area to the east.

**Highway 401 Frontage:** The lands located at the southern boundary of the Premier Gateway Employment Area along Highway 401, south of Steeles Avenue.

**Potential Highway 413 Frontage:** The potential Highway 413 is proposed to run north-south through the

### Gateway Areas

Gateway Areas are located at key intersections within the Premier Gateway Employment Area. These are:

#### **Steeles Avenue, at Winston Churchill Boulevard:**

A major Gateway Area located at the eastern boundary of the Employment Area, north of Highway 407.

**Steeles Avenue, at Trafalgar Road:** A major Gateway Area located in the centre of the Employment Area, with access from Highway 401.

**Steeles Avenue, at James Snow Parkway:** A major Gateway Area located at the western boundary of the Employment Area, with access from Highway 401.

These streets and gateway areas will significantly contribute to the character of the Premier Gateway Employment Area. Where appropriate, the guidelines provide specific direction for these areas.

## 3.1.1 Boulevards, Sidewalks, and Pedestrian Crossings

### Design Guidelines

#### General

- a. Public boulevards for future local streets other than Steeles Avenue and Trafalgar Road and within Gateway Areas, should be a minimum of 4.5 metres wide including a minimum 2.1 metre sidewalk, and should be provided on both sides of arterial and collector streets (existing and proposed)
- b. Sidewalks and walkways should be continuous, universally accessible, barrier-free and clearly designated.
- c. Sidewalk surfaces should be constructed of poured, brushed concrete, in accordance with Town standards.
- d. A minimum landscape strip of 3.0 metres should be provided within the public boulevard to accommodate street trees and landscaping.
- e. A continuous row of street trees is encouraged within the minimum landscape strip. All trees should have access to a minimum of 30 cubic metres of good quality soil (can be shared).

- f. Amenities like street furniture, wayfinding signage, lighting fixtures, and street trees should be incorporated in public boulevards throughout the Premier Gateway Employment Area.
- g. Curb ramps must provide barrier-free connections between the roadway and the boulevard.
- h. Mid-block pedestrian crossings with pedestrian activated signals should be included on long blocks.



*Trees help create an enjoyable pedestrian experience.*

#### Special Streets and Gateway Areas

- i. Public boulevards should be a minimum of 6 metres with a minimum 2.1 metre sidewalk, and should be provided on both sides of Steeles Avenue and Trafalgar Road.
- j. Boulevard materials should incorporate higher-quality materials to demarcate these important locations.
- k. Gateway Areas should include distinct wayfinding and branding elements including banners, flags, and signage, as well as lighting standards and other street furniture, to reinforce their role as the entrances into the Premier Gateway Employment Area, and to create a distinct visual presence from 400-series highways.
- l. Gateway Areas should include public art installations in the areas with the highest visibility.
- m. Special paving treatments for pedestrian crossings should be used in Gateway Areas to demarcate these important locations, and to facilitate safe pedestrian movement.

### Phase 2B Area

- n. Where possible, local roads and multi-use trail networks should frame and connect natural features and open spaces within the Phase 2B Area.
- o. Local streets and collector roads should provide public green boulevards with a minimum width of 5.0 metres and sidewalks with a minimum width of 2.1 metres, which should be provided on both sides of the street.
- p. Steeles Avenue should provide public green boulevards with a minimum width of 6.5 metres with a minimum 2.1 metre sidewalk, which should be provided on both sides of the street.
- q. Winston Churchill Boulevard should provide a 3.0 metre wide multi-use path buffered by a green planting area where feasible, to the satisfaction of the Regional Municipality of Halton.
- r. The public green boulevard should buffer the pedestrian sidewalk from the right-of-way and contain a row of contiguous tree plantings, providing each tree with access to 30 cubic metres of soil volume to promote a healthy and mature tree canopy.
- s. Rain gardens and bioswales are encouraged to be located in the green boulevard to enhance intersections, pedestrian crossings, and vehicular entrances.
- t. Street furniture may be located in the green boulevard. Where possible, street furniture should be high quality and grouped together to ensure minimal disruption to the tree canopy.
- u. Where bicycle lanes are proposed on local streets and collector roads, they are to be separated from vehicular traffic by a curb, and planted medians are encouraged.
- v. In concert with Transportation Report recommendations, pedestrian crossings near *Residential Special Policy Areas* should be clearly marked.



*Bioswale in green boulevard (Photo: MMSD).*



*Bicycle lane with planted median.*

## 32 Building Location and Orientation

### Design Guidelines

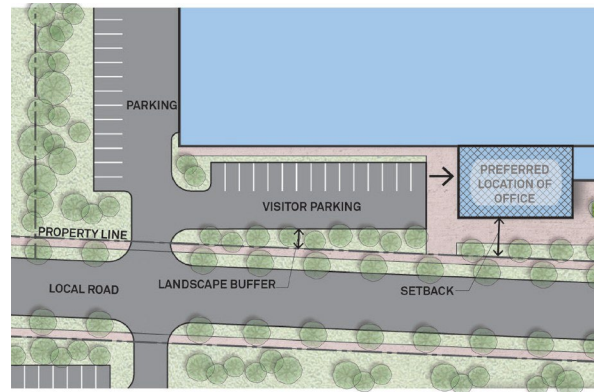
#### General

- Buildings should generally be located close to the front property line to define the street edge.
- Buildings should generally occupy a minimum of 60% of the total lot frontage.
- Industrial buildings should locate offices and entrance elements towards streets and intersections.
- Site development should be as efficient as possible including minimizing setbacks and maximizing building coverage. The minimum required development setback from 400-series highways should reflect the Ministry of Transportation (MTO) minimum requirements.
- Site layout and building orientation should be designed to maximize solar gain, the capture of solar energy through building elements like windows, as a form of passive heating for built form.
- Building orientation should respect existing sensitive land uses.

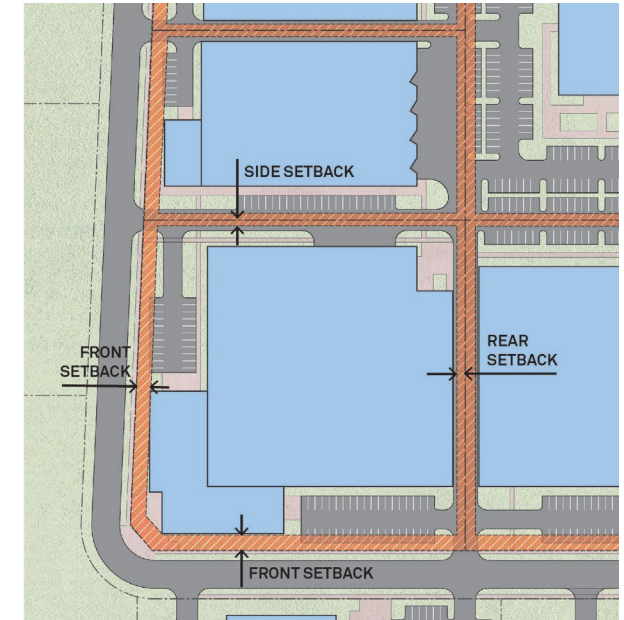
#### Special Streets and Gateway Areas

- Buildings on lots at the intersection of Steeles Avenue and Trafalgar Road shall be oriented to the intersection, and entrance elements shall be encouraged to front on both Steeles Avenue and Trafalgar Road to enhance this major Gateway Area.
- Where a building has a frontage on Steeles Avenue or Trafalgar Road and a local street, office and entrance elements are encouraged to front both on Steeles Avenue or Trafalgar Road and the adjacent local streets to engage the corner.

