



BY-LAW No. 2014-0020

A Bylaw to adopt Amendment No. 20 to the
Town of Halton Hills Official Plan - Revised Norval Secondary Plan

WHEREAS the Council of the Corporation of the Town of Halton Hills, is empowered to enact this By-law by virtue of the provisions of the Planning Act, 1990, R.S.O., c.P. 13, as amended;

AND WHEREAS the Regional Municipality of Halton, as the approval authority, has exempted this Official Plan Amendment from their approval;

AND WHEREAS on April 14, 2014, Council for the Town of Halton Hills approved Report No. PDS-2014-0012, dated March 5, 2014, regarding the revised Norval Secondary Plan and related Official Plan and Zoning By-law Amendments.

NOW, THEREFORE, BE IT RESOLVED THAT THE COUNCIL OF THE CORPORATION OF THE TOWN OF HALTON HILLS ENACTS AS FOLLOWS:

1. That Amendment No. 20 to the Official Plan of the Town of Halton Hills as amended, being the attached text and schedules, is hereby adopted as an exempt Local Official Plan Amendment in accordance with Regional By-law 18-99, except for the properties known as Lots 11-15, Plan 64 Louisa St., and 16 Adamson St. N, Norval shown on Schedule A to this Amendment as Decision Withheld 1 and Decision Withheld 2, which are hereby deferred, with Official Plan Amendment No. 1 to the Town of Halton Hills continuing to apply to the deferred properties in the interim;
2. That the Town Clerk is hereby authorized to circulate the Official Plan Amendment as provided for by the Planning Act regulating the appeal process.

BY-LAW read and passed by the Council for the Town of Halton Hills this 14th day of April , 2014.

MAYOR – Rick Bonnette

TOWN CLERK – Suzanne Jones

**AMENDMENT NO. 20
TO THE OFFICIAL PLAN FOR THE TOWN OF HALTON HILLS**

The attached text and schedules constitutes Amendment No. 20 to the Official Plan of the Town of Halton Hills as amended, which was adopted by the Council of the Town of Halton Hills by By-law 2014-0020 in accordance with the provisions of the Planning Act, 1990, R.S.O., c.P. 13, as amended.

THE CORPORATION OF THE TOWN OF HALTON HILLS

MAYOR – R. Bonnette

CLERK – S. Jones

AMENDMENT NO. 20

TO THE OFFICIAL PLAN FOR THE TOWN OF HALTON HILLS

PART I - THE PREAMBLE does not constitute part of the Amendment.

PART II - THE AMENDMENT consisting of the attached text and Schedule "A" to the amendment, constitutes Amendment No. 20 to the Town of Halton Hills Official Plan as amended.

PART III - THE APPENDICES do not constitute part of the Amendment. The appendices contain the Hamlet Design and Heritage Protection Guidelines which support the Secondary Plan.

PART I- THE PREAMBLE

1. PURPOSE

The purpose of this Amendment is to update the Norval Secondary Plan which was originally adopted by Town Council in 2001 and approved by the Region of Halton in 2003.

There has been significant changes to the Provincial, Regional, and local planning environment since the Secondary Plan was originally adopted including, the adoption of the Provincial Greenbelt Plan, the completion of the Halton Peel Boundary Area Transportation Study, a revision to the floodplain mapping by Credit Valley Conservation, and a request from the public to review many of the policies in the Secondary Plan.

A review of the Secondary Plan was therefore undertaken in 2012-2013 to develop updated goals, objectives, and a policy framework which better reflects the existing policy environment and the community's needs.

2. LOCATION

The Amendment affects the lands within the Hamlet of Norval as delineated by the Town of Halton Hills Official Plan.

The Hamlet of Norval is located at the crossroads of Highway 7 and Adamson Street (Winston Churchill Boulevard), and at the confluence of the Credit River and Silver Creek, in the Town of Halton Hills and the Region of Halton. It is situated directly east of the Georgetown Urban Area and directly west of the Region of Peel and City of Brampton municipal boundaries. A small part of the area historically associated with the Hamlet is under the jurisdiction of the City of Brampton and the Region of Peel.

3. BASIS

3.1 Review of Secondary Plan

Since the Secondary Plan was adopted in 2001 and approved by the Region of Halton in 2003, a number of factors indicated that a review was warranted including:

- The Hamlet of Norval Secondary Plan was due for a five year review as prescribed by the Planning Act, to ensure that it conformed to provincial plans, had regard for matters of provincial interest and was consistent with provincial policy. Conformity with the Provincial Greenbelt Plan was particularly important as Norval is within the Greenbelt Plan Area.
- Consideration of the outcome of the Halton Peel Boundary Area Transportation Study (HPBATS) which included options for the Norval by-pass and other transportation routes in the vicinity of Norval.
- Revised floodplain mapping was prepared by Credit Valley Conservation that impacted the Hamlet of Norval.
- Consideration of comments received from the Norval community through consultation on the Comprehensive Zoning By-law relating to a desire to revisit the maximum square footage per commercial premises, and the extent of the Hamlet Community Core land use designation.

On May 24, 2011 Council approved (Report No.: PDS-2011-0029) the undertaking of the Norval Secondary Plan Review to re-evaluate the land use designations and policies in the Hamlet of Norval Secondary Plan with public input to develop an updated Secondary Plan.

The Terms of Reference for the study set out a five phase process as follows:

- Phase 1 - Background Analysis and Issue Identification
- Phase 2 - Land Use Alternatives
- Phase 3 - Preferred Land Use Alternative
- Phase 4 - Initial Policy Formation
- Phase 5 - Final Policy Formulation

Public input has been considered throughout the multiphase review and was solicited through a series of open houses/workshops, mail outs, a project website, newspaper ads and a statutory public meeting.

Updated goals, objectives and a policy framework have been created which reflects the existing policy environment and the community's needs.

3.2 Central Issues to Norval

As part of the Secondary Plan Review, the central issues to the community and the basic assumptions of the Secondary Plan were reviewed and updated based on public and stakeholder input. The updated central issues of the community and assumptions of the

Secondary Plan are as follows:

- Traffic volumes and heavy truck traffic are a major concern. The future of Highway 7 and the proposed Norval By-Pass will have a tremendous impact on the community. Future environmental assessments will be undertaken to determine the By-pass alignments;
- The character of the Hamlet of Norval is closely related to the valley system of the Credit River;
- Norval is a predominantly residential community that includes a number of existing businesses which collectively contribute to the character of the Hamlet;
- Many of the residents have a deep sense of community, attributed largely to the distinctive rural/hamlet character of the area and its sense of place;
- No major growth of the residential component of the community is appropriate at this time;
- Norval's setting at the confluence of Silver Creek and the Credit River presents unique opportunities for linking the preservation and enhancement of its natural areas with tourism and economic development initiatives;
- Strengthening of the commercial core, and support for the existing businesses and the tourism industry is a major theme of the Secondary Plan. Existing automotive uses along Guelph Street will be recognized but no new automotive uses will be permitted;
- The Secondary Plan implements Provincial and Credit Valley Conservation policy for floodplain management and allows for minor expansions of existing development within flood impacted areas subject to flood proofing criteria but does not allow for the creation of new sensitive land uses or the expansion of existing ones within the floodplain;

- The preservation of the Hamlet core is critical to the maintenance and protection of Norval’s heritage character and tourism potential. The Secondary Plan provides clear guidelines aimed at creating a consistent hamlet image;
- The Secondary Plan does not envision any future new use of lands within the Hamlet for industrial uses due to compatibility issues; and,
- Planning for urban growth in the City of Brampton and the Georgetown Urban Area adjacent to Norval must take into consideration the preservation of the character of the Hamlet.

3.3 The Town of Halton Hills Official Plan

The Town of Halton Hills Official Plan designates the community of Norval as “Hamlet Area”. The plan provides policies for these areas but directs that all development will be subject to the policies in the approved Secondary Plan.

3.3 The Region of Halton Official Plan

The Region of Halton Official Plan provides a framework that ensures that growth is managed in a way that is environmentally and economically sustainable. The plan sets criteria for the management of growth throughout the Region. Norval is designated as a Hamlet within the Regional structure of the Regional Plan.

In the Region of Halton Official Plan, Hamlets are defined as compact rural communities which are intended to accommodate future residential growth in the Rural system and small scale industrial, commercial and institutional uses serving the farming and rural communities.

The Region requires that Secondary Plans for Hamlets be prepared that provide for compact growth that maintains the hamlet character of the community.

3.4 The Greenbelt Plan

The Hamlet of Norval is within the area covered by the Provincial Greenbelt Plan. The Greenbelt Plan identifies where urbanization should not occur in order to provide permanent protection to the agricultural land base and the ecological features and functions within the boundaries of the Plan. Under the Greenbelt Plan, the municipal Official Plan delineates the boundaries of the hamlets and minor rounding out is permitted during the conformity exercise. The conformity was completed by the Town in 2008 and therefore no further expansion of the Hamlet boundaries is permitted.

The Greenbelt Plan does not apply to lands with the boundaries of hamlets as the municipal Official Plan continues to govern land uses within them; however the policies relating to external connections do apply. The policies dealing with external connection indicate that within urban areas the ecological features and functions of river valleys should be maintained and enhanced, redevelopment abutting valleys should establish or increase the extent or width of vegetation protection zones especially in the most ecologically sensitive areas, improve fish habitat, include landscaping restoration, and minimize the impacts of storm water run-off. The Plan permits infill and intensification of hamlets subject to appropriate water and sewage services.

PART II – THE AMENDMENT

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1. INTRODUCTION

All of this part of the document entitled Part II – The Amendment, consisting of the following text and attached schedule, constitutes Amendment No. 20 to the Official Plan of the Town of Halton Hills.

2. DETAILS OF THE AMENDMENT

The Hamlet of Norval Secondary Plan of the Town of Halton Hills is hereby amended as follows:

1. By replacing the words “Amendment No. 104” with “Amendment No. 1”, adding the words “as amended by Official Plan Amendment 20” after the words “Town of Halton Hills” and replacing the words “Appendices A and B are” with “Appendix A (Hamlet Design and Heritage Protection Guidelines) is” in the first paragraph of Part B.
2. By adding the phrase “existing businesses focused on Guelph Street and” after the words “promotion of” in the first paragraph of Section 1.0 Overall Goal of the Secondary Plan.
3. By deleting and replacing the objectives listed in Section 2.0 with the following:
 - “To reduce traffic congestion and heavy truck traffic within the Hamlet to the extent feasible and to fully support the further evaluation of by-pass opportunities;
 - To recognize that the boundary of Norval has been fixed through the Greenbelt Plan and to continue to ensure the preservation of hamlet character by addressing the interface between the Hamlet area, surrounding land uses, the Georgetown urban area and lands in the City of Brampton;
 - To maintain and enhance the existing character of the Hamlet and to recognize the existing street and lot pattern which contribute to the character;

- To improve the visual aesthetics through the use of Hamlet Design Principles and Hamlet Design Guidelines;
 - To strengthen Norval's tourism presence within Halton Hills and the Greater Toronto Area;
 - To provide for increased tourist-pedestrian related amenities and facilities;
 - To enhance the vitality of the commercial core through the support of small, independent businesses focused on Guelph Street and maintain the core as a focal point for the community;
 - To maintain, enhance and restore the health of the natural environment;
 - To provide an environmental framework which serves both the existing and future community, which is formed by linking existing open spaces, natural features, and parks and the developed areas of the Hamlet;
 - To recognize the constraints of the floodplain and to balance the need to protect public safety with the need to recognize historic development within the Hamlet;
 - To recognize the heritage resources within the Hamlet which form an integral component of its character and ensure that new buildings are in keeping with the heritage character including height, built form, massing, scale and setbacks; and,
 - To encourage active transportation and to provide facilities within the core which connect to Georgetown and the surrounding communities.”
4. By deleting the first sentence of the paragraph in Section 3.0 General Policies, and adding the words “to this Plan” after the words “Schedule A”.
 5. By deleting Schedule A in the Secondary Plan and replacing it with Schedule A attached to this amendment.

