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## PREAMBLE

## INTRODUCTION

This document comprises the 2008 Town of Halton Hills Official Plan, consolidated to April 2024. The Plan repeals and replaces the Town of Halton Hills Official Plan as adopted by Town Council in 1982.

## ORGANIZATION OF THE PLAN

This document consists of three components:

- Part I  
The PREAMBLE, which does not constitute part of the Official Plan;
- Part II  
The TOWN OF HALTON HILLS OFFICIAL PLAN, comprised of text and Schedules A1-A17, B1, B2, H2, H3, H4-1, and H4-2, H5, H6-1, H6-2, H6-3, H6-4, H7-1, H7-2, H7-3 constitutes the operative part of the document; and,
- Part III  
The APPENDICES, which do not constitute part of the Official Plan, but provide additional information to assist in implementation of this Official Plan.

## LOCATION

This Official Plan affects all lands within the boundary of the Town of Halton Hills.

## BASIS

The first Halton Hills Official Plan was adopted in 1982 and approved with modifications and deferrals by the Province in 1985. Subsequent to the approval of the Official Plan, Secondary Plans were approved in 1987 for the Georgetown West and Georgetown South Planning Districts. A Municipal Housing Statement was completed in 1991. Subsequently, a comprehensive review of the residential and commercial land use policies for the communities of Acton and Georgetown was completed in 1993 with the approval of the Urban Area Study and a series of implementing Official Plan Amendments. In 1998, the Town completed the Industrial Strategy.

Other initiatives in support of the Town's long range planning program included the preparation of detailed land use policies for the Premier Gateway Employment Area, Secondary Plans for Norval and Glen Williams, a Municipal Housing Statement Update, the Residential Care Facilities Study, and the Adult Entertainment Study. All of the aforementioned initiatives have been specific to an area or issue. None of these amendments involved a major review of the goals of the 1982 Official Plan on a comprehensive basis.

The 1982 Halton Hills Official Plan included a policy commitment that the Official Plan will undergo a comprehensive review within five years to measure the achievement of the Plan's policies against its goals, and to revise its goals, policies and methods of implementation where necessary. Town Council authorized the undertaking of a comprehensive review of the Official Plan with the approval of Report

No. PD-99-125 in November 1999. Given that the Official Plan that was in effect at that time was adopted in 1982, a comprehensive review was justified on the basis of the age of the document.

Other factors that led to the review included:

- revisions to the Planning Act and the release of the 1996 Provincial Policy Statement;
- updates to the Niagara Escarpment Plan;
- a new Regional Official Plan (1995) as well as a 5 year review of that document;
- completion of a Community Strategic Plan in 1999;
- growth pressures resulting from Halton Hills' location in the Greater Toronto Area; and,
- community concerns regarding the protection and enhancement of the Town's environmental resources.

Recommended program objectives for the Official Plan Review included:

- to develop the Official Plan with a community based approach;
- to prepare the Official Plan so that it provides guidance for a twenty year period (approximately 2021);
- to address the environment, quality of life, and economic potential of Halton Hills as key elements of the Official Plan;
- to use a watershed basis and ecosystem approach;
- to update and consolidate the Secondary Plans in concert with the Official Plan review; and,
- to develop user friendly, plain language format for the Official Plan with clear mapping.

The Official Plan program was formally initiated in May of 2000. The program was based, in part, on the Community Strategic Plan that established a clear vision of the kind of future that the Town wanted to achieve. This vision was developed through a focused strategic plan process where the public, community leaders, stakeholders, staff and elected representatives actively engaged in dialogue about the choices and challenges that were likely to face the community over the following twenty years.

The Official Plan program was also reliant on the findings of the Silver Creek Subwatershed Study, the Rural Study, the Commercial Policy Review, the Urban Design Study and the Municipal Housing Statement Update. Public workshops were also held in the key topic areas of Growth Management, Environment and Economic Development. Discussion Papers were prepared regarding the aforementioned topic areas along with Housing to provide policy options for public and agency consideration.

The aforementioned studies and discussion papers served as a basis for agency and public input to the planning process including detailed input from a Technical Advisory Committee made up of agency representatives and a Steering Committee comprised of community residents. Various staff reports summarized the main policy direction that was incorporated into the Draft Official Plan dated April 2004.

On April 17, 2004, Council authorized the release of the Draft Official Plan, dated April 2004, for formal public and agency comments. The draft document was subject to extensive agency and public review, including three public open houses. The consultation process generated thirty-six written submissions. This correspondence was incorporated along with a staff response/recommendation into a document entitled Submissions Matrix dated February 2005. The recommendations contained in the Submissions Matrix, provided the basis for the Draft Official Plan, dated May 2005. The Draft Official Plan was also updated to reflect the Province's Greenbelt Plan and the 2005 Provincial Policy Statement.

A statutory public meeting was held on Tuesday, June 28, 2005. Staff received thirty-eight written submissions on the revised Draft Official Plan, dated May 2005. Correspondence received before the deadline was incorporated verbatim along with a staff response/recommendation into the document entitled Official Plan Program Submissions Matrix, dated June 2006. The recommendations contained in the Submissions Matrix as well as other input including comments received from the Region of Halton, provided the basis for this Official Plan, which was adopted by Council on September 18, 2006. This Official Plan came into force upon approval by Halton Region on March 28, 2008, save and except for seven deferrals.

## DEFERRALS

As noted above, this Town of Halton Hills Official Plan came into force upon approval by Halton Region on March 28, 2008, save and except for seven deferrals. The status of each of these deferrals is outlined below:

- Deferral 1 (D1) has been lifted from Schedule A1 as it was not carried forward in Regional Official Plan Amendment No. 38 (ROPA 38), which incorporates the results of Sustainable Halton – Halton Region's growth management and land use response to the Growth Plan for the Greater Golden Horseshoe, Greenbelt Plan, and Provincial Policy Statement.
- Deferral 2 (D2) has been lifted from Schedule A1 as it was not carried forward in Regional Official Plan Amendment No. 38 (ROPA 38), which incorporates the results of Sustainable Halton – Halton Region's growth management and land use response to the Growth Plan for the Greater Golden Horseshoe, Greenbelt Plan, and Provincial Policy Statement.
- Deferral 3 (D3) has been lifted from Schedule A1 as it was not carried forward in Regional Official Plan Amendment No. 38 (ROPA 38), which incorporates the results of Sustainable Halton – Halton Region's growth management and land use response to the Growth Plan for the Greater Golden Horseshoe, Greenbelt Plan, and Provincial Policy Statement.
- Deferral 4 (D4) has been lifted from Schedule A2 as it was removed through the approval of Official Plan Amendment No. 23 (Dufferin Acton Quarry Expansion).
- Deferral 5 (D5) applies to the definition of Significant Woodland, as defined in Section G13.7 (Glossary) of this Official Plan, as it relates to lands shown on Schedule A2 of this Official Plan.
- Deferral 6 (D6) applies to the definition of Significant Woodland, as defined in Section G13.7 (Glossary) of this Official Plan, as it relates to lands shown on Schedule A2 of this Official Plan.
- Deferral 7 (D7) has been lifted from Schedule A2 to reflect a Decision of the Ontario Municipal Board.

## 2010 OFFICIAL PLAN REVIEW

Between March 28, 2008 and January 1, 2017, the Town of Halton Hills Official Plan was amended by several Official Plan Amendments, many of which were initiated as part of the 2010 Official Plan Review. The amendments that resulted from the 2010 Official Plan Review were categorized into three categories, and are summarized below:

### Category 1: Growth Plan Conformity Amendments

- Official Plan Amendment No. 7 (OPA 7) implemented the GO Station Secondary Plan, which included land use policies pertaining to the Georgetown Major Transit Station Area, as identified in the Growth Plan for the Greater Golden Horseshoe.
- Official Plan Amendment No. 9 (OPA 9) implemented the recommendations of the Town's Intensification Opportunities Study, which included delineation of the Built Boundary, minimum intensification targets for the area within the Built Boundary, and mapping of intensification areas.
- Official Plan Amendment No. 10 (OPA 10) implemented Sustainable Halton as set out in Regional Official Plan Amendment No. 38 (ROPA 38), which included the 2031 planning horizon, revised population and employment targets, minimum density targets for Designated Greenfield Areas, and designation of the *Future Residential/Mixed Use Area* and Phases 1B and 2B of the Premier Gateway Employment Area.

### Category 2: Amendments Arising from Town-initiated Studies

- Official Plan Amendment No. 5 (OPA 5) implemented the recommendations of the Town's Community Improvement Plan, which included designation of a Community Improvement Project Area and eight Community Improvement Project Sub-Areas;
- Official Plan Amendment No. 6 (OPA 6) implemented the recommendations of the Acton Downtown Area Land Use Study, which included revision of the boundary and sub-area designations for the *Acton Downtown Area*, and introduction of the *Downtown Redevelopment Sub-Area*, *Downtown Residential/Office Sub-Area*, and *Downtown Medium Density Residential/Commercial Sub-Area*; and,
- Official Plan Amendment No. 14 (OPA 14) implemented the recommendations of the School Site Land Use Planning Study, which included the introduction of policies that preserve school sites for their intended educational purposes in consideration of their importance as community hubs and neighbourhood gathering places.
- Official Plan Amendment No. 15 (OPA 15) implemented the recommendations of the Stand-Alone Aggregate Related Uses Study, which included land use policies pertaining to aggregate related uses, asphalt plants, concrete batching plants, aggregate transfer stations, outdoor storage uses, and contractors' establishments.
- Official Plan Amendment No. 19 (OPA 19) implemented the recommendations of the Green Development Standards Study, which included sustainable development practices through enhanced performance related to energy conservation, water conservation, air quality, waste management, transportation, community design, natural environment, innovation and communication.

### Category 3: Amendments Arising from ROPA No. 38 – Rural and Environmental Policy Matters

- In March 2016, the Town initiated the Agricultural and Natural Heritage Systems Review, which will further amend the Town's Official Plan to achieve conformity with Halton Region's Official Plan, as it relates to the *Agricultural Area*, *Mineral Resource Extraction Area*, Rural Clusters, the Natural Heritage System within the Agricultural/Rural Area, and the Greenbelt *Protected Countryside Area* (as required).

## 2024 OFFICIAL PLAN CONSOLIDATION

This 2024 consolidation of the 2008 Town of Halton Hills Official Plan incorporates Official Plan Amendments Nos. 1--56, save and except for certain matters which have not yet been approved by Halton Region or the Ontario Land Tribunal, as identified below. Figure 1 (in the Preamble) identifies the areas on Schedules A1, A2, A3, A3-1, A8, A17, B1, and B2 that are currently pending approval.

### Official Plan Amendment No. 21

All text in this Plan highlighted in pink and areas identified on Figure 1 (in the Preamble) are part of Official Plan Amendment No. 21 (OPA 21). OPA 21 has not yet been approved by Halton Region, and therefore is not in force. OPA 21 was adopted by Town Council in July 2014.

OPA No.	Description of Official Plan Amendment	By-Law No.	Date of Council Adoption	Date of Regional or OLT Approval	Inclusion in this Plan?
1.	Norval Secondary Plan	2006-0108	18 September 2006	7 March 2008	Yes
2.	Glen Williams Secondary Plan	2006-0108	18 September 2006	7 March 2008	Yes
3.	Menkes Georgetown Estates	2008-0080	14 July 2008	13 August 2008	Yes
4.	79 Main Street North	2008-0134	8 December 2008	6 January 2009	Yes
5.	Community Improvement Plan	<a href="#">2010-0027</a>	12 April 2010	Exempt	Yes
6.	Acton Downtown Area Land Use Policy Review	<a href="#">2012-0023</a>	6 March 2012	Exempt	Yes
7.	Georgetown GO Station Area Secondary Plan	<a href="#">2010-0055</a>	11 May 2010	19 August 2011	Yes
8.	13383 Fourth Line	<a href="#">2010-0041</a>	26 April 2010	Exempt	Yes
9.	Intensification Areas & Policies	<a href="#">2010-0056</a>	11 May 2010	9 February 2015	Yes
10.	Provincial Growth Plan Conformity	2010-0077	28 June 2010	21 January 2016	Yes

OPA No.	Description of Official Plan Amendment	By-Law No.	Date of Council Adoption	Date of Regional or OLT Approval	Inclusion in this Plan?
11.	Automotive Commercial Uses	<a href="#">2012-0068</a>	27 August 2012	Exempt	Yes
12.	Esqueasing Developments	2011-0069	11 July 2011	20 July 2012 (OMB Approval)	Yes
13.	11410 Trafalgar Road, 12995 17 Side Road	2013-0058	N/A	23 September 2013 (OMB Approval)	Yes
14.	School Site Land Use Planning Study	<a href="#">2011-0101</a>	12 December 2011	Exempt	Yes
15.	Standalone Aggregate Related Uses Study	2013-I	18 March 2013	13 December 2022 (OLT Dismissal)	Yes
16.	Re-designation of Former Aggregate Extraction Sites	2013-0012	18 March 2013	13 September 2018 (OLT Dismissal)	Yes
17.	Churchill Valley Estates	2013-0040	8 July 2013	15 August 2013	Yes
18.	CRAFT Development Corporation		N/A	28 January 2014 (OMB Approval)	Yes
19.	Green Development Standards Study	2014-0045	7 July 2014	Exempt	Yes
20.	Revised Norval Secondary Plan	2014-0020	14 April 2014	Exempt	Yes
21.	GTA West Corridor Protection	2014-0050	7 July 2014	Deferred by Region at Council request	Yes
22.	Mature Neighbourhoods Character Study	2017-0032	29 May 2017	Exempt	Yes
23.	Dufferin Acton Quarry Expansion	2015-0032	6 July 2015	October 11, 2016 (Joint Board Approval)	Yes
24.	Memorial Arena lands	2015-0025	6 July, 2015	Exempt	Yes
25.	16 Adamson Street North	2015-0042	24 August 2015	Exempt	Yes
26.	Minor Revisions to the Official Plan	2016-0070	12 December 2016	Exempt	Yes
27.	224 Maple Avenue	2016-0001	11 January 2016	Exempt	Yes
28.	8 & 10 Lindsay Court and 13758 & 13764 Highway 7, 12 Lindsay Court	2016-0019	N/A	8 April 2016 (OLT Approval)	Yes

OPA No.	Description of Official Plan Amendment	By-Law No.	Date of Council Adoption	Date of Regional or OLT Approval	Inclusion in this Plan?
29.	193, 195, 197 Mountainview Road North and 111, 115 John Street (Georgetown)	2016-0073	12 December 2016	Exempt	Yes
30.	Additional 75 ha to Urban Area (Premier Gateway Phase 1B lands)	2018-0034	11 June 2018	16 March 2021	Yes
31A.	Premier Gateway Phase 1B1 Secondary Plan	2018-0035	11 June 2018	16 March 2021	Yes
31B.	Premier Gateway Phase 1B2 Secondary Plan	2018-0036	11 June 2018	16 March 2021	Yes
32.	Vision Georgetown Secondary Plan	2018-0048	9 July 2018 9 July 2018	19 January 2024	Yes
					Yes
33.	25 James Street (Georgetown)	2018-0039	15 April 2019	Exempt	Yes
34.	Glen Williams Mature Neighbourhoods Study	2019-0017	8 July 2019	Exempt	Yes
					Yes
35.	Cannabis Cultivation and Processing	2019-0034	9 September 2019	Exempt	Yes
36.	284 Queen Street East (Acton)	2019-0038	9 September 2019	Exempt	Yes
37.	Destination Downtown Secondary Plan	2019-0050	20 January 2020	Exempt	Yes
38.	Accessory Dwelling Units	2019-0048	9 July 2018	Exempt	Yes
39.	12144 Steeles Avenue (Esquesing)	2020-0001	9 July 2018	Exempt	Yes
40.	Hold for Intensification Study	_____	_____	_____	No
41.	42 Mill Street	2020-0027	25 May 2020	Exempt	Yes
42.	20 Ransom Street Acton	_____	_____	OLT Approved 6 (OLT Approval)	Yes
43.	Habitat for Humanity - 37 King Street (Georgetown)	2021-0024	25 May 2021	4 June 2022 (OLT Approval)	Yes
44.	Glen Williams Secondary Plan Review	2021-0046	4 October 2021	Under Appeal	No

OPA No.	Description of Official Plan Amendment	By-Law No.	Date of Council Adoption	Date of Regional or OLT Approval	Inclusion in this Plan?
45.	17 Guelph Street, Georgetown	2022-0006	28 February 2022	Exempt	Yes
46.	Delegated Authority for ZBAs	2022-0038	13 June 2022	Exempt	Yes
47.	47 Maria Street	2022-0045	1 June 2023	Exempt	Yes
48.	Remington development OPA	2023-0033	8 May 2023	Exempt	Yes
49.	Bill 109, More Homes for Everyone Act, 2022	2023-0010	13 February 2023	29 May 2023	Yes
50.	Premier Gateway Phase 2B	2023-0089	October 10, 2023	Under Appeal	No
51	Gellert Park Expansion	2023-0044	29 May 2023	Exempt	Yes
52	Gilbach Waterpark	2023-0064	21 June 2023	Exempt	Yes
53	59 & 61 King Street (Georgetown)	2024-0002	22January 2024	Exempt	Yes
54	Glen Williams Estates - 102 Confederation St	Draft 2023-xxxx	N/A	11-Apr-2024 (OLT Approval)	Yes
55	Alternative Notice OPA	2024-0007	12 February 2024	Exempt	Yes
56	Dayfoot Drive	2024-0021	9 February 2024	Exempt	Yes



## INTRODUCTION

An Official Plan is a policy document, which is intended to serve as the basis for making land use decisions and managing change in any municipality in Ontario. According to the Planning Act, an Official Plan “shall contain goals, objectives and policies established primarily to manage and direct physical change and the effects on the social, economic and natural environment of the municipality”. In preparing the Official Plan for the Town of Halton Hills Planning Area, Council has had regard to matters of Provincial interest as set out in Section 2 of the Planning Act and as listed below:

- The protection of ecological systems, including natural areas, features and functions;
- The protection of the agricultural resources of the Province;
- The conservation and management of natural resources and the mineral resource base;
- The conservation of features of significant architectural, cultural, historical, archaeological or scientific interest;
- The supply, efficient use and conservation of energy and water;
- The adequate provision and efficient use of communication, transportation, sewage and water services and waste management systems;
- The minimization of waste;
- The orderly development of safe and healthy communities;
- The accessibility for persons with disabilities to all facilities, services and matters to which the Planning Act applies;
- The adequate provision and distribution of educational, health, social, cultural and recreational facilities;
- The adequate provision of a full range of housing, including affordable housing;
- The adequate provision of employment opportunities;
- The protection of the financial and economic well-being of the Province and its municipalities;
- The co-ordination of planning activities of public bodies;
- The resolution of planning conflicts involving public and private interests;
- The protection of public health and safety;
- The appropriate location of growth and development; and
- The promotion of development that is designed to be sustainable, to support public transit and to be oriented to pedestrians.