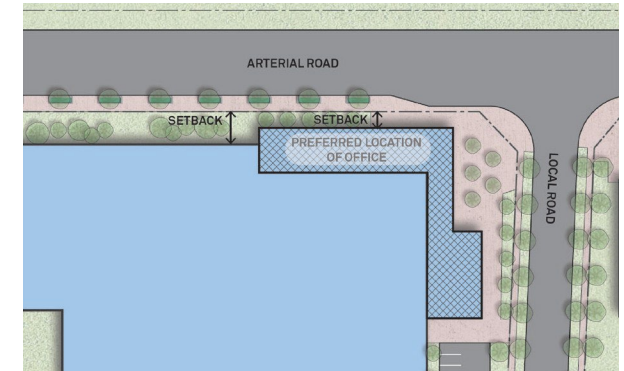
#### Orientation of Entrance Towards the Street



#### Typical Street Setbacks



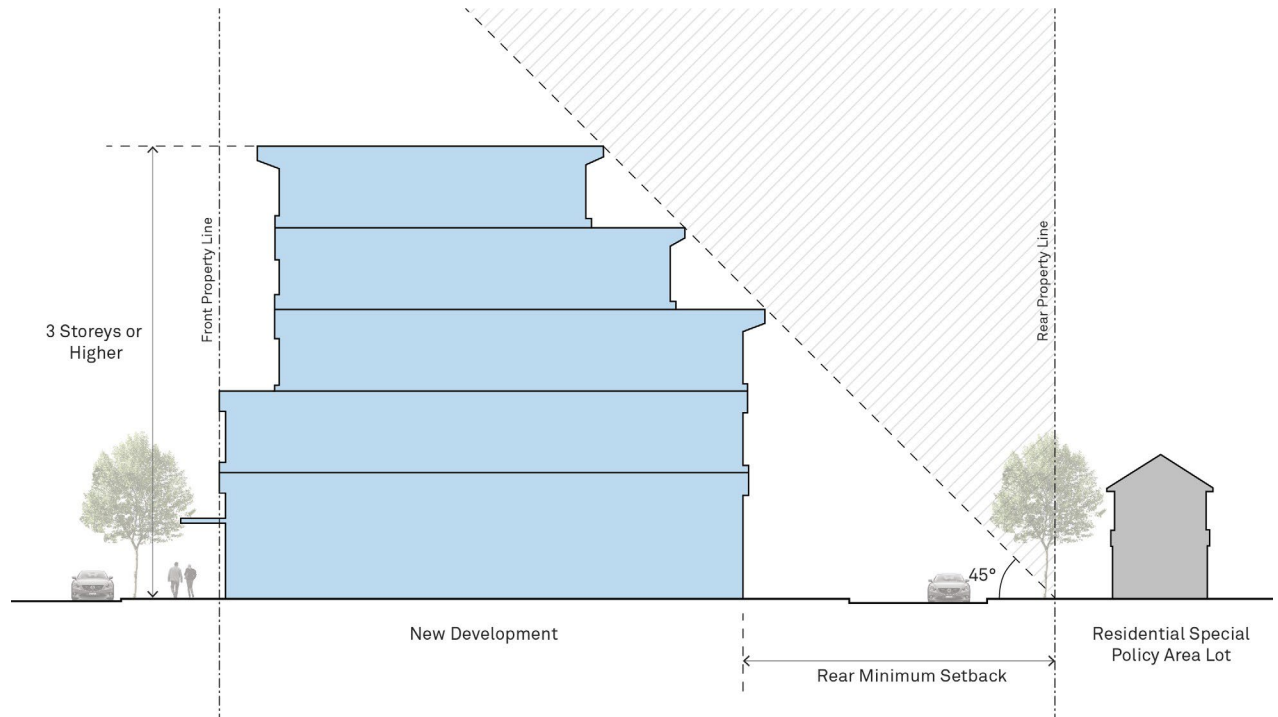
#### Setbacks from Arterial Roads



### Phase 2B Area

- i. Buildings on lots at the intersection of Steeles Avenue and Winston Churchill Boulevard, shall be oriented to the intersection, and entrance elements shall be encouraged to front on both streets to enhance this Gateway Area.
- j. Where a building has a frontage on a Special Street and a local street, entrance elements are encouraged to front both on the Special Street and the adjacent local streets to engage the corner.
- k. Buildings on lots adjacent to a Residential Special Policy Area with heights greater than 3-storeys shall provide appropriate height transitions by adhering to a 45-degree angular plane, established from the adjacent property line of the *Residential Special Policy Area* lot.
- l. Buildings with multiple tenants on a Special Street should provide multiple barrier-free pedestrian entrances along the primary frontage of the building to further activate the street.

Angular Plane Diagram



# 33 Open Space and Landscaping

## Design Guidelines

### General

- a. Where on-site open spaces are provided, or where development abuts public open space, buildings should be located to frame and address these open spaces. At least 50% of open spaces should be fronted by public streets and/or active building edges.
- b. High standards for planting density, quality and variety at main building facades on public street frontages, and in landscape buffers shall be applied, meeting or exceeding the applicable Halton Hills standards.
- c. Use of low maintenance, drought resistant, non invasive plants and trees native to Halton Region shall be encouraged for at least 75% of landscaped areas. For a list of native species refer to Conservation Halton Landscaping and Tree Preservation Guidelines, Appendix 1 or the Credit Valley Conservation Plant Selection Guideline.
- d. On-site open spaces are encouraged to provide outdoor amenity space for employees in accessible areas.
- e. The design of parking areas for employee and customer parking should include landscaped islands and strips to screen the parking and mitigate the impact of paved areas.
- f. There should be a significant minimum landscape buffer provided where industrial uses abut residential uses. This buffer shall consist of plantings such as high shrubs and dense tree coverage that obscure views of industrial developments from the adjacent residential development.
- g. The perimeter of parking lots should be heavily landscaped with a minimum 3.0 metre buffer that includes a range of trees and vegetation.
- h. A landscape buffer of 15 metres is required adjacent to the historic sites of St. Stephen's Church and the historic house south of the church.
- i. The setback required by the Ministry of Transportation (from 400-series highways) should be landscaped.
- j. Open space and landscape design should adhere to the principles of LID.



*Surface parking lot with bioswale and landscaped buffer.*

### *Special Streets and Gateway Areas*

- k. The view of the Town from 400-series highways is important in presenting an image of the community for both residents and visitors. To enhance that image:
  - i. The Town will expect a high level of architectural design and finish on the facades of buildings that are visible from the highways;
  - ii. Properties that abut a highway must be designed to have two “faces”, one to the internal street network, and one to the highway;
  - iii. High-quality, well-maintained landscaping will be required along all lot lines that abut a highway; and
  - iv. Parking and loading facilities will be permitted between the highway right of way and the building, but these must be limited in nature, and screened from view of the highway.