6. By renaming Section 3.1 from “Public Utilities” to “Servicing”.
7. By deleting the last sentence in the first paragraph under Section 3.1 Servicing, and replacing the sentence with “The Region of Halton has reserved a limited capacity in the Georgetown Wastewater Pollution Control Plant to service the Hamlets when the Region, in consultation with the Town, determines it to be prudent and feasible.”.
8. By deleting the title “3.2 Servicing” but retaining the following text as part of Section 3.1.
9. By adding a comma after the words “Where feasible” in the first sentence of the second paragraph of Section 3.1 Servicing.
10. By deleting the last sentence in the second paragraph of Section 3.1 Servicing, and replacing it with “In such cases the policies of this Section and Sections 3.2 and 3.3 shall apply”.
11. By deleting the words “or private” and adding the words “services or private” after the word “municipal” in the first sentence of the third paragraph in Section 3.1 Servicing.
12. By deleting the last paragraph in Section 3.1 Servicing and replacing it with:

“The minimum lot size for new development on full urban services (municipal water and sewer) shall be 929 square metres.”
13. By renumbering Sections 3.3 to 3.6 as Sections 3.2 to 3.5, respectively.
14. By deleting the entire text of Section 3.2 Groundwater Protection and replacing it with the following:

“Prior to any development on private or partial services a hydrogeological study may be required to the satisfaction of the Town of Halton Hills and Regional Municipality of Halton in consultation with Credit Valley Conservation to ensure that groundwater resources will not be negatively impacted.
Any residential proposal containing three lots or more shall

be subject to a hydrogeological study that will address the protection of the existing groundwater supply.

Reference should be made to the Region of Halton's Hydrogeological Studies and Best Practices Guidelines when preparing a hydrogeological study in support of a development application."

15. By replacing the entire text of Section 3.3 Non Residential Uses with the following:

"Existing non-residential uses can occupy up to 500 square metres of gross floor area. New non-residential uses can occupy up to 250 square metres of gross floor area. New non-residential uses proposing to exceed the 250 square metre limit, but less than 500 square metres of gross floor area must also satisfy the criteria in Section 4.2 of this plan.

Prior to any development on private or partial services a hydrogeological study may be required in accordance with Section 3.2 of this Plan.

A "new" non-residential use refers to:

- the introduction of a use which is not currently permitted on a property, or
- the re-designation and/or rezoning of a property from one land use to another."

16. By replacing the entire text of Section 3.4 Stormwater Management with the following:

"All commercial, industrial, institutional, recreational and residential development shall be supported by a Stormwater Management (SWM) report unless waived by the Town through a pre-consultation process. The report shall be prepared to the satisfaction of the Town of Halton Hills in consultation with the appropriate agencies in accordance with the policies in Section C8 of the Halton Hills Official Plan.

In order to encourage more sustainable development, stormwater best management practices will be encouraged.

This may include Low Impact Development (LID) stormwater management techniques with the objective to address stormwater at the source rather than solely in

traditional end of pipe measures (e.g. stormwater management ponds). Stormwater best management practices such as LID techniques may include the use of measures such as bioretention facilities, permeable pavers, vegetated filter strips, rainwater harvesting, and disconnected downspouts.

In addition, a report on the impact on fisheries within the Credit River and Silver Creek may also be required. A Stormwater Management report may be required for a lot severance, if determined to be necessary by the Town of Halton Hills in consultation with Credit Valley Conservation.”

17. By renaming Section 3.5 to “Floodplain, Valleylands and Natural Features”.
18. By deleting italics on the word “Greenlands” throughout the Secondary Plan.
19. By replacing the entire text of Section 3.5 with the following text:

“The Hamlet of Norval has historically developed within the valley of the Credit River. Development is normally prohibited within valleylands given the potential for impacts on the watershed. However, the long-term establishment of a community in this area makes the prohibition of development undesirable due to the impact of such a prohibition on the viability of the community and the potential loss of important cultural assets. A balance has been sought between the protection of the valleylands and the allowance of limited development to ensure the continued viability of the Hamlet.

The Regulatory Floodplain has been illustrated on Schedule A in order to identify all areas that are susceptible to flooding. Schedule A further designates areas of environmental importance as Greenlands. Section 11 of this Plan defines and provides planning policies for the Greenlands.

Areas that have been identified as wetlands, significant woodlands, Environmentally Sensitive Areas (ESAs) or steep slopes, have been designated as Greenlands on Schedule A. Lands which are located within the floodplain have only been included in the Greenlands designation if they are not developed or if they have other natural heritage

features listed above which also applies to them. Developed lands whose only environmental constraint is the floodplain have been given an underlying land use designation with a floodplain overlay designation as outlined in Section 12.0 of this Plan.

In accordance with the Provincial Policy Statement and Credit Valley Conservation's Regulation 160/06 (Regulation of Development, Interference with Wetlands and Alterations to Shorelines and Watercourses), no new or expanded nursing homes, day care facilities, group homes, seniors homes, schools, fire, police or ambulance stations, or other sensitive uses shall be permitted within the regulatory floodline established by Credit Valley Conservation.

Areas that are designated Greenlands are recognized as generally unsuitable for development given their importance to:

- Human and ecological health;
- The protection of property from damage due to all natural hazards including erosion and slope instability as well as flooding; and,
- Human safety.

The Georgetown Credit River Valley and Hungry Hollow Ravine ESAs are located partially within and adjacent to the Norval Secondary Plan area.

It is a policy of this plan to provide for the protection and enhancement of features within the Greenlands designation through the limitation of permitted uses, the provision of building setback requirements and other development requirements, such as the need for more detailed studies. The protection and enhancement of natural features can also be achieved through the restoration of watercourse corridor or riparian zones, and landowner stewardship.

An Environmental Impact Study may be required for development applications adjacent to the Greenlands to the satisfaction of the Town in consultation with the Region of Halton and Credit Valley Conservation in accordance with Section C2 of the Town's Official Plan. Reference should be made to the Region of Halton's Environmental Impact Assessment guidelines when preparing terms of reference

for the study and when undertaking and completing the study.

Lands not designated as Greenlands may still have valuable natural features that should be considered for protection and enhancement. This is especially true for woodlands in the Hamlet which are not deemed significant woodlands. These woodlands perform important ecological functions both individually and as a whole that assist in erosion control, mitigating the impacts of wind and dust, providing habitat for local and migratory wildlife and improving aesthetics. Applicants for Site Plan Approval, Subdivision Approval or severance, may be required to submit a tree inventory and preservation plan and a proposed planting plan to the satisfaction of the Town. The tree inventory and preservation plan shall include tree protection, enhancement and replacement measures.”

20. By moving Section 9.6 Expansion or Replacement of Existing Buildings within the Regulatory Floodplain under Section 3.0 General Policies and renumbering it as Section 3.6.

21. By deleting the first paragraph in Section 3.6 and replacing it with the following text:

“A portion of Norval’s historical core area is within the regulatory flood line established by Credit Valley Conservation. Generally, new development and/or the creation of new lots is prohibited by Credit Valley Conservation within the floodplain given the potential danger to life and property and impacts on the Credit River Watershed. However, it is recognized that the existing buildings provide the homes and businesses that make up the community and are also important from a tourism and heritage preservation perspective.”

22. By deleting the second paragraph in Section 3.6 and replacing it with the following text:

“In balancing these issues a limited amount of expansion or, under certain conditions, replacement of buildings will be permitted subject to approval by Credit Valley Conservation. The following Credit Valley Conservation requirements as may be amended from time to time are applicable:”

23. By deleting the words “18.6 square metres” and replacing them with the words “a ground floor addition of 50% or less of the original habitable ground floor area to a maximum of 100 square metres and does not include a basement” in subsection a) of Section 3.6 Expansion or Replacement of Existing Buildings within the Regulatory Floodplain.
24. By deleting the words “footprint providing the” and replacing the words with “footprint provided that” in Section 3.6, subsection c).
25. By adding the words “and there is no increase in the number of dwelling units” to the end of Section 3.6, subsection d).
26. By deleting subsection f) of Section 3.6 and replacing it with:

No new sensitive land uses including nursing homes, day care facilities, group homes, seniors homes, schools, fire, police or ambulance stations, or other similar uses shall be permitted, and any existing sensitive uses shall not be allowed to expand or be replaced in the event of destruction; and,”
27. By deleting the word “chemical” from Section 3.6, subsection g).
28. By adding the words “to achieve no negative impact” after the words “mitigation strategies” in the second paragraph of Section 3.7 Protection of Fish Habitat.
29. By deleting the first paragraph of Section 3.8 Urban Separators and replacing it with “A rational Hamlet boundary has been established based upon municipal boundaries and identifiable features such as valley top-of-bank, watercourses and roads”.
30. By deleting the second paragraph of Section 3.8 and replacing it with:

“In the case of Norval, the Silver Creek valley, Willow Park, and the Greenbelt Plan Area form an urban separator between Norval and the Georgetown Urban Area.”
31. By deleting and replacing the third paragraph and subsections of Section 3.8 with the following:

“If the Hamlet of Norval is to retain its character, these separators are viewed as important elements. To this end, the following policies will apply:

- a) Wherever possible, environmental areas that represent a logical buffer between the Hamlet and both future and existing urban development will be used as an “urban separator”;
 - b) Wherever possible, buffers will be included within the boundaries of the Hamlet;
 - c) The Town of Halton Hills will engage in discussions with the City of Brampton and the Region of Peel to examine ways that a distinct separation between the Hamlet of Norval and the City of Brampton can be preserved;
 - d) Planning for the Southeast Georgetown new growth area shall consider the interface with the Hamlet of Norval.”
32. By creating a new Section 3.10 Adult Entertainment Uses with the following text:
33. By renumbering the Transportation Section as 3.11 and the following Sections in numerical order.
34. By deleting the text of the Transportation Section and replacing it with the following text:

“Adult Entertainment Uses shall be prohibited in all land use designations in the Hamlet.”

“Highway 7 is a Provincial Highway with access restrictions. New entry points to Highway 7 are subject to the approval of the Ministry of Transportation.

The Halton-Peel Boundary Area Transportation Study (HPBATS) was undertaken to look at accommodating future travel demands generated by growth in Brampton and Halton Hills. Highway 7 and Adamson Street (Winston

Churchill Boulevard) through Norval were identified as “bottlenecks”. Means of addressing congestion were discussed in the Study and included shifting traffic outside of peak hours (using travel demand management), shifting modes of travel (to carpooling, transit, cycling, or walking), reducing demand (typically addressed through employer-based programs such as telecommute), or shifting traffic elsewhere (i.e., a bypass). The HPBAT Study indicated that the roadways through Norval should remain two lanes serving local traffic and restricting capacity which will help to ensure the long term preservation of the Hamlet. It recommended additional capacity be provided via bypasses, new arterials, or new freeway options which included the Norval By-pass, the Adamson Street North By-pass and the Winston Churchill Blvd By-pass.

Environmental Assessments are required in order to undertake the by-pass options. The Town will request as part of any Environmental Assessments that specific attention to north-south and east-west connections be made to determine how the connections will be achieved through either a combination of road connections or a new corridor. Possible conflicts between these connections and the existing housing and environmental features in the Hamlet must be given careful consideration in order to ensure that the river valley setting and the current built form character of the community is maintained. The Town will also request that truck traffic be restricted in the Hamlet core.

The Town of Halton Hills and the Region of Halton will implement corridor protection for future north-south transportation facilities and related connections (East/West Connection by-pass or Norval West By-pass). Schedule A of the Secondary Plan has identified a corridor protection area within the Hamlet boundaries.