These matters of Provincial interest are further articulated in the Provincial Policy Statement. The Policy Statement is intended to promote a policy led system that recognizes and addresses the complex interrelationships among environmental, economic and social factors in land use planning. The Policy Statement deals with matters pertaining to Building Strong Communities, Wise Use and Management of Resources, and Protecting Public Health and Safety and a series of related subcategories. This Official Plan implements the Provincial Policy Statement through more detailed policies that appropriately reflect the Town's unique character.

Section 27 of the Planning Act requires the Town of Halton Hills Official Plan to conform to the Region of Halton Official Plan. The intent of the Regional Official Plan is to set out how growth and development, natural resources and the natural environment are to be managed throughout Halton Region. The intent of the Town of Halton Hills Official Plan is to refine and expand upon the policy direction contained within the Regional Official Plan in a manner that reflects the Town's character, role and location within the Region of Halton and the Greater Toronto Area. Upon approval by the Region, the Town of Halton Hills Official Plan shall be deemed to conform to the Regional Official Plan. In the event of a conflict between the provisions of the Town of Halton Hills Official Plan and the Regional Official Plan, the more restrictive provision shall apply.

The Town of Halton Hills Official Plan has also been prepared in accordance with the land use designations and policies of the Niagara Escarpment Plan. Pursuant to the Niagara Escarpment Planning and Development Act, the Niagara Escarpment Plan prevails over any local plan or zoning by-law where conflicts occur between them. Town policies in this Official Plan that are more rigorous or restrictive than the Niagara Escarpment Plan are considered not to be in conflict with the Niagara Escarpment Plan and continue to apply.

This Official Plan has also been prepared in accordance with the Greenbelt Plan, which came into effect on February 28, 2005. The Greenbelt Plan was established under Section 3 of the Greenbelt Act, 2005, which received Royal Assent on February 24, 2005, and was deemed to have come into force on December 16, 2004. According to the Greenbelt Plan, "the Greenbelt is a cornerstone of Ontario's proposed Greater Golden Horseshoe Growth Plan which is an overarching strategy that will provide clarity and certainty about urban structure, where and how future growth should be accommodated, and what must be protected for current and future generations. The Greenbelt Plan identifies where urbanization should not occur in order to provide permanent protection to the agricultural land base and the ecological features and functions occurring on this landscape."

The Greenbelt Plan contains one designation – Protected Countryside. In a Halton Hills context, and as set out in the Greenbelt Plan, lands within the *Protected Countryside Area* designation are intended to enhance the spatial extent of agriculturally and environmentally protected lands within the Niagara Escarpment Plan while at the same time improving linkages between these areas and the surrounding major lake systems and watersheds. Within this designation there is an Agricultural System, a Natural System, and Settlement Areas. The policies of this Official Plan are intended to implement the Greenbelt Plan to the extent possible. A further amendment to this Plan is anticipated following the completion of a more detailed Greenbelt Plan implementation exercise by the Region of Halton, in consultation with the Town and the other Local Municipalities.

Pursuant to the Greenbelt Act, the Greenbelt Plan prevails over any local plan or zoning by-law where conflicts occur between them. Town policies in this Official Plan that are more rigorous or restrictive

than the Greenbelt Plan are not considered to be in conflict with the Greenbelt Plan and continue to apply subject to the applicable limitations set out in Section 5.3 of the Greenbelt Plan.

The preparation of this Official Plan involved many dedicated and committed people with an interest in the future of the Town of Halton Hills. In January of 1999, Council initiated a strategic planning process to establish a clear Vision of the kind of future that the Town would like to achieve. This Vision was developed through a focused strategic plan process where the public, community leaders, stakeholders, staff and elected representatives actively engaged in dialogue about the choices and challenges that faced their community over the next twenty years.

The strategic planning process and the Official Plan process have resulted in the development of a Land Use Vision for the future of the Town that is expressed in this Plan. This Vision is based on the following planning principles:

- a) Protect and enhance the natural environment and cultural heritage features of the Town;
- b) Manage growth by directing it to appropriate locations that have access to full urban services;
- c) Protect and enhance the character of both urban and rural areas in the Town;
- d) Protect good agricultural land from incompatible development;
- e) Provide appropriate municipal services to support an excellent quality of life;
- f) Foster the development of a prosperous economy by encouraging appropriate economic development in the best locations; and,
- g) Deliver responsive and effective local government.

In addition to the Strategic Plan, a number of comprehensive Discussion Papers were prepared prior to the writing of this Plan. These Discussion Papers dealt with growth management, housing, the environment, economic development and the rural area. In addition, an Urban Design Study, a Retail Market Demand Study, a Municipal Housing Statement Update and the Silver Creek Subwatershed Study were completed.

On June 16, 2006 the Growth Plan for the Greater Golden Horseshoe came into effect in accordance with the legislative framework provided by the Places to Grow Act. The Growth Plan provides a vision and sets out a supporting policy framework for managing growth in one of the fastest growing regions in North America until 2031. Detailed policy direction is provided pertaining to population and employment forecasts, intensification, employment lands, greenfield development, settlement area expansions, infrastructure, resource management and natural systems. All municipal official plans must be brought into conformity with the Growth Plan within defined time frames.

In order to address the requirements of the Growth Plan, along with the Greenbelt Plan and the 2005 Provincial Policy Statement, between 2006 and 2009, the Region of Halton completed a multi-year work program, entitled Sustainable Halton. This exercise culminated in the selection of a Preferred Growth Option in June 2009, and the adoption of Regional Official Plan Amendment No. 38 in December 2009, implementing the Preferred Growth Option, as well as a number of other policy changes arising from the multi-faceted work program.

In order to respond to the Growth Plan and provide input to the Sustainable Halton exercise, the Town undertook a number of strategic and land use planning initiatives that are related to and are intended to respond to the Growth Plan and provide input to the Sustainable Halton Plan process. These initiatives included the development of a new Strategic Plan, approved by Council in November 2007, setting out a broad vision for the community contained in the following eight equally important strategic directions:

- Foster a Healthy Community;
- Preserve, Protect and Enhance our Environment;
- Foster a Prosperous Economy;
- Preserve, Protect and Promote our Distinctive History;
- Preserve, Protect and Enhance our Countryside;
- Achieve Sustainable Growth;
- Provide Sustainable Infrastructure and Services; and,
- Provide Responsive, Effective Municipal Government.

In addition to the foregoing, the Town commenced a series of studies and planning initiatives which primarily have an urban focus, including the Intensification Opportunities Study, the GO Station/Mill Street Corridor Secondary Plan, and the Community Improvement Plan. The Town also prepared Discussion Papers, reports, and consulted with its residents through a variety of means in providing input to the selection of a Preferred Growth Option for Sustainable Halton as implemented through ROPA No. 38.

The goals, objectives and policies contained in this Plan are intended to guide the decisions of public authorities and private interests until 2031, which is the planning period established by this Plan. All new public works and the passage of any zoning by-law must conform to this Official Plan. This Plan applies to all lands within the Town of Halton Hills.

## STRUCTURE OF THE PLAN

The Introduction is considered part of the Town of Halton Hills Official Plan. This Official Plan is further divided into eight parts, each of which is described below:

### PART A - COMMUNITY VISION, GOALS AND STRATEGIC OBJECTIVES

Part A contains the Vision of the Town. This Vision was prepared by Council and is based on an understanding of past and future trends and the wishes of the Town's residents. The goals and strategic objectives that form the basis of the Plan flow from the Vision. These goals and strategic objectives also establish a framework for the other objectives and policies in the Plan. This section of the Plan also describes how it is to be implemented through a series of land use designations.

### PART B - ENVIRONMENTAL AND OPEN SPACE LAND USE POLICIES

Part B contains policies that apply to the natural heritage features and public and private open space lands within the Town, including lands within the *Niagara Escarpment Plan Area* and lands within the Town's Greenlands System.

### PART C - ENVIRONMENTAL MANAGEMENT POLICIES

Part C contains the policies dealing with watercourses, natural hazards, watershed planning and surface and groundwater resources throughout the Town. In addition, policies that specify the requirements for matters such as stormwater management reports, environmental impact studies, tree preservation, the development of contaminated sites, land use compatibility, noise and vibration reports and energy conservation are contained in this section. The policies of this Section of the Official Plan apply to all lands within the Town of Halton Hills, unless specifically modified by the policies contained within Section E2 (*Protected Countryside Area*).

### PART D - URBAN LAND USE POLICIES

Part D contains the land use policies that apply to the urban areas of Georgetown, Acton and the Premier Gateway Employment Area.

### PART E - AGRICULTURAL/RURAL LAND USE POLICIES

Part E contains the land use policies that apply to the Town's Agricultural/Rural Area.

### PART F - GENERAL DEVELOPMENT POLICIES

Part F contains policies that, where appropriate, must be considered as part of the land use planning process. This includes subdivision of land, urban design, cultural heritage resources, transportation, parkland, community facilities, servicing, public uses and development phasing. These policies apply to the whole of the Town.

### PART G - PLAN IMPLEMENTATION AND ADMINISTRATION

Part G describes how the Vision, Goals, Objectives and Policies of the Official Plan will be implemented.

## PART H - SECONDARY PLANS

Part H contains more detailed planning policies for specific geographic areas in the Town and includes the Secondary Plans for the Hamlets of Glen Williams and Norval and the Georgetown GO Station Area.

## SCHEDULES

The following schedules, which are to be read in conjunction with the text of this document, also constitute an operative part of this Official Plan:

Schedule A1	Land Use Plan
Schedule A2	Greenbelt Plan
Schedule A3	Georgetown Urban Area Land Use Plan
Schedule A3-1	Georgetown Built Boundary and Intensification Areas
Schedule A5	Georgetown Community Node Land Use Plan
Schedule A6	Acton Urban Area Land Use Plan
Schedule A6-1	Acton Built Boundary and Intensification Areas
Schedule A7	Acton Downtown Area Land Use Plan
Schedule A8	Premier Gateway Employment Area Land Use Plan
Schedule A9	Ashgrove Land Use Plan
Schedule A10	Ballinafad Land Use Plan
Schedule A11	Bannockburn Land Use Plan
Schedule A12	Crewsons Corners Land Use Plan
Schedule A13	Henderson's Corners Land Use Plan
Schedule A14	Limehouse Land Use Plan
Schedule A15	Silvercreek Land Use Plan
Schedule A16	Terra Cotta Land Use Plan
Schedule A17	Future Strategic Employment Areas
Schedule B1	Functional Plan of Major Transportation Facilities
Schedule B2	Right-Of-Way Classifications
Schedule H2	Norval Land Use Plan

Schedule H3	Georgetown GO Station Area Land Use Plan
Schedule H4-1	Glen Williams Land Use Plan
Schedule H4-2	Glen Williams Environmental Areas
Schedule H5	Premier Gateway Phase 2B Secondary Plan
Schedule H6.1	Vision Georgetown Community Structure
Schedule H6-2	Vision Georgetown Land Use Plan
Schedule H6-3	Vision Georgetown Transportation Network
Schedule H6-4	Regulated Watercourse and Associated Flooding Hazard Southwest Quadrant
Schedule H7-1	Downtown Georgetown Land Use Plan
Schedule H7-2	Downtown Georgetown Built Form
Schedule H7-3	Downtown Georgetown Active Transportation Plan





The primary purpose of the Official Plan is to provide the basis for managing growth that will support and emphasize the Town's unique character, diversity, civic identity, rural lifestyle, natural heritage and cultural heritage and to do so in a way that has the greatest positive impact on the quality of life in Halton Hills. The Official Plan is one of a series of municipal policies, guidelines and regulations that will direct the actions of the Town and shape growth and development. The Official Plan establishes a vision for the future land use structure of the Town and, as a result, is intended to serve as the basis for managing change until 2031, which is the planning horizon established by this Plan.

The Town of Halton Hills is comprised of several settlement areas of varying size, and a substantial rural area, with a distinctive and enviable identity that comes from the beauty and tranquility of the rural setting, rolling hills, rivers and valleys. It is a place where residents enjoy safe family living, scenic beauty and active community life. The community recognizes the unique attributes that set it apart from other places and is passionate about preserving the small town character and rural feeling while accommodating a moderate scale of growth to 2031 that is in keeping with the Town's urban and rural character, as articulated in the Town Strategic Plan. The preservation and enhancement of these features while keeping pace with the diverse needs of the community is at the forefront of the Town's Vision for its future.

The Town and its citizens view its long-term future to be more self-reliant and supports managed growth that preserves the unique features of the community, uses land wisely, elevates the quality of the built environment and provides diverse economic opportunities.

The aim is to provide choices for employment, housing, shopping and services. The intent is to diversify and create a more vibrant local economy through collaborative partnerships with existing businesses and through proactive efforts to attract new industries and services.

The Town and its residents view community services as being fundamental to maintaining and enhancing the quality of life for all age groups. The citizens provide vitality to the community by their committed volunteerism. Halton Hills enjoys a thriving arts community, local theatre and cultural events, which provides a balance with the abundance of passive and active recreational and leisure pursuits. The Town and its citizens recognize the need for new and enhanced community services and physical infrastructure to support the existing and changing population. This includes ensuring that schools and other similar community facilities are planned for in all new neighbourhoods and retained where possible in existing neighbourhoods in recognition of the role they play as a focus of community activity and neighbourhood interaction.

The Town has a number of significant environmental and topographical features that contribute to the 'sense of place' felt by many of the Town's residents. These features include the Credit River Valley system, the Niagara Escarpment, the Bruce Trail and the vast forest tracts, smaller woodland areas and wetland areas that support diverse wildlife communities. The protection of these attributes is a key underlying principle in this Official Plan and for this reason, this Official Plan establishes an 'environment-first' philosophy in the Town. The environment-first philosophy is a land use planning approach that is to be considered in making all planning decisions whereby the importance of maintaining, restoring and where possible, enhancing or improving natural heritage features and ecological functions is recognized and promoted as a Town priority to be considered in conjunction with other interests in accordance with Provincial policy and legislation.

As with the Region of Halton Official Plan, the Niagara Escarpment Plan and the Greenbelt Plan, this Official Plan recognizes and enshrines the concept of landform permanence and the need to

permanently protect certain landforms in keeping with the principles of land stewardship or where appropriate through public ownership. These include the Niagara Escarpment, Environmentally Sensitive Areas, wetlands, areas of natural and scientific interest, and streams and valley systems which are to be maintained in their current form and extent with no or as little displacement or encroachment as possible. Other lands to be preserved in large measure so that they will always form part of the Town's landscape include farms, countryside, forested areas and other open space areas.

The agricultural areas of the Town are considered to be an important component of what makes up the character of the community. These areas must be protected for future agricultural use so that they can continue to serve an important role in the local economy. The fragmentation of agricultural areas and the introduction of incompatible uses in these areas are strongly discouraged by the Official Plan.

The rural landscape provides a vital natural function as a source of oxygen, clean water and food and is considered an asset to the community. This Official Plan establishes the long-term role and function of farming and natural areas within the Town. It is the intent of the Town to only permit development that is compatible with the character, role and function of these areas. It is also the intent of this Plan to permit the continued functioning of natural systems, maintain the rural pattern of large land holdings and a landscape dominated by open fields and forests. The protection of the rural landscape is an important element of the permanent community structure.

The Town also recognizes that mineral aggregate extraction is a rural based activity and an important component of the local economy. Mineral aggregate extraction must be undertaken in a manner that minimizes social and environmental impacts. In keeping with the 'environment-first' philosophy, this Plan requires that where a mineral aggregate extraction operation impacts the Greenlands System, the application will result in a net environmental gain to the features or functions of the Greenlands System through a combination of compensation and enhancements that are initiated prior to and/or during extraction, and the proposed progressive and final rehabilitation of the site to uses that benefit the community.

The Town has three primary urban areas and a number of rural settlement areas of varying size. This Official Plan directs the majority of new residential and employment growth to the urban areas, where lands are available for development on full municipal services.

In keeping with the Places to Grow Act (2005), municipal official plans must be brought into conformity with the Growth Plan for the Greater Golden Horseshoe within three years of the final approval of that Plan (June 16, 2009). Between 2006 and 2009, the Region of Halton completed a multi-year work program, entitled Sustainable Halton, to achieve Growth Plan conformity, as well as conformity with the Greenbelt Plan and 2005 Provincial Policy Statement. This planning exercise culminated in the selection of a Preferred Growth Option in June 2009, and the adoption of Regional Official Plan Amendment No. 38 in December 2009, implementing the Preferred Growth Option, as well as a number of other policy changes arising from the multi-faceted work program.

The Preferred Growth Option in the context of Halton Hills consisted of 20,000 population growth, 370 hectares of residential/mixed use area contiguous to the Georgetown Urban Area, and 340 hectares of employment land contiguous to the Premier Gateway Employment Area to the 2031 planning horizon. In addition, a minimum intensification requirement of 5,100 units between 2015 and 2031 within the Built-up Areas of Acton and Georgetown was established. Through a series of amendments to the Halton Hills Official Plan, the Town has worked to achieve conformity with the Provincial Growth Plan and the Regional Official Plan, as amended by Amendment No. 38.

Encouraging additional economic development in the Town is also a key goal of this Official Plan. The establishment of a positive business environment that provides jobs and prosperity to Town residents is a key component of this Plan. On this basis, the Official Plan promotes the development of the newly emerging Premier Gateway Employment Area for prestige industrial uses and encourages general industrial uses within employment areas located in Acton and Georgetown. This Plan also encourages, through a flexible policy regime, opportunities for additional development in the urban areas, particularly in a *Community Node* designation that is focused at the intersection of Guelph Street and Mountainview Road in Georgetown and the Acton and Georgetown *Downtown Areas*.

The new Official Plan assumes that the high quality of life now enjoyed by the Town's residents can be maintained and enhanced if the Town's distinct urban and rural character is maintained and enhanced. However, change is inevitable and it must be managed in an efficient and orderly manner to maximize the benefits of new development and minimize the impacts. It is therefore the intent of this Plan to provide Council with the tools to consider and mitigate the impacts of change on the qualities that make the Town a desirable place to live.