- l. Steeles Avenue, Trafalgar Road, Winston Churchill Boulevard and Gateway Areas should incorporate unique softscape and hardscape landscaping treatments that distinguish these important areas, including feature paving, planters, trees, shrubs, and other vegetation.

### *Phase 2B Areas*

- m. Properties within the Phase 2B Area are to be planted with a minimum of 75% native and/or drought-tolerant vegetation, as outlined in the Town of Halton Hills Green Development Standards.
- n. Properties adjacent to headwater drainage features, wetlands, and natural heritage system lands are to incorporate greater than 75% native and/or drought-tolerant plants in their open space and landscaping.
- o. Where possible, properties should locate open spaces adjacent to headwater drainage features, wetlands, and natural heritage system lands to promote contiguous habitat for all species.

- p. Retain existing mature and healthy vegetation and trees where possible, especially native species.
- q. Properties with frontage on local streets and collector roads should provide a contiguous row of trees to frame the public sidewalk, forming a green and shaded pedestrian realm.



*High quality architectural finishes should be visible from the 400-series highways (Photo: Hixson Inc.).*

# 34 Access and Circulation

## Design Guidelines

### General

- Access to development sites on major arterial roads such as Steeles Avenue, Trafalgar Road and Winston Churchill Boulevard should be provided from local streets and/or private streets to minimize intersections and entrances.
- Joint access driveways between adjacent development lots should be encouraged where possible.
- Access for trucks (i.e. for servicing and loading) should not conflict with general passenger vehicle circulation. Separate entrances are encouraged where possible.
- Provide pedestrian connections from buildings to parking, public transit, and public spaces.
- Pedestrian walkway paving treatments should be clearly articulated and should differ in material and appearance from vehicular routes.
- The implementation of clear signage will assist orientation on public walkways and through public spaces.

- Tree planting and other landscaping must not be an obstacle to the barrier-free path of travel.

### Special Streets and Gateway Areas

- New street connections to Steeles Avenue, Trafalgar Road and Winston Churchill Boulevard shall generally be limited to a minimum interval between intersections of 120 metres and shall utilize existing or relocated streets where possible. This requires approval of the Region and must be supported by a traffic study.



Access should be provided from local streets where possible.

Typical Individual Access Driveways



Recommended Driveway with Joint Access Easement



## 35 On-Site Parking

### 35.1 Surface Parking

#### Design Guidelines

##### *General*

- a. Parking should generally be located at the rear or in the interior side yard of the building. However, parking in yards abutting 400-series highways shall be limited with a significant landscape buffer between the parking area and the highway right-of-way, a portion of which can be in the required minimum MTO setback.
- b. Parking between the primary building elevation and the public street is discouraged.
- c. Opportunities to reduce the total number of parking spaces required within each site should be explored including shared parking between users and provision of formal parking to serve identified initial employees with reserve unpaved areas available, should the nature of the use change in the future.
- d. Large areas of surface parking for employees and customers should be broken-up through landscaping islands and pedestrian pathways to minimize their visual impacts.
- e. Dedicated pedestrian walkways should be incorporated through surface parking areas for employees and customers and defined through differentiated paving materials and landscaping. The amount of landscaping should be proportionate to the overall parking lot size.
- f. Where large surface parking areas are provided, including parking areas for trucks, opportunities to integrate LID technologies should be implemented, including permeable paving and bioswales.
- g. Parking and access design should mitigate potential pedestrian and vehicular conflicts through clearly demarcated circulation routes, pavement marking, and signage.
- h. Pick-up and drop-off areas should be designed to not interfere with pedestrian circulation.
- i. Site and parking area design should address hydrant location and snow storage.
- j. On larger sites, way-finding signage should be included in all parking areas.

##### *Phase 2B Area*

- k. Surface parking lots should provide a minimum of 1 tree for every 8 parking spaces, to be planted within a landscape buffer. Each tree will have a minimum of 30 cubic metres of healthy soil to promote growth.
- l. Dedicated pedestrian walkways should be incorporated through surface parking areas for employees and customers and defined through differentiated permeable paving materials and landscaping. Landscaping should include trees to shade the walkways and infiltrate stormwater runoff.
- m. The use of permeable paving throughout the surface parking lot is encouraged to reduce stormwater runoff.

## 352 Structured Parking

### Design Guidelines

#### General

- a. Where a significant amount of parking is required for employees and customers, structured parking is preferable to surface parking.
- b. Above-grade parking structures fronting onto public streets and public open space should avoid the presence of blank facades, including providing active uses along the facades in those areas where feasible, and include a range of approaches including landscaping, public art and architectural detailing.
- c. Access to structured parking should be from secondary streets or the interior of blocks. Pedestrian entrances should be visible and accessible, accessed from public streets or near main building entrances.
- d. Street trees and pedestrian-scaled lighting should be provided on all boulevards flanking a parking structure.

#### Phase 2B Area

- e. Where feasible, structured parking should be part of the primary building to allow for safe access from the interior of the building.
- f. Structured parking should include green features to promote a more appealing and sustainable built form.
- g. Pedestrian entrances to structured parking should be visible, accessible, and well lit.



*Green buffer and trees maintain a pedestrian-scale at this parking structure.*

## 353 Bicycle Parking

### Design Guidelines

#### General

- a. Locate visitor bicycle spaces in visible and accessible locations at-grade, near building entrances and pedestrian walkways.
- b. Locate employee bicycle parking in a weather protected secure area with controlled access.
- c. The design of bicycle racks should not impede pedestrian movement and snow clearing.
- d. Bicycle lockers are encouraged, particularly for large office or industrial developments.

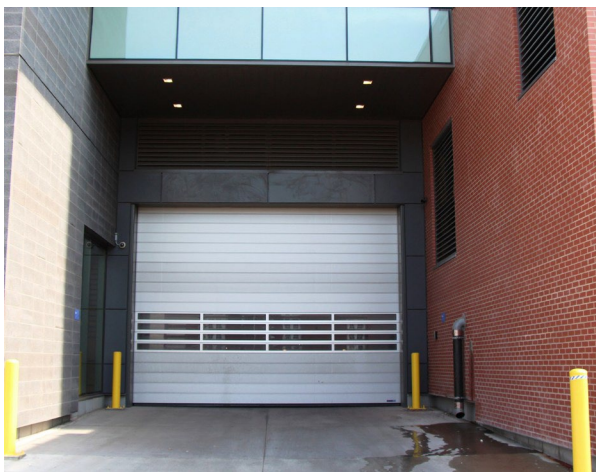
#### Phase 2B Area

- e. Where properties have frontages along Steeles Avenue, Winston Churchill Boulevard or Ninth Line, bicycle parking should be located in view of public realm.

