

X6

HAMLET OF GLEN WILLIAMS DESIGN AND HERITAGE PROTECTION GUIDELINES

The heritage character of the hamlet is rooted in Glen Williams' history as a mill town of the 1800's, a character that is reflected in the architectural tradition of buildings from time. The consultant team, through the public participation process, has recognized that the community has a strong desire to preserve this history and the social, intimate characteristics typical of a small town.

The hamlet design analysis has revealed that, despite the strong impact of heritage buildings in the hamlet centre, the overall architectural character of Glen Williams is a variety of building forms and styles, representative of Glen Williams' organic pattern of growth over the last century.

The guidelines below describe how the heritage character of the community should be retained both in its infrastructure and its building forms, both existing and future

X6.1

STREET TYPE AND PATTERN

Older streets in the hamlet have street sections as narrow as 12 and 15m. Narrower streets allow for houses to have "eyes on the street", which contributes to a safe and intimate pedestrian environment. There is a strong sense of this "community supervision" in the hamlet. The following guidelines should be considered when developing or improving new roads:

- Consider the use of rural road or rolled curb sections to promote the rural character of the hamlet;
- Consider modifying road engineering and lot grading standards, without compromising safety standards, where new streets encounter topographical features, woodlots, single trees, and other natural features, to preserve the natural character of streets;
- Utilize a range of street/block types including irregular blocks, short orthogonal blocks, winding streets, and rural cul-de-sacs to preserve the varied character of the hamlet street system; and,
- Limit block lengths to 175m, in keeping with the smaller block lengths of the hamlet centre.

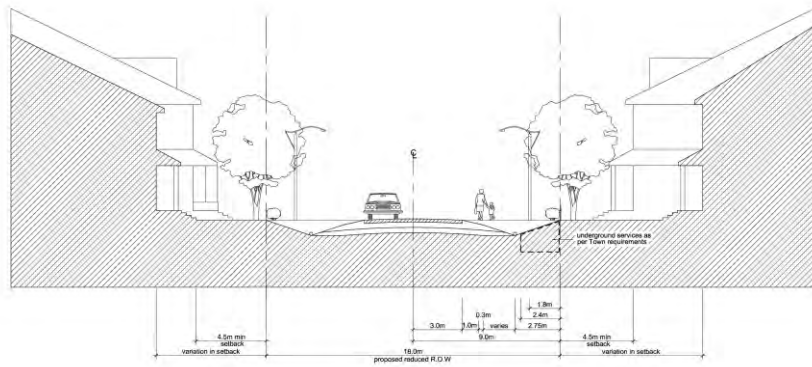


Figure X6.1: Proposed Rural Cross Section. Note: This sketch is for illustrative purposes, and does not replace Town standards.

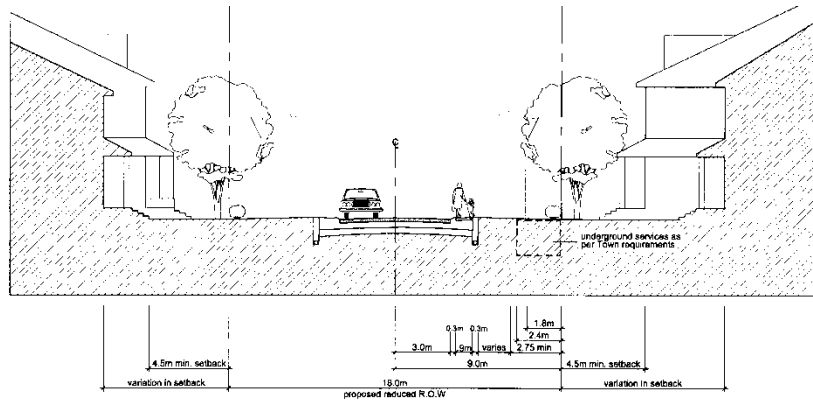


Figure X6.2: Proposed Rolled Curb Street Section. Note: This sketch is for illustrative purposes, and does not replace Town standards.

X6.2 LOT CONFIGURATION

- Vary lot frontages and depths within each streetscape to maintain the hamlet's random lot pattern. Allow adjacent lots to vary in lot configuration.
- Ensure that lot sizes allow for the safe and effective installation/connection of sanitary services (private, communal or municipal), per regulatory requirements.
- Avoid streetscapes with uniform lot frontages. Permit adjacent lot frontages to vary up to 50%. It is recommended that no more than four consecutive lots shall have the same frontage. Beyond a maximum of four lots, allow adjacent lot frontages to vary by 50%.

X6.3 SETBACKS

X6.3.1 Front Yard

- Encourage flexibility of front yard setbacks to maintain the variety of setbacks found on hamlet streetscapes. It is recommended that no more than four consecutive lots shall have the same front yard setback.
- Ensure that no front wall of a house shall be set further back than half the length of the adjacent house to maintain privacy of rear yards.
- On one streetscape, ensure that a minimum of 30% of the front wall of houses are located at the minimum setback to provide a sense of enclosure to the street and a pedestrian oriented environment. Consider revising the current 50ft (15m) minimum front yard requirement to 4.5m.



Figure X6.3: Setbacks

X6.3.2 Side Yard

- Side yard setbacks should allow for access, servicing requirements, variations in grading and natural features.
- Side yard setbacks in the hamlet vary from as low as 2m up to 35m. Consider revising the current 15ft (4.5m) interior setback to 2.25m to allow for flexibility of siting of the main house. Refer to "Houses at Corner Lots and Pedestrian/Open Space Links" for reference to exterior side yard setbacks.

X6.3.3 Rear Yard

- The current 25ft (7.6m) rear yard setbacks can be maintained for all lot depths.

X6.4 HOUSES AT FOCAL LOCATIONS

X6.4.1 Corner Lots

- Houses at corner lots are important within a streetscape as they are visible from both streets and create the entrance condition or a “gate” to the street. Flanking elevations, garages and private yard enclosures are exposed to the public realm at these locations. The design of these buildings and elevations should have special consideration.
- Exposed elevations should have equal importance with respect to openings and attention to detail. The use of wrap-around porches and corner bay windows is encouraged to link the two facades and to accentuate the corner condition. The main entrance should be located on the long frontage to avoid blank sections of walls.

X6.4.2 Houses at Pedestrian Trails/Links and Open Space Areas

- Houses that border upon open space are also visible from two sides, similar to corner lots. Both elevations should have equal importance with respect to openings and attention to detail.
- The main entrance should face the street. The use of wrap-around porches and corner bay windows is encouraged to link the two facades, to accentuate the corner condition; to link the two elevations; and, to provide a visual connection from the house to these public areas.
- For both corner and pedestrian link locations, consider a reduction of the current exterior side yard setback of 30ft (9.1m) to 4.5m to increase the sense of community supervision at these public space connections.

Note: A 10 metre setback will be required from valley top of bank to lot lines to allow adequate space for pedestrian trails.

X6.5 GARAGES AND AUXILIARY BUILDINGS

- Encourage the use of detached garages that are located at the rear of the lot. Many garages in the hamlet are detached and to the rear and/or side of the lot. To encourage this design strategy, consider exemption of the area of rear yard garages from calculations for maximum coverage, under the zoning by-law.
- Where garages are attached, they shall be recessed a minimum of 1.0 m from the face of the house. Avoid garages that project forward from the front wall of the house.

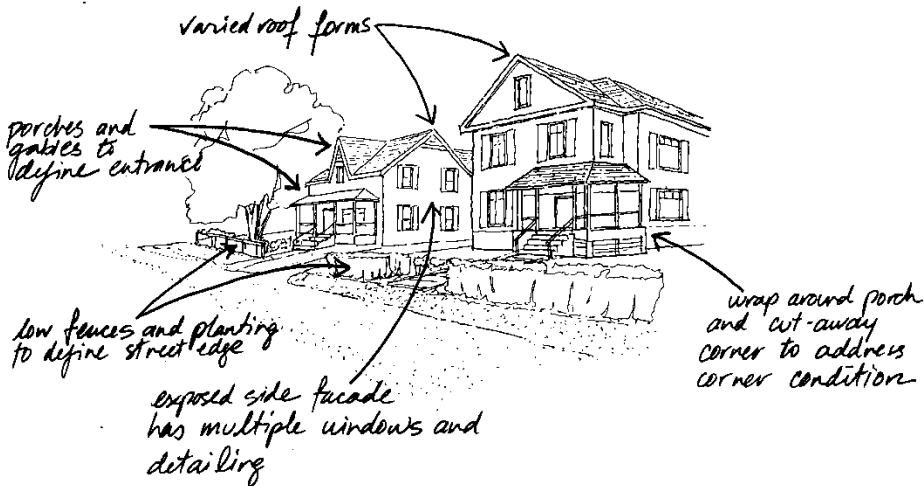


Figure X6.4: Urban Design Principles

X6.6 ARCHITECTURAL DESIGN PRINCIPLES FOR NEW DEVELOPMENT

The following guidelines are intended to aid developers and builders to create buildings and streets that contribute to the quality of intimate, random and individual qualities of streetscapes that can be found in the heritage district and that could foster the kind of atmosphere that can be defined by “hamlet character”.

X6.6.1 Street Address

The main elevation of houses shall address the lot frontage to provide a clear identification of the street address. Architectural elements such as the front entrance; habitable spaces with windows to the street; porches and stairs; and, terraces and balconies, convey the sense of houses “looking out onto the street”.

X6.6.2 Entrance Architecture

- The design of houses should accentuate the main entrance. Attention should be given to the architectural detailing of entrances and their importance in setting the character, or “identity” of the streetscape.
- The use of porches is encouraged as a means to define the entrance and create well-proportioned front elevations. The detailing of porches should be integrated with that of the house as a whole.
- Porches foster social activity between the house and the street, which is very common along the streets of Glen Williams. Porches should be generous enough in depth and length to allow for furniture and planting. A minimum depth of 1.8m is recommended.
- Walkways from the entrance to the street are encouraged as a means of linking street and property at a pedestrian scale.



Figure X6.5: Entrance Architecture

X6.6.3 Relationship to Grade

The relationship of the house to grade is important in the streetscape. The main floors of houses in the hamlet tend to be at grade or close to grade. In cases of strong topography, entrance levels are related to grade through terracing. Basement garages or high service floors do not appear in the hamlet and should be avoided.

X6.6.4 Windows and Projecting Elements

- The design, placement and size of windows are important in creating architecturally well-proportioned streetscapes and affect the sense of privacy between properties. Special attention should be given to the location and detailing of windows.
- Projections such as bay windows and balconies, chimney elements, projecting cornices and roof eaves are encouraged to create variety along the streetscape.
- Bay windows, balconies, porches and porticoes may project up to 1.8m from the main building face into the front yard setback. This is intended to encourage houses to have these elements and be located close to the street edge.
- Bay windows may be single or double storey in height. Their proportions should be appropriate to the building from which they project.

X6.6.5 Roofs

Because of the various ages of houses in Glen Williams, no single roof type or pitch is prevalent. Heritage homes typically have steeply-pitched roofs with a variety of roof forms such as dormers and gables, while bungalows have shallower hip roofs. A variety of roof forms appropriate to the scale and architecture of the built form is encouraged.

X6.6.6 Construction Materials

- A variety of building materials is used throughout Glen Williams. To promote the character of the hamlet, the use of materials found in heritage buildings, such as brick, stone and wood is encouraged.

- Some houses and porches in the hamlet, mostly those of wood siding, are painted light colours. This creates an attractive, lively streetscape. The use of colour is encouraged for building facades and/or for architectural details to create streetscapes that are in keeping with those of the hamlet centre.
- Materials for garages and outbuildings should be similar to those used for the main house.

X6.6.7 Landscaping

- The use of fences and landscaped elements, used in combination, is encouraged to delineate between properties.
- Where an existing natural feature is located within a property, such as a woodlot or single tree, it should be integrated into the landscaping design as a means to promote and preserve Glen Williams' natural setting.
- Many paths to houses in the hamlet are identified with planted features. Where walkways extend to the street, they should be augmented with planting both to provide an alternate means of street address and to bring natural elements to the street edge.

X6.7 IMPLEMENTATION

X6.7.1 Design Review

The Design Review process shall occur in conjunction with applications for Draft Plan Approval and prior to application for building permits. The Design Review process shall monitor the realization of the vision for Glen Williams including:

- preservation and promotion of the character of Glen William's built form;
- protection and enhancement of Glen William's open space network and natural environment;
- improvement of pedestrian connections to the commercial and community facilities of the hamlet core and to Glen William's open space system; and,
- preservation of the balance between development in the hamlet and adjacent natural lands.

The Design Review process shall determine how new development fulfills the hamlet design recommendations for:

X6.7.1.1 Street System

- A street pattern that responds to significant natural features and / or topography.
- Street sections and streetscape elements.
- Connection of parcel to other neighbourhoods and/or open space with pedestrian paths, where possible.

X6.7.1.2 Lot Configuration, Setbacks, Housing Types, and Garages

- Variation of lot sizes.
- Setbacks.
- Proposed housing types and garage locations.

X6.7.1.3 Houses at Focal Locations

- Treatment of houses at corner lots, those abutting pedestrian trails and open spaces and at other focal locations.

X6.7.1.4 Architectural Principles

- Response of new development to heritage character, where new development is in proximity to, or includes within the site, buildings of heritage character.
- Intended architectural character of built form.



Figure X6.6: Landscaping

X6.7.1.5 Landscaping Principles

- Integration of natural features on the site.
- Character of new landscaping and streetscape features.

X6.8 GATEWAYS

Gateway features should be established at key entry points within the community to help strengthen the character and identity of Glen Williams Development at gateways should therefore help shape this sense of identity by the nature and quality of landscaping, built forms and design features such as public art.

Depending on location and available space, gateway features may include taller architectural elements that symbolize entry-like gateposts such as columns and customized lighting fixtures. Landscape features (such as plantings, flags, special signage, and banners) may also accentuate gateways.

In order to strengthen the gateway image, different public art features such as sculptures, fountains, and decorative walls with murals may be used. Gateways should be given first priority when considering the placement public art features.

Building developments within Gateway areas must incorporate streetscape improvements that will serve to provide shelter to pedestrians at these major intersections. For example, this can be accomplished by setting back the building and developing a public space that incorporates landscaping, public art, lighting and/or shelters.

X6.9

DOCUMENTATION

The information required for the Design Review process, in addition to normal requirements of draft plan application, shall be prepared by a qualified architect and or landscape architect and include:

- a) Site Plan(s) indicating:
 - i) setbacks, heights and housing locations;
 - ii) clear location and site dimensioning of septic beds, if privately serviced;
 - iii) indication of pedestrian connections and access to open space;
 - iv) topography and new grading;
 - v) existing vegetation and proposed character of landscaping; and,
 - vi) roads, walkways, driveways, terraces and other impervious surfaces location of public features such as postal kiosks and any above grade utilities; and,
- b) cross sections of each street type showing:
 - i) width of right-of-way;
 - ii) type of road section being used with all services located as per the Town's requirements;
 - iii) location of street lighting and furniture;
 - iv) boulevards; and,
 - v) streetscape elevations illustrating proposed residential character, including:
 - entrances and porch locations;
 - heights;
 - roof form; and,
 - fenestration.