Through a variety of successful initiatives, the Town is taking numerous steps towards achieving a sustainable community characterized by cultural, economic, environmental and social well-being. Sustainable development is an important component of achieving this objective. Sustainable development is often defined as development meeting the needs of the present generation, without compromising the ability of future generations to meet their own needs. The benefits of sustainable development include a reduction in the environmental footprint through a variety of measures that include reducing water, energy and land consumption, minimizing construction waste and contaminants, and improving air quality and the natural environment. Additional benefits include improved community design, support of active transportation, efficient infrastructure use, stormwater management and lower long-term building operating costs.

As part of achieving a sustainable community there has been recognition of the need for sustainable building and development practices to use fewer resources during the construction process, and to reduce the greenhouse gas emissions and long-term operating costs of buildings.

## A1A POPULATION AND EMPLOYMENT

This Plan provides a land use planning framework to direct and manage growth to the 2031 planning horizon based on the population and employment targets contained in Table A1A.

TABLE A1A: POPULATION AND EMPLOYMENT TARGETS

Year	2006	2031
Population	58,000	94,000
Employment	20,000	43,000

**Note:** Population numbers are Total Population including approximately 4% undercoverage from the official Census Population reported by Statistics Canada.

## A2 GOALS AND STRATEGIC OBJECTIVES

### A2.1 NATURAL HERITAGE AND WATER RESOURCES

#### A2.1.1 GOAL

To protect, enhance and where possible restore, significant natural heritage features and related ecological functions in the Town for present and future generations.

#### A2.1.2 STRATEGIC OBJECTIVES

- a) To protect significant natural heritage and hydrologic features and their associated ecological functions;
- b) To ensure that a comprehensive understanding of the natural environment, including the values, opportunities, limits and constraints that it provides, guides land use decision-making in the Town;
- c) To require that land use planning contributes to the protection, maintenance and enhancement of water and related resources and aquatic ecosystems on an integrated watershed management basis;
- d) To maintain and enhance significant woodland features and the habitats and ecological functions they provide;
- e) To recognize that a healthy community is made up of an interconnected system of open spaces and natural heritage features;
- f) To promote the retention of natural heritage features so that they can be enjoyed by future generations and serve as a legacy of the community's desire to protect their role and function;
- g) To increase awareness of the role natural heritage features play in establishing the character of the community;
- h) To minimize negative changes to the water quality and hydrological and hydrogeological characteristics of watercourses, lakes, aquifers and wetlands;
- i) To prohibit development that will result in a negative impact to the critical functions and processes of watercourses, lakes, aquifers and wetlands;
- j) To prohibit the loss or fragmentation of provincially significant wetlands and significant habitat of endangered and threatened species;
- k) To maintain and enhance significant areas of natural and scientific interest, significant valleylands, escarpment slopes and related landforms, and significant wildlife habitat areas;
- l) To maintain and enhance surface and groundwater resources in sufficient quality and quantity to meet existing and future needs on a sustainable basis;
- m) To promote land use decisions that incorporate water conservation measures and the efficient use of water resources on a watershed and sub-watershed basis;

- n) To encourage the establishment of a greenspace network that links environmental and recreational resources both within and beyond the boundaries of the Town as part of the development of a Natural Heritage Strategy;
- o) To encourage the development of a safe, convenient, affordable, efficient and energy-conserving transportation system that minimizes impacts on the natural environment;
- p) To encourage the municipal acquisition of valleylands and other Environmentally Sensitive Areas within the Town, particularly in the urban areas; and,
- q) To consult with the Region, the Conservation Authorities, the Town Environmental Advisory Committee and, where appropriate, neighbouring municipalities when making land use decisions that may impact on natural heritage features and related ecological functions.

## A2.2 GROWTH AND SETTLEMENT

### A2.2.1 GOAL

To direct most forms of development to urban areas where full wastewater and water services are available and to support the efficient use of land in these areas.

### A2.2.2 STRATEGIC OBJECTIVES

- a) To direct the majority of new residential and employment growth to Urban Areas, where full services exist;
- b) To permit a limited amount of development, in the form of infilling or plans of subdivision, within the Rural Clusters;
- c) To permit development within the Hamlets that maintains and enhances hamlet character and scale in accordance with the policies contained within this Plan and approved Secondary Plans;
- d) To ensure that new country residential subdivisions are not established outside of the *Hamlet Area* and *Rural Cluster Area* designations;
- e) To reinforce the function of the two downtown areas as cultural, administrative, entertainment, retail and social focal points of the community;
- f) To encourage the establishment of a new and enhanced community focus point at the intersection of Guelph Street and Mountainview Road in Georgetown;
- g) To encourage development and redevelopment in the Town's commercial corridors that incorporates excellence in urban design and complements the *Community Node* and the two *Downtown Areas*;
- h) To encourage the establishment of an optimum balance between residential and non-residential assessment in the Town; and,
- i) To ensure that all urban development is appropriately phased and in conjunction with required infrastructure improvements where appropriate.

## A2.3 URBAN CHARACTER

### A2.3.1 GOAL

To protect and enhance the character of existing urban areas and to maintain them as diverse, livable, safe, thriving and attractive communities.

### A2.3.2 STRATEGIC OBJECTIVES

- a) To encourage the further intensification and use of the lands within the *Downtown Areas*, as appropriate;
- b) To ensure that all new urban development has a positive contribution on urban life in the Town;
- c) To maintain and enhance the character and identity of existing residential areas;
- d) To ensure that all urban streets are defined by buildings and public spaces wherever possible and appropriate;
- e) To encourage a high quality of site and building design for all forms of development within the Town;
- f) To exercise appropriate municipal development control in order to achieve a consistently high standard of site, building and landscape design;
- g) To ensure that new development areas are integrated into the fabric of the existing community;
- h) To ensure that neighbourhoods are compact and pedestrian-friendly with a mix of housing types, community facilities, public schools, commercial centres and open spaces; and,
- i) To foster a sense of civic identity and pride through a high standard of urban design in all future developments.



## A2.4 RURAL CHARACTER

### A2.4.1 GOAL

To protect, maintain and enhance the open space character of lands outside of the Urban Areas and the *Hamlet Areas* for enjoyment by present and future generations.

### A2.4.2 STRATEGIC OBJECTIVES

- a) To preserve and enhance the Niagara Escarpment as a unique and important feature of the Town;
- b) To protect the open and natural character of the existing rural landscape;
- c) To require that aggregate extraction operations are designed to minimize visual impact;
- d) To restrict development that requires the expansion of urban services into the rural area; and,
- e) To discourage the intrusion of land uses that are incompatible with the rural character and/or resource activities of the area.

## A2.5 AGRICULTURE

### A2.5.1 GOAL

To preserve areas demonstrating high capability for agricultural production for that purpose.

### A2.5.2 STRATEGIC OBJECTIVES

- a) To ensure that agriculture remains an integral part of the economy of the Town and the Region;
- b) To ensure that non-agricultural uses which may have an impact on the viability of farm operations are not permitted;
- c) To prohibit the further fragmentation of good agricultural lands and encourage consolidation of farming parcels to improve efficiencies and productivity where possible;
- d) To encourage the maintenance of the character of agricultural areas by maintaining farm buildings and other elements of the built and natural landscapes that contribute to that character;
- e) To encourage the establishment of small-scale secondary uses on farm properties to improve the livelihood of area farmers;
- f) To encourage the establishment of uses and activities on farm properties that highlights the importance of agriculture and its history in the Town;
- g) To preserve the farm community as an important part of the Town's rural fabric;
- h) To work with all levels of government and the agricultural community to promote and enhance the agricultural industry through marketing;
- i) To encourage the participation of the agricultural industry and community in dealing with concerns of an agricultural nature; and,
- j) To promote environmentally sensitive and sustainable farm practices.

## A2.6 CULTURAL HERITAGE

### A2.6.1 GOAL

To identify, conserve and enhance the Town's cultural heritage resources and promote their value and benefit to the community.

### A2.6.2 STRATEGIC OBJECTIVES

- a) To enhance the character of the Town by protecting and maintaining the Town's cultural heritage resources;
- b) To encourage the retention of cultural heritage resources wherever possible to provide continuity between the past and the present;
- c) To foster civic pride by recognizing the contribution that cultural heritage resources make to the rural and urban fabric of the Town;
- d) To use the preservation of cultural heritage resources as a tool to enhance the character and vitality of neighbourhoods and districts;
- e) To educate the public on the Town's history and achievements;
- f) To ensure that the nature and location of cultural heritage and archaeological resources are known and considered before land use decisions are made;
- g) To pursue the designation, or other means of protection, under the Ontario Heritage Act, of significant cultural heritage resources;
- h) To prevent the demolition, destruction, inappropriate alteration or use of designated cultural heritage resources;
- i) To promote the conservation of significant cultural heritage resources on lands that are adjacent to proposed development and to encourage the adjacent development to be of an appropriate scale and character;
- j) To continue to develop a comprehensive inventory of the Town's built heritage and cultural heritage landscape resources; and,
- k) To consult with Heritage Halton Hills and other established heritage organizations when making decisions regarding the conservation of cultural heritage resources in the Town.

## A2.7 ECONOMIC DEVELOPMENT

### A2.7.1 GOAL

To provide opportunities for economic development in a manner that fosters competitiveness and a positive business environment.

### A2.7.2 STRATEGIC OBJECTIVES

- a) To ensure that the quality and character of life in the Town is sustained and improved in context of the Greater Toronto Area and the global economy;
- b) To facilitate opportunities to provide a range of goods and services to the public within Halton Hills through the use of a flexible policy regime;
- c) To facilitate the establishment of a competitive business environment that is able to easily adapt to changing circumstances and priorities;
- d) To encourage the development of the business infrastructure required to attract uses that will contribute to the quality of life in the Town;
- e) To promote the establishment of more live-work relationships that reduce commuting;
- f) To establish, maintain and enhance employment areas that provide a range of job opportunities and a broad range of commercial and service facilities that meet the needs of residents of the Town and the wider area;
- g) To encourage wherever possible through the land use planning process the retention and expansion of existing businesses in the Town;
- h) To ensure that a sufficient supply of serviced employment generating lands is available for development at all times;
- i) To carefully monitor local and regional trends with respect to the supply of land for employment to ensure that an adequate supply in appropriate locations is available at all times;
- j) To encourage further industrial development in the Acton Urban Area on the basis of full or partial services, as appropriate;
- k) To protect lands that have the potential of being used for agricultural purposes from incompatible development to ensure that farming operations can operate with the maximum degree of flexibility and efficiency;
- l) To encourage the development of home-based businesses provided the proposed use is compatible with adjacent uses;
- m) To encourage the protection of the Town's natural attributes, such as its rural character and its natural heritage features in order to maintain and enhance recreational and tourism opportunities that rely upon these attributes;

- n) To encourage the protection of the Town's cultural heritage resources in order to maintain and enhance economic development and tourism opportunities;
- o) To protect and recognize the Town's mineral aggregate resource industry as an important component of its economic base;
- p) To encourage the continued revitalization of the *Downtown Areas*, which reflects their heritage significance and promote a mix of uses and attractions for community activities both in the commercial core and in immediately adjacent areas; and,
- q) To improve the aesthetic quality of the retail corridors in the Town to ensure that they function as attractive destinations for shoppers from the Town and the surrounding area.

## A2.8 INFRASTRUCTURE

### A2.8.1 GOAL

To provide infrastructure that meets the needs of present and future residents and businesses in an efficient, environmentally-sensitive, cost effective and timely manner.

### A2.8.2 STRATEGIC OBJECTIVES

- a) To consider the economics of providing services to the people of the Town as part of the review of any development proposal to ensure that the development pattern is efficient and does not lead to a decline in the level of municipal service;
- b) To ensure that all necessary infrastructure required to serve the urban area is built as necessary prior to, or coincident with, urban development;
- c) To establish an integrated transportation system that safely and efficiently accommodates various modes of transportation including trains, automobiles, trucks, public transit, cycling and walking;
- d) To establish a street pattern within new development areas that is based on a permeable grid pattern of arterial, collector and local roads;
- e) To ensure that the construction of all infrastructure, or expansions to existing infrastructure, occurs in a manner that is compatible with adjacent land uses and with a minimum of social and environmental impact;
- f) To encourage the provision of infrastructure that is energy efficient, promotes water conservation and water efficiencies, and supports improvements to air quality; and,
- g) To ensure that public school facilities and other community infrastructure are provided in both older and newer neighbourhoods and that these facilities are available for all current and future residents of the Town.

## A2.9 HOUSING

### A2.9.1 GOAL

To provide an adequate housing supply and range of housing choices to meet the needs of present and future residents.

### A2.9.2 STRATEGIC OBJECTIVES

- a) To ensure that there is an adequate supply of land for residential development subject to the availability of water and wastewater capacity;
- b) To assist in the achievement of residential intensification and affordable housing by encouraging opportunities for mixed-use development in appropriate locations;
- c) To encourage the use of surplus public lands for affordable housing only if the site is appropriate for such a use and located where the use would be compatible with adjacent uses;
- d) To ensure that a full range of housing opportunities are available for residents in the Town in accordance with the Town's Municipal Housing Statement;
- e) To encourage the development of seniors housing in the Town;
- f) To carefully control the conversion of rental housing to ownership housing to ensure that a viable amount of rental housing continues to be available;
- g) To encourage the provision of additional rental housing, within the financial capabilities of the Town, through the use of incentives to the private sector and the entering into of public/private partnerships;
- h) To support the Region in its responsibilities as the provider and manager of assisted housing;
- i) To participate in Regional, Provincial and/or Federal housing programs that support appropriate housing development in the Town;
- j) To support the provision of an adequate supply of residential care facilities and emergency housing, subject to appropriate senior government level funding, the provision of adequate community services for future residents of these facilities, and proper land use planning principles; and,
- k) To support universal physical access and encourage the building industry to incorporate such features in new residential structures.

## A2.10 MINERAL AGGREGATE RESOURCES

### A2.10.1 GOAL

To protect mineral aggregate resource areas for long term use while ensuring that extraction occurs in a manner that minimizes environmental and social impacts.

### A2.10.2 STRATEGIC OBJECTIVES

- a) To ensure that as much of the mineral aggregate resources in the Town as is realistically possible shall be made available to supply mineral aggregate needs as close to markets as possible provided the extraction can occur in a manner that minimizes environmental and social impacts;
- b) To ensure the proper management of mineral aggregate operations to minimize potential negative environmental and social impacts;
- c) To protect areas having high potential aggregate resource deposits from incompatible development that would preclude or hinder access to the resource;
- d) To ensure that all operations are progressively rehabilitated in an environmentally responsible fashion;
- e) To ensure that roads are appropriate and built to a standard that can accommodate truck traffic associated with mineral aggregate extraction operations;
- f) To foster a strategic partnership with all the approval agencies to ensure that the objectives of this Plan can be met;
- g) To recognize existing mineral aggregate operations and protect them from activities that would preclude or hinder their expansion or continued use; and,
- h) To promote the conservation of mineral aggregate resources through the recovery of these resources wherever feasible.



## A2.11 SUSTAINABLE COMMUNITY DEVELOPMENT

### A2.11.1 GOAL

To promote community development in a manner that is sustainable for present and future generations.

### A2.11.2 STRATEGIC OBJECTIVES

- a) To develop an energy efficient mix of land uses in urban areas by integrating land use planning with energy conservation practices that take into account community layout, building types and densities, mix of uses and other factors that contribute to creating efficient, vibrant, compact, complete and healthy communities;
- b) To reduce the consumption of energy, water and land, and non-renewable resources;
- c) To promote sustainable site and building design and construction techniques in new development that reduce energy and water consumption, improve air and water quality, encourage alternative modes of transportation, provide for enhanced natural environment conditions, and improve waste management; and,
- d) To promote a total and per capita reduction in energy and water consumption in all sectors by encouraging retrofitting of existing buildings and facilities.

### A3 LAND USE CONCEPT

The land use designations in this Plan are divided into three broad categories. The Environmental and Open Space Area applies to lands throughout the Town that are considered to be significant from a natural heritage and/or open space perspective. The Urban Area applies to the three primary urban areas in the Town - Georgetown, Acton and the Premier Gateway Employment Area, as well as the Mansewood employment area at the northeast corner of Regional Road 25 and 5 Side Road. The Agricultural/Rural Area applies to lands outside of the Urban Area excluding the Environmental and Open Space Area and includes lands that are subject to the Greenbelt Plan.

The land use designations in each category are intended to implement the Vision, Goals and Strategic Objectives of this Plan.

#### A3.1 ENVIRONMENTAL AND OPEN SPACE AREA

The land use designations within the Environmental and Open Space Area are described below:

##### A3.1.1 GREENLANDS SYSTEM

The Greenlands System is where the majority of the natural heritage features within the Town are located. Designations within this classification include *Greenlands A*, *Greenlands B*, *Greenbelt Greenlands*, *Escarpment Natural Area*, and a single-tier *Greenlands* designation within the Urban Areas of Georgetown, Acton and the Premier Gateway Employment Area.

##### A3.1.2 MAJOR PARKS AND OPEN SPACE AREA

This designation applies to lands, which are the site of large land holdings owned by a public authority that are used, or have the potential to be used for passive or active recreational uses.

##### A3.1.3 PRIVATE OPEN SPACE AREA

This designation applies to lands in private ownership that are maintained in a natural state or accommodate manicured open space uses such as golf courses and private cemeteries.

##### A3.1.4 NIAGARA ESCARPMENT PLAN AREA

This area, which bi-sects the Town in a north-south direction, is the subject of the Provincially-approved Niagara Escarpment Plan. This Plan applies three primary land use designations to the area consisting of *Escarpment Natural Area*, *Escarpment Protection Area* and *Escarpment Rural Area*. Lands within the *Escarpment Protection Area* and *Escarpment Rural Area* designations below the escarpment brow are considered by this Plan to be part of the Town's Prime *Agricultural Area*. Pits and quarry operations licensed under the Aggregate Resources Act are designated by the Niagara Escarpment Plan as *Mineral Resource Extraction Areas*. In addition, the Rural Clusters of Henderson's Corners, Limehouse and Silvercreek are designated as Minor Urban Centres by the Niagara Escarpment Plan. The policies in this Official Plan comply with those in the Niagara Escarpment Plan.