## 3.6 Servicing, Loading and Storage

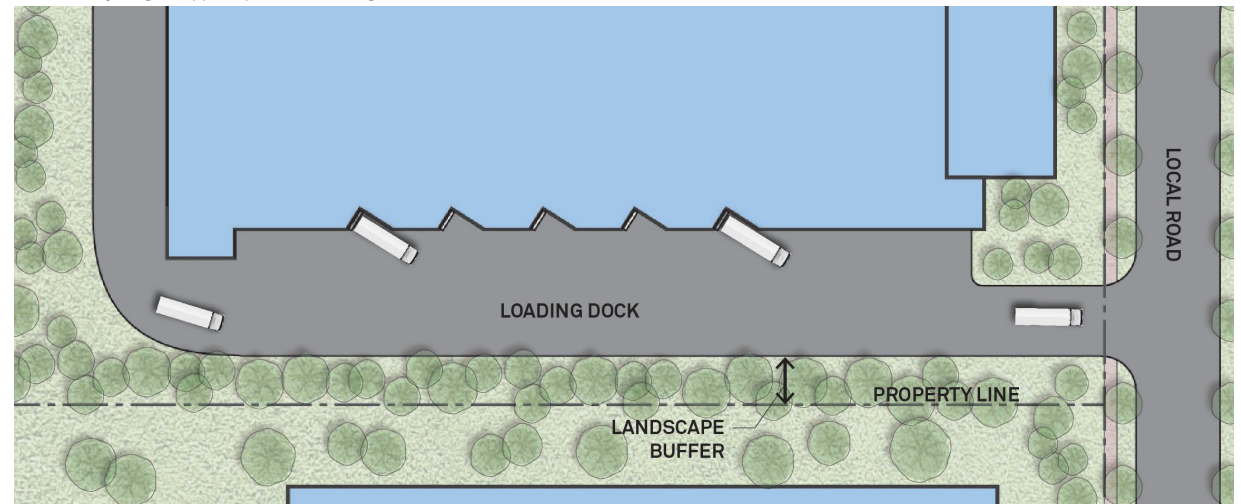
### Design Guidelines

- a. Service, loading, and garbage areas should be located within main building structures, where possible, or in separate enclosed buildings where it is not possible to locate them in the main building.
- b. Loading, service, and garbage storage areas should not face Steeles Avenue, Trafalgar Road, Winston Churchill Boulevard, or 400-series highways, and should be in areas of low visibility
- c. The coordination of service areas for delivery, loading and garbage pick-up is encouraged, to reduce the number of entrances along public streets and within parking areas.
- d. Outdoor garbage containers should be limited and screened by wall features or incorporated into built form, and should feature significant landscaping. These areas must be compatible with building design and materials.
- e. Service areas should be separated from pedestrian amenity areas, open spaces, and walkways.
- f. When occupied, loading areas should not impede on-site vehicular circulation.



A service and loading area within the building structure.

Landscaping Buffer for Loading Areas



## 3.7 Stand-Alone Signage

### Design Guidelines

#### General

- a. Permitted signage types must comply with the Halton Hills Sign By-Law.
- b. Signage along 400-series highways must comply with MTO standards.
- c. Free standing signs and site signs should generally be limited to a single free standing monument sign with graphic consistency.
- d. For free standing and site signs, where the principal frontage of a site exceeds 100 metres, one additional site sign should be permitted for every additional 80 metres of frontage in excess of 100 metres.
- e. Front lit signage, back lit individual block letter signs and/or logo signage is required, particularly those that face the public street or are parallel to a pedestrian walkway.
- f. Electronic messages on digital signage with visible effects during the message transition, including fading, flashing, or motion are discouraged for safety reasons.
- g. To reduce the impacts of light pollution, the illumination of signage is discouraged during off-peak hours.

#### Special Streets and Gateway Areas

- h. As part of a broader gateway design strategy, a signage feature may be erected to identify Gateway Areas as entry points to the Town of Halton Hills. The feature should be well-integrated into a broader gateway feature, including landscaping, seating and/or public art, and be of a scale that is visible from Trafalgar Road, Winston Churchill Boulevard, James Snow Parkway, or Steeles Avenue.



*A free standing sign with graphic consistency.*

## 3.8 On-Site Lighting

### Design Guidelines

- a. All pedestrian and parking areas shall be well lit to promote safety, security and comfort during hours of office use.
- b. Pedestrian-scale light standards or illuminated bollards should be used in employee and customer parking areas, along pedestrian walkways, and other pedestrian zones.
- c. Accent lighting to emphasize built forms and landscape elements is encouraged.
- d. The design and location of lighting must consider the impacts of light pollution, energy efficiency, and other potential negative impacts. The mitigation of light pollution is particularly important where commercial and industrial uses abut residential areas.
- e. Light emitting diodes (LEDs), solar power, road reflectors and other alternative lighting and energy sources should be encouraged for energy efficiency and should be dark sky compliant.
- f. Lighting shall comply with applicable Town standards.
- g. Site lighting should be located to limit light incursion into the Residential Special Policy Area.



*Pedestrian-scale light standard in a parking area.*

# 4.0 Built Form Design Guidelines



*Built form within the Premier Gateway Employment Area should frame public streets and open spaces and create a well-defined street edge. Buildings located within Gateway Areas should be distinguished by special architectural treatments.*

# 4.1 Sustainability

## Design Guidelines

### Phase 2B Area

- a. Adhere to and, where possible, exceed the Town of Halton Hills' Green Development Standards.
- b. New development should meet net-zero standards or provide net-zero ready buildings.
- c. New development should consider the use of geothermal heating. Where available, development should connect to district heating and cooling systems.
- d. Simplified built form and massing with fewer complex junctions are encouraged to minimize building envelope heat loss. Building massing must also meet relevant stepback guidelines found in Section 4.2 Height and Massing.
- e. Building facade design and articulation should respond to changes in solar orientation and increase opportunities for natural ventilation.
- f. Consider passive heating, cooling, and lighting design principles in landscape and building designs, including, but not limited to:
  - i. Orienting for maximum solar-gain potential to reduce heating demand in colder months.
  - ii. Using deciduous trees to provide natural shading to reduce over-heating in warmer months.
- g. Where upper storeys are contemplated, a lower window-to-wall ratio should be considered to reduce heat gain and loss through the building envelope by increasing the area of insulated wall.
- h. Include operable windows, where possible, to provide natural ventilation and help reduce mechanical heating and cooling requirements.
- i. Incorporate light coloured materials and/or planted walls to reduce heat island effect.
- j. Design and orientation of roofs and surfaces to accommodate solar energy and collection devices are encouraged.
- k. Integrate green or white roof practices into the design of built form.
- l. New development should reflect best practices in bird-friendly design, including bird strike deterrent strategies such as fritted glazing. Building systems should automatically turn off lighting after hours to limit interference with the flight pattern of migratory birds.



*Green roofs can provide habitat for some animals, reduce stormwater run off, and reduce the urban heat island effect.*

# 42 Height and Massing

## Design Guidelines

### General

- a. The maximum building height permitted within the Premier Gateway Employment Area ranges from 2 to 8 storeys.
- b. Building massing within, and between, developments should be varied and may incorporate stepbacks, changes in height, or strategic setbacks of sections of the building.
- c. All roof top mechanical units are to be screened in all directions. Such screening shall be compatible with building design in form, materials and colour.