The HPBAT Study indicated that no bypass option will bring the volume-to-capacity ratio through Norval to less than one (functional capacity). As a result, there will continue to be

on-going traffic congestion. Traffic Calming measures have therefore been initiated within Norval on streets under the jurisdiction of the Town in order to slow traffic. If traffic calming is to be provided on Highway 7 and Adamson Streets yet still maintain function and capacity, the streets should be made visually narrower. On-street parking, street trees, landscaping, and street furniture should be investigated to make the road feel narrower and convey the message to slow down. Additional traffic calming measures which do not place any form of structure within the right of way such as pavement markings and pavement colouring should also be considered. As Guelph Street is under the jurisdiction of the Ministry of Transportation (MTO) then Provincial approval will be required for any changes within the Guelph Street right of way.

Active transportation will be encouraged by improving pedestrian connections including movements into and out of the Hamlet and the expansion and enhancement of the trail systems, and by facilitating the addition of bicycle lanes as identified in the Town's Cycling Master Plan."

35. By deleting the word "three" and replacing it with the word "six" in the second paragraph of Section 3.12 Tourism Development.
36. By adding the sentence "Other former residents important to the Hamlet include Peter Adamson and Robert Noble whose names have been given to local streets". At the end of the second paragraph in Section 3.12 Tourism Development.
37. By modifying the third paragraph of Section 3.12 Tourism Development by deleting the words "also developing into" in front of the words "an educational".
38. By modifying the fourth paragraph of Section 3.12 Tourism Development by deleting the words "experiencing increased tourism" and replacing them with the words "attracting tourists".
39. By replacing the word "Hamlet" with "hamlet" in the fourth paragraph of Section 3.12 Tourism Development.

40. By modifying the subsections a), b), d) of Section 3.12 by replacing the word “Urban” with the word “Hamlet”.
41. By modifying Section 3.13 Archaeological Resources by deleting the words in the first paragraph “Stage 2 Field Assessment, as outlined in the Provincial Archaeological Assessment Technical Guidelines 1993,” and replacing them with “an archaeological impact assessment in accordance with the Halton Hills Official Plan.”
42. By modifying Section 3.14 Urban Design and Heritage Protection by deleting the words “of Norval” after the words “built form” in the first sentence.
43. By modifying Section 3.14 Urban Design and Heritage Protection by replacing the word “Urban” in the title with “Hamlet” and by adding, at the end of the first paragraph, the following text:

“In order to maintain the character of the Hamlet, all development will recognize the existing built form context and protect the character of the heritage buildings and landscapes. Development will respect and reinforce:

- a) the height, massing, and scale of nearby buildings,
- b) the average setbacks of buildings from the street,
- c) the prevailing side and rear yard setbacks,
- d) the landscaped open space patterns,
- e) the size and configuration of lots, and
- f) the existing street pattern.

All development must be in keeping with the physical character of the Hamlet and no approvals will be granted under the Planning Act for development which does not respect the existing built form.”

44. By deleting the first sentence of the fourth paragraph of Section 3.14 Hamlet Design and Heritage Protection and replacing it with the sentence “Further details regarding the appropriate built form and the protection and enhancement of the community features are established in the Hamlet Design and Heritage Protection Guidelines.”

45. By deleting the last two paragraphs of Section 3.14 Hamlet Design and Heritage Protection and replacing them with the following text:

“The approval process for all planning applications within the Hamlet will include the application of the Hamlet Design and Heritage Protection Guidelines. An architectural and urban design brief/study may be required in support of an application indicating how the proposed development conforms with the Hamlet Design and Heritage Protection Guidelines to the satisfaction of the Town. The Town may require a third party “peer review” of the proposed development.

It is a policy of this Plan that the cost of a peer review and implementation of on and off-site improvements consistent with the Hamlet Design and Heritage Protection Guidelines will be the responsibility of the proponent to the satisfaction of the Town.”

46. By renaming Section 3.15 from “Expansion of Hamlet Boundaries” to “Hamlet Boundaries” and by deleting the text in Section 3.15 and replacing it with:

“Under the policies of the Greenbelt Plan, the boundaries of the Hamlets are delineated by the local Official Plan. Minor rounding out to a hamlet’s boundary is only permitted through an Official Plan Conformity exercise with the Greenbelt Plan. The Town of Halton Hills has undertaken a Greenbelt Plan conformity exercise and therefore no further expansion of the Hamlet boundaries is permitted.”

47. By adding a new Section 3.16 as follows:

“3.16 Trail Systems

The development of improved trail systems which encourage walking and bicycling has been identified by the Town of Halton Hills through the Cycling Master Plan and the earlier Trails and Cycling Master Plan.

The existing trails and conceptual alignments for future bicycle and pedestrian trails/routes are shown on Schedule A. Reference should be made the Cycling Master Plan and the earlier Trails and Cycling Master Plan for details on how and when future trails are expected to be developed. The actual implementation of any trail system will be the subject of further assessment and additional public consultation will occur during that period. All trails will be developed to municipal standards and with regard to potential environmental impacts. Additional information on trail development is also provided in the Hamlet Design and Heritage Protection Guidelines.”

48. By adding the word “Area” after the words “Hamlet Community Core” in the title of Section 4.0.
49. By adding the word “Area” after the words “Hamlet Community Core” in the first paragraph in Section 4.1 Purpose.
50. By modifying the second sentence of Section 4.1 by adding the words “supporting and” after the words “at the same time” and adding the words “the existing businesses and” after the words “the commercial viability of”.
51. By deleting the word “area” after the word “Hamlet” in the first paragraph of Section 4.1 Purpose.
52. By replacing the word “Urban” with “Hamlet” in the last sentence of the second paragraph in Section 4.1 Purpose.
53. By deleting the first paragraph under Section 4.2 and adding the following text:

“Uses permitted within the Hamlet Community Core Area designation include tourism and cultural uses, retail services, residential uses and office space. Existing non-residential uses can occupy up to 500 square metres of gross floor area. New non-residential uses can occupy up to 250 square metres of gross floor area. New non-residential uses proposing to exceed the 250 square metre limit, but less than 500 square metres of gross floor must satisfy the following criteria:

- a) A design study must be provided that clearly demonstrates that the development meets the criteria of Section 3.14 and the Hamlet Design and Heritage

Protection Guidelines and that the built form respects and reinforces the character of the community, is visually consistent with surrounding uses and is sensitive to existing vegetation and topography;

- b) The development is compatible with adjacent uses;
 - c) A traffic impact study must be provided that addresses traffic impacts, mitigation measures and ensures that there is acceptable vehicular and pedestrian access to and within the site;
 - d) Adequate Servicing must be provided for the proposed use in accordance with Section 3.1 Servicing of this Plan; and,
 - e) Any adverse noise impacts can be appropriately mitigated.”
54. By deleting the words “uses such as” after the words “The permitted uses include” in Section 4.2 Permitted Uses.
55. By modifying Section 4.2 by deleting the listed permitted uses and replacing them with the following permitted uses:
- a) “Animal Clinics;
 - b) Antique and furniture stores;
 - c) Artist studios and art galleries;
 - d) Bakeries;
 - e) Banks;
 - f) Bed and breakfast establishments;
 - g) Business or professional offices;
 - h) Cafes;
 - i) Community centres;
 - j) Craft shops;
 - k) Custom workshops;
 - l) Day care centres;
 - m) Dwelling unit located in a non-residential building;
 - n) General stores;
 - o) Home occupations and cottage industries;
 - p) Ice cream parlours;
 - q) Professional medical or dental offices;
 - r) Museums;
 - s) Personal service uses such as hair stylist and shoe repair;

- t) Private parks;
 - u) Public parks;
 - v) Public parking areas;
 - w) Recreational uses;
 - x) Residential care facilities meeting the definition of a Group Home Type 1 subject to the requirements of the implementing Zoning By-law and the Official Plan;
 - y) Restaurants, but not a drive through restaurant;
 - z) Retail showrooms;
 - aa) Single detached dwellings;
 - bb) Tourist attractions.”
56. By modifying Section 4.2 by deleting the reference to “Section 3.4” and replace it with “Section 3.3” in the paragraph after the list of permitted uses.
57. By deleting the fourth paragraph in Section 4.2 Permitted Uses.
58. By inserting the following text after paragraph 3 in Section 4.2 Permitted uses:

“Within the area identified as Hamlet Community Core Area, automotive uses that legally existed on the day of the passing of the revised Secondary Plan (Official Plan Amendment 20) will be recognized and permitted to continue their existing operation and function in their current location. No additional automotive services or uses will be permitted with the exception of limited motor vehicle sales accessory to the main permitted automotive use at 488/490/492 Guelph St.”

No new automotive uses will be permitted within the Hamlet Community Core Area designation.

Notwithstanding the permitted uses listed above, no new or expanded nursing homes, day care facilities, group homes, seniors homes, schools, fire, police or ambulance stations, or other sensitive uses shall be permitted within the

regulatory floodline established by Credit Valley Conservation.”

59. By modifying the first sentence in Section 4.3 by deleting the words “Hamlet Community Core Designation” and replacing with “Hamlet Community Core Area designation”.
60. By deleting and replacing the subsections under Section 4.3 with the following subsections:
 - a) “Where commercial and residential uses abut, site planning to achieve adequate buffering in the form of landscaping, or architectural screening shall be required;
 - b) Adequate off street parking will be required. Parking facilities will be oriented to the side or rear of all commercial buildings;
 - c) Adequate servicing for the proposed use must be provided in accordance with Section 3.1 Servicing of this Plan;
 - d) The subdivision of land will only be permitted as infilling where the lots meet the minimum criteria specified in Section 3.1 Servicing of this Plan;
 - e) New commercial uses shall generally be compatible with the character and scale of the Hamlet;
 - f) Home occupations and cottage industries may be permitted as a secondary use to the primary residential use in accordance with the Halton Hills Official Plan;
 - g) Outdoor Storage is not permitted within the Hamlet Community Core Area designation;
 - h) The development of office space will be encouraged to occur on second storeys above retail uses;
 - i) All development will be consistent with the policies in Section 3.14 and the Hamlet Design and Heritage

Protection Guidelines contained in Appendix A and shall not exceed two storeys;

- j) No drive through operations will be permitted within the Hamlet Community Core Area designation; and,
- k) Any development adjacent to Highway 7 is subject to approval from the Ministry of Transportation for new or changes to access on to Highway 7.”

- 61. By moving and renumbering Section 5.0 Hamlet Residential as Section 6.0 and renumbering the subsection accordingly.
- 62. By deleting Section 6.0 Hamlet Residential – Special.
- 63. By adding a new Section 5 as follows:

“5.0 Hamlet Commercial Area

5.1 Purpose

The Hamlet Commercial Area designation applies to existing automotive uses located on Highway 7 or existing commercial uses which are located outside of the Hamlet Community Core. The purpose of the designation is to recognize the existing uses and to permit limited additional commercial uses which are compatible with surrounding residential uses.

Existing non-residential uses can occupy up to 500 square metres of gross floor area. New non-residential uses can occupy up to 250 square metres of gross floor area. New non-residential uses proposing to exceed the 250 square metre limit, but less than 500 square metres of gross floor area must satisfy the criteria in Section 4.2 of this Plan in addition to the land use policies of this designation.

5.2 Permitted Uses

The uses permitted within the Hamlet Commercial Area designation include:

- a) Automotive uses that legally existed on the day of the passing of the bylaw adopting this plan;
- b) Commercial uses that legally existed on the day of the passing of the bylaw adopting this plan;
- c) Day care centres;
- d) Single detached dwellings;
- e) Bed and breakfast establishments;
- f) Home occupations and cottage industries;
- g) Animal clinics;
- h) Business or professional offices;
- i) Custom workshops;
- j) Personal service uses such as hair stylists and shoe repair; and,
- k) Retail stores.