## A3.2 URBAN AREA

The land use designations within the Urban Area are described below:

### A3.2.1 LIVING AREA

This designation applies to lands that are primarily used for residential purposes in the urban areas of Acton and Georgetown. A number of sub-designations, including *Low Density Residential Area*, *Medium Density Residential Area* and *High Density Residential Area* are applied as appropriate to all of the lands that comprise the Living Area of the Town of Halton Hills.

### A3.2.2 COMMUNITY AREA

This designation applies to lands that serve primarily as community focal points in Acton and Georgetown. The following sub-designations are included within this designation: *Downtown Area*, *Community Node*, *Secondary Node*, *Corridor Commercial Area*, *Local Commercial Area*, *Major Institutional Area* and *Civic Centre Area*.

### A3.2.3 EMPLOYMENT AREA

This designation applies to lands that are primarily used for industrial uses that are on or eligible for full municipal services within Acton, Georgetown, the Premier Gateway Employment Area, and the Mansewood area.

### A3.2.4 SPECIAL STUDY AREA

This designation applies to undeveloped lands located within the Urban Area for which a specific land use has yet to be determined. On this basis, special policies have been established to ensure consideration of a range of land use options prior to development occurring.

## A3.3 AGRICULTURAL/RURAL AREA

The land use designations within the Agricultural/Rural Area category are described below:

### A3.3.1 AGRICULTURAL AREA

This designation applies to lands that are predominantly utilized for agricultural purposes and which have an agricultural character. The *Agricultural Area* primarily consists of lands that are classified as Class 1, 2 or 3 soils according to the Canada Land Inventory. The lands within this designation are considered by this Plan to form a major component of the Town's Prime *Agricultural Area*.

### A3.3.2 PROTECTED COUNTRYSIDE AREA

This designation applies to lands that are within the Greenbelt Plan established by the Province of Ontario. Lands within the *Protected Countryside Area* designation that are located south and east of the *Niagara Escarpment Plan Area* are considered by this Plan to be part of the Town's Prime *Agricultural Area*. Much of the land within the *Protected Countryside Area* designation is within the Natural System, which is an overlay designation, identified by the Province of Ontario in the Greenbelt Plan. The Natural System is comprised of the Natural Heritage System and the Water Resources System. Within the Natural System are lands that are identified as *Greenbelt Greenlands* on the Schedules.

#### A3.3.3

#### HAMLET AREA

This designation applies to the Hamlets of Glen Williams and Norval.

#### A3.3.4

#### RURAL CLUSTER AREA

This designation applies to the Rural Clusters of Ashgrove, Ballinafad, Bannockburn, Crewsons Corners, Henderson's Corners, Hornby, Limehouse, Silvercreek and Terra Cotta.

#### A3.3.5

#### COUNTRY RESIDENTIAL AREA

This designation only applies to lands that have been developed, Draft Approved or approved through a site-specific Official Plan Amendment prior to the adoption of this Plan for Country Residential development.

#### A3.3.6

#### MINERAL RESOURCE EXTRACTION AREA

This designation only applies to lands that are currently licensed for aggregate extraction by the Ministry of Natural Resources. Once the license has been surrendered or revoked in accordance with the Aggregate Resources Act, the Town will take the necessary steps to redesignate the subject lands from the Mineral Resource Extraction Area designation to another appropriate land use designation.

#### A3.3.7

#### MAJOR INSTITUTIONAL AREA

This designation only applies to one area of land in the broader Agricultural/Rural Area that is the site of a major institutional complex that undertakes educational/religious instruction and produces, stores and distributes literature and programs along with accessory uses including a place of worship and residential accommodation for employees.

## B1

### GREENLANDS SYSTEM

The Town's Greenlands System represents a network of natural areas that continues to exist after the area was settled and the lands and waters that support the ecological functions critical to the survival of these areas. This Plan is based on the recognition that a healthy Natural Heritage System is an important attribute of the community's wellbeing and makes a positive contribution to the quality of life enjoyed by Town residents and business owners.

In order to ensure that the tools are available to maintain, improve and potentially restore elements of the ecological features and functions of the Natural Heritage System, this Plan establishes a Greenlands System in accordance with the requirements of the Region of Halton Official Plan. The intent of the Greenlands System is to maintain, as a permanent landform, an interconnected system of natural and open space areas that will preserve areas of significant ecological value while providing, where appropriate, some opportunities for recreation.

The Greenlands System in the Town of Halton Hills includes the following designations:

- *Greenlands A*;
- *Greenlands B*;
- *Escarpment Natural Area*;
- *Greenbelt Greenlands*;
- *Greenlands* within certain Special Policy Areas listed in Section B1.3.7 of this Plan; and,
- *Greenlands* within the Premier Gateway Employment Area and the Georgetown and Acton Urban Areas.
- The policies of the *Greenlands A* and *Greenlands B* designations are contained in this section of the Plan.

## B1A

### GREENLANDS WITHIN EXISTING URBAN AREAS

A single tier *Greenlands* designation in the existing Acton and Georgetown Urban Areas, as shown on Schedules A3, A6, and all other schedules applicable to lands within these Urban Areas of this plan, and Phases 1A and 2A of the Premier Gateway Employment Area as shown on Schedule A8 of this Plan, implements the Regional Natural Heritage System contained in the Regional Official Plan.

Notwithstanding the policies of Section B1.2 of this Plan, the *Greenlands* designation is subject to the policies contained in Sections 115.2, 115.3, 115.4(2), 116, 116.1, 117.1, 118(1.1), 118(2), 118(3), 118(3.1), 118(4), 118(5) through 118(13), 139.11, 139.12, Map 1G, and the applicable definitions of the Regional Official Plan, which are hereby incorporated by reference into this Plan.

## B1B

### GREENLANDS WITHIN URBAN AREAS TO 2031

A single tier *Greenlands* designation within Phases 1B and 2B of the Premier Gateway Employment Area shown on Schedule A8 of this Plan, and the *Future Residential/Mixed Use Area* of the Georgetown Urban Area shown on Schedule A3 of this Plan, implements the Regional Natural Heritage System contained in the Regional Official Plan. This designation is subject to Sections D3.5.4.4.4, D6.3.4, and D6.4.4.4.4 of this Plan.

## B1C

### GREENLANDS IN THE GREENBELT PLAN AREA

An additional single tier *Greenbelt Greenlands* designation, comprised of Key Natural Heritage Features and Key Hydrologic Features, is contained within the Natural System identified by the Province in the Greenbelt Plan and is subject to Section E2 of this Plan. The policies applying to the *Escarpment Natural Area* designation are contained within Section B4 of this Plan. Environmental management policies that complement the policies of this section of the Plan are contained within Section C of this Plan.

## B1.1

### OBJECTIVES

It is the objective of this Plan to:

- a) establish a balanced relationship between development and nature by preserving natural features and ecological systems and protecting people and property from environmental hazards;
- b) maintain, improve and where possible, restore the health, diversity, size and connectivity of natural heritage features, hydrologically sensitive features and related ecological functions;
- c) maintain natural stream form and flow characteristics and the integrity and quality of watercourses;
- d) ensure that only land uses that maintain, improve or restore the ecological and hydrological functions of the Greenlands System are permitted;
- e) ensure that the Greenlands System is maintained as a series of inter-connected natural features for the benefit of present and future generations;
- f) encourage the acquisition of land that is the site of significant natural heritage features by public authorities for conservation purposes;
- g) provide the tools to properly assess development applications located in close proximity to natural heritage features;
- h) ensure that development is appropriately setback from significant natural heritage features;
- i) eliminate the potential for the loss or fragmentation of significant wetlands and the habitats and ecological functions they provide;
- j) encourage and support the Ministry of Natural Resources and the appropriate Conservation Authority in conducting evaluations of unevaluated wetlands within the Town;
- k) provide opportunities, where appropriate, for passive outdoor recreational activities;
- l) protect significant scenic and cultural heritage resources; and,
- m) implement the 'Environment-First' philosophy of this Plan.

## B1.2

### GENERAL POLICIES APPLYING TO THE GREENLANDS SYSTEM

### B1.2.1

#### GREENLANDS RECONFIGURATION

Council may utilize a systems approach in identifying and protecting *Greenlands A* and *B* within a Secondary Plan area by using criteria other than those under Sections B1.3.1 and B1.3.2 of this Plan provided that:

- a) the Region is consulted throughout the process;
- b) the public and affected agencies are consulted on the proposed process in identifying the alternative *Greenlands A* and *B*;
- c) the alternative is based on the policies contained in the Regional Official Plan and this Plan pertaining to the Greenlands System and utilizes a systems approach that is based on a detailed and comprehensive analysis carried out as part of a Subwatershed Study covering at least the Secondary Plan Area;
- d) it is demonstrated to the satisfaction of the Region that the alternative *Greenlands A* and *B*:
  - i) will result in no overall negative impact on the ecological functions of the Greenlands System based on *Greenlands A* and *B* as defined through Sections B1.3.1 and B1.3.2 of this Plan;
  - ii) will enhance the Greenlands System by incorporating linkages, buffers, and other features that would improve the overall function and sustainability of the System.
- e) the alternative is in keeping with the 'environment-first' philosophy, and the intent of the Goals and Strategic Objectives pertaining to natural heritage, contained within this Plan;
- f) the redefined *Greenlands A* and *B* are introduced into this Plan by an amendment that is in conformity with the Regional Official Plan to the satisfaction of the Region; and,
- g) once the Amendment to this Plan is approved, the alternative *Greenlands A* and *B* are deemed to be consistent with the Regional Official Plan and have the effect of replacing the designations of *Greenlands A* and *B* within the affected area on Map 1 to the Regional Official Plan.

#### B1.2.2 USE OF LANDS IN PRIVATE OWNERSHIP

Where any land within the Greenlands System is held under private ownership, this Plan shall not be construed as implying that such areas are free and open to the general public.

#### B1.2.3 TRANSFER OF GREENLANDS INTO PUBLIC OWNERSHIP

Council shall work with the Region and the appropriate conservation authorities and other public agencies, such as the Province, to develop and implement a land securement strategy that would result in the transfer of lands within the Greenlands System into public ownership. However, given the financial limitations of every level of government and consistent with the principle of land stewardship, this policy does not imply that all lands within the Greenlands System will be purchased by the Town or other public agency.

Council shall consider every opportunity to obtain the dedication of lands within the Greenlands System through the development approval process. However, such dedications will not be considered as part of

the required parkland dedication set out by the Planning Act and Section F7.2.6 (Dedication of Land Through the Development Process) of this Plan.

Where appropriate, Council shall also work with the Region and the appropriate Conservation Authority to encourage the use of conservation easements to protect lands within the Greenlands System.

#### B1.2.4 ADJACENT LANDS

Adjacent lands are the lands adjacent to a natural heritage feature within which potential impacts of a development proposal must be considered. For the purposes of this Official Plan, adjacent lands are defined as all lands partially or wholly within:

- 120 metres of the boundary of a provincially significant wetland;
- 50 metres of the boundary of a regionally significant wetland;
- 50 metres of the boundary of an Environmentally Sensitive Area;
- 50 metres of the boundary of a provincially or regionally significant area of natural and scientific interest;
- 50 metres of the significant habitat of an endangered or threatened species;
- 50 metres of the boundary of a significant woodland;
- 50 metres of the boundary of a significant valleyland;
- 50 metres of the boundary of an area of fish habitat; and,
- 50 metres of the boundary of an area of significant wildlife habitat.

Within the Natural System of the Greenbelt Plan, adjacent lands are lands within 120 metres of Key Natural Heritage Features and Key Hydrologic Features, and the applicable policies are contained in Section E2.5.3.3 (d) of this Plan.

It is recognized that not all of the above natural heritage features have been identified by the Region and/or the appropriate Conservation Authority at the date of the adoption of this Plan and therefore they are not shown on the Schedules to this Plan. The incorporation of additional natural heritage features shall be in accordance with Section B1.3.4 (Incorporation of Additional Lands into the Greenlands System) and Section B1.3.5 (Significant Woodlands) of this Plan.

No development shall be permitted on adjacent lands unless an Environmental Impact Study and/or a Subwatershed study and/or a Geotechnical study is completed and approved by Council, subject to the comments of the appropriate agencies. The requirements for an Environmental Impact Study are contained in Section C2 (Environmental Impact Studies) of this Plan. Provisions relating to watershed and subwatershed studies are contained within Section C7 (Watershed Planning) of this Plan.

The scale and the contents of the required studies shall be determined at the time the development is proposed. The width of the adjacent lands may be increased/decreased, depending on the feature and the nature of the proposed development. This determination shall be made in consultation with the appropriate agencies at the time the development is proposed.



#### B1.2.5 SIGNIFICANT NATURAL FEATURES

It is the policy of this Plan to:

- a) prohibit development within significant wetlands and significant habitat of endangered and threatened species; and,
- b) prohibit development within other significant natural heritage features within the Greenlands System unless it is demonstrated through an EIS approved by the Town and the Region in consultation with the appropriate Conservation Authority, that there will be no negative impacts on the feature or their related ecological function.

In keeping with the 'environment-first' philosophy enshrined in this Plan, reductions to the spatial extent and ecological function of significant natural heritage features are not contemplated by this Plan. Notwithstanding the foregoing, the boundaries of lands within the Greenlands System can be refined through the development process without an Official Plan Amendment subject to the approval of the Town and the Region in consultation with the appropriate Conservation Authority and adjacent municipalities where appropriate, of an Environmental Impact Study in accordance with Section C2 (Environmental Impact Studies) of this Plan. Any such refinement shall be in keeping with the intent of this Plan including the environment-first philosophy that is enshrined within this Plan.

#### B1.2.6 INTEGRATION OF GREENLANDS AND PARKLAND

In order to enhance the function of the Greenlands System located within the Urban Area, the Town shall seek to locate local and non-local parkland adjacent to or near the Greenlands System as identified on the Schedules to this Plan.

#### B1.2.7 TRAILS

The development of trails within the Greenlands System shall be encouraged provided that:

- a) the trails are located on publicly owned lands or are part of the Bruce Trail;
- b) the trails and associated activities do not impact negatively on natural heritage features and ecological functions or resource uses such as agricultural operations as demonstrated through the preparation of Master Plans approved by Council in consultation with the Region and the appropriate Conservation Authority;
- c) proper consideration is given to issues of trespassing on private properties and liability in the event of property damages or personal injuries; and,
- d) adjacent landowners potentially affected by the trails are consulted.

#### B1.2.8 ZONING BY-LAW IMPLEMENTATION

The boundaries of the *Greenlands A* and *B* designations are delineated on the Schedules to this Plan. The boundaries are intended to be more precisely delineated in the implementing Zoning By-law in accordance with detailed mapping provided by the appropriate Conservation Authorities and Provincial agencies, and will not require an amendment to this Plan. Such lands will be placed in appropriate environmental zones in the implementing Zoning By-law.

The zoning by-law shall also incorporate general setbacks for lot lines, buildings, structures, parking areas and other similar facilities from lands within the Greenlands System in relation to the extent and sensitivity of the natural heritage features and ecological functions of the area. The Town and the Region of Halton in consultation with the appropriate Conservation Authority will evaluate this general setback for specific development applications, and as established in the Zoning By-law, to provide the appropriate setback required to protect the area from the impacts of development. A greater setback may be required to reflect specific circumstances.

### B1.3 DESIGNATIONS

#### B1.3.1 GREENLANDS A

##### B1.3.1.1 Location

The *Greenlands A* designation is intended to include the following components of the Natural Heritage System:

- a) areas included within Regulatory Floodplains, as determined by the appropriate Conservation Authority, and as amended from time to time;
- b) significant habitat of endangered or threatened species, as determined by the Ministry of Natural Resources, and refined from time to time;
- c) provincially significant wetlands, as identified by the Ministry of Natural Resources, and refined from time to time; and
- d) areas defined through Section B1.2.1 of this Plan.

Lands designated *Greenlands A* are shown on the Schedules to this Plan. The location of provincially significant wetlands are shown on Appendix X1A to this Plan.

##### B1.3.1.2 Permitted Uses

The following uses may be permitted in accordance with the objectives set out in Section B1.1 of this Plan, other policies of this Plan and Conservation Authority Regulations:

- a) existing agricultural operations;
- b) passive non-motorized recreational uses, such as nature viewing and pedestrian trail activities, only on publicly owned lands or on the Bruce Trail;
- c) forest, wildlife and fisheries management;
- d) archaeological activities;
- e) where applicable, uses permitted in an approved Niagara Escarpment Park and Open Space Plan;
- f) essential transportation and utility facilities; and,

- g) essential watershed management and flood and erosion control projects carried out or supervised by a public authority.

### B1.3.2 GREENLANDS B

#### B1.3.2.1 Location

The *Greenlands B* designation is intended to include the following components of the Natural Heritage System in accordance with the objectives set out in Section B1.1 to this Plan:

- a) Environmentally Sensitive Areas, outside of *Escarpment Natural Area* or *Greenlands A*, as identified by the Region of Halton and refined from time to time;
- b) regionally significant wetlands, as determined by the Ministry of Natural Resources, and refined from time to time;
- c) Provincially and Regionally Significant Areas of Natural and Scientific Interest, as determined by the Ministry of Natural Resources, and refined from time to time;
- d) fish habitat;
- e) significant woodlands in accordance with Section B1.3.5 of this Plan;
- f) significant valleylands;
- g) significant wildlife habitat areas;
- h) Carolinian Canada sites;
- i) Halton Regional Forests; and,
- j) areas defined through Section B1.2.1 of this Plan.

Lands designated *Greenlands B* are shown on the Schedules to this Plan. Some of the various types of natural heritage areas that comprise *Greenlands B* are shown on Appendix X1A to this Plan.

#### B1.3.2.2 Permitted Uses

The following uses may be permitted, in accordance with the objectives set out in Section B1.1 of this Plan, other policies of this Plan and Conservation Authority Regulations:

- a) existing agricultural operations;
- b) single detached dwellings on existing lots;
- c) home occupations and cottage industries subject to Section E1.4.4 of this Plan;
- d) passive non-motorized recreational uses, such as nature viewing and pedestrian trail activities, only on publicly owned lands or on the Bruce Trail;
- e) forest, wildlife and fisheries management;

- f) watershed management and flood and erosion control projects carried out or supervised by a public agency;
- g) archaeological activities;
- h) transportation and utility facilities;
- i) small-scale public uses above the escarpment brow; and,
- j) where applicable, uses permitted in an approved Niagara Escarpment Park and Open Space Plan.