### Special Streets and Gateway Areas

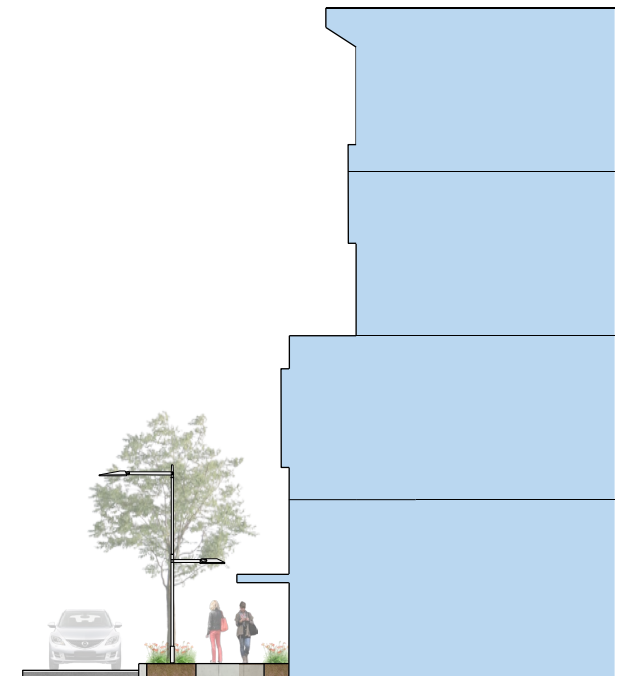
- d. Where taller buildings are proposed in the Premier Gateway Employment Area, they should be concentrated at the key Gateway Areas, as well as along 400-series highways and Trafalgar Road.
- e. Consideration may be given to increased height limits beyond 6 storeys for buildings within Gateway Areas based on review of a specific development.

- f. Gateway Areas should feature appropriate massing, building projections, recesses at grade, lower storey design and open space treatments to reinforce the prominence of these locations.

### Phase 2B Area

- g. Buildings taller than 3 storeys should be massed appropriately and should be designed to include stepbacks, architectural articulation, and fenestration that add visual interest and help to diminish the perceived height and width of the building.
- h. Buildings taller than 3 storeys should establish a streetwall height that is of a pedestrian-scale and appropriate for the surrounding context.

*Massing Diagram*



*Taller buildings should be varied through the use of stepbacks and changes of height, and should feature open space treatments at grade.*

# 4.3 Street-Oriented Design

## Design Guidelines

### General

- a. Buildings should generally be designed and oriented to address Steeles Avenue and other local roads. Where a building has frontage on 400-series highways, it should be treated as a highly-visible secondary elevation as outlined in Section 4.3.
- b. Buildings and main entrances should be designed to create a consistent street edge and frame adjacent streets, as well as amenity spaces, and parking areas.
- c. Primary building elevations are those elevations that face a public street. Blank facades along the length of a public street are to be broken up. Primary building elevations should be designed with significant architectural features that articulate horizontal and vertical elements of the building face, including windows, awnings and canopies, projections and recesses, architectural detail, and change of materials.
- d. Buildings that have frontage on public streets should emphasize architectural elements that promote pedestrian comfort. These may include the use of canopy structures and arcades.

- e. Exterior building materials should be low maintenance and visually appealing.

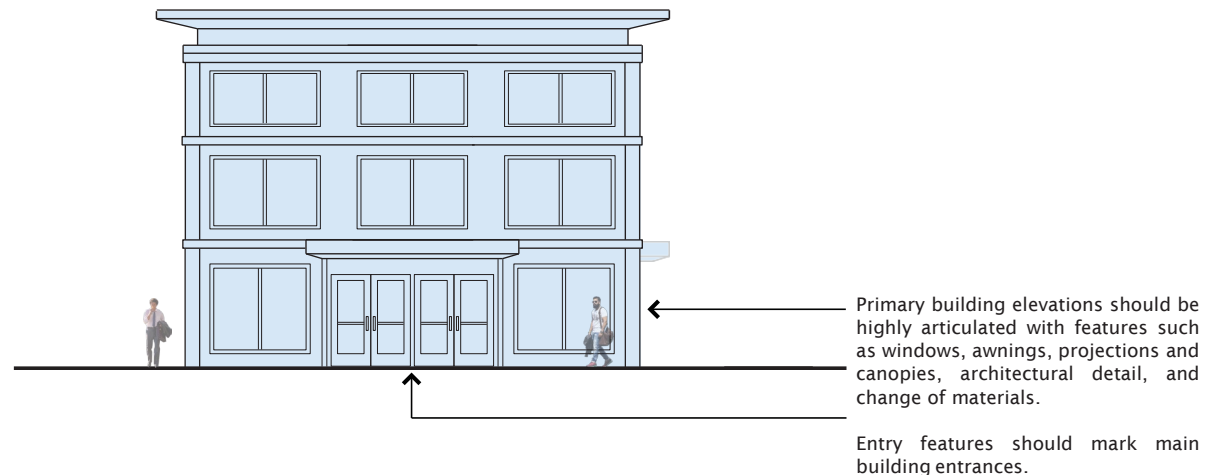
### Special Streets and Gateway Areas

- f. Buildings fronting on Steeles Avenue and Trafalgar Road that include office uses should be encouraged to locate such uses at-grade adjacent to the street, and should provide a significant number of windows in order to establish a strong connection to the street.

### Phase 2B Area

- g. Buildings fronting Steeles Avenue and Winston Churchill Boulevard should be oriented toward the street. Parking should be located at the rear and, where feasible, accessed from collector or local roads.

Primary Building Elevations

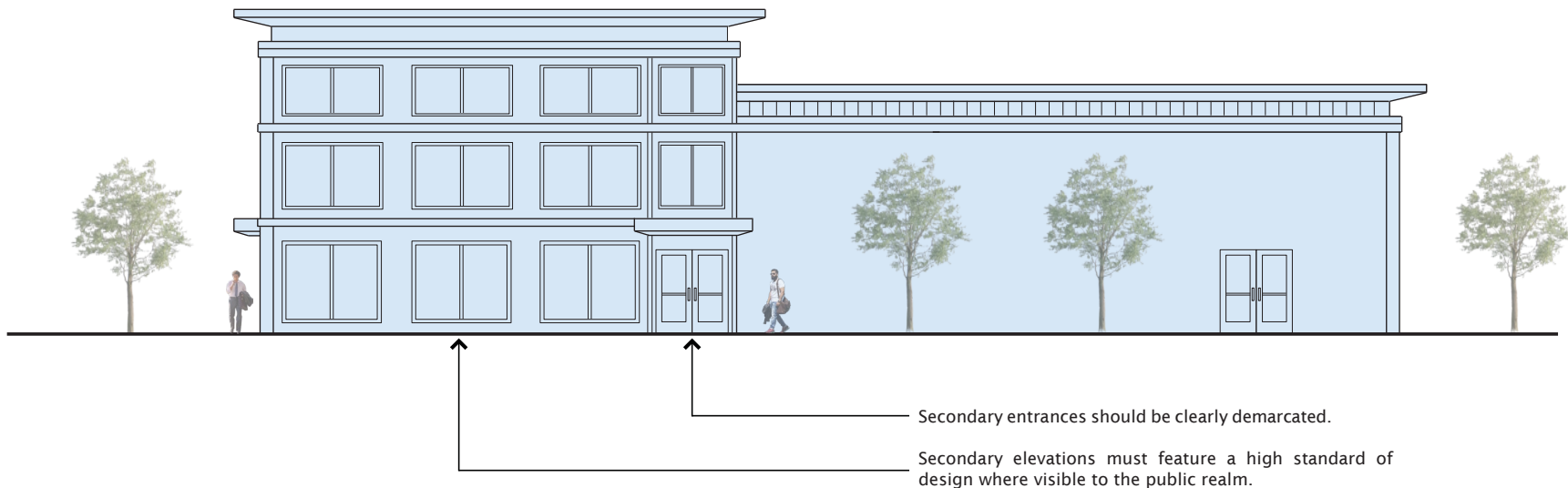


# 4.4 Secondary Elevations

## Design Guidelines

- Secondary building elevations are those elevations that do not face a public street. Such elevations should complement the primary building elevation, and where visible to the public, should feature a comparable level of design and be articulated by horizontal and vertical building elements including windows, projections and recesses, and architectural detail.
- Where a building has a secondary frontage on 400-series highways, a significant amount of articulation is encouraged to create visual interest, and may include building projections, material variations, windows, signage, and interior active uses (i.e. offices, common space).
- Where secondary entrances are provided within secondary elevations, they should be clearly demarcated.
- Exterior building materials should be low maintenance and visually appealing.