5.3 Land Use Policies

Development within the Hamlet Commercial Area designation shall be consistent with the following land use policies:

- a) Notwithstanding the permitted uses listed above, no new or expanded nursing homes, day care facilities, group homes, seniors homes, schools, fire, police or ambulance stations, or other sensitive uses shall be permitted within the regulatory floodline established by Credit Valley Conservation;
- b) Within the area identified as Hamlet Commercial Area, automotive uses that legally existed on the day of the passing of the revised Secondary Plan (Official Plan Amendment 20) will be recognized and permitted to continue their existing operation and function in their current location. No additional automotive services or uses will be permitted with the exception of limited motor vehicle sales accessory to the main permitted automotive use at 546/548 Guelph St.;

- c) No new automotive uses are permitted in the Hamlet Commercial Area designation;
- d) Outdoor storage is not permitted in the Hamlet Commercial Area designation;
- e) Where commercial and residential uses abut, site planning to achieve adequate buffering in the form of landscaping, or architectural screening shall be required;
- f) All development will be consistent with the policies in Section 3.14 and the Hamlet Design and Heritage Protection Guidelines contained in Appendix A, and shall not exceed two storeys;
- g) Adequate off street parking will be required. Parking facilities should be oriented to the side or rear of all commercial buildings;
- h) Adequate servicing for the proposed use must be provided in accordance with Section 3.1 Servicing of this Plan;
- i) New commercial uses shall generally be compatible with the character and scale of the Hamlet;
- j) Home occupations and cottage industries may be permitted as a secondary use to a primary residential use in accordance with the Halton Hills Official Plan;
- k) No drive through operations will be permitted within the Hamlet Commercial Area designation; and,
- l) Any development adjacent to Highway 7 is subject to approval from the Ministry of Transportation for new or changes to access on to Highway 7.”

64. By renaming Section 6.0 “Hamlet Residential” to “Hamlet Residential Area”.

65. By deleting the word “with” and replacing it with the word “within” in the first sentence of Section 6.1 Purpose.
66. By deleting the words “uses such as” from the end of the first sentence in Section 6.2 Permitted Uses.
67. By adding the word “Area” after the word “Hamlet Residential” throughout Section 6.0.
68. By deleting the permitted uses in Section 6.2 and replacing them with the following:
 - a) Single detached dwellings;
 - b) Bed and breakfast establishments; and,
 - c) Home occupations and cottage industries; and,
 - d) Residential care facilities meeting the definition of Group Home Type 1 subject to the requirements of the implementing Zoning By-law and the Official Plan.
69. By modifying Section 6.3 b) Land Use Policies by deleting the words “Section 3.2” and replacing them with the words “Section 3.1” and by deleting Section 6.3 d) and replacing it with “Home occupations and cottage industries may be permitted as a secondary use to the primary residential use in accordance with the Halton Hills Official Plan.”
70. By modifying Section 6.3 Land Use Policies by adding the following new subsections:
 - e) “All development will be consistent with the policies in Section 3.14 and the Hamlet Design and Heritage Protection Guidelines contained in Appendix A, and shall not exceed two storeys; and,
 - f) Adequate servicing for the proposed use must be provided in accordance with Section 3.1 Servicing of this Plan.”
71. By renumbering Section 7.0 Institutional to Section 9.0 and the associated subsections accordingly.
72. By inserting a new Section 7.0 as follows:

“7.0 Hamlet Residential/Office Area

7.1 Purpose

Lands that are designated Hamlet Residential/Office Area are intended to encourage the retention of the existing built form in those areas so that it is consistent and compatible with the adjacent residential and commercial uses.

Existing non-residential uses can occupy up to 500 square metres of gross floor area. New non-residential uses can occupy up to 250 square metres of gross floor area.

7.2 Permitted Uses

The uses permitted within the Hamlet Residential/Office Area designation include:

- a) Single detached dwellings;
- b) Bed and breakfast establishment;
- c) Home occupations and cottage industries;
- d) Business or professional office; and,
- e) Dwelling unit located in a non-residential building.

7.3 Land Use Policies

Development within the Hamlet Residential/Office Area designation shall be consistent with the following land use policies:

- a. Where office and residential uses abut, site planning to achieve adequate buffering in the form of landscaping, or architectural screening shall be required;
- b. Adequate on-site parking will be required. Parking facilities should be oriented to the side or rear of all office buildings;
- c. All development will be consistent with the policies in Section 3.14 and the Hamlet Design and Heritage

Protection Guidelines contained in Appendix A, and shall not exceed two storeys;

- d. Adequate servicing for the proposed use must be provided in accordance with Section 3.1 Servicing of this Plan;
 - e. The subdivision of land will only be permitted as infilling where the lots meet the minimum criteria specified in Section 3.1 Servicing of this Plan;
 - f. New office uses shall be compatible with the character and scale of the Hamlet;
 - g. Home occupations and cottage industries may be permitted as a secondary use to the primary residential use in accordance with the Halton Hills Official Plan;
 - h. Any development adjacent to Highway 7 is subject to approval from the Ministry of Transportation for new or changes to access on to Highway 7;
 - i. Outdoor Storage is not permitted within the Hamlet Residential/Office Area designation.”
73. By renumbering Section 8.0 Open Space to 10.0 and the corresponding subsections accordingly.
74. By renumbering Section 9.0 to Section 11.0 and the corresponding subsection accordingly.
75. By renumbering Section 10.0 Implementation to Section 13.0.

76. By inserting a new Section 8.0 as follows:

“8.0 Corridor Protection Area

8.1 Purpose

The purpose of the Corridor Protection Area designation is to recognize that this area will be the subject of a future environmental assessments to determine if this is an appropriate location for a transportation by-pass corridor.

8.2 Permitted Uses

The uses permitted within the Corridor Protection Area are limited to:

- a) Uses that legally existed on the day of the passing of the bylaw adopting Official Plan Amendment 20 updating this Plan;
- b) Existing agricultural operations;
- c) Non-intensive recreation uses such as nature viewing and pedestrian trail activities;
- d) Public uses.

8.3 Land Use Policies

The erection of new buildings and structures shall be prohibited within lands designated Corridor Protection area.”

77. By adding the word “Area” after “Institutional” in the title of Section 9.0.
78. By adding the word “Area” after “Institutional” throughout Section 9.0 Institutional Area and the corresponding subsections.
79. By adding the following text after the list of permitted uses in Section 9.2 Permitted Uses:

“Notwithstanding the permitted uses listed above, no new or expanded nursing homes, day care facilities, group homes, seniors’ homes, schools, fire, police or ambulance stations, or other sensitive land uses shall be permitted within the

regulatory flood line established by Credit Valley Conservation.”

80. By adding the word “sewage” after the words “and sanitary” to the newly renumbered Section 9.3 c) and by deleting the word “when” and replacing it with “if”.
81. By adding the word “and,” at the end of subsection 9.3 c).
82. By adding a new subsection 9.3 d) as follows:

“All development will be consistent with the policies in Section 3.14 and the Hamlet Design and Heritage Protection Guidelines contained in Appendix A.”
83. By adding the word “Area” after the words “Open Space” in the title for Section 10.0.
84. By adding the word “Area” after “Open Space” throughout Section 10.0 and the corresponding subsections.
85. By deleting the word “and” after the words “Hillcrest Cemetery” and replacing it with a “,” in Section 10.1 Purpose.
86. By adding the words “, and St. Paul’s Anglican Pioneer Cemetery” after the words “McNab Pioneer Cemetery” in Section 10.1 Purpose.
87. By adding the words
“and,
c) public uses.”
to the end of Section 10.2 Permitted Uses.
88. By modifying Section 11.1 Greenlands Purpose subsection a) by adding the word “undeveloped” after the words “To identify”.
89. By modifying Section 11.2 Criteria for Designation subsection a) by adding the words “which are not currently developed” after the words “refined from time to time,”.
90. By deleting and replacing the word “Flood Plains” with “Floodplains” in Section 11.2, subsection a).

91. By deleting Subsection d) in Section 11.2 and replacing it with “d) Significant Woodlands; and,”.

92. By deleting the wording in Section 11.3 Setbacks and replacing it with the following wording:

“All new development in the Hamlet shall meet the following setbacks:

- a) A 10 metre building setback from the stable top and bottom of the bank of valley;
- b) A 10 metre building setback from any identified erosion allowance associated with a watercourse;
- c) A 10 metre building setback from the regulatory floodline;
- d) In non-valley situations a 30 metre building setback from the bank of the watercourse, or 10 metres from the flood line, whichever is greater unless the setback is within the Greenlands designation;
- e) A 10 metre building setback from Environmentally Sensitive Areas and wetlands;
- f) A 30 metre building setback from provincially significant wetlands; and,
- g) Where lands are being subdivided or severed, a setback of 10 metres from the lot line to the greater of any flood hazard, erosion hazards or significant natural features.

These setback requirements must be met for all new development unless more appropriate setbacks are recommended in accordance with an approved report or study (eg. subwatershed study, environmental impact study, geotechnical study) to the satisfaction of the Town in consultation with the Region of Halton and Credit Valley Conservation. For proposals minor in scale, study or report requirements may be scoped or waived by the Town in consultation with the Region of Halton and Credit Valley Conservation.”

93. By replacing the reference to “9.3” with “11.3” in Section 11.5 Land Use Policies, Subsection vi).

94. By modifying the first paragraph of Section 11.6 Development Evaluation Criteria by adding the words “or adjacent to lands designated” after the words “proposed development within”, and deleting the word “designation” after the word “Greenlands”.
95. By modifying the newly renumbered Section 11.6 a) by adding the words “in consultation with” after the words “satisfaction of the Town,”.
96. By deleting the wording in Section 11.6 b) and replacing it with the following wording:

“An Environmental Impact Study prepared to the satisfaction of the Town in consultation with the Region of Halton and Credit Valley Conservation, in accordance with Section C2 of the Halton Hills Official Plan. Reference should be made to the Region of Halton’s Environmental Impact Assessment Guidelines when preparing terms of reference for the study and when undertaking and completing the study;”
97. By modifying Section 11.7 Zoning by deleting the first paragraph and replacing it with:

“The boundaries of the Greenlands are delineated on Schedule A based on the best mapping currently available. The boundaries shall be delineated in the Zoning By-law and may be refined at the time of a development application based on studies and/or field investigations conducted by the proponent and approved by the Town, in consultation with the Region of Halton, Credit Valley Conservation and Provincial agencies. Refinement to the boundaries will not require an amendment to this Plan.”
98. By modifying the second paragraph of Section 11.7 Zoning by: adding the words “in consultation with” after the word “Town”, deleting the word “and” in front of “the Region” and by adding the words “and the” in front of “Credit Valley”.
99. By deleting the last sentence of Section 11.7 Zoning.

100. By introducing a new Section 12.0 Regulatory Flood Plain Overlay which states:

“12.0 Regulatory Floodplain Overlay

12.1 Purpose and Effect

The purpose of the Floodplain Overlay is to identify developed properties which are located within the Regulatory Floodplain and identify that the properties are subject to Credit Valley Conservation’s Regulation of Development, Interference with Wetlands and Alterations to Shorelines and Watercourses (160/06) as may be amended.