### **B1.3.3 LANDS PARTIALLY WITHIN GREENLANDS B IN SETTLEMENT AREAS**

It is the policy of this Plan to direct development in Urban Areas, *Hamlet Areas* or *Rural Cluster Areas* away from *Greenlands B* areas on those parcels of land that are partially within the *Greenlands B* designation.

### **B1.3.4 INCORPORATION OF ADDITIONAL LANDS INTO THE GREENLANDS SYSTEM**

Not all of the natural heritage features identified under *Greenlands B* have been incorporated on the Schedules at the date of the adoption of this Plan. As new information regarding natural heritage features becomes available through Watershed Management Plans, Subwatershed studies or a comprehensive Natural Heritage Strategy, additional lands shall be incorporated into the *Greenlands* designation as part of the five-year review of the Official Plan required by the Planning Act or a comprehensive Secondary Plan exercise in accordance with Section G3.1 of this Plan. More specific policies pertaining to the identification of significant woodlands are referenced in Section B1.3.5.

### **B1.3.5 SIGNIFICANT WOODLANDS**

All woodlands 0.5 hectares or larger, have been identified by the Region to be an important natural heritage feature and candidates for assessment as significant woodlands. It is the policy of this Plan to:

- a) Identify and show on the Urban Land Use Schedules to this Plan, significant woodlands as a component of *Greenlands B* through Watershed Management Plans, Subwatershed Studies, or individual site specific Environmental Impact Studies; and,
- b) Require the submission and approval of an Environmental Impact Study that identifies or refines the boundaries of significant woodlands and implements measures to protect such significant woodlands for any development proposal, other than individual consents or uses conforming to this Plan and Zoning By-laws, located wholly or partially within 50 metres of woodlands 0.5 hectares or larger.

### **B1.3.6 ENVIRONMENTALLY SENSITIVE AREAS**

Environmentally Sensitive Areas (ESAs) are land and water areas within the Greenlands System containing natural features or ecological functions of such significance as to warrant their protection. An area must meet at least one of the primary criteria set out in the Regional Official Plan to be classified as an ESA. The permitted uses in ESAs are governed by the underlying land use designations shown on the

Schedules of this Plan. The precise boundaries of ESAs are to be established through an Environmental Impact Study. The requirements for an Environmental Impact Study are set out in Section C2 (Requirements for an Environmental Impact Study) of this Plan. The general boundaries of ESAs are shown in Appendix X1A to this Plan.

#### B1.3.7 SPECIAL POLICY AREAS

Special Policy Areas apply to those lands within the Greenlands System that are the subject to a land use policy specific to a property or area. All other relevant policies of this Plan are applicable unless otherwise modified or exempted by the provisions applicable to each Special Policy Area.

##### B1.3.7.1 Special Policy Area 1

The wetland features referred to as W7 and W8, which are designated as *Greenlands A* and *V2*, which is designated as *Escarpment Rural Area*, located in Part of Lot 13, Concession 1 (Esquesing) and their associated buffer area, as shown on Schedule A2 of this Plan, which are identified in the site plan as a setback, shall be excluded from the extraction area in the license to be issued by the Ministry of Natural Resources under the Aggregate Resources Act. Setbacks shall be established around the three (3) wetlands to act as a buffer area between the extraction areas and the wetlands. The setbacks surrounding the wetlands shall be no less than 15 metres for the W7 and W8 wetlands and no less than 25 metres for the V2 wetland.

##### B1.3.7.2 Special Policy Area 2

Notwithstanding the policies of Section B1.2 of this Plan, and notwithstanding any provisions of Section E2.5 of this Plan to the contrary, the single-tier Greenlands designation contained within Special Policy Area 2, as shown on Schedule A2 to this Plan, is subject to the Natural Heritage System policies contained in Sections 115.2, 115.3, 115.4, 116, 116.1, 117.1, 118(1.1), 118(2), 118(3), 118(3.1), 118(4), 118(6) through 118(9), 139.3.6, 139.11, 139.12, Map 1G and the applicable definitions of the Regional Official Plan which are hereby incorporated by reference into this Plan.

In addition, the following uses may be permitted within the single-tier Greenlands designation:

- a) Development related to the installation of, access to, facilities and servicing for, the water management and monitoring system associated with the quarry located in the Mineral Resource Extraction Area, and,
- b) On lands forming the setback for the Mineral Resource Extraction Area only; berming, screening, temporary stockpiling of earthen material, accessory structures and facilities normally associated with a mineral extraction operation, and facilities and servicing for, and related to the water management and monitoring system and associated infrastructure for the quarry extension, which may be in the license area approved under the Aggregate Resources Act. Extraction shall be prohibited in this area.

##### B1.3.7.5 Special Policy Area 5

Notwithstanding the policies of Section B1.2 of this Plan, the single-tier Greenlands designation contained within Special Policy Area 5, as shown on Schedule A3 to this Plan, is subject to the Regional Natural Heritage System Policies contained in Sections 115.2, 115.3, 115.4(2), 116, 116.1, 117.1, 118(1.1), 118(2), 118(3), 118(3.1), 118(4), 118(5) through 118(13), 139.11, 139.12, Map 1G, and the

applicable definitions of the Regional Official Plan which are hereby incorporated by reference into this Plan.

## B2 MAJOR PARKS AND OPEN SPACE AREA

### B2.1 OBJECTIVES

It is the objective of this designation to:

- a) ensure that the use and development of major parks and open space lands is consistent with the 'environment-first' philosophy of this Official Plan;
- b) ensure that all major parks and open space lands are used in a manner that reflects their capacity for public use;
- c) ensure that the impacts of the use of major parks and open space lands on adjacent land uses and the character of the surrounding neighbourhood are minimized; and,
- d) ensure that the residents of the Town have access to a properly planned and accessible public open space network.

### B2.2 LOCATION

The *Major Parks and Open Space Area* as shown on the Schedules to this Plan applies to Non-Local Parkland, including Community Parks and Town Wide Parks, which function as major active and/or passive recreational areas in the Town.

### B2.3 PERMITTED USES

Permitted uses in the *Major Parks and Open Space Area* are limited to passive and active recreational uses and accessory uses. Community facilities, such as public cemeteries and public community centres, infrastructure and utilities may also be permitted in the *Major Parks and Open Space Area* provided any adjacent natural features and functions are protected and enhanced and the scale of the use is compatible with the character of adjacent development. The development of new Community Parks and Town Wide Parks or expansions to existing facilities outside of the Urban Area, *Hamlet Area*, and *Rural Cluster Area* designations shall require an amendment to this Plan in accordance with the applicable criteria set out in Section E1.4.9 and E2.4.4 of this Plan.

### B2.4 LAND USE POLICIES

#### B2.4.1 COMPREHENSIVE PLANNING

Each parcel of land designated *Major Parks and Open Space Area* shall be planned to both accommodate public use and minimize the impacts of that public use on the environment and adjacent residential areas. Given the 'environment-first' philosophy of this Plan, the development of additional parking areas, community facilities and other infrastructure should be carried out in a manner that protects and enhances any adjacent natural heritage features and functions in accordance with the recommendations contained in an Environmental Impact Study.

The development of new *Major Parks and Open Space Area* shall be conditional upon the preparation of a Council approved Master Plan that assesses:

- a) the potential arrangement of uses, activities and facilities on the site;
- b) the preservation/incorporation of existing features;
- c) the adequacy and location of parking on the site;
- d) the need for public washroom and change room facilities in the area;
- e) the adequacy of the road network and site servicing in the area; and,
- f) the means by which pedestrian movement in the area can be accommodated in a safe manner through neighbourhood linkages, clear sightlines, visibility and lighting.

#### B2.4.2 PUBLIC CEMETERIES

Public cemeteries and accessory uses such as crematoriums, columbariums and mausoleums are permitted in the *Major Parks and Open Space Area* designation subject to the policies of this Section:

- a) Public cemeteries and related facilities shall be sited to minimize conflicts with adjacent existing and planned land uses and transportation systems; and,
- b) Proposals for new public cemeteries, or the enlargement of existing public cemeteries, may be considered, provided that:
  - i) the cemetery can be accessed by roads that are designed to accommodate high volumes of traffic in short periods of time;
  - ii) adequate off-street parking and an appropriate internal traffic circulation system are provided;
  - iii) the tree planting and landscaping on the site is designed to complement the plot plan, the existing contours and the use of abutting lands; and,
  - iv) appropriate hydrogeological studies have been completed which indicate that the public cemetery will not have a negative impact on the quality and quantity of groundwater and surface water in the surrounding area.

#### B2.5 ZONING BY-LAW IMPLEMENTATION

All lands in the *Major Parks and Open Space Area* designation shall be placed in an appropriate Open Space Zone in the implementing Zoning By-law.

#### B2.6 SPECIAL POLICY AREAS

Special Policy Areas apply to those lands designated for *Major Parks and Open Space Area* purposes that are subject to a land use policy specific to a property or area. All other relevant policies of this Plan are applicable unless otherwise modified or exempted by the provisions applicable to each Special Policy Area.



#### B2.6.1

#### SPECIAL POLICY AREA 1

The lands designated as *Major Parks and Open Space Area* and identified as Special Policy Area 1, as shown on Schedule A3 of this Plan, form part of the Credit River Valley trail/open space system serving the residents of the community. Notwithstanding this designation, the lands are classified as a Neighbourhood Park and are subject to the policies contained in Section F7.3.4.2 of this Plan.

## B3 PRIVATE OPEN SPACE AREA

### B3.1 OBJECTIVES

It is the objective of this designation:

- a) to protect and enhance woodlands and other private open space lands that are not incorporated within the Greenlands System identified by this Plan; and,
- b) to recognize existing private recreational uses such as golf courses that contribute to the open space character of the Town.

### B3.2 LOCATION

The *Private Open Space Area* as shown on the Schedules to this Plan applies to lands in private ownership that are maintained in a natural state or accommodate manicured open space uses such as golf course and private cemeteries.

### B3.3 PERMITTED USES

Permitted uses in the *Private Open Space Area* are limited to passive and active recreational uses including existing golf courses, private cemeteries, conservation uses and forestry uses in accordance with good management practices and accessory uses. The development of new golf courses and other recreational uses or expansions to existing facilities shall require an amendment to this Plan in accordance with the applicable criteria set out in E1.4.9 and E2.4.4 of this Plan. Residential dwelling units and overnight accommodation uses are not permitted within the *Private Open Space Area*.

### B3.4 LAND USE POLICIES

Where lands are designated *Private Open Space Area*, this Plan does not mandate that this land will remain as open space indefinitely, nor shall it be construed as implying that open space areas are free and open to the general public or will be purchased by the Town.

Where proposals are made to develop lands designated *Private Open Space Area* and the Town does not wish to purchase such lands in order to maintain the open space, Council may approve an application for the re-designation of such land for other purposes. In evaluating such applications, Council shall take into account:

- a) any existing environmental and/or physical hazards;
- b) the potential impact of these hazards;
- c) the proposed methods by which these impacts may be overcome in a manner consistent with accepted engineering techniques and resource management practices;
- d) any other applicable policies of this Plan; and,
- e) any new environmental information regarding the appropriateness of incorporating the lands within the Greenlands System.

### B3.5 ZONING BY-LAW IMPLEMENTATION

All lands in the *Private Open Space Area* designation shall be placed in an appropriate Open Space Zone in the implementing Zoning By-law.

### B3.6 SPECIAL POLICY AREAS

Special Policy Areas apply to those lands within the *Private Open Space Area* that are subject to a land use policy specific to a property or area. All other relevant policies of this Plan are applicable unless otherwise modified or exempted by the provisions applicable to each Special Policy Area.

#### B3.6.1 SPECIAL POLICY AREA 1

The following uses shall be permitted on lands designated as *Private Open Space Area* and identified as Special Policy Area 1, as shown on Schedule A2 of this Plan:

- a) agricultural uses;
- b) forestry; and,
- c) cemeteries and related accessory uses, excluding a crematorium.

The Niagara Escarpment bisects the Town of Halton Hills from the southwest to the northeast. It is easily considered the main topographical feature in the Town of Halton Hills.

According to the Niagara Escarpment Plan, "the combination of geological and ecological features along the Niagara Escarpment results in a landscape unequalled in Canada. It is also a source of some of southern Ontario's prime rivers and streams and one of the province's principal outdoor recreation areas." On February 8, 1990, the Bureau of the United Nations Educational, Scientific and Cultural Organisation (UNESCO) Man and Biosphere (MAB) program approved the designation of the Niagara Escarpment as a Biosphere Reserve.

It is the intent of this Plan to both implement and complement the policies of the Niagara Escarpment Plan as appropriate. On this basis, this Section of the Plan contains the following three primary designations, which relate to lands in the Town that are subject to the Niagara Escarpment Plan:

- *Escarpment Natural Area;*
- *Escarpment Protection Area; and,*
- *Escarpment Rural Area.*

This Plan provides direction on how lands within these three designations are to be used and managed. The policies of the Niagara Escarpment Plan should also be consulted to ensure that all applicable policies are considered. In the event of a conflict between this Plan and the Niagara Escarpment Plan, the latter shall prevail in accordance with Section 14 of the Niagara Escarpment Planning and Development Act. Policies in this Plan that are more restrictive or rigorous than those found in the Niagara Escarpment Plan are not considered to be in conflict with the Niagara Escarpment Plan and continue to apply.

Land use policies applying to lands designated *Mineral Resource Extraction Areas* within the Niagara Escarpment Area are contained within Section E6 (*Mineral Resource Extraction Area*) of this Plan. Land use policies applying to the Henderson's Corners, Silvercreek and Limehouse Rural Clusters, which are considered to be Minor Urban Centres by the Niagara Escarpment Plan, are contained within Section E4 (*Rural Cluster Area*) of this Plan.

Lands within the *Niagara Escarpment Plan Area* are also within the Greenbelt Plan established by the Province of Ontario.

#### B4.1

#### OBJECTIVES

It is the objective of this Plan to:

- a) protect unique ecologic and historic areas;
- b) maintain and enhance the quality and character of natural streams and water supplies;
- c) provide adequate opportunities for outdoor recreation;
- d) maintain and enhance the open landscape character of the Niagara Escarpment in so far as possible, by such means as compatible farming or forestry and by preserving the natural scenery;

- e) ensure that all new development conforms with the intent of the Niagara Escarpment Plan and this Plan;
- f) provide for adequate public access to the Niagara Escarpment; and,
- g) ensure that change is managed in accordance with the 'environment-first' philosophy established by this Official Plan.

## B4.2 LAND USE DESIGNATIONS

### B4.2.1 ESCARPMENT NATURAL AREA

#### B4.2.1.1 Objectives

It is the objective of this designation to:

- a) protect and enhance the most natural Escarpment features, stream valleys, wetlands and related significant natural areas and associated cultural heritage features;
- b) encourage compatible recreation, conservation and educational activities; and,
- c) maintain and enhance the landscape quality of Escarpment features.

#### B4.2.1.2 Location

The *Escarpment Natural Area* designation as shown on Schedule A2 to this Plan applies to lands that:

- a) are the site of Escarpment slopes and related landforms associated with the underlying bedrock that are in a relatively natural state;
- b) are forested and within 300 metres from the brow of the Escarpment slope;
- c) are the site of the most significant Areas of Natural and Scientific Interest (Life Science); and,
- d) are the location of the most significant stream valleys and wetlands associated with the Escarpment.

#### B4.2.1.3 Permitted Uses

The following uses are permitted, subject to the other policies of this Plan and the appropriate development criteria contained within the Niagara Escarpment Plan:

- a) existing agricultural operations;
- b) existing uses;
- c) single detached dwellings on existing lots;
- d) bed and breakfast establishments subject to Section E1.4.3 of this Plan;
- e) home occupations and cottage industries subject to Section E1.4.4 of this Plan;

- f) passive non-motorized recreational uses, such as nature viewing and walking trails, only on publicly owned lands or on the Bruce Trail;
- g) forest, wildlife and fisheries management;
- h) essential watershed management and flood and erosion control projects carried out or supervised by a public authority;
- i) archaeological activities;
- j) essential transportation and utility facilities;
- k) uses permitted in Park or Open Space Master or Management Plans that are not in conflict with the Niagara Escarpment Plan; and,
- l) nature preserves owned and managed by an approved conservation organization.

#### B4.2.2 ESCARPMENT PROTECTION AREA

##### B4.2.2.1 Objectives

It is the objective of this designation to:

- a) maintain and enhance the open landscape character of Escarpment features;
- b) provide a buffer between prominent Escarpment features and other land uses;
- c) maintain natural areas of regional significance and associated cultural heritage features; and,
- d) encourage agricultural activities and compatible recreation, resource management and conservation and educational activities.

##### B4.2.2.2 Location

The *Escarpment Protection Area* designation as shown on Schedule A2 to this Plan applies to lands:

- a) where existing land uses have significantly altered the natural environment associated with Escarpment slopes and related landforms;
- b) which are in close proximity to Escarpment slopes that are visually a part of the landscape unit; and/or,
- c) which are the site of Regionally Significant Areas of Natural and Scientific Interest (Life Science).

##### B4.2.2.3 Permitted Uses

The following uses are permitted, subject to the other policies of this Plan and the appropriate development criteria contained within the Niagara Escarpment Plan:

- a) agricultural operations;
- b) existing uses;

- c) single detached dwellings on existing lots;
- d) accessory residential uses on farm properties subject to Section E1.4.2 of this Plan, provided they are designed to be mobile or portable;
- e) bed and breakfast establishments subject to Section E1.4.3 of this Plan;
- f) home occupations and cottage industries subject to Section E1.4.4 of this Plan;
- g) home industries subject to Section E1.4.5 of this Plan;
- h) veterinary clinics;
- i) animal kennels in conjunction with a single detached dwelling and subject to Section E2.4.2 of this Plan;
- j) passive non-motorized recreational uses, such as nature viewing and walking trails, only on publicly owned lands or on the Bruce Trail;
- k) watershed management and flood and erosion control projects carried out or supervised by a public agency;
- l) small scale public uses only on lands above the escarpment brow;
- m) forest, wildlife and fisheries management;
- n) archaeological activities;
- o) transportation and utility facilities;
- p) commercial uses on farm properties subject to Section E1.4.6 of this Plan;
- q) residential care facilities subject to Section B4.3 of this Plan;
- r) uses permitted in Park or Open Space Master or Management Plans that are not in conflict with the Niagara Escarpment Plan; and,
- s) nature preserves owned and managed by an approved conservation organization.