### *Secondary Building Elevations*



# 45 Heritage

## Design Guidelines

- a. The conservation and appropriate adaptive re-use of cultural heritage resources is strongly encouraged.
- b. The repair, preservation, or general conservation of heritage fabric should be guided by best practices outlined in the *Parks Canada Standards and Guidelines for the Conservation of Historic Places in Canada*.
- c. Appropriate buffers and transitional elements should be provided between new development and existing heritage fabric.
- d. Where feasible, the preservation and imaginative integration of historic structures within the context of commercial and industrial development is encouraged. The demolition or replacement of cultural heritage resources is discouraged.
- e. The design of new structures should be compatible with adjacent heritage sites and structures. However, the artificial application of historical building styles to modern construction methods is to be discouraged.



St. Stephen's Church

## 4.6 Access and Entrances

### Design Guidelines

#### General

- a. Main building entrances should be located along the primary building elevation.
- b. Architectural features to emphasize entry areas and other special building areas and relieve large expanses of solid wall are encouraged. Considerations may include canopies, awnings, double-height windows, or taller architectural elements. Entrances shall be appropriate to building scale and form.
- c. Building entrances should be barrier-free, and should incorporate ramps, automatic doors, and other elements that contribute to accessibility.
- d. Main building entrances must be accessible by public walkways and/or pedestrian sidewalks.

#### Phase 2B Area

- e. Canopies should shelter entrances to provide weather protection for pedestrians accessing the buildings.



*Barrier-free entrances should incorporate ramps, automatic doors, and other elements that contribute to accessibility.*

## 4.7 Signage and Lighting

### Design Guidelines

#### General

- a. Signage and lighting design should adhere to the relevant by-laws for the Town of Halton Hills and MTO requirements.
- b. Signage should be integrated into building design to reduce clutter.
- c. Accent lighting to emphasize built forms and landscape elements is encouraged.
- d. Rooftop signs are not encouraged.
- e. Signage on sites adjacent to a highway should be of a scale appropriate to the speed and scale of the highway environment. On sites adjacent to 400-series highways, the size limits of signage on building facades facing the highways may be increased to a total surface area of no more than 20% of the building wall face.
- f. Electronic messages on digital building signage with visible effects during the message transition, including fading, flashing, or motion are discouraged for safety reasons.
- g. To reduce the impacts of light pollution, the illumination of digital signage is discouraged during off-peak hours.
- h. Light emitting diodes (LEDs), solar power, road reflectors and other alternative lighting and energy sources should be encouraged for energy efficiency.

#### Phase 2B Area

- i. Illuminated signage should not increase the light levels within 10 metres of all points of the sign face by more than 3.0 lux above the ambient lighting levels.



*Signage should be integrated into building design to reduce clutter.*



*Existing Building Signage within the Premier Gateway Employment Area*

# 5.0 Environment and Open Space Guidelines



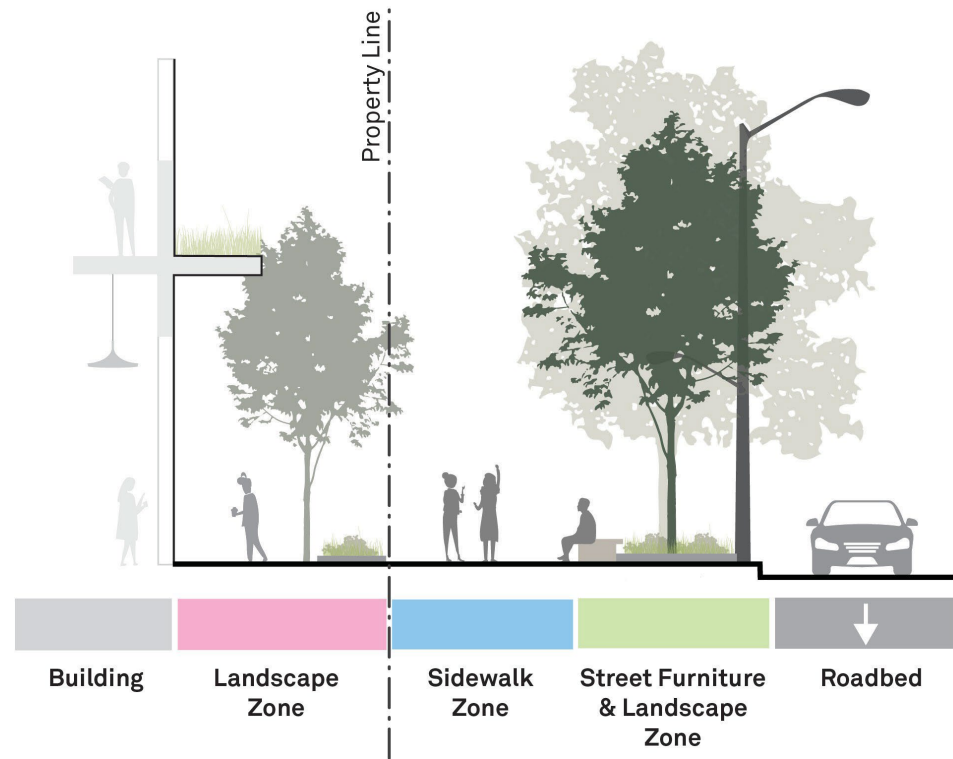
*The protection and regeneration of the natural landscape should be encouraged through site planning and design.*

# 5.1 Sustainability

## Design Guidelines

### General

- a. Sustainable site and building design and construction techniques in new development that reduce energy and water consumption, and improve air quality, water quality, and waste management should be encouraged and promoted.
- b. In addition to the protection and enhancement of trees and other natural features in the Greenlands and Major Parks & Open Space designations, additional trees should be planted on sites and public streets as part of the development process.
- c. Pedestrian, cycling, and transit should be promoted to encourage alternatives to automobile use.
- d. The use of recycled materials for building construction is encouraged.
- e. Construction of solar-ready buildings should be encouraged including consideration of required roof loads, as appropriate.
- f. Use LID techniques in the site design process to reduce impermeable surfaces and stormwater runoff.