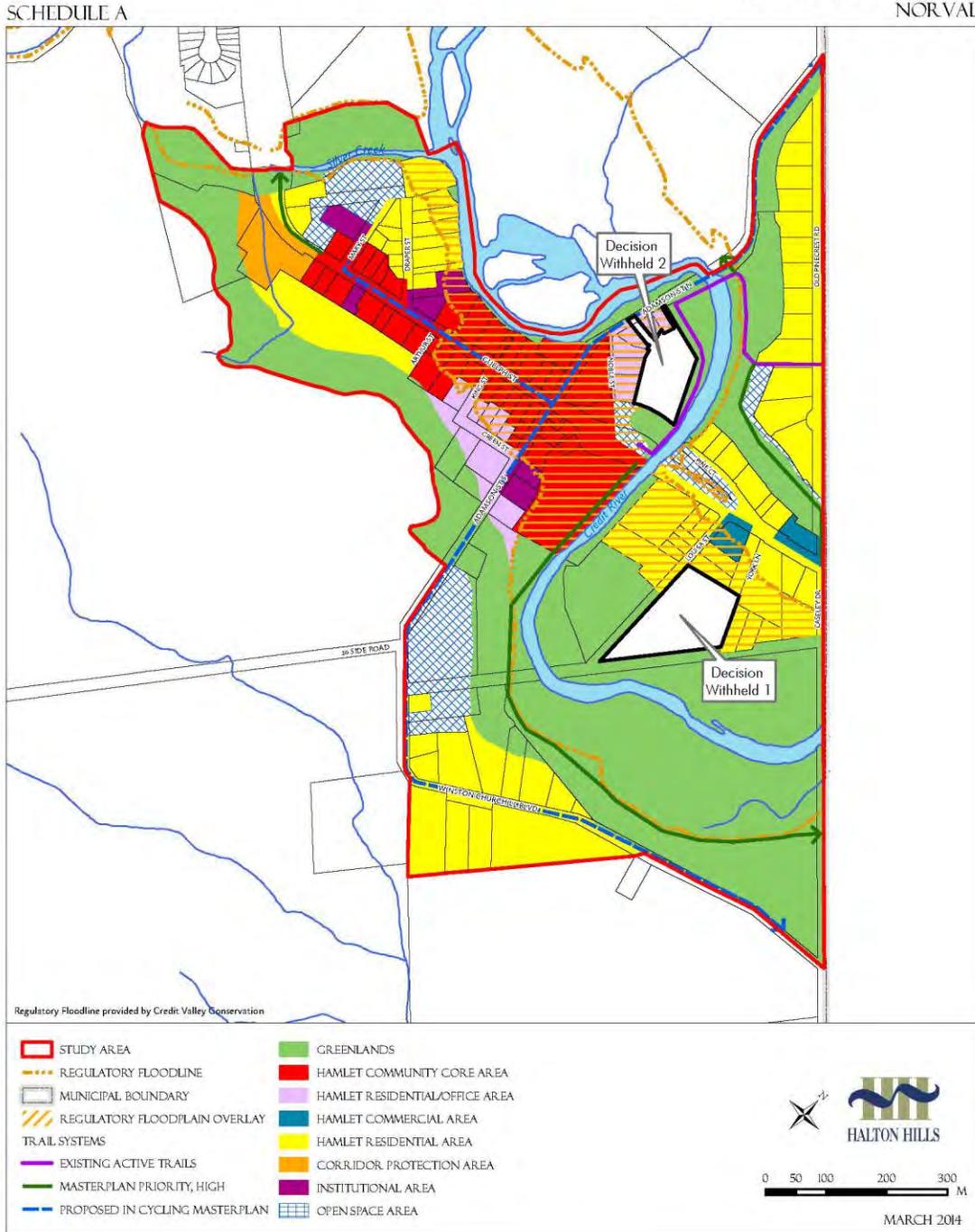
Properties subject to the Regulatory Floodplain Overlay are shown on Schedule A1 to the Secondary Plan.”

3. IMPLEMENTATION AND INTERPRETATION

This Official Plan Amendment shall be implemented and interpreted in accordance with the implementation and interpretation provisions set out in the Amendment and the relevant sections of the Official Plan.

SCHEDULE A to OPA 20

TOWN OF HALTON HILLS
NORVAL



This is Schedule "A" to Halton Hills Official Plan Amendment No. 20, passed this ____ of April, 2014.

Mayor

Clerk

APPENDIX A

**HAMLET DESIGN AND
HERITAGE PROTECTION GUIDELINES**



The Hamlet of Norval
Secondary Plan Review

Hamlet Design and Heritage Protection Guidelines

SUBMITTED BY:
Brook McIlroy/

November, 2013



Looking east along Guelph Street towards the Community Core.

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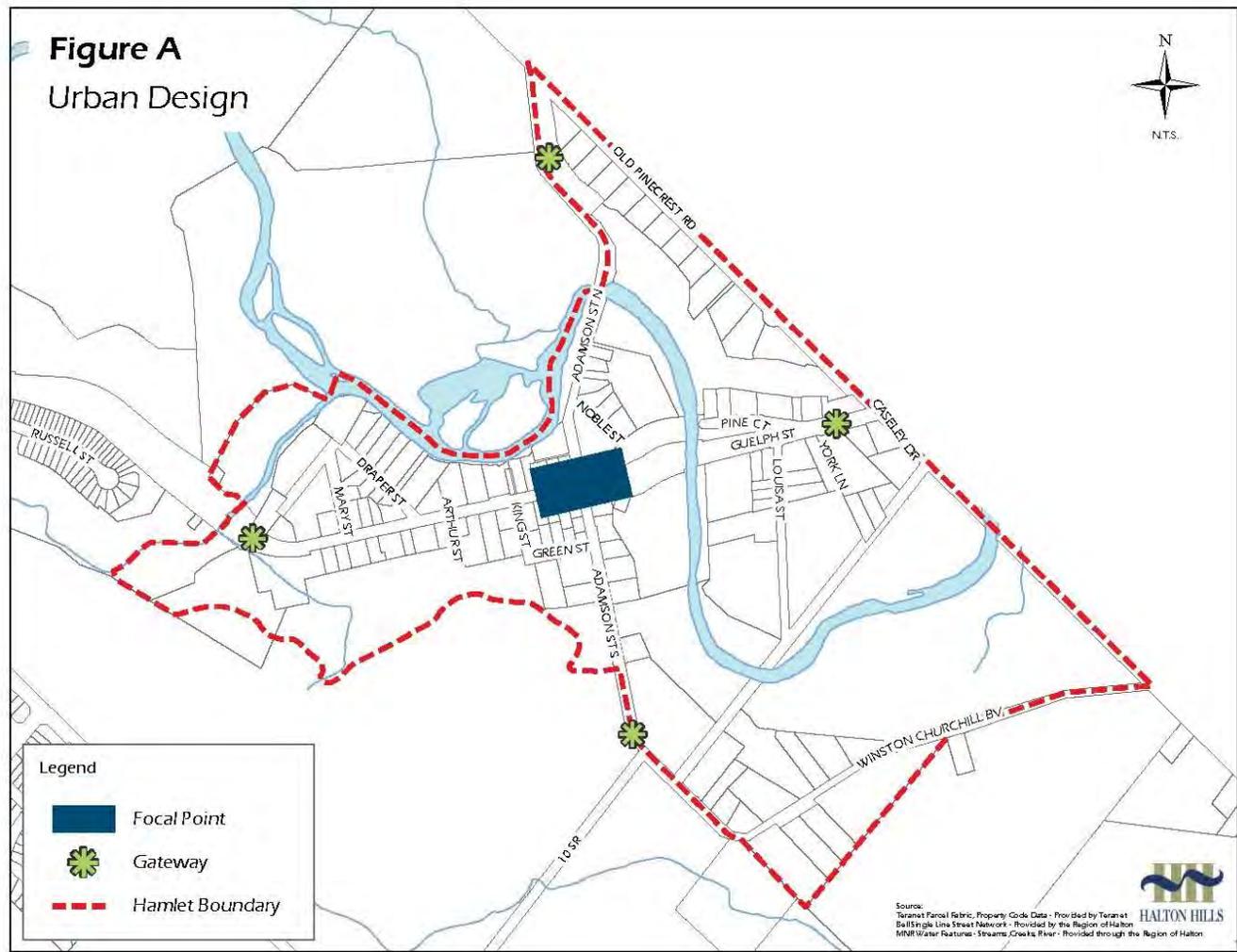
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Introduction

Much of Norval's character is defined by the heritage architecture displayed by its buildings, as well as their scale and their relationship to each other. An important objective of these guidelines is to ensure new development, both public and private, is compatible with the heritage character of the area.

The following guidelines are designed to protect the unique hamlet character prevalent in the community while still allowing room for individual architectural impression. They are to be used by residents and developers in the design of projects as well as Town staff in reviewing zoning by-law amendments and site plan control applications.

Figure A identifies “gateways” and “focal points” as important elements towards achieving urban design objectives. Reference should be made to Figure A when reading the following guidelines.



Design Guideline # 1

Sustainability

A sustainable community is diverse, well connected and walkable and characterized by a strong respect for local identity and natural heritage.

Sustainable Building Design

- New buildings are encouraged to reduce the energy consumption of building and site systems (HVAC, hot water, lighting) through the use of appropriate mechanical and construction technology (natural cooling, light recovery, passive solar design, etc.);
- Water use reduction technologies are encouraged, including water-efficient appliances, such as aerators, low-flow shower heads, dual-flush toilets, front-loading washers, waterless urinals and high-efficiency dishwashers;
- Waste water technologies, such as rain barrels or cisterns, are encouraged in new buildings to collect and filter rain water to be recycled for non-potable domestic uses;
- All buildings should have conveniently located waste management facilities to support the separation of waste into different streams according to reuse and recycling regulation (i.e. compost, paper, plastics, etc.);

Sustainable Material Choice

- Where possible, construction materials should be recycled to reduce the environmental impacts of extracting and manufacturing new materials. If there are no salvageable materials available, efforts should be made to purchase materials from demolition sales, salvage contractors and used materials dealers;
- New construction materials should be locally sourced to reduce the impacts of transportation. Canadian products are generally designed to withstand our climate;
- Construction materials should be durable and should be considerate of life cycle costing to avoid premature replacement.

Sustainable Landscaping

- Recommended landscape materials should include non-invasive, non-cultivar species that are native to the area to support sustainable urban biodiversity. Species that are generally drought resistant and require minimal maintenance are encouraged;
- Landscape design should incorporate strategies to minimize water consumption (i.e. use of mulches and compost, alternatives to grass and rainwater collection systems);
- Existing significant trees, tree stands and vegetation should be protected and incorporated into site design where feasible.

- New trees should be planted to contribute to the Town's existing tree canopy. Where the rhythm of existing trees is interrupted, new trees should be planted as infill to maintain a continuous canopy;
- Site design should minimize impervious hard surfaces. The surface area of driveways and parking areas should be as small as possible within allowable standards, and porous pavement and landscaped areas should be maximized.

Note: For additional information related to sustainability, please refer to the Halton Hills Green Development Standards.



Solar panels, bioswales, permeable pavement and adaptive landscaping are encouraged in Norval.

Design Guideline # 2

Gateways

Gateways are important features that symbolically define Norval, create identity and help people find their way around.

As the basis of first impressions, gateways play an important role in the economic development of a community. Development at gateways should therefore help shape this sense of identity by the nature and quality of landscaping, built form and urban design features such as public art.

- Gateway features should include taller architectural elements, which symbolize entry into the Hamlet, including customized lighting fixtures, landscape features (i.e. tree plantings), flags, special signage, and banners;
- Where buildings are located at gateways, they should prominently address streets through enhanced design treatments, such as taller corner elements, enhanced entry treatments and large expanses of glazing;
- Buildings within Gateway designations must incorporate streetscape improvements that will serve to provide shelter to pedestrians at these major intersections. For example, this can be accomplished by setting back the building and developing a public space that incorporates, landscaping, public art, lighting and/or shelters;
- In order to strengthen the gateway image, different public art features such as sculptures, fountains, and decorative walls with murals may be used. Gateways should be given first priority when considering the placement of public art features;
- Gateway areas should be centres for information and wayfinding, directing visitors to key destinations within the Hamlet (i.e. Norval Park, Credit River, L.M. Montgomery Garden);
- Special ambient lighting and light effects may be employed in order to strengthen the “nightscape” of gateways.