#### B4.2.3 ESCARPMENT RURAL AREA

##### B4.2.3.1 Objectives

It is the objective of this designation to:

- a) maintain and enhance the open landscape character and scenic value of lands in the vicinity of the Escarpment;
- b) provide a buffer for the more ecologically sensitive areas of the Escarpment;
- c) encourage agricultural activities and compatible recreation, resource management, conservation and educational activities; and,

- d) to provide for the designation of new *Mineral Resource Extraction Areas* which can be accommodated in accordance with the policies of this Plan and by amendment to the Niagara Escarpment Plan, the Region of Halton Official Plan and this Plan.

#### B4.2.3.2 Location

The *Escarpment Rural Area* designation as shown on Schedule A2 to this Plan applies to lands that are:

- a) the site of minor Escarpment slopes and landforms;
- b) located in vicinity of the Escarpment which contribute to the open landscape character of the area; and/or,
- c) the site of features of ecological importance to the environment of the Escarpment.

#### B4.2.3.3 Permitted Uses

The following uses are permitted, subject to the other policies of this Plan and the appropriate development criteria contained within the Niagara Escarpment Plan:

- a) agricultural operations;
- b) existing uses;
- c) single detached dwellings on existing lots;
- d) accessory residential uses on farm properties subject to Section E1.4.2 of this Plan, provided they are designed to be mobile or portable;
- e) bed and breakfast establishments subject to Section E1.4.3 of this Plan;
- f) home occupations and cottage industries subject to Section E1.4.4 of this Plan;
- g) home industries subject to Section E1.4.5 of this Plan;
- h) veterinary clinics;
- i) animal kennels in conjunction with a single detached dwelling and subject to Section E2.4.2 of this Plan;
- j) passive non-motorized recreational uses, such as nature viewing and walking trails, only on publicly owned lands or part of the Bruce Trail;
- k) uses permitted in Park or Open Space Master or Management Plans that are not in conflict with the Niagara Escarpment Plan;
- l) forest, wildlife and fisheries management;
- m) watershed management and flood and erosion control projects carried out or supervised by a public authority;
- n) archaeological activities;



- o) transportation and utility facilities;
- p) commercial uses on farm properties subject to Section E1.4.6 of this Plan;
- q) residential care facilities subject to Section B4.3 of this Plan;
- r) wayside pits and quarries;
- s) small-scale public uses only on lands above the escarpment brow;
- t) nature preserves owned and managed by an approved conservation organization;
- u) berming, screening, temporary stockpiling of earthen material, accessory structures and facilities normally associated with a mineral extraction operation, and facilities and servicing for, and related to the water management and monitoring system and associated infrastructure for the quarry extension, on lands forming the buffer and setback for the Mineral Resource Extraction Area, which may be included within the license area approved under the Aggregate Resources Act, in Part of Lot 24, Concession 3 and Part of Lots 21 and 22, Concession 4, in the Town of Halton Hills, Regional Municipality of Halton. Extraction shall be prohibited in this area;
- v) development related to the installation of, access to, facilities and servicing for, the water management and monitoring system associated with the mineral aggregate operation located in the Mineral Resource Extraction Area, in Part of Lots 24 and 25, Concession 3 and Part of Lots 21 and 22, Concession 4, in the Town of Halton Hills, Regional Municipality of Halton; and
- w) accessory apartments in single detached dwellings subject to Section 2.2 (11) of the Niagara Escarpment Plan.

#### B4.2.3.4 Golf Courses and Private Recreation Uses

The development of new golf courses or expansions to existing facilities and other private recreation uses within the *Escarpment Rural Area* shall require an amendment to this Plan and be in accordance with Sections E1.4.9 and E2.4.4 of this Plan.

### B4.3 RESIDENTIAL CARE FACILITIES

Residential care facilities, not including those categorized as Second Level Lodging Homes, are permitted in single detached dwellings, subject to the appropriate development criteria within the Niagara Escarpment Plan, the requirements of Section G11 of this Plan and a minimum separation between facilities of 1,000 metres.

### B4.4 SPECIAL POLICY AREAS

Special Policy Areas apply to those lands within the *Niagara Escarpment Plan Area* that are subject to a land use policy that is specific to a property or area. All other relevant policies of this Plan are applicable unless otherwise modified or exempted by the provisions applicable to each Special Policy Area.

#### B4.4.1 SPECIAL POLICY AREA 1

The *Escarpment Rural Area* lands forming the setback for a mineral resource extraction area delineated in the site plan of a license area approved under the Aggregate Resources Act on Part of Lots 13 and 14, Concession 1 (Esquesing), Town of Halton Hills, and identified as Special Policy Area 1, as shown on Schedule A2 of this Plan, may subject to Condition 42 of Appendix F of the Joint Board Decision, as amended by Cabinet, be used for berming, screening, temporary stockpiling of earthen material, accessory structures and facilities normally associated with a mineral extraction operation, and facilities and servicing for, and related to, the water management and monitoring system for the quarry extension. Extraction shall not be permitted in the area.

It is the objective of this Plan to:

- a) recognize watercourses in the Town and protect them through the planning process from development that may have an impact on their function as an important component of the Natural Heritage System;
- b) ensure that development does not occur on lands that are unstable or susceptible to flooding;
- c) encourage improvements to air quality wherever possible, through facility management, land use planning, transportation management, roadway design, operation and maintenance, and other complementary programs;
- d) together with the Federal, Provincial and Regional governments, public interest groups and the private sector, work to reduce greenhouse gas emissions;
- e) ensure that land use change is managed on the basis of approved watershed and sub-watershed studies;
- f) achieve integrated watershed management through partnership with stakeholders within the watershed;
- g) ensure that agricultural operations are appropriately developed and managed;
- h) ensure that development does not occur on or near hazardous slopes;
- i) protect the quality and quantity of water available for drinking water purposes;
- j) protect water resources as an integral component of the natural environment;
- k) ensure that the integrity of the Town's watersheds are maintained or enhanced;
- l) encourage the further study of natural heritage features and related ecological functions to provide information that may assist in determining appropriate measures to protect them;
- m) encourage the regeneration of natural areas near watercourses;
- n) preserve trees and wooded areas and facilitate the planting of trees, to the extent practicable, including through the development process;
- o) encourage the development of alternatives to automobile use through the establishment of transit, pedestrian and bicycle routes and the development of housing at densities that support the use of these alternatives;
- p) encourage energy efficiency in the design of public and private buildings;
- q) identify the tools that are required to evaluate an application for development in or adjacent to natural heritage features and related ecological functions that are considered to be an integral component of the Natural Heritage System of the Town;
- r) identify what information is required to support an application that may have an impact on the hydrological features of the Town; and,

- s) identify means for ensuring that new development is built in a more sustainable manner, including the implementation of Green Development Standards.

## C2

## ENVIRONMENTAL IMPACT STUDIES

Where the policies of this Plan require that an Environmental Impact Study (EIS) be prepared, such an EIS shall be prepared in accordance with the requirements of this section and Terms of Reference approved by the Town and the Region, in consultation with the appropriate Conservation Authority and the proponent. In the preparation of an EIS, reference shall be made to the Environmental Impact Assessment Guidelines of the Region of Halton.

### C2.1

#### PURPOSE OF AN EIS

The purpose of an EIS is to:

- a) collect and evaluate the appropriate information in order to have a complete understanding of the boundaries, attributes and functions of natural heritage features and associated ecological and hydrological functions; and,
- b) make an informed decision as to whether or not a proposed use will have a negative impact on the natural heritage features and ecological and hydrological functions.

Any EIS required by this Plan must describe the natural heritage features and ecological functions, identify their significance and sensitivities and describe how they could be affected by a proposed use. The EIS should give consideration to the relevant aspects and inter-relationships of various components of the Natural Heritage System on and off the site. In addition, the EIS must address how the proposed development will protect, maintain or restore the natural features and ecological functions of the Natural Heritage System.

The Town and the Region, in consultation with the appropriate Conservation Authority, must approve an EIS prior to the granting of development approvals. The recommendations of an EIS shall be implemented through official plan amendments, zoning by-laws, subdivision conditions, site plan control, or applicable regulations.

### C2.2

#### WHAT AN EIS SHOULD DEMONSTRATE

Before development is approved in the area subject to the EIS, the EIS shall demonstrate that the relevant policies of this Plan are met, including, for mineral aggregate extraction operations, the principle of net environmental gain as set out in Section E6.4.3.5 of this Plan. The EIS should demonstrate that the use will:

- a) not have a negative impact on significant natural heritage features and related ecological functions;
- b) not discharge any substance that could have an adverse effect on air quality, groundwater, surface water and associated plant and animal life;
- c) be serviced by an adequate supply of water and that the groundwater taking associated with the use will not have an adverse effect on existing water supplies, surface water features and associated plant and animal life;
- d) not cause erosion or siltation of watercourses or unacceptable changes to watercourse morphology;

- e) not interfere with groundwater recharge to the extent that it would adversely affect groundwater supply for any use;
- f) not cause an increase in flood potential on or off the site;
- g) maintain/enhance/restore/rehabilitate the natural condition of affected watercourses, and protect/enhance/restore/rehabilitate aquatic habitat;
- h) not encourage the demand for further development that would negatively affect wetland function or contiguous wetland areas;
- i) enhance and restore endangered terrestrial and aquatic habitat where appropriate and feasible;
- j) not unduly interfere with the function of existing or potential natural corridors that are determined to be of significance;
- k) not lead to a reduction to the extent of significant woodlands within the Greenlands System; and,
- l) not lead to species loss or negative impacts on endangered, threatened or rare species and/or their habitat.

In determining whether the above criteria have been satisfied, reference may be made to applicable federal and provincial standards and regulations as well as referring to the relevant policies of this Plan.

## C3 WATERCOURSES

### C3.1 FUNCTION OF WATERCOURSES

Watercourses that flow within the boundaries of the Town are an integral part, and contribute to the health and function of the Town's Natural Heritage System since they:

- a) contain fish and wildlife habitat areas;
- b) function as corridors for wildlife habitat movement and vegetation dispersal;
- c) serve to maintain the quality and quantity of surface and groundwater resources;
- d) assist in the improvement of air quality;
- e) provide base flow and food supply to downstream areas; and,
- f) provide stormwater conveyance and control.

Where Regulatory Floodplain mapping is available, the floodplains of watercourses are protected within the *Greenlands A* designation as shown on the Schedules to this Plan and are subject to the policies of Section B1 of this Plan.

### C3.2 PROTECTION OF WATERCOURSES

It is the intent of this Plan to recognize the importance of the ecological function of all watercourses, and their associated floodplains, valleys and stream corridors, which can serve as key components and linkages in the Town's Natural Heritage System. Watercourses are generally shown on Appendix X1B to this Plan. Permanent and intermittent watercourses are distinguished on Appendix X1B for information purposes only.

It the policy of this Plan to encourage the regeneration of natural areas near watercourses and the protection of headwater areas for maintaining natural hydrological processes within a watershed.

In addition, it is the policy of this Plan that evaluations of the ecological function of watercourses, as shown on Appendix X1B, through a Subwatershed Study or in the absence of a Subwatershed Study, an Environmental Impact Study, be prepared to support Planning Act applications. Based upon the findings of an approved Subwatershed Study or Environmental Impact Study, additional watercourses may be incorporated into the Greenlands System by an amendment to this Plan.

Notwithstanding the policies of this Plan and the generality of the watercourses shown on Appendix X1B, federal and provincial statutes and Conservation Authority regulations will be applied to any watercourse determined by the relevant agency to be subject to such statutes and regulations whether or not it is shown on Appendix X1B to this Plan.

## C4 NATURAL HAZARDS

### C4.1 DEVELOPMENT BELOW THE STABLE TOP OF BANK AND IN FLOODPLAINS

It is the intent of this Plan to not permit new development or site alteration below the stable top of bank of a valley/watercourse.

In addition, it is also the policy of this Plan to not permit new development within the floodplain of a watercourse in accordance with the One-Zone concept. Areas that are within the Regulatory Floodplain as determined and mapped by the appropriate Conservation Authority are located within the *Greenlands* A designation. The intent of this policy is to minimize and eliminate any risks to life and property resulting from flooding and other natural hazards, such as erosion and slope failure.

In order to implement the objectives of this Plan, the implementing Zoning By-law shall place all lands within the Regional Storm floodplain and all lands below the stable top of bank of any valley/watercourse in an appropriate Environmental Zone consistent with the applicable land use designation in this Plan. Uses permitted in this Environmental Zone shall be limited to agricultural operations that existed on the date the implementing Zoning By-law is passed by Council and open space uses, excluding those uses that necessitate tree cutting. No buildings or structures, with the exception of essential structural works required for flood and/or erosion or sediment control, will be permitted in this Zone.

Notwithstanding this policy, any alteration of the existing channel of a watercourse shall require the approval of the appropriate Conservation Authority in accordance with applicable statutes and its development, Interference with Wetlands and Alterations to Shorelines and Watercourses Regulation.

### C4.2 EXISTING DEVELOPMENT IN FLOODPLAINS AND BELOW STABLE TOP OF BANK

Notwithstanding the policies of C4.1 above, minor additions or replacement structures, swimming pools, and non-habitable accessory structures may be permitted on lots of record that are within the Regional Storm floodplain and/or below the stable top of bank of a valley/watercourse subject to policies and regulations of the appropriate Conservation Authority.

### C4.3 STABLE TOP OF BANK SETBACKS

It is the policy of this Plan that all new lots be located a minimum of 15 metres from the stable top of bank of a major valley/watercourse and a minimum of 7.5 metres from the stable top of bank of a minor valley/watercourse. These setback requirements must be met by all development unless more appropriate setbacks are recommended in accordance with an approved Subwatershed study, Environmental Impact Study or Geotechnical study in consultation with the appropriate Conservation Authority. Setback requirements for the Premier Gateway Employment Area Phases 1A and 2A shall be in accordance with Section D3.5.4.3.3 of this Plan.

In all cases where development is proposed adjacent to a watercourse, Council shall be satisfied that the proposed development can be safely accommodated without there being a negative impact on the features and functions of the watercourse and its associated valley corridor.



At the present time, many of the Town's residents obtain drinking water from private wells. It is a policy of this Plan to protect existing sources of drinking water for future use.

On this basis, all major applications for development in close proximity to development serviced by private wells shall be supported by a Hydrogeological Report. Major applications shall include any development requiring an Amendment to this Plan. In addition, a Hydrogeological Report may be required for other applications in accordance with the policies of this Plan.

The Report shall be prepared to the satisfaction of the Town and the Region of Halton. The purpose of the Hydrogeological Report is to investigate the impacts of the proposed development on water quality and quantity and provide recommendations on:

- a) how to protect or enhance the hydrological function of the water resource;
- b) how to minimize or eliminate the effect of the proposed use on the groundwater recharge and discharge function;
- c) how to minimize or eliminate the effect of the proposed use on the quality and quantity of drinking water in adjacent private and municipal wells;
- d) how to protect, enhance or restore sensitive groundwater recharge/discharge areas, aquifers and headwater areas;
- e) whether it is necessary to monitor water budgets for groundwater aquifers and surface water features; and,
- f) how to ensure that the quality of the sensitive surface water features affected by the development are protected, improved or restored.

## C6

### GROUNDWATER MANAGEMENT

All residents of the Town of Halton Hills obtain their drinking water from groundwater sources. The majority of the water used is extracted from municipal wells in the Georgetown and Acton urban areas. Given this reliance on groundwater primarily for human use and its ecological importance, it is the objective of this Plan to:

- a) manage water resources in a manner that ensures an adequate sustainable supply of clean water for both human use and the natural environment;
- b) support the preparation of watershed and subwatershed plans to assist in the proper management of water as a key resource;
- c) protect existing surface and ground water quality from degradation and to improve and restore water quality where degraded;
- d) to incorporate source protection objectives into the land use planning process to ensure that the sources of water are not compromised in the future as a result of land use decisions;
- e) avoid competition for water supplies through appropriate Regional planning initiatives and decision-making processes;
- f) encourage the establishment of water conservation measures as a way to meet water supply needs in the same manner that alternatives to create new supplies are investigated; and,
- g) educate the public on the value of protecting the resource and how they can contribute to its protection.

## C6.1

### POLICIES

The Region of Halton has long recognized the value of groundwater resources and has prepared an Aquifer Management Plan to provide for an overall understanding of the resource and to provide the basis for policies in the Region of Halton Official Plan.

Policies based on the Aquifer Management Plan have been incorporated into the Region of Halton Official Plan. These include identifying Municipal Wellhead Protection Zones based on the groundwater flow patterns within the vicinity of each of the active wells supplying water to the Region. Municipal Wellhead Protection Zones that are located within Halton Hills are identified on Appendix X1B to this Plan. It is the policy of this Plan:

- a) to encourage the Region to update the AMP as new information becomes available;
- b) to show the boundaries of the Municipal Wellhead Protection Zones in the Comprehensive Zoning By-law, once incorporated into the Regional Official Plan and this Plan by amendments, and prohibit or restrict within these areas land uses that have the potential to release or discharge contaminants to significantly affect groundwater quality;
- c) to require those land uses that have the potential to release or discharge contaminants to significantly affect the quality of groundwater be subject to a review by the Region to assess the risks of such uses to potentially contaminate groundwater and, based on this assessment, to determine whether: the landowner is required to undertake a hydrogeological study to the

satisfaction of the Region and implement its recommendations. Such recommendations may result:

- i) in the prohibition of the use;
  - ii) in the restriction of the use; or,
  - iii) an agreement to adopt Best Management Practices as prescribed by the Region; and,
- d) to cooperate with the Region in the extension of Municipal Wellhead Protection Zones for municipal wells serving neighbouring municipalities and request similar protection from neighbouring municipalities for municipal wells serving the residents of Halton Hills.

## C7 WATERSHED PLANNING

### C7.1 OBJECTIVES

It is the objective of this Plan to:

- a) support the preparation and implementation of Watershed and Subwatershed Plans within the Town of Halton Hills and the surrounding area;
- b) ensure that there is an appropriate balance between the objectives of water supply management, habitat protection, flood management and land use to protect and enhance water quality and quantity for future generations;
- c) protect and/or restore streams, groundwater discharge and recharge areas and wetlands for the benefit of fish, wildlife and human uses;
- d) ensure that any Watershed and Subwatershed planning process is open and transparent and includes opportunities for public input;
- e) ensure that necessary resources are provided for the implementation of Watershed and Subwatershed Plans;
- f) ensure that Watershed and Subwatershed Plans contain goals and objectives that are appropriate for the Watershed and Subwatershed and can be implemented by the Town, agencies and landowners; and,
- g) ensure that Watershed and Subwatershed Plans are scientifically defensible and designed to be continuously improved as new information and science becomes available.