*A landscape zone should buffer the public realm and reinforce the sustainable and green nature of the Employment Area.*

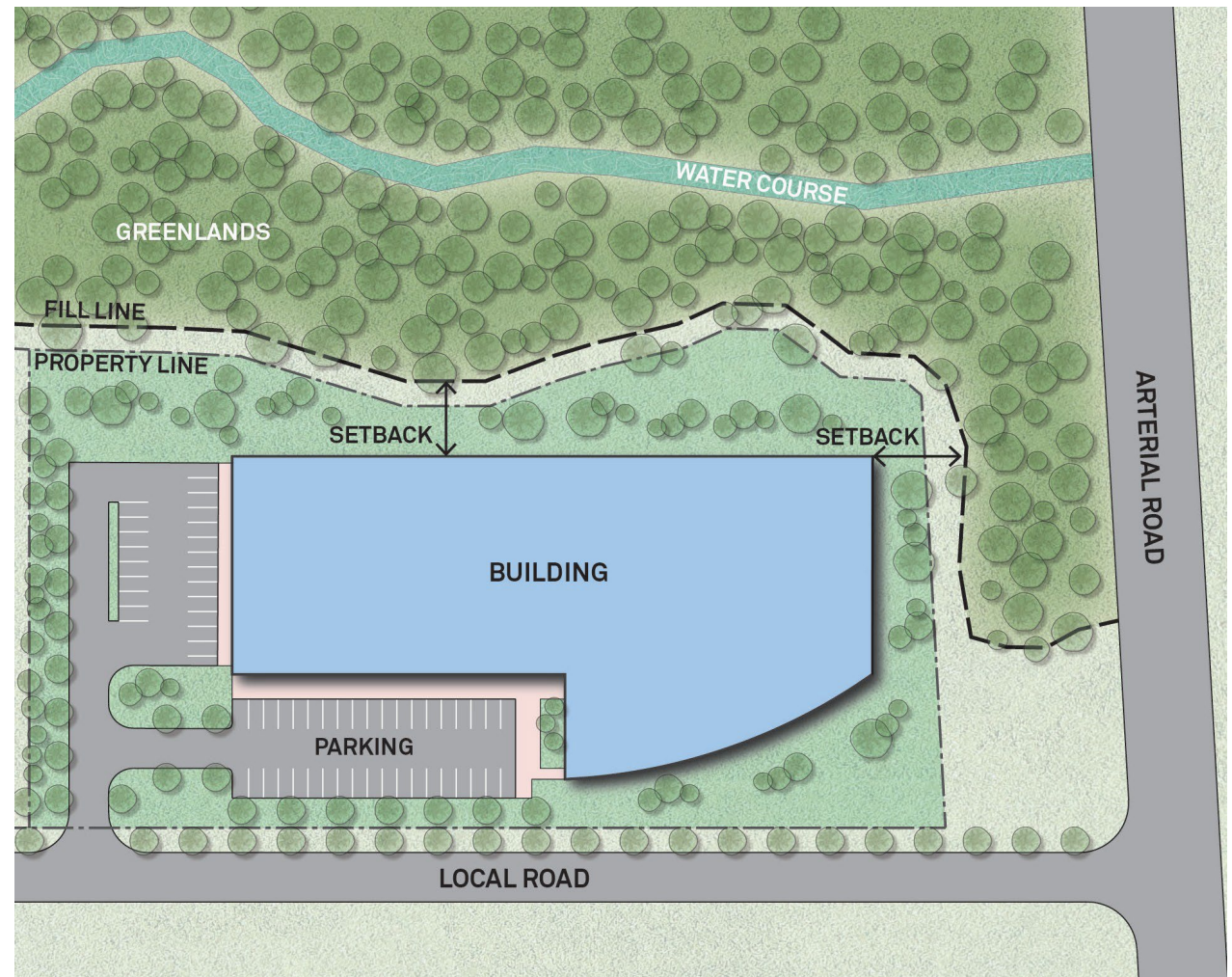
- g. Integrate white roof practices into the site and built form design to contribute to reduced energy use.
- h. Built form should be oriented to maximize exposure to natural light.
- i. Open spaces should be oriented to maximize exposure to natural light and minimize wind impacts.
- j. Green roofs are encouraged to contribute to stormwater retention.
- k. The on-site storage and filtration of stormwater is strongly encouraged.
- l. Parking infrastructure should include, where possible, biofiltration trenches, electric vehicle charging stations, and solar panel shade structures to minimize urban heat island effect.
- m. A landscape zone should buffer the public realm and provide a contiguous row of trees to support a comfortable pedestrian experience. The landscape zone may also contain bioswales and will support the attenuation of stormwater to further reduce run-off.

# 52 Greenlands

## Design Guidelines

- a. Contribute to the preservation and enhancement of lands in the Greenlands designation through sensitive site planning and design including:
  - i. Prohibiting the planting of non-native and invasive species particularly adjacent to the top of bank of valleys and ravines or natural features.
  - ii. Providing naturalized buffers between development and Greenlands.
  - iii. Providing physical and visual connections to lands in the Greenlands designation.
  - iv. Encouraging outdoor uses and facilities such as picnic areas, patios, employee recreation facilities and pedestrian areas in locations adjacent to Greenlands so that these natural features may act as amenities for users.

Site Planning Near Greenlands



## 5.3 Major Parks & Open Space

### Design Guidelines

- a. Provide strong pedestrian and recreational pathways to connect parks and open spaces to the natural lands, as part of a pathway system that connects to the Sixteen Mile Creek system. In addition, provide physical and visual connections from parks to adjacent public streets and other features.
- b. Provide built elements such as trellises or pavilions to act as landscape markers that define the focal importance of parks and provide passive recreational elements.



# 5.4 Stormwater Management Ponds

## Design Guidelines

### General

- a. Stormwater management ponds should be integrated into developments using sustainable design strategies.
- b. Where appropriate, stormwater management ponds should be considered important site amenities and should be accessible and visible to employees and site visitors. Accessible walking paths should allow for views and recreation near stormwater management ponds where appropriate.
- c. Edges of stormwater management ponds abutting Greenlands should remain naturalized.



*Stormwater management ponds should be integrated into developments (Photo: Ontario Tech University).*