Gateways should be defined by neighbourhood features, such as parks and landmark buildings.

Design Guideline # 3

Focal Points

Built forms at focal points should be of the highest architectural quality to make it memorable and recognizable.

The intersection of Guelph St. (Highway 7) and Adamson St. (Winston Churchill Boulevard) is an important focal point of the community that provides a visual anchor, a point of interest and open space opportunities with access to the Credit River. The strengthening of this intersection through good urban design will improve its attractiveness in terms of tourism as well as general commerce and community pride.

- Built form and development (especially commercial, cultural or entertainment on the ground floor space) should be oriented toward public streets and spaces in order to make public space vibrant and pedestrian-friendly;
- On corner sites, a similar level of architectural expression should be used on both frontages, including enhanced facade articulation, a significant amount of glazing (minimum of 40%), signage etc.;
- Main building entrances of corner buildings should be close to the corner. Alternatively, access can be provided from both frontages;
- Corners should be accentuated by developing to the maximum height limits or with the positioning of entrances;
- Frame prominent intersections by locating new buildings or other structures closer to the street and defining the intersection space;
- Activities that attract or generate pedestrian traffic such as cafes, retail functions and public art are highly desirable at the focal points.



New buildings at focal points should be vibrant and pedestrian friendly.

Design Guideline # 4

Crosswalks

Clearly marked crosswalks provide safe opportunities for pedestrian movement.

- Crosswalks should be a minimum of 3.0 metres wide, with visible edge bands to identify them as a continuation of the pedestrian surface;
- In certain locations, crosswalks may be completely raised to accommodate easier access for seniors and children as well as to serve as a traffic calming measure;
- Additional mid-block pedestrian signals and courtesy crossings with specialized markings and signage should be considered within the Community Core;
- Within the focal area, opportunities to integrate public art into crosswalk design should be explored.



The design of crosswalks may be used to celebrate the history of Norval through artistic interpretations.

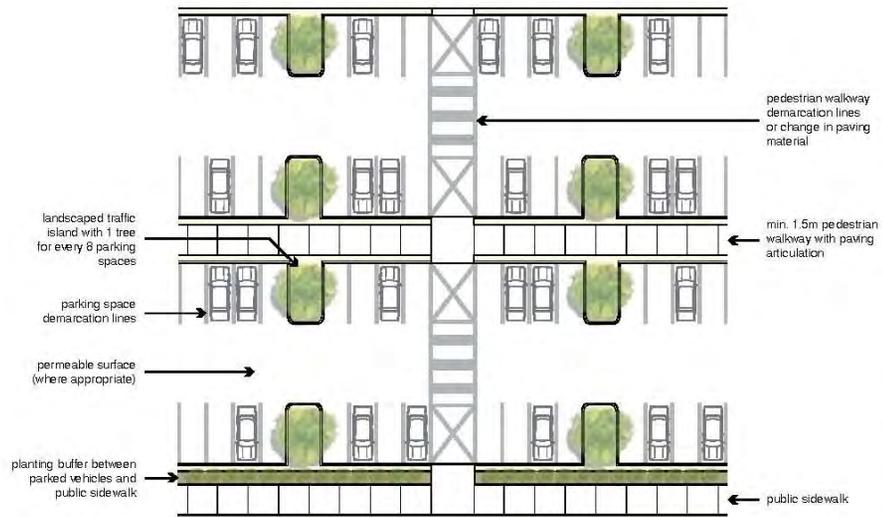
Design Guideline # 5

Surface Parking and Site Circulation

Surface parking lots should be appropriately located, well landscaped and visually divided into smaller courts to minimize their impact on the public streetscape.

Access to parking should be from rear lanes and side streets. Shared entrances to parking areas and loading areas (for 2 or more properties) are encouraged, in order to minimize the number of curb-cuts and to minimize impact on street and pedestrian traffic.

- Parking lots be oriented to the rear or side lot areas of the building site. Parking lots should not dominate the frontage of streets. Where parking areas must be situated adjacent to the sidewalk, a landscaped buffer should be located between parked vehicles and the sidewalk. This buffer should be located within the private realm to not reduce the total sidewalk width;
- Planting strips, landscaped traffic islands and/or paving articulation should be used to define smaller parking 'courts' that provide pedestrian walkways, improve edge conditions and minimize the aesthetic impact of surface parking;
- The amount of landscaping should be proportionate to the overall parking lot size, but generally, 1 tree for every 8 parking spaces is recommended. These can be clustered to facilitate snow clearing;
- Pedestrian-scaled lighting should be provided along pathways to enhance visibility and security;
- Where appropriate, permeable paving should be considered to promote drainage. Well-drained snow storage areas should be provided or snow should be removed off-site.



The illustration above demonstrates the key components used to mitigate the negative impacts of surface parking lots.



To minimize their impact on the public realm, surface parking lots should be located behind buildings, and should be designed as more intimate parking 'courts.'

Design Guideline # 6

Streets and Boulevards

Create boulevards that combine safe, unobstructed pedestrian travel routes with places to stop and socialize.

Boulevards are the interface between private and public spaces and other circulation systems. They represent one of the most important elements of the streetscape performing functional, aesthetic and social roles in the daily lives of the residents of Norval.

- Street and boulevard grades must be designed not to obstruct the movement of pedestrians;
- Within the Community Core, new development should maintain continuous sidewalks on both sides of Highway 7 and Adamson Street. Outside of the Community Core, sidewalks on one side of the street is acceptable.
- At points of congestion (focal points with outdoor patios, entrances to civic or entertainment buildings), sidewalks should be at least 1.85 metres wide to accommodate increased numbers of pedestrians and activities;
- Wherever possible, sidewalks should have elements for weather protection (permanent porticos or arcades, fixed or removable hanging canopies, permanent or temporary awnings);
- On-street parking may be situated within bump-outs, where appropriate. The bump-outs should be landscaped with mature street trees or low level ground cover;
- On-street parking should not conflict with bicycle/ pedestrian travel;
- Street furniture and landscaping should be located between the sidewalk and vehicle traffic. This zone may contain landscaped areas with site furnishings and infrastructure facilities such as benches, bicycle locks, transit stops, and utilities;
- In the long-term, where sidewalks are located directly adjacent to vehicle travel lanes, they should be relocated to accommodate the above furniture and landscape zone (please refer to Page 15 for example street sections).
- Street trees should be offset a minimum of 1.5 metres from the curb to accommodate snow storage, large vehicle movements and to minimize salt damage. Trees should be spaced consistently at 6.0-9.0 metre intervals;
- A transition zone between the sidewalk and the building or property line provides a dedicated area for window shopping, spill-out retail, building entrances, street furniture and signage. In areas not bounded by buildings, this transition zone may include landscaping or a second row of trees;
- Sidewalk surface textures should be designed to be sufficiently smooth and flat to accommodate safe and pleasant use for people of all ages and abilities. Similarly, surface textures should provide additional visual qualities through use of different colors, shapes or materials;
- Pedestrian-scaled boulevard lighting should be provided in areas of high use, such as focal points, and where the future tree canopy may impact light levels;
- All boulevards should be designed to accommodate snow storage.

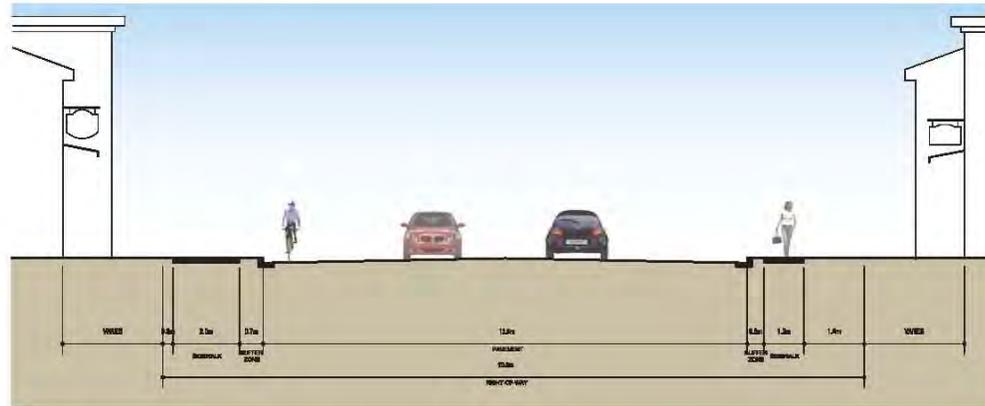


Street furniture and landscaping should be located between the sidewalk and vehicle traffic and parking.

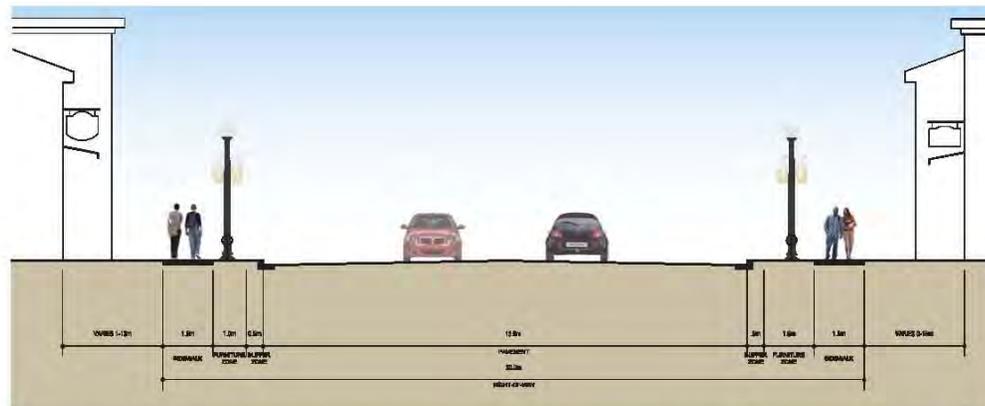


Street furniture and bicycle parking are encouraged throughout Norval, particularly at key destinations (i.e. Community Core) and should be situated where they will not interfere with pedestrian circulation.

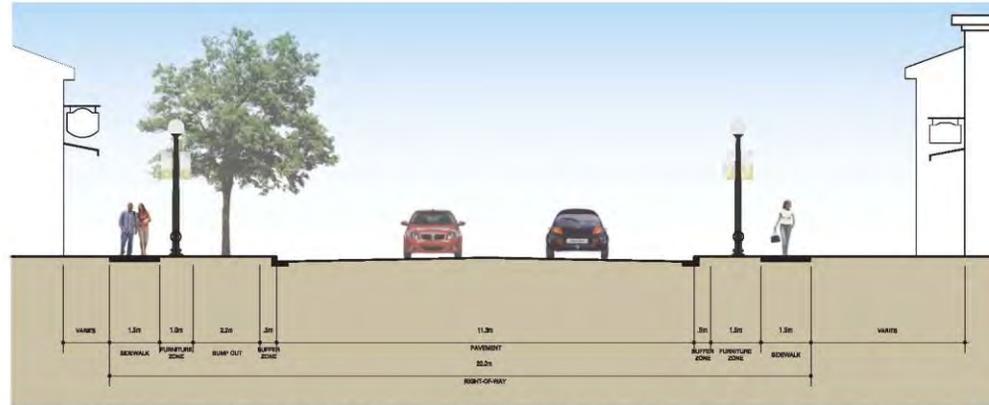
Potential Boulevard Improvements



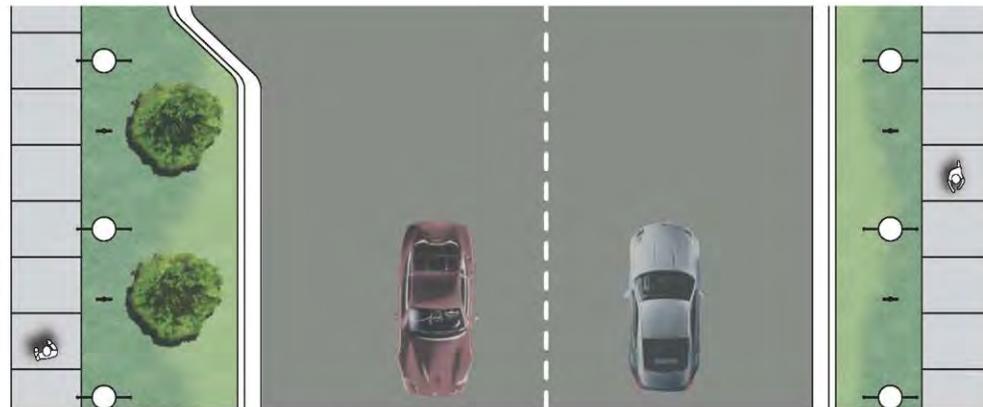
Existing Conditions: A narrow boulevard width limits the potential for an enhanced boulevard treatment.