### C7.2 THE INTENT OF WATERSHED PLAN AND SUBWATERSHED PLANS

A Watershed is an area of land that drains into a watercourse or body of water. Unlike municipal boundaries, watershed boundaries are defined by nature and, as a result, watersheds often overlap a number of jurisdictions. A Subwatershed is an area of land that drains into a tributary of a larger watercourse or body of water. The intent of Watershed and Subwatershed Plans is to provide direction and target resources for the better and effective management and restoration of a given Watershed and Subwatershed. Such plans are intended to:

- a) serve as a guide for improving water quality, reducing flood damage and protecting natural resources in a watershed;
- b) prevent existing watershed problems from worsening as a result of future land development, public work activities and agricultural and other activities; and,
- c) provide an opportunity for multiple jurisdictions with varying priorities to coordinate their efforts and accept their responsibility for the impact their actions have both on upstream and downstream areas.

Council shall have regard to the above when making decisions or commenting on the priority given to the preparation of Watershed and Subwatershed Plans. The responsibility for preparing Watershed Plans generally rests with the Region in partnership with the appropriate Conservation Authority and in

consultation with the Town. Subwatershed Plans shall be prepared by the Town in cooperation with the appropriate Conservation Authority.

### C7.3 CONTENT OF WATERSHED PLANS

All Watershed Plans should contain the following:

- a) a detailed assessment and integrated analysis of the natural heritage features and functions and linkages on a watershed basis for incorporation into the Greenlands System;
- b) an assessment of the opportunities for and constraints to development;
- c) an estimate of the development capacity of the watershed based on defined water quality objectives;
- d) a water budget analysis;
- e) a description of the tools that are available to improve conditions in the Watershed and address the cumulative impacts of development; and,
- f) an action plan that contains a series of recommended programs and projects for improving the Watershed.

It is the intent of the action plan to set out the responsibilities of the various jurisdictions within the Watershed so that each can contribute to the prevention and remediation of Watershed problems and opportunities.

### C7.4 CONTENT OF SUBWATERSHED PLANS

All Subwatershed Plans shall contain the following:

- a) a detailed assessment and integrated analysis of the natural heritage features and functions and linkages on a subwatershed basis for incorporation into the Greenlands System;
- b) a characterization of the Subwatershed in terms of its environmental resources;
- c) a summary of the existing environmental issues;
- d) preparation and testing of a predictive impact assessment model to assess the potential impacts from stressors within the Subwatershed;
- e) the identification of alternative management solutions for the Subwatershed; and,
- f) detailed management strategies for the implementation of the Subwatershed Plan that incorporate guidelines for development, Terms of Reference for the preparation of Environmental Implementation Reports, including storm water management strategies, and ongoing monitoring programs for the Subwatershed Plan.

Relevant recommendations contained in Watershed and Subwatershed Plans shall be incorporated by Amendment into this Plan. A Subwatershed Plan shall be in place prior to the approval of a Secondary Plan that implements an Urban or Hamlet expansion. In addition, all applications for development shall conform with the recommendations made in an approved Watershed or Subwatershed Plan that have been incorporated by Amendment into this Plan. Within the Credit Valley Watershed, where an approved Subwatershed Plan is in place, an Environmental Implementation Report (EIR) may be required to implement the Subwatershed Plan. Where an Environmental Implementation Report is required, this may supersede the requirement for an EIS in accordance with Section C2 of this Plan, with the approval of the Town and the Region, in consultation with the appropriate Conservation Authority.

All commercial, industrial, institutional, recreational and residential development proposals shall be supported by a Stormwater Management (SWM) report unless waived by the Town through a preconsultation process in accordance with Section G12 of this Plan. The content and scope of the SWM report shall be determined when the development is proposed or through the completion of an EIR where required by an approved Subwatershed Plan.

The SWM Report shall be prepared to the satisfaction of the Town and the appropriate agencies and be prepared in accordance with The Ministry of Environment Stormwater Management Planning and Design Manual, 2003, or its successor, or through the completion of an EIR where required by an approved Subwatershed Study, and shall:

- a) provide recommendations on a stormwater quantity system that ensures that post-development peak flow will not be greater than the pre-development levels for storms up to and including the Regional storm and the 1:100 year storm event;
- b) document the possible impacts of development on watershed flow regimes including their interconnection with groundwater resources;
- c) provide recommendations on how to maintain post-development water quality and improve run-off where appropriate;
- d) document the means by which stormwater volume control will be provided;
- e) determine and describe the necessary site management measures required to be undertaken during construction to mitigate the potential negative impact of development; and,
- f) where applicable, describe how the requirements of the Watershed and/or Subwatershed Plan, or EIR will be implemented in the stormwater management plan.

All stormwater management facilities in a Plan of Subdivision shall be placed in an appropriate Environmental Zone in the implementing Zoning By-law to reflect the potential for these lands to be flooded and to ensure that their intended use is recognized. Stormwater management facilities for condominium developments and other large single uses may be privately owned and maintained. Agreements with the Town shall be required as a condition of approval, to provide for their continued maintenance.

It is the policy of this Plan to support the enactment of a tree conservation by-law by the Region of Halton to regulate tree cutting, including the requirement of a permit for the removal of trees, within the Greenlands System as shown on the Schedules to this Plan and woodlands 0.5 hectares or larger outside of the Greenlands System.

There are wooded areas within the Town that are not within the Greenlands System, primarily because of their small size or their location within urban areas. However, these areas also contribute to the character of the community. It is the policy of this Plan that:

- a) treed areas outside of the Greenlands System shall be retained in their natural state and additional trees shall be planted in accordance with good forestry management practice, whenever possible and appropriate, as a condition of development approval. To this end, all development proposals shall be accompanied by a tree inventory and preservation plan and proposed planting program;
- b) the Region and Town, in making public work decisions, shall have regard to the existence of trees and make every reasonable effort to protect them. If trees will be lost due to a public work, it is a policy of this Plan that replanting programs be initiated to compensate for the loss of trees;
- c) the Town require the planting of trees of appropriate quality, size and variety as a condition of development approvals wherever possible. Particular attention will be paid to surface parking lots and the softening of their visual impact on abutting lands and streets;
- d) Council may undertake a tree-planting program along arterial and collector roads wherever possible to create, over time, a canopy of trees that will enhance the character of the community and contribute to the aesthetics of the area; and,
- e) Council promote the development of vegetative cover along watercourses.



It is the policy of this Plan to:

- a) encourage the preparation of a Site Alteration By-law in accordance with the Municipal Act that regulates the removal of topsoil, restricts the stripping or mining of agricultural soils and restricts activities that erode, deplete, render inert or contaminate soils; and,
- b) encourage development and construction practices that minimize the levels of soil erosion and siltation.

The following policies apply to agricultural operations and non-agricultural operations near such uses in the Town:

- a) In order to provide farmers with the ability to carry out normal farm practices, all new development, including development on existing lots of record, shall be set back from agricultural operations in accordance with the Minimum Distance Separation One formula, as amended. Development on lands within the Urban Area, *Rural Cluster Area*, *Hamlet Area* and *Country Residential Area* existing or draft plan approved at the date of the adoption of this Plan is exempt from this policy;
- b) New and/or expanded livestock facilities shall be set back from existing non-agricultural uses in accordance with the Minimum Distance Separation Two formula, as amended;
- c) The Minimum Distance Separation One and Two formulas shall be included within the implementing Zoning By-law. An Amendment to this Plan shall not be required to vary the setbacks required by the formulas, provided the variation is minor and will not affect the viability of agricultural operations in the area;
- d) It is a policy of this Plan to require the preparation of Nutrient Management Plans prior to the development of new intensive livestock facilities or the expansion of existing facilities in the Town. Such a Nutrient Management Plan would establish a strategy for proper nutrient distribution and shall be prepared in accordance with Nutrient Management Act;
- e) Nothing in this Plan shall limit the ability of farmers to carry out normal and reasonable farm practices in accordance with the Farming and Food Production Protection Act; and,
- f) Agricultural uses are permitted within the Urban Area until lands are required for the orderly phasing of urban development.

Open and closed waste disposal sites in the Town are shown on Appendix X3 to the Plan. The following policies apply to these sites and the lands in the vicinity of each:

- a) The development of new uses or new or enlarged buildings or structures within an assessment area of approximately 500 metres from the property boundary of an open site or 500 metres from the fill area of a closed site, may be permitted provided an assessment is completed to determine:
  - i) the impact of any potential methane gas migration;
  - ii) whether the proposed use will be adversely affected by noise, odour, dust or other nuisance factors from the waste disposal site;
  - iii) potential traffic impacts;
  - iv) whether the proposed use will be adversely affected by ground and surface water contamination by leachate migrating from the waste disposal site; and,
  - v) the impact of the proposed use on leachate migration from the landfill site;
- b) The assessment is intended to address these matters and other items outlined in the Province's Guideline D-4, April 1994, or its successor as required to ensure that the proposed land uses are compatible in nature and do not adversely impact upon each other;
- c) The studies required to support a development application within the assessment area shall be based on the type and/or scale of the development proposed or the studies may be waived as appropriate. The nature of the required studies and their scope shall be determined by the Town in consultation with the Region of Halton at the time the development is proposed;
- d) All lands within the assessment area shall be subject to a Holding provision in the implementing Zoning By-law. The lifting of a Holding provision permitting the development of any new use or new or enlarged buildings or structures within the assessment area shall not occur until Council is satisfied that all of the required studies have been completed to the satisfaction of the Town and the Region;
- e) In cases where an amendment to the Official Plan and/or Zoning By-law is required to permit a proposed use, appropriate studies dealing with the matters set out above shall be submitted for Council's consideration; and
- f) An amendment to this Plan will be required for a new waste disposal site. Notwithstanding the foregoing, new waste disposal sites are not permitted in lands subject to the Niagara Escarpment Plan.

Prior to the Town and the Region considering a development application, the proponent shall identify whether there is any potential for contamination on the site using information available from the Region, Province, and other sources in accordance with the Region's protocol pertaining to contaminated or potentially contaminated sites. If the site of a proposed use is known or suspected to be contaminated, Council shall require the proponent to prepare a study prepared in accordance with the Ministry of Environment regulation that determines the nature and extent of the contamination and the identification of a remedial plan if required. Where the need for remediation is identified, the site shall be restored in accordance with the remedial plan before development occurs to ensure that there will be no adverse effects on the proposed uses or adjacent land uses. On-site soil rehabilitation of contaminated sites, rather than off-site removal of contaminated soil, shall be encouraged. Development shall only be permitted if the Town and the Region of Halton are satisfied through appropriate studies that there will not be any adverse effects and that the site complies with Ministry of Environment regulations, Regional standards and other requirements regarding soil, surface water and groundwater quality.

Land uses such as residential, schools, hospitals, and day nurseries, are considered sensitive to noise, vibration, odour or other emissions associated with various types of industries. It is a policy of this Plan that incompatible land uses be separated or otherwise buffered from each other to prevent adverse effects from odour, noise and other contaminants, and minimize risk to public health and safety. Where a proposed development is located adjacent to a potentially incompatible land use, an assessment of the compatibility of the proposal shall be required in accordance with guidelines prepared by the Ministry of Environment.

Such a study shall be prepared to the satisfaction of Council and the appropriate agencies and shall include recommendations on how the impacts can be mitigated. The approval of development proposals shall be based upon the achievement of adequate separation distances and the implementation of the recommendations of the required studies.

It is a policy of this Plan to minimize any adverse noise and vibration impacts from highways, arterial roads and railways on sensitive land uses. To this end, a Noise Impact Study in accordance with Ministry of Environment guidelines shall be undertaken for proposed development involving sensitive land uses that are adjacent to or in proximity to a highway, arterial road or industrial use to the satisfaction of Council and the Region. The approval of development applications shall be based on the implementations of the required study, such as the restriction of new residential and other sensitive land uses and the provision of appropriate safety and mitigating measures and notices on title.

A Noise Impact Study shall be required where proposed development involving sensitive land uses is within 300 metres of a railway right-of-way or 1,000 metres of a railway yard. A Vibration Study shall be required where proposed development involving sensitive land uses is within 75 metres of a railway right-of-way. Noise and vibration studies shall be prepared to the satisfaction of Council and the Region in consultation with the appropriate railway agency. The approval of development applications shall be based on the implementation of the recommendations of the required studies, such as the restriction of new residential and other sensitive land uses and the provision of appropriate safety measures, such as setbacks, berms and security fencing, mitigating measures, and notices on title.

The Town shall encourage energy conservation by promoting:

- a) compact urban form in new greenfield areas that is transit supportive;
- b) mixed use development in appropriate locations and live-work relationships that reduce automobile use;
- c) lot and building design that maximizes direct access to sunlight during the winter;
- d) the use of vegetation that will reduce energy consumption of buildings; and,
- e) cycling and walking.

Solid waste management is the responsibility of the Region of Halton and is subject to detailed policies in the Regional Official Plan. It is the policy of this Plan to support local and Regional efforts to promote the principles of reduce, reuse and recycle including the recycling of aggregate wastes (including used concrete and used asphalt) and solid wastes.



**C18                    WAYSIDE PITS AND QUARRIES, PORTABLE ASPHALT PLANTS AND PORTABLE CONCRETE PLANTS**

Wayside pits and quarries, portable asphalt plants and portable concrete plants used on public authority contracts shall be permitted, without the need for an Official Plan amendment or rezoning in all areas of the Town, except the Niagara Escarpment Plan Area and those areas of existing development or particular environmental sensitivity, which have been determined to be incompatible with extraction and associated activities.

## C19

### GREEN DEVELOPMENT

To ensure that the goals and strategic objectives of this Plan to facilitate sustainable development practices are addressed through development applications, all applicable development applications in the Town shall promote the following goals through the use of technologies, innovations, features, landscaping, site plans, subdivision plans and any other means available to the developer:

- Energy conservation;
- Water conservation and quality;
- Natural environment;
- Air quality;
- Waste management;
- Communication; and,
- Transportation and/or community design.

## C19.1

### GREEN DEVELOPMENT STANDARDS

A development application will be deemed to have met the above goals if it meets the requirements of the Green Development Standards adopted by Council, and as amended from time to time, to provide detailed direction for the implementation of this policy.

## D1 LIVING AREAS

### D1.1 OBJECTIVES

It is the objective of this Plan to:

- a) maintain and enhance the character of Mature Neighbourhood Areas by ensuring that new housing, replacement housing, additions, and alterations are compatible, context sensitive, and respectful of the existing character of the neighbourhood;
- b) maintain and enhance the character and stability of existing and well established residential neighbourhoods by ensuring that development and redevelopment is compatible, in terms of built form, with the character of adjacent buildings and neighbourhoods and the scale and density of existing development;
- c) encourage the provision of a range of housing types to accommodate persons with diverse social and economic backgrounds, needs and desires while promoting the maintenance and improvement of existing housing;
- d) encourage the provision of a full range of housing opportunities to meet the Town's projected housing needs;
- e) promote the efficient use of existing and planned infrastructure by creating the opportunity for various forms of residential intensification, where appropriate;
- f) encourage more intensive residential development to locate at the boundaries of existing residential areas;
- g) ensure that all residential areas are served by schools, parks and other facilities which are intended to function as community hubs and neighbourhood gathering places;
- h) ensure that residential areas permit a variety of complementary and compatible land uses including special needs housing, community facilities, schools, small-scale commercial uses and open space areas; and,
- i) establish a comprehensive set of design guidelines and policies that foster the establishment of an urban environment that is safe, functional and attractive.

### D1.2 LOCATION

The Living Area of the Town of Halton Hills applies to developed and undeveloped lands on municipal water and wastewater services in Georgetown and Acton. The Living Area is divided into the following three designations:

- *Low Density Residential Area;*
- *Medium Density Residential Area;* and,
- *High Density Residential Area.*

Each of these designations is intended to provide opportunities for residential development at densities that ensure a full range of housing types is provided.

## D1.3 DESIGNATIONS

### D1.3.1 LOW DENSITY RESIDENTIAL AREA

#### D1.3.1.1 MAIN PERMITTED USES

The main permitted uses in the *Low Density Residential Area* designation are limited to:

- a) single detached dwellings;
- b) semi-detached dwellings; and,
- c) duplex dwellings.

#### D1.3.1.2 Density and Height

The maximum permitted density shall not exceed 20 units per net residential hectare in the *Low Density Residential Area* designation as shown on Schedule A3: Georgetown Urban Area. The maximum permitted density shall not exceed 15 units per net residential hectare in the *Low Density Residential Area* designation as shown on Schedule A6: Acton Urban Area. The maximum building height shall not exceed three storeys in the *Low Density Residential Area* designation.

Notwithstanding the foregoing, this Plan recognizes that certain lands in the Acton Urban Area have developed at densities higher than those identified by this section. Additional development within these lands shall be permitted in accordance with the Zoning By-laws existing at the date of the adoption of this Plan and other applicable policies of this Plan.

#### D1.3.1.3 Complementary Uses

Complementary uses that are permitted, subject to criteria in the *Low Density Residential Area* designation include the following:

- a) home occupations subject to Section D1.3.1.4 of this Plan;
- b) bed and breakfast establishments in single detached dwellings subject to Section D1.3.1.5 of this Plan;
- c) accessory apartments in single detached dwellings and semi-detached dwellings, subject to Section D1.3.1.6 of this Plan;
- d) garden suites subject to Section D1.3.1.7 of this Plan;
- e) special needs housing subject to Section D1.3.1.8 of this Plan;
- f) local parkland subject to Section F7.3.4 of this Plan;
- g) institutional uses which generally serve the needs of the surrounding residential areas such as private and public elementary schools, day nurseries and places of worship, subject to Section D1.3.1.9 of this Plan;
- h) local commercial uses subject to Section D2.5.5 of this Plan; and,
- i) motor vehicle service stations subject to Section D1.3.1.10 of this Plan.

#### D1.3.1.4 Home Occupations

Home occupations are permitted in accordance with the following criteria:

- a) the use shall not have a negative impact on the enjoyment and privacy of neighbouring properties;
- b) the use is clearly secondary to the primary residential use in terms of floor space utilization and is compatible with surrounding uses;
- c) the use is completely located in the principal residence of the person conducting the home occupation;
- d) the character of the dwelling as a private residence is preserved;
- e) adequate on-site parking facilities are provided for the use, in addition to the parking required for the principal use on the property, and such parking is provided in locations compatible with surrounding land uses;
- f) the use will not cause a traffic hazard; and,
- g) the signage advertising the use is to be designed and located in accordance with the Town's sign by-law.