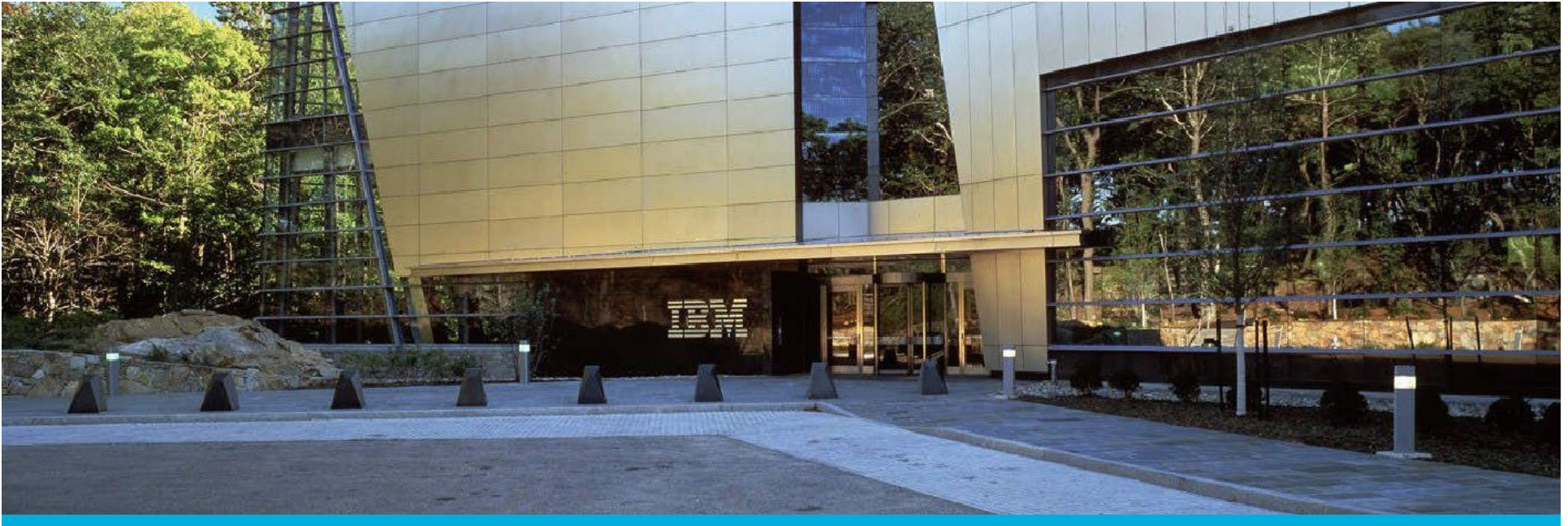
### *Phase 2B Area*

- d. Stormwater management ponds should incorporate naturalized plantings to manage access, improve aesthetics, and promote biodiversity.
- e. Stormwater management ponds should be designed to provide bird and fish friendly habitats. This may be achieved through the design of naturalized channels, the use of native species, and the provision of shade by planting trees along the east, south, and west edges of the ponds. Overbank vegetation is encouraged.
- f. An access road with a width of approximately 4.0 metres shall be provided where stormwater pond servicing is required.



*Naturalized stormwater management pond.*

# 6.0 Implementation



The Premier Gateway Employment Area Urban Design Guidelines are to be utilized in conjunction with the Official Plan and Zoning By-law to act as a framework for development implementation.

These guidelines should be considered a flexible tool to illustrate both the general and site specific framework. The urban design guidelines aim to fulfill the Town's vision and objectives for the Premier Gateway Employment Area, in order to ensure high-quality development with a strong visual identity.

The intent of the guidelines is to establish a planning framework that will help the area thrive as a dynamic, accessible, and economically competitive employment area that reflects urban design aesthetics and sustainable development practices appropriate for this area. They are not to be viewed as requirements forcing adherence to all guidelines. Specific plans for site development and individual buildings will vary, and this diversity is encouraged.

## 6.1 Review and Approvals Process

The guidelines will be utilized at the various review stages of development approvals, from Draft Plan of Subdivision through to Site Plan Approvals. In addition, they offer a review function for design input from the municipality into detailed design at the building permit stage.

Applications for approvals for specific properties within the Premier Gateway Employment Area must demonstrate how their planning and design fulfills the Urban Design Vision and Guiding Principles, and the specific guidelines applicable to the application. Third party peer review may be required for large projects.

The process involves the following stages:

### 1. Draft Plan of Subdivision

Applications requesting approval for Draft Plan of Subdivision should be evaluated in the context of these guidelines. Specific review should be given to the applicable guidelines within Site Plan Design Guidelines, Built Form Design Guidelines, and Environment and Open Space Guidelines.

Applications should provide a description of how the Draft Plan of Subdivision implements the applicable guidelines for the site in question.

### 2. Site Plan Review

Applications for Site Plan Approval will be subject to review of these guidelines and the Town's applicable planning, construction and engineering requirements.

Applications should provide a description of how the development implements the guidelines within both their general and site specific context. Applications shall be assessed with respect to the Site Plan Design Guidelines, Built Form Design Guidelines, and Environment and Open Space Guidelines.

### 3. Building Permit

Building permits will be reviewed for confirmation that individual developments will be implemented in accordance with Site Plan Approvals and with the design intent of the guidelines for built form and landscaping.

## 6.2 Partnership Opportunities

To assist in the implementation of the guidelines, the Town of Halton Hills should consider potential partnership opportunities with other governmental bodies, agencies, and key stakeholders to realize shared goals and to uphold the document's Urban Design Vision.

Partnership opportunities may include:

### **The Ministry of Transportation:**

- The Ministry of Transportation (MTO) is responsible for establishing setbacks from Highway 401 and 407 and the potential 413. Continued cooperation and engagement between the Town of Halton Hills and MTO is required to ensure that all design guidelines reflect a shared vision for these highways and abutting land uses.

### **Developers, land owners, business owners, employees, and residents:**

- The Town should monitor the implementation of the urban design guidelines on an ongoing basis to ensure that it results in development that implements the Urban Design Vision and reflects a high quality of design.



## 6.3 Urban Design Review Process

This document **should be reviewed** every five years and updated where required to reflect changes to building design standards, sustainable design, and the Premier Gateway Employment Area's existing and emerging context.

Updates to the guidelines must reflect the intent of the Official Plan and the Zoning By-Law, and should reflect the overall Urban Design Vision and Guiding Principles established for the area.



# 7.0 Definitions

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**Angular Plane:** A conceptual inclined plane rising over a lot, drawn at a specific angle from the horizontal, which helps to shape the maximum bulk and height of buildings, to ensure adequate access to privacy, sun, and sky views.

**Boulevard:** A boulevard is the area of the street between the building face or the front property line, and the edge of the curb.

**Compatibility:** Characteristics of buildings, including scale, height, materials, and landscaping, which allow buildings to be complementary in design with the existing area but does not require them to look exactly the same.

**Development:** The creation of a new lot, a change in land use, or the construction of buildings and structures requiring approval under the Planning Act, R.S.O. 1990, c. P.13. This does not include activities that create or maintain infrastructure used by a public body and authorized under an environment assessment process; or, works subject to the Drainage Act. (PPS, 2005, amended).

**Facade:** The exterior wall of a building that faces public view, usually referring to the front wall.

**Loading Area:** An area other than a street or laneway used for the loading or unloading of vehicles.

**Low Impact Development (LID):** A design approach to manage stormwater runoff and promote green infrastructure. LID techniques promote increased evapotranspiration, infiltration, and groundwater recharge and also lower surface runoff volumes and flow rates.

**Scale:** The relative size of a building as perceived by pedestrians, which is a product of multiple factors including size, height, bulk, massing, material use, and local context.

**Servicing Area:** The portion of a building or site that contains services critical to building function. This may include waste storage and pickup areas, as well as material loading and unloading areas.

**Setback:** The distance from the property line from which a building is built.

**Stepback:** A recess at the top of a building's podium, base, or upper levels that ensures an appropriate built form scale from the property edge and reduces the perception of mass in a building's upper levels.

**Streetwall:** The condition of enclosure along a street created by the fronts of buildings, and enhanced by the continuity and height of the lower facades. Upper levels, when set back, have less impact on the streetwall.

**Storage Area:** A portion of a building or site used for the storage of equipment, materials or products.



*Steeles Avenue and Sixth Line South*