Alternative 1 (Short-Term): The relocation and narrowing of the sidewalk provides a buffer between pedestrian and vehicle traffic and accommodates basic pedestrian amenities (i.e. lighting and banners).



Alternative 2: Providing bump-outs along Guelph Street provides the opportunity for on-street parking, and the addition of street trees, seating and pedestrian amenities.



Landscaped bump-outs provide space for additional boulevard treatments.



Alternative 3 (Long-Term): As part of the reconstruction of the Guelph Street right-of-way (in combination with the Norval Interchange) a wider right-of-way width could allow the proper boulevard width to accommodate street trees, lighting and banners, and pedestrian amenities on both sides.

Design Guideline # 7

Lighting Fixtures

Well-placed lighting standards create safe, active streetscapes.

Exterior lighting is an important and relatively inexpensive way to improve streetscapes and open spaces. This is particularly true during winter periods where daylight is reduced. Lighting, properly employed, attracts people, and provides safety and comfort.

- At gateways and focal areas, the Town Standard for decorative lighting should be applied to reinforce the cultural character of the Hamlet;
- In the Community Core where buildings are built to the edge of the sidewalk fixtures may be mounted directly on buildings;
- Alternatively, light fixtures should be placed regularly between sidewalk and curb cuts to allow unobstructed pedestrian movement;
- Lighting fixtures should be no more than 0.6 metres from the curb;
- Spacing of lighting fixtures should vary according to the intensity of pedestrian use. For the typical situation, spacing will be approximately 10 metres;
- An average luminary mounting height should be 3.6 metres (4.2 metre maximum);
- Downcast, pedestrian-scaled lighting enhances safety and visibility on streets. At gateways and focal points, lighting can be used to accent special features, such as heritage properties, landscaping and signage;
- Private property lighting should ensure safe and well lit pedestrian areas, including parking areas and building entrances;
- All pedestrian and street lighting should be "dark sky" friendly to minimize light pollution.



Above: Appropriately designed lighting fixtures add to the facade's aesthetics while offering nighttime visibility.
Below: Light standards throughout Norval should reflect the historic character of the community.

Design Guideline # 8

Signage

The Hamlet of Norval has a rich and diverse cultural heritage, which should be enhanced by appropriate signage.

In most cases, communities lack proper community identification, which can increase civic pride and awareness of the community. Such identification provides direction to visitors and through traffic and provides basic information on local history and architectural heritage.

- Signs which obscure architectural elements on heritage building are not permitted;
- Traditional materials such as wood, brass, or bronze are the most appropriate materials for signage within the Hamlet Community Core. Some modern materials may be considered if they blend with the material of built structure upon which the sign is to be located;
- Utilization of symbols, and historic lettering is encouraged;
- In historical areas, in general, each building is permitted one ground sign, canopy sign, one projecting sign, one soffit sign, one wall sign and window sign;
- In a case where the operations of a store have expanded into a number of adjoining storefronts, individual repeating signboards should be considered for each of the original storefronts;
- Pedestrian scale signs (window, hanging, awning signs) should be small and positioned to interfere as little as possible with neighbouring signs;
- Free-standing signage should not interfere with pedestrian circulation or accessibility;
- Within the Hamlet Community Core area animated, portable or roof signs are discouraged, as well as billboards and internally illuminated signs;
- The amount of information on signs should be limited - the shortest message has the greatest impact;
- Historical photographs may be used to establish the styles and types of signage appropriate to a building within its district during the era of its construction and early life and use these models for contemporary signs.

Note: Notwithstanding the above guidelines, all signage along Guelph Street/Highway 7 must conform to the Ministry of Transportation's Corridor Signage Policy.



Ensuring signage is high quality and is appropriately located within the Hamlet will enhance wayfinding, celebrate the history of Norval, and help to facilitate tourism. Below: Portable signs like these "sandwich boards" provide interest and vitality to the streetscape, though their use should be regulated.

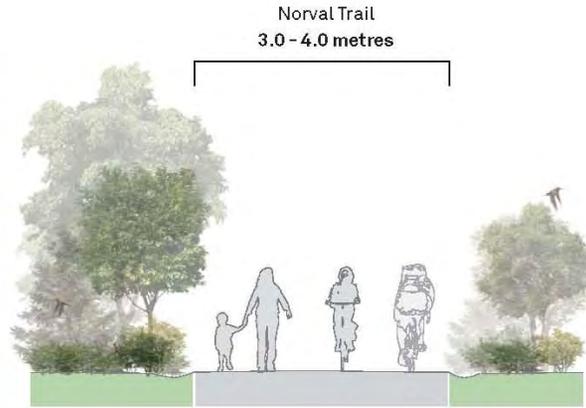
Design Guideline # 9

Trails

Provide alternative transportation options, and recreational and tourism opportunities through a well-connected trail network.

Trail development is an important component of providing non-vehicular access through the Hamlet of Norval and along the Credit River Valley. Trail development in the Norval Secondary Plan Area must be consistent with the Halton Hills Trails and Cycling Master Plan and the following policies.

- Trails within Norval should have minimum widths of 3.0-4.0 metres to accommodate pedestrians, bicyclists, and other types of recreational users;
- Trails should be designed, wherever possible, as separate linkages from other vehicular traffic;
- Development of trails that link existing natural areas, parks and open spaces, particularly along the Credit River Valley, should be encouraged;
- Design of trails should allow easy access for every user group. Surfaces of the trails should be carefully graded and finished to allow full accessibility except where such finishes have an impact on Greenlands areas. In these sensitive areas, the trails should be constructed of low impact materials that are porous and stable, such as crushed rock, wood chip paths or board walks;
- Design of trails should consider elements of public safety, avoiding creation of entrapment spots by non-transparent landscaping or through creation of walls or similar built features and should be designed to have frequent, clearly-marked exits to areas of high pedestrian and car traffic;
- Trails should be clearly marked with attractive way-finding (signage) systems;
- Trail development should explore opportunities to introduce environmental or cultural / historical learning experience and other educational experience (flora, fauna, local history) such as those that have developed at the Willow Park Ecology Centre;
- Trails should generally have lighting, except in environmentally sensitive areas where light could negatively impact on natural habitat areas. The necessity for and level of lighting, as well as the type of light fixtures will depend on the size and character of the trail.



Trails should provide links throughout the Hamlet and region with easy access for a range of users.

Design Guideline # 10

Building Character

Create an active, attractive public realm through buildings that have a distinct image and quality.

The Hamlet of Norval has a wide variety of building styles. These styles, while different, have a variety of similar elements that should be reflected through high quality building design that supports the unique character and scale of the Hamlet. Uniqueness, achieved through creative use of forms, details and colours should enhance pedestrian enjoyment along the street.

- New buildings, particularly in the Community Core, should reflect the scale, and common elements that define the heritage building character (please refer to the diagram on the right);
- Character should be achieved through creative and sensitive architectural design utilizing:
 - Building silhouette;
 - Spacing between buildings;
 - Setbacks from street property line;
 - Massing of building form;
 - Location and treatment of entrances;
 - Surface materials, textures and finishes;
 - Shadow patterns from massing and decorative features;
 - Style of architecture; and,
 - Landscaping on the site.
- Existing buildings within the Hamlet reflect a variety of building styles, including Post-War American, smaller “cottage” character, and a more traditional Victorian style (predominantly found on the Hamlet’s heritage churches). These styles have a number of key elements that should be reflected in new development, including:
 - Gables roofs
 - Protruding eavestroughs
 - Facades with vertically oriented windows and a wide variety of wall detailing
 - Double-hung windows
 - Stone lintels
 - Columns
 - Bending and arches of same or contrasting colour
 - Ground arches
 - Wide front porches
 - Wood detailing, etc.



Sample Building # 1: Post-War American Character

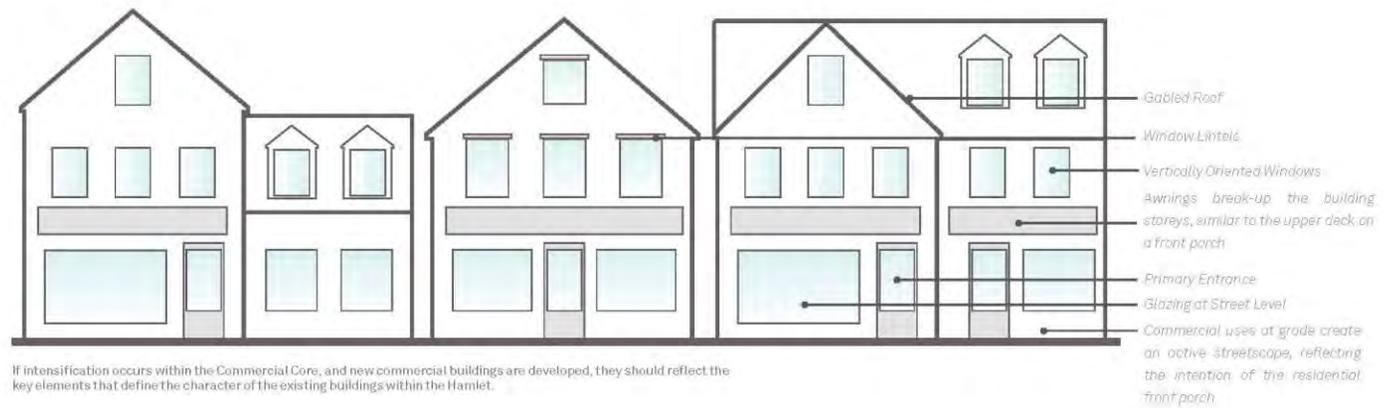
- Gabled Roof
- Eavestrough
- Stone Lintel
- Double Hung Windows
- Vertically Oriented Windows
- Wood Detailing
- Columns with Base
- Primary Entrance
- Brick Facade
- Wide Front Porch



Sample Building # 2: 'Cottage' Character

- Gabled Roof
- Vertically Oriented Windows
- Double Hung Window
- Lightly Coloured Siding
- Wood Detailing
- Columns
- Primary Entrance
- Wide Front Porch

Typical heritage façade articulation elements in the Hamlet.



- In addition, door lintels, window lintels and sills, window shutters, horizontal bands and cornices, different types of decorations (tiles or sculptural elements), light lamps, fences or balustrades should be taken into consideration during architectural design of new buildings in Norval;
- Set-backs should enhance the streetscape if they denote an important/public building or should create a well-defined public realm which is highly usable and pedestrian friendly;
- All front yard and side property lines in residential areas should be delineated with low hedges or similar plantings (rows of shrubs, linear flower beds). High, solid, fencing (over 2.0 metres) in front yards is strongly discouraged unless the property is adjacent to an industrial or commercial use that requires visual buffering.



Buildings in the Community Core should reflect the heritage character of Norval.



Window lintels and sills, window shutters, wood and stone detailing and facades with vertically oriented windows are important character components of residential buildings in the Hamlet.

Design Guideline # 11

Building Height

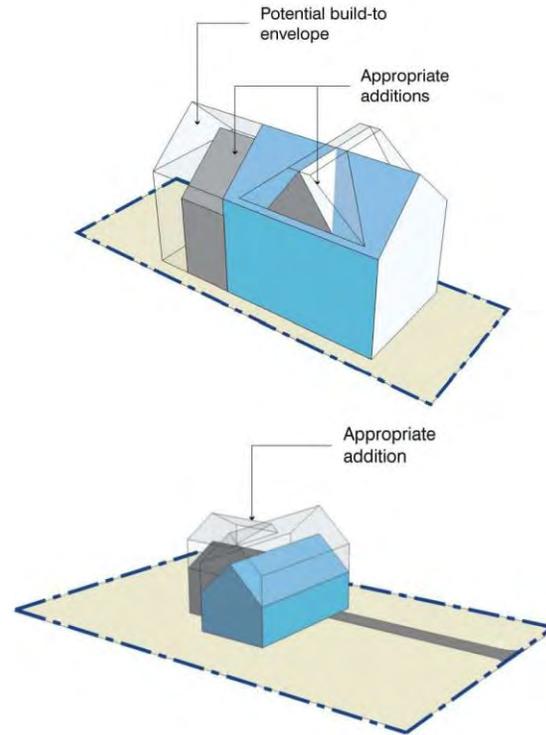
Create a strong street edge and a human scaled environment through appropriate built form, height and massing.

Building heights are regulated in the Official Plan and should not exceed 2 storeys. Within these restrictions, the following guidelines should be considered.

- Buildings should generally be of uniform height that does not vary more than 25 % from each other to define a street "ceiling";
- Infill buildings abutting existing structures at the building line should generally match the adjacent building height, or provide a clear offset in height so as to maintain the visual integrity of the existing structure;
- Buildings abutting lower scale buildings should ensure a transition in scale. The location of windows, horizontal lines and cornices, gables and roofs can be used to scale and proportion buildings and create transitions;
- Additions and renovations to existing buildings should ensure a final building that reflects the height, scale and massing of adjacent buildings;
- Additions/renovations to existing buildings should not be greater than 1/3 of the total building volume, and should be limited to no more than 1 storey above the existing height of the building (to a maximum of 2 storeys).
- Within the Community Core, vertical additions should result in building heights no greater than 2 storeys (7.5 metres);
- The height of planned buildings should comply with the permitted number of stories allowed by these design guidelines. Since the Secondary Plan proposes mixed use development in the "Hamlet Community Core" area it is advised that the approximate maximum allowed heights per individual storey be:
 - For commercial ground floors: 4.2m;
 - For office use second floors: 3.6m;
 - For standard residential floors: 3.3m;
 - For non-standard residential lofts (mansards, attics): 3.6m max, although all above-mentioned heights may be doubled if the architectural concept proposes loft-type of space.
- Mechanical penthouses, clock towers or similar architectural features shall not be subject to these height restrictions, but their massing and proportions should be well integrated and in direct relation to the building;
- Building heights should be used as a tool for assuring a minimum of 5 hours of daily sunlight in the public realm. Building envelope and height should be derived from the sun angle (on the shortest day in the year - Dec. 21) desired for a particular part of the street;
- Building height should be utilized in conjunction with setback control in order to establish proper sun radiation to public spaces.



Buildings should be of uniform height to create a defined street ceiling.



Renovations and additions to existing buildings should not be more than 1/3 of the existing building volume. The illustrations above demonstrate an example of an appropriate addition that is consistent with the existing character of the building.

Design Guideline # 12

Building Materials

Finishing materials should be of a high quality and should extend to all sides of the building, including projections.

Building materials are one of the predominant factors which determine character and quality of the building exterior. Careful consideration of materials, especially with respect to colour and texture, will make a significant contribution to the overall streetscape.

- Extensive mixtures of different materials should be discouraged. Exterior materials should be limited to no more than two complimentary materials, to avoid cluttering or an overly complex appearance;
- Acceptable exterior materials are: solid brick, cut stone, stucco, wood shingles, wood siding, glass, ceramic tiles or adequate combinations of them;
- Use of building materials should complement each other and complement traditional materials in the Hamlet Community Core. Although, there are many new materials on the market, more natural, traditional materials (brick, stone, wood) are encouraged in Norval;
- Imitation materials are discouraged. Building materials should not be used to replicate other materials (i.e. shingles that resemble bricks, etc.);
- Contemporary materials, such as aluminum, steel panels, coloured glass, ceramic tiles, etc. may be considered for use in future developments in conjunction with traditional materials. They should be used as accents for no more than 30% of front facades.



Preferred cladding materials include brick, stone, metal, glass, in-situ concrete, pre-cast concrete, and stucco.



Building materials should be complementary to the heritage and character of the Hamlet, especially in the Community Core.

Design Guideline # 13

Residential Buildings

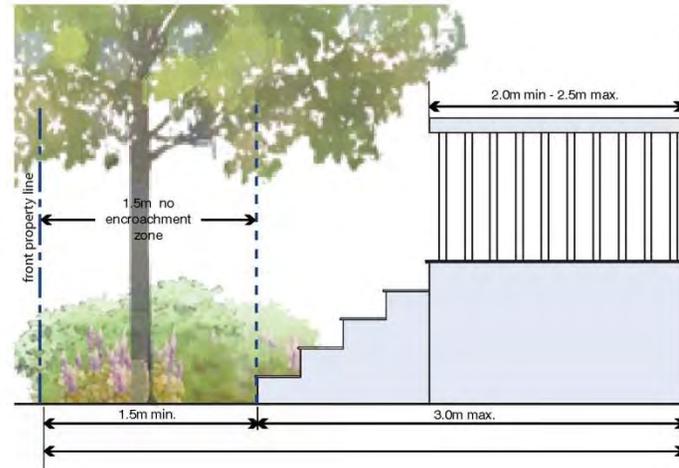
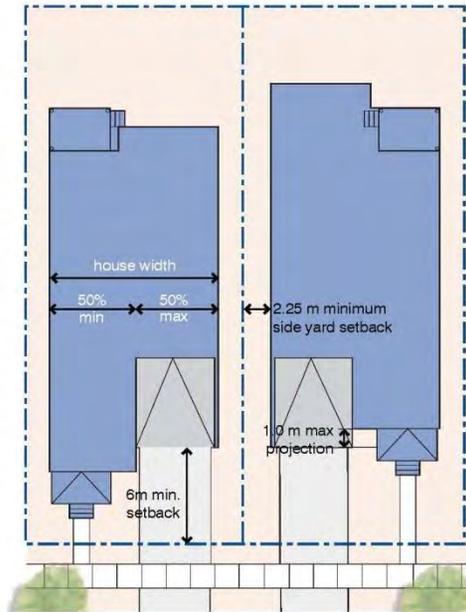
The built form, height and massing of residential buildings should be sensitive to adjoining areas, and the Hamlet of Norval as a whole.

General

- The mass and scale of residential buildings should be sensitive to adjoining areas. Currently, the majority of residential dwellings in Norval are between 1 and 2 storeys and future buildings should respect this 2 storey height limit;
- Dwellings should be oriented towards streets and/or open spaces to provide a sense of enclosure and enhanced safety through "eyes on the street";
- Primary building facades, particularly those which face streets, parks, and open spaces, should exhibit increased architectural detailing and generous amount of window openings to give attention to the prominence of these building faces and encourage strong visual connections between the private dwelling and public street;
- Flanking façades should have a design and materials standard equal to the front façade through the use of wrap-around porches, sun rooms, bay windows and side entrances.

Single-Detached Dwellings

- A range of front yard setbacks currently exists within Norval, and should continue to create a diversity of setbacks on the streetscape. However, front yard setbacks should range between 4.5 to 7.5 metres.
- Within the Community Core, front yard setbacks should range between 3.0 to 5.0 metres to create an appropriate transition between the public and private realm, while still maintaining a connection to the street.
- 1.5 metres of this minimum setback, from the front property line, should be a "no encroachment" zone. The remaining setback may contain non-habitable building elements (e.g. porches, steps, roof elements, etc.);
- Where dwellings have a front yard garage, a minimum 6.0 metre setback is recommended between the front of the garage and the front property line to accommodate one vehicle without disrupting the sidewalk.
- Interior side yard setbacks should be a minimum of 2.25 metres (including roof overhangs), or 3.0 metres where a garage is accessed by a side-yard driveway. Exterior side yard setbacks should be a minimum of 4.5 metres.
- There should be a minimum rear yard setback of 7.5m measured either to the rear property line or, in instances where a garage is present in the rear yard, to the face of the garage which is closest to the residential dwelling;
- Garage design should be complementary in character and quality of detail to the principal dwelling. To ensure garages are not a dominant feature of the community, they should be no wider than one half the width of the house;
- The minimum depth for porches and decks should be 2.0 metres;
- The top of the front porch should not be higher than 1/2 a storey above grade.



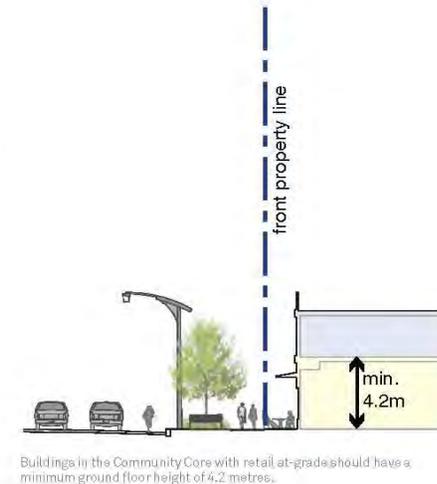
Design Guideline # 14

Commercial Buildings

Commercial buildings should promote attractive, pedestrian-supportive streetscape with a variety of public amenities.

Commercial buildings should have a high quality of architectural design to enhance and activate the streetscape.

- Commercial-related features that detract from the streetscape, such as excessive or illuminated signage, are discouraged. Pedestrian amenities, including walkways that connect entries, seating landscaping and human scaled lighting are encouraged wherever possible;
- Buildings should incorporate architectural detail such as vestibules, recessed entrances and covered walkways, canopies and awnings to reflect the heritage character of the Hamlet, and to provide weather protection;
- Commercial buildings should not have blank façades facing the street. The facades should have distinct architectural detailing, including entrance and window design;
- A significant amount of the building frontage on the ground floor and at the building base levels should be glass to allow views of the indoor uses and create visual interest for pedestrians;
- Landmark buildings are encouraged at the intersection of Guelph Street and Adamson Street, and should reinforce the prominence of this location through appropriate massing, building projections, recesses at-grade, lower storey design and open space treatments;
- A 4.2 metre floor-to-ceiling height is recommended at-grade to create a strong street presence and allow for flexible commercial space;
- Where setbacks vary on both sides of a proposed commercial building, the average of the two setbacks should be used.



Design Guideline # 15

Institutional Buildings

Institutional buildings should be designed to reflect their civic role through prominent, high quality architecture.

Existing institutional uses, such as places of worship and educational facilities, are focal points in the Hamlet. As required, opportunities for additional institutional uses (i.e. community centre, museum, etc.) should be explored, and can be attractive destinations within the Hamlet.

- Institutional buildings should be located at gateways and focal points, and should be highly visible;
- Building design should promote safety and ease of access through well defined entrances and windows facing the public street and primary walkways;
- Main entrances should be highly visible and distinguished through the building's architecture and detailing (i.e. door size, entry and windows);
- Façades should maximize the use of operable windows for natural illumination and ventilation.



New institutional buildings should be attractive landmark sites within the Hamlet.