The implementing Zoning By-law shall further detail appropriate performance standards for home occupations.

#### D1.3.1.5 Bed and Breakfast Establishments

Bed and breakfast establishments are permitted subject to Site Plan Control and the following criteria:

- a) the use shall not have a negative impact on the enjoyment and privacy of neighbouring properties;
- b) the use is clearly secondary to the primary use of the dwelling as a residence and is compatible with surrounding uses;
- c) the bed and breakfast establishment must be the principal residence of the owner and operator;
- d) the character of the dwelling as a private residence is preserved;
- e) adequate parking facilities are available on the lot for the proposed use;
- f) no more than three rooms are available for guests;
- g) the use will not cause a traffic hazard;
- h) the signage advertising the use is to be designed and located in accordance with the Town's sign by-law; and,
- i) municipal water and wastewater services are adequate and available.

The implementing Zoning By-law shall further detail appropriate performance standards for bed and breakfast establishments.

D1.3.1.6 Accessory Apartments

An accessory apartment is permitted subject to the regulations of the Zoning By-law and the following criteria:

- a) the accessory apartment shall comply with the Ontario Building and Fire Codes;
- b) adequate parking is available on the lot for both dwelling units and minimizes the loss of outdoor amenity areas or landscaping;
- c) the accessory apartment is designed and located in such a manner to not have a negative impact on the character of the surrounding neighbourhood and to that end any building addition shall be compatible with the massing, height, and setbacks of adjacent dwelling units; and,
- d) municipal water and wastewater facilities are adequate and available.

Accessory apartments shall not be subject to the density provisions of this Plan. As a condition of approval, the Town shall require that dwelling units containing an accessory apartment be registered in accordance with the provisions of the Municipal Act.

D1.3.1.7 Garden Suites

Garden suites may be permitted in conjunction with a single detached dwelling in the *Low Density Residential Area* designation provided that:

- a) the suite is located in the rear yard and appropriate buffering and siting of the suite relative to adjacent properties is provided;
- b) adequate parking is available on the lot for both the single detached dwelling and the garden suite;
- c) a site-specific temporary use by-law is passed pursuant to the Planning Act;
- d) the applicant enters into an agreement with the Town which addresses site location, buffering and installation/removal and maintenance during the period of occupancy; and,
- e) municipal water and wastewater facilities are adequate and available.

Garden suites shall not be subject to the density provisions of this Plan.

D1.3.1.8 Special Needs Housing

D1.3.1.8.1 Long-Term Care Facilities and Retirement Homes

Long term care facilities and retirement homes are permitted subject to an amendment to the implementing Zoning By-law and provided Council is satisfied that:

- a) the site has access and frontage onto an Arterial or Collector road as shown on Schedule B1 to this Plan;
- b) the site has adequate land area to accommodate the building, an outdoor amenity area, on-site parking and appropriate buffering such as setbacks, landscaping and fencing to ensure compatibility of the use with adjacent land uses;
- c) the building does not exceed 3 storeys in height;
- d) the use will not cause traffic hazards or an unacceptable level of congestion on surrounding roads; and,
- e) municipal water and wastewater facilities are adequate and available.

D1.3.1.8.2 Residential Care Facilities

Residential care facilities, not including those categorized as Group Home Type 2, are permitted in single detached dwellings, subject to the requirements of the implementing Zoning By-law and Section G11 of this Plan, including:

- a) a minimum separation between facilities;
- b) a minimum floor space per resident; and,
- c) a requirement that Treatment Centres must be located on a lot with frontage on an Arterial or Collector Road as shown on Schedule B1 to this Plan.

The implementing Zoning By-law shall further detail appropriate performance standards for residential care facilities that have regard to:

- a) limitations of the existing housing stock and design;
- b) The objective of community integration; and,
- c) The prevention of an undue concentration of residential care facilities in certain areas of the Town.

The establishment of residential care facilities in other suitable locations, which are not permitted by the implementing Zoning By-law, may be permitted through site-specific amendment to the applicable Zoning By-law, provided Council is satisfied with respect to:

- a) the adequacy of the separation distance between the proposed facility and all other residential care facilities;
- b) the suitability of the dwelling size and lot size, in relation to the number of residents to be accommodated; and,

- c) the adequacy and proximity of transportation and community support services, including medical and educational services, required by the residents.

#### D1.3.1.8.3 Emergency Housing

Emergency housing facilities are permitted subject to an amendment to the implementing Zoning By-law, and provided Council is satisfied that:

- a) the facility has access and frontage onto an Arterial or Collector road as shown on Schedule B1 to this Plan;
- b) the facility is located a minimum of 800 metres from any Residential Care Facility;
- c) the facility is located a minimum of two kilometres from any other emergency housing facility;
- d) the dwelling size and lot size is adequate, in relation to the number of residents to be accommodated;
- e) the facility has access to adequate and proximate community support services, including medical and educational services, required by the residents;
- f) the facility is located no more than half a kilometre from a public transit route, or the facility operator can provide adequate transportation services;
- g) the facility will not cause a traffic hazard; and,
- h) municipal water and wastewater facilities are adequate and available.

#### D1.3.1.9 Day Nurseries and Institutional Uses

Day nurseries and institutional uses which generally serve the needs of surrounding residential areas such as private and public elementary schools, nursery schools, and places of worship, may be permitted subject to an amendment to the implementing Zoning By-law and provided Council is satisfied that:

- a) the site has access and frontage onto an Arterial or Collector Road as shown on Schedule B1 to this Plan;
- b) the use is intended to serve and support surrounding residential areas and is not more appropriately located in the *Major Institutional Area* designation;
- c) the use is designed to complement the low density nature of the neighbourhood;
- d) the use will not cause any traffic hazards or an unacceptable level of congestion on surrounding roads;
- e) the site is large enough to accommodate the building, on-site parking, play areas, where required, and appropriate buffering such as setbacks, landscaping and fencing to ensure the compatibility of the use with adjacent land uses;



- f) the noise generated by the use will not adversely affect the enjoyment of neighbouring properties;
- g) the use will be a positive addition to the neighbourhood and will provide a service that is required by residents;
- h) in the case of public elementary schools, the site is located adjacent to a neighbourhood park to provide a neighbourhood focal point and encourage the provision of complementary facilities such as sports fields, hard surface play areas and components and play equipment; and,
- i) municipal water and wastewater services are adequate and available.

#### D1.3.1.10 Motor Vehicle Service Stations

Motor Vehicle Service Stations may be permitted subject to an amendment to the implementing Zoning By-law and provided Council is satisfied that:

- a) the site has frontage on an Arterial Road as shown on Schedule B1 to this Plan and is located at the intersection with an Arterial Road, a Collector Road or a Local Road;
- b) the use will not cause traffic hazards or an unacceptable level of congestion on surrounding roads;
- c) the use is located on a site that has a minimum lot area of 0.6 hectares to incorporate required parking, waste management facilities, landscaping and buffering on-site; and,
- d) the scale, massing and siting of the development is compatible with development on adjoining lands.

### D1.3.2 MEDIUM DENSITY RESIDENTIAL AREA

#### D1.3.2.1 Main Permitted Uses

The main permitted uses in the *Medium Density Residential Area* designation are limited to:

- a) triplex dwellings;
- b) quattroplex dwellings;
- c) multiple dwellings;
- d) street townhouse dwellings;
- e) block townhouse dwellings;
- f) low-rise apartment dwellings; and,
- g) long term care facilities and retirement homes.

D1.3.2.2 Density And Height

The density range shall be 21 to 50 units per net residential hectare and the maximum building height shall not exceed four storeys. The maximum density and/or height may be increased subject to Section G4.3 of this Plan.

D1.3.2.3 Complementary Uses

Complementary uses that are permitted subject to certain criteria in the *Medium Density Residential Area* designation include:

- a) home occupations subject to Section D1.3.1.4 of this Plan;
- b) local parkland subject to Section F7.3.4 of this Plan; and,
- c) accessory apartments in townhouse dwellings subject to Section D1.3.1.6 of this Plan.

D1.3.2.4 Criteria for Designation

New medium density residential areas shall be established by way of Amendment to the Official Plan in accordance with Section D1.4.3 of this Plan.

D1.3.3 **HIGH DENSITY RESIDENTIAL AREA**

D1.3.3.1 Main Permitted Uses

The main permitted uses in the *High Density Residential Area* designation are limited to:

- a) apartment dwellings; and,
- b) long term care facilities and retirement homes.

D1.3.3.2 Density and Height

The density range shall be 51 to 100 units per net residential hectare and the maximum building height shall not exceed eight storeys. The maximum density and/or height may be increased subject to Section G4.3 of this Plan.

D1.3.3.3 Complementary Uses

Complementary uses that may be permitted in the *High Density Residential Area* designation include the following:

- a) home occupations subject to Section D1.3.1.4 of this Plan; and,
- b) local parkland subject to Section F7.3.4 of this Plan.

D1.3.3.4 Criteria for Designation

New high-density residential areas shall be established by way of Amendment to the Official Plan in accordance with Section D1.4.3 of this Plan.

#### D1.4 GENERAL RESIDENTIAL AND HOUSING POLICIES

##### D1.4.1 URBAN DESIGN AND DESIGN FOR NEW COMMUNITIES

All new development requiring a Planning Act approval shall conform to Section F2 (Urban Design) and Section F3 (Design for New Communities) of this Plan as appropriate.

##### D1.4.2 NEW HOUSING, REPLACEMENT HOUSING, ADDITIONS, AND ALTERATIONS IN MATURE NEIGHBOURHOOD AREAS

New housing, replacement housing, additions, and alterations within Mature Neighbourhood Areas shall be permitted provided they are compatible, context sensitive, and respectful of the existing character of the neighbourhood.

The implementing Zoning By-law shall further detail appropriate standards for new housing, replacement housing, additions, and alterations within Mature Neighbourhood Areas, including lot coverage, building height, and side yard setbacks amongst other standards.

Minor variances from the implementing Zoning By-law associated with new housing, replacement housing, additions, and alterations in Mature Neighbourhood Areas shall consider, where applicable:

- a) compatibility with existing building orientation and building setbacks;
- b) that the scale, massing, building height, and built form features are compatible with the existing character of the neighbourhood;
- c) the preservation of landscaped open space areas and the protection of existing trees; and,
- d) that impacts on adjacent properties are minimized.

##### D1.4.3 INFILL DEVELOPMENT IN ESTABLISHED RESIDENTIAL NEIGHBOURHOODS

Infill development, in accordance with the applicable land use designation in this Plan, shall be encouraged provided Council is satisfied that:

- a) the proposed development, including building form and density, is compatible with the character of the existing neighbourhood;
- b) new buildings are designed in a manner that is sensitive to the location, massing and height of adjacent buildings;
- c) a suitable transition in lot sizes, densities, building forms and heights is provided from adjacent development;
- d) existing trees and vegetation will be retained and enhanced where possible and additional landscaping will be provided to integrate the proposed development with the existing neighbourhood;
- e) the proposed development will not create a traffic hazard or an unacceptable increase in traffic on local roads; and,

- f) significant views and vistas which help define a residential neighbourhood are preserved.

#### D1.4.4

#### NEW MEDIUM AND HIGH DENSITY RESIDENTIAL AREAS

In considering the development of new medium and high density residential development by way of Amendment to this Official Plan, Council shall be satisfied that the proposal:

- a) is located on and has direct access to a Collector or Arterial Road as shown on Schedule B1 to this Plan;
- b) respects the character of adjacent residential neighbourhoods, in terms of height, massing and setbacks;
- c) can be easily integrated with surrounding land uses;
- d) will not cause traffic hazards or an unacceptable level of congestion on surrounding roads;
- e) can easily be accessed by public transit if available;
- f) is located in close proximity to community facilities, such as parks, schools and open spaces;
- g) is located on a site that has adequate land area to incorporate the building, on-site parking, an outdoor amenity area, and appropriate buffering such as setbacks, landscaping and fencing to ensure the compatibility of the use with adjacent land uses;
- h) where appropriate, considers the role of topography and natural vegetation in minimizing the impacts of tall buildings on adjacent land uses;
- i) has demonstrated that the potential shadow impacts associated with tall buildings will be at an acceptable level on adjacent properties; and,
- j) municipal water and wastewater services are adequate and available.

#### D1.4.5

#### MONITORING OF HOUSING

A program shall be established in conjunction with the Region in order to monitor:

- a) historic housing production levels by location, type and tenure in the Town;
- b) the location and spatial distribution of vacant land that is designated for residential use within the Official Plan;
- c) the supply of draft approved and vacant registered residential lots/units;
- d) the price of housing available on the market;
- e) the location, spatial distribution, supply and pricing of available rental housing;

- f) the type, location and spatial distribution of infill housing development that has occurred;
- g) how many accessory apartments have been legally created in accordance with Section D1.3.1.6 of this Plan; and,
- h) progress towards the achievement of the housing targets identified in the Municipal Housing Statement.

The results of this monitoring program will be reported to Council through an Annual Housing Report. The Annual Housing Report will assist Council in evaluating the performance of the Town in meeting the housing objectives of this Plan.

#### D1.4.6 MUNICIPAL HOUSING STATEMENT

The Town shall update its Municipal Housing Statement, as required, in cooperation with the Region. The Municipal Housing Statement will contain:

- a) a detailed demographic profile of the Town, including age structure, household income and household size;
- b) a description of trends in the affordability of housing through an analysis of housing prices and market rents;
- c) a profile of the Town's housing stock by density, type and tenure;
- d) an analysis of the adequacy of the Town's housing and residential land supply, in relation to forecasted housing demand;
- e) an evaluation of the need for affordable, seniors and special needs housing; and,
- f) the establishment of an optimum housing mix, and 5-year housing targets by housing type consistent with the 2031 forecasted population indicated in this Plan.

#### D1.4.7 HOUSING SUPPLY

It is a policy of this Plan to maintain a ten year supply of land through residential intensification, redevelopment, and if necessary lands designated and available for residential development, and a three year supply of land zoned for residential intensification and residential lots/units in registered and draft plan approved subdivisions within the context of the population target contained in this Plan and provided that there is sufficient water and wastewater capacity available to service development in the Georgetown and Acton Urban Areas.

#### D1.4.8 HOUSING MIX

It is the policy of this Plan to ensure the provision of a complete range of housing within the Town of Halton Hills. On this basis, the optimum housing mix target to be achieved by the 2031 planning horizon is:

- 65% low density housing;

- 20% medium density housing; and,
- 15% high-density housing.

This housing mix is to be achieved by targeting the following housing mix for development over the 2006 to 2031 planning period, within Halton Hills as a whole, through intensification within the Built Boundary, as well as development within the Designated Greenfield Area:

- 46% low density housing;
- 32% medium density housing; and,
- 22% high-density housing.

New development that assists in achieving this housing mix shall be encouraged. In addition, Official Plan Amendment applications that propose the down-designation of sites for medium and high density housing, outside of Intensification Areas, shall be discouraged, since these sites will ultimately assist in achieving an appropriate balance of housing in the Town and the population target identified by this Plan. All such applications shall be accompanied by a planning justification report in accordance with Terms of Reference approved by the Town in consultation with the Region that assesses the implications of the proposal on:

- a) the Town's ability to meet the housing mix and population targets outlined in this Plan; and,
- b) the impact such a down-designation will have on other sites that are designated for medium and/or high density purposes in the surrounding area.

#### D1.4.9 CONVERSION AND DEMOLITION OF RENTAL HOUSING

The conversion of rental housing to condominium or other forms of tenure, or the demolition of rental housing, shall only be considered by Council if a minimum threshold rental housing vacancy rate of 3% is maintained in the Town.

#### D1.5 IMPLEMENTING ZONING BY-LAW

All lands in each of the residential designations shall be placed in specific residential zones in the implementing Zoning By-law. Complementary uses will also be placed in appropriate zones in the implementing Zoning By-law.

#### D1.6 RESIDENTIAL SPECIAL POLICY AREAS

Residential Special Policy Areas apply to those lands designated for residential purposes that are subject to a land use policy that is specific to a property or area. All other relevant policies of this Plan are applicable unless otherwise modified or exempted by the provisions applicable to each Residential Special Policy Area.

##### D1.6.1 RESIDENTIAL SPECIAL POLICY AREA 1

A maximum of 56 apartment units shall be permitted on lands designated as *High Density Residential Area* and identified as Residential Special Policy Area 1, as shown on Schedule A3 of this Plan; and shall comply with zoning by-law and site plan requirements in effect at the time of adoption of this Plan.

#### D1.6.2 RESIDENTIAL SPECIAL POLICY AREA 2

A maximum of 65 units shall be permitted within buildings that existed at the date of the adoption of this Plan on lands identified as Residential Special Policy Area 2, as shown on Schedule A3 of this Plan.

#### D1.6.3 RESIDENTIAL SPECIAL POLICY AREA 3

A maximum of 54 street townhouses and a minimum of 16 detached or semi-detached dwelling units shall be permitted on lands identified as Residential Special Policy Area 3, as shown on Schedule A3 of this Plan.

#### D1.6.4 RESIDENTIAL SPECIAL POLICY AREA 4

The maximum density shall not exceed 16 units per net residential hectare on lands identified as Residential Special Policy Area 4, as shown on Schedule A3 of this Plan.

#### D1.6.5 RESIDENTIAL SPECIAL POLICY AREA 5

The lands designated as *Trafalgar Road Redevelopment Area* and identified as Residential Special Policy Area 5, as shown on Schedule A3 of this Plan, form part of the Civic Centre District Intensification Area as shown on Schedule A3-1 of this Plan.

#### D1.6.6 RESIDENTIAL SPECIAL POLICY AREA 6

The development of single and semi-detached dwellings or other forms of horizontal multiple dwellings suitable for seniors housing or any combination of these dwelling types may be permitted within a condominium development on lands identified as Residential Special Policy Area 6, as shown on Schedule A3 of this Plan. Notwithstanding Section D1.3.1.2, the maximum permitted density shall not exceed 25 units per net residential hectare. Through the site plan and condominium approval processes, the street and block layout on the subject lands shall provide for an attractive appearance from 10 Side Road through the use of measures such as service roads and/or the location of private open space elements.

##### D1.6.5.1 Permitted