

Norval Secondary Plan Review

June 19th

Public Workshop #3

Summary

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1.0 Introduction

On Wednesday, June 19th, the third public workshop was held for the Hamlet of Norval Secondary Plan Review. The objectives of the workshop were to:

- Provide a status/progress update on the study to date;
- Present the draft Secondary Plan, Zoning By-law amendments and revised Hamlet Design & Heritage Protection Guidelines;
- Solicit public and stakeholder input on the Hamlet Design & Heritage Protection Guidelines, the Draft Secondary Plan and the Draft Zoning By-law Amendment.

1.1 Workshop Outline

The workshop began with a drop-in session to allow for participants to arrive and meet the consultant team. A presentation was then made by Lorelei Jones (Macaulay Shiomi Howson). Following the presentation, attendees were invited to participate in a workshop with small round table discussions. People were assigned to four different tables to encourage a mix of participants and ideas at each table. Groups were asked to engage in discussions and complete worksheets to obtain their input on the draft documents using a set of key questions. A member of the consulting team or planning staff was at each table to answer questions and assist with the discussion if needed.

1.2 Who Came to the Workshop?

Approximately 100 people attended the workshop, including a mix of Town Staff, Councillors, residents, key stakeholders, and representatives from the development community. Members of the Town planning staff were on hand to help facilitate the workshop and answer questions.

1.3 What was Presented?

The workshop began with remarks from Councillor Robson, Steering Committee Chair, on the process to date, including previous Steering Committee meetings and two open houses to maximize opportunities for public input. Next, a presentation was made by Lorelei Jones (Macaulay Shiomi Howson). The presentation addressed the following topics:

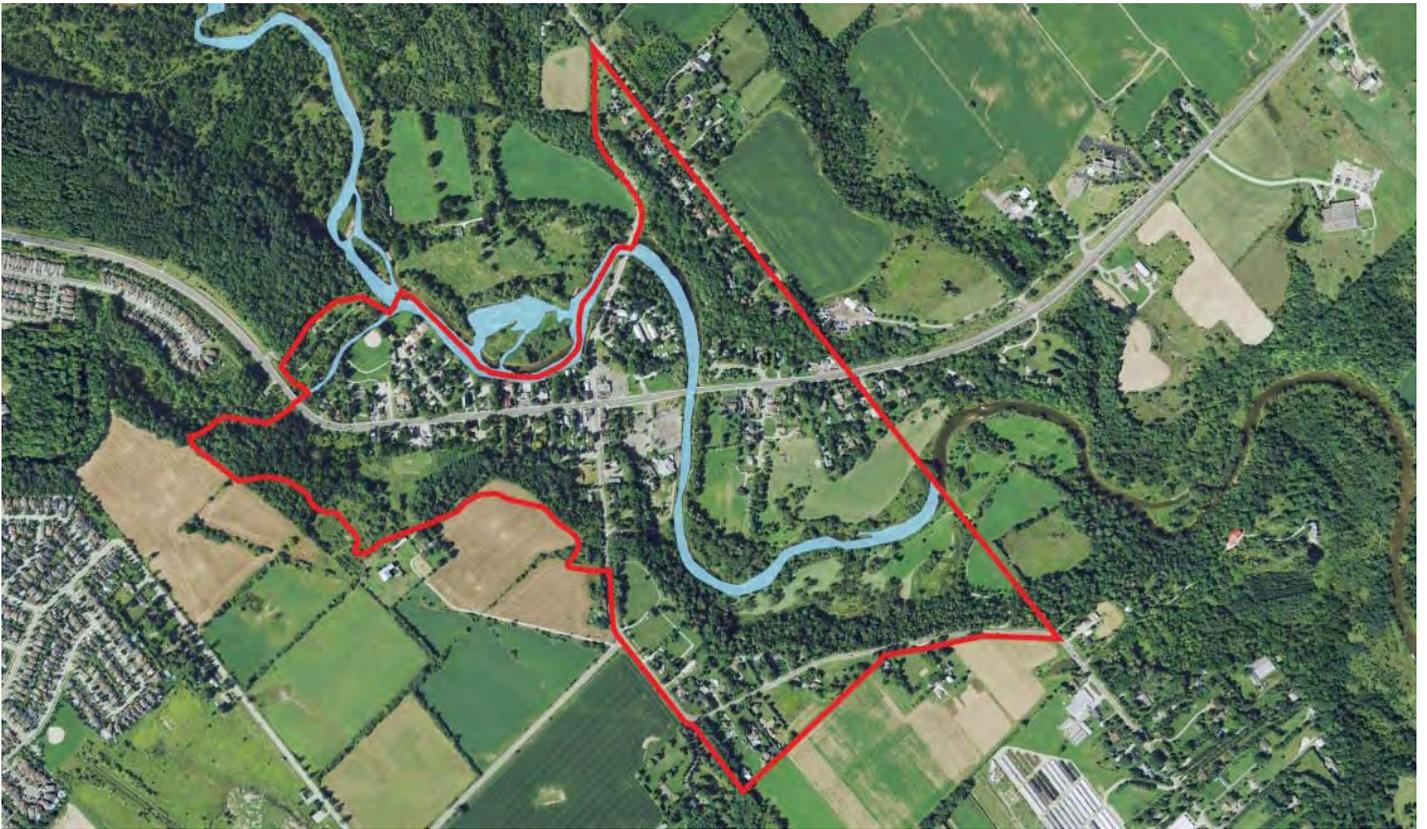
- Study Process to date
- Revised Objectives
- Valleyland & Natural Features Protection
- Draft Secondary Plan and Zoning By-law Amendments
- Land Use Policies and Mapping
- Transportation
- Heritage Protection and Design
- Next Steps

2.0 Workshop Exercise

In small groups, and using two large worksheets for guidance, participants were encouraged to discuss and respond to six key considerations from the draft Secondary Plan, draft Zoning By-law and Hamlet Design Guidelines, including:

- Consideration # 1: Automotive Uses
- Consideration # 2: Proposed Hamlet Residential/Office Area
- Consideration # 3: Size of Commercial Uses
- Consideration # 4: Building Height/Character
- Consideration # 5: Parking/Streetscape
- Consideration # 6: Multi-Use Trails

(Please see Appendix A for the worksheets, including brief descriptions of the design guideline criteria, as well as the Draft Secondary Plan and Draft Zoning By-law Amendment Maps).



2.1 Key Findings

Although a wide-range of feedback was generated from each of the four working groups, there were some notable points of consensus. These key findings are summarized below, and (in Section 2.2) added and/or integrated into the Priority Directions that resulted from the June 19th Vision Workshop.

Consideration #1: Automotive Uses

- Many participants felt there were too many restrictions on automotive land uses as automotive uses had always been part of the hamlet character and that existing zoning rights should not be taken away. Some participants did not want to see new automotive uses but felt existing uses should be allowed to continue. Many suggested allowing accessory used car sales but some wanted restrictions on the number of used cars that could be sold from a lot.



Consideration #2: Proposed Hamlet Residential/Office Area

- Most participants liked this concept, but were concerned about parking for these uses (3 out of 4 groups).

Consideration #3: Size of Commercial Uses

- There was no consensus between the groups on whether a 250 square metre cap should be placed on commercial buildings. Some felt that this should be left up to lot size.

Consideration #4: Building Height/Character

- All groups felt that new buildings should fit into the character of the Village; however, some were concerned with how the Guidelines would affect existing uses.

Consideration #5: Parking/Streetscapes

- The groups were generally in favour of the guidelines for parking and streetscape design. Although, participants were concerned about where this space for parking and streetscape improvements would be found and how much money this would require.



Consideration #6: Multi-Use Trails

- Most groups were in favour of the proposed trail system, as long as effort was taken to protect the natural environment.

2.3 Group Discussion Questions

In small discussion groups, participants were asked to consider a number of key land use and policy changes to help prepare the updated Secondary Plan for the Hamlet of Norval. The findings for each group are summarized in the following sections.



Participants were asked to comment on, from top to bottom, existing automotive uses, heritage preservation, and commercial uses.

Group 1

Automotive Uses: Do you agree that the Secondary Plan/Zoning By-law approach to recognizing automotive uses strikes the right balance between the objectives of protecting Hamlet character and supporting existing businesses? Why or Why not?

- Disagree. Keep previous zoning provisions.
- Hamlet was created on commercial uses, not residential.
- Uses on auto properties are being restricted and others are not being restricted as much.
- Increases in restrictions are decreasing property values.

Proposed Hamlet Residential/Office Area: Do you agree with the conversion of part of the Hamlet Community Core designation to a new Hamlet/Residential Office Area designation which permits residential uses, but also medical or business offices?

- There is no parking for these uses.
- Prefer "Hamlet Commercial Core" to be strictly residential.

Commercial Uses: Do you agree with the proposed cap of 250 square metres for new commercial uses, requiring a re-zoning with design, traffic, services, noise and land use compatibility studies to support commercial uses between 250 and 500 square metres? Why or Why not?

- Should be based on lot size – let each property develop on its own merit.
- Region of Halton allows 500 square metres, so the Town should too.

Building Height/Character: Do you agree with the Design Guideline approach to building height and architectural character? Why or Why not?

- Maximum height up to 2-storeys for all uses.
- Should be determined by lot size.

Parking/Streetscapes: Do you agree with the Design Guideline approach to parking and streetscapes? Why or Why not?

- Concerned with existing street width for street furniture.
- Only agree if funds are available from MTO.
- Need more street lights.
- Properties east of the 4 Corners do not receive same investment as west of the 4 Corners.

Multi-Use Trails: Do you agree with the Design Guideline approach to trails as illustrated above?

- No more trails – keep as is.
- More trails will encroach on natural habitat.

Additional Comments:

- No need to change anything – leave as is.
- Project is "ripping" community apart.
- Add designated crosswalks at Mary Street and on the east side of Norval.

Group 2

Automotive Uses: Do you agree that the Secondary Plan/Zoning By-law approach to recognizing automotive uses strikes the right balance between the objectives of protecting Hamlet character and supporting existing businesses? Why or Why not?

- Automotive uses take away the feeling of the village (visual impact).
- Whatever rights they have now they should keep.
- View that past uses on these properties contributed to identity of Norval.

Proposed Hamlet Residential/Office Area: Do you agree with the conversion of part of the Hamlet Community Core designation to a new Hamlet/Residential Office Area designation which permits residential uses, but also medical or business offices?

- Ok. Subject to addressing parking and traffic concerns.

Commercial Uses: Do you agree with the proposed cap of 250 square metres for new commercial uses, requiring a re-zoning with design, traffic, services, noise and land use compatibility studies to support commercial uses between 250 and 500 square metres? Why or Why not?

- General group agreement.
- Concerns about how this will affect existing businesses.

Building Height/Character: Do you agree with the Design Guideline approach to building height and architectural character? Why or Why not?

- New buildings should fit into the village.
- Tourism is important – concerned about design and location of Carpet Palace (what not to do).
- Concern about additional costs of complying with guidelines.

Parking/Streetscapes: Do you agree with the Design Guideline approach to parking and streetscapes? Why or Why not?

- Good idea, but Guelph St. is busy – where would the space come from?
- What happens to parking (on street) for businesses, community gathering places/churches?
- If bypass is built, then there is support for this idea.
- Like parking lot design.

Multi-Use Trails: Do you agree with the Design Guideline approach to trails as illustrated above?

- As long as they are not over private property.
- Safety risks at certain locations.
- Limit to where it can go.
- Where to park when walking?

Additional Comments:

- If servicing comes to Norval, how will that impact the look and feel of the community?
- Would rather see crosswalk at Mary Street than left turn lane.
- Flow of traffic is a huge concern.



The Hamlet has a strong stock of buildings with heritage value. Participants were in agreement that these buildings should be protected.



Group 3

Automotive Uses: Do you agree that the Secondary Plan/Zoning By-law approach to recognizing automotive uses strikes the right balance between the objectives of protecting Hamlet character and supporting existing businesses? Why or Why not?

- Yes, but existing rights should not be removed (i.e. accessory auto sales).

Proposed Hamlet Residential/Office Area: Do you agree with the conversion of part of the Hamlet Community Core designation to a new Hamlet/Residential Office Area designation which permits residential uses, but also medical or business offices?

- Yes

Commercial Uses: Do you agree with the proposed cap of 250 square metres for new commercial uses, requiring a rezoning with design, traffic, services, noise and land use compatibility studies to support commercial uses between 250 and 500 square metres? Why or Why not?

- No because it's a non-issue. Property size limits this.

Building Height/Character: Do you agree with the Design Guideline approach to building height and architectural character? Why or Why not?

- Yes.

Parking/Streetscapes: Do you agree with the Design Guideline approach to parking and streetscapes? Why or Why not?

- Yes, great to have design guidelines!
- No space for parking.

Multi-Use Trails: Do you agree with the Design Guideline approach to trails as illustrated above?

- Great idea!

Additional Comments:

- Traffic is a large concern.
- Heritage guidelines are good to have for Norval.
- Need more heritage designations for heritage homes in Norval.
- The 2 year limit from CVC to replace buildings in the floodplain is too short.

Group 4

Automotive Uses: Do you agree that the Secondary Plan/Zoning By-law approach to recognizing automotive uses strikes the right balance between the objectives of protecting Hamlet character and supporting existing businesses? Why or Why not?

- Do not like options removed (specifically used car sales)
- Limit the number of used cars they have.
- We want freedom of use within "character" designation.
- Restrictions cause re-sale problems.
- Auto uses encourage economic growth but don't allow new auto uses.

Proposed Hamlet Residential/Office Area: Do you agree with the conversion of part of the Hamlet Community Core designation to a new Hamlet/Residential Office Area designation which permits residential uses, but also medical or business offices?

- Yes

Commercial Uses: Do you agree with the proposed cap of 250 square metres for new commercial uses, requiring a rezoning with design, traffic, services, noise and land use compatibility studies to support commercial uses between 250 and 500 square metres? Why or Why not?

- Agree.

Building Height/Character: Do you agree with the Design Guideline approach to building height and architectural character? Why or Why not?

- More design freedom is better.
- Most group members in support.

Parking/Streetscapes: Do you agree with the Design Guideline approach to parking and streetscapes? Why or Why not?

- Yes.
- Add crosswalks.

Multi-Use Trails: Do you agree with the Design Guideline approach to trails as illustrated above?

- Yes, I love it!
- Would like sidewalks north and south, as well as east-west.

Additional Comments:

- Sidewalks please!
- Traffic is a major issue.
- Changes should not have a negative impact on current businesses and property values.
- Crosswalks as traffic calming

Norval Secondary Plan Review June 19/ Public Open House

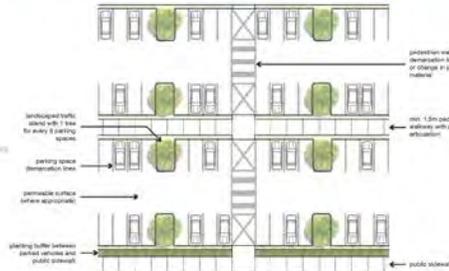
Exercise: Hamlet Design and Heritage Protection Guidelines

Using the graphics below, please discuss and respond to the questions on the recommendations of the draft Hamlet Design and Heritage Protection Guidelines.



- Shingled Roof
- Flowerbox
- Shuttered Windows
- Decorative Shutters
- Decorative Shutters
- Decorative Shutters
- Wide Front Porch

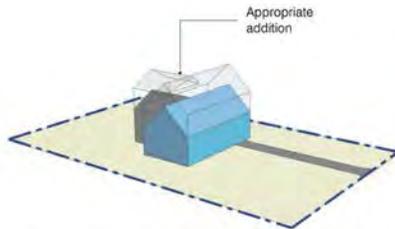
Development should be sympathetic to the heritage character described above.



The illustration above demonstrates the key components used to mitigate the negative impacts of surface parking lots.



Trails should link natural areas, parks, open spaces.



Renovations and additions to existing buildings should not be more than 1/3 of the existing building volume. The illustrations above demonstrate an example of an appropriate addition that is consistent with the existing character of the building.



Street furniture and landscaping should be located between the sidewalk and vehicle traffic and parking. Maximum sidewalk widths should be 1.85 metres with trees spaced consistently at 6 to 9 metre intervals.



1 Building Height/Character: Do you agree with the Design Guideline approach to building height and architectural character? Why or Why not?

2 Parking/Streetscapes: Do you agree with the Design Guideline approach to parking and streetscapes? Why or Why not?

3 Multi Use Trails: Do you agree with the Design Guideline approach to trails as illustrated above?

4 Do you have any other comments that you would like to share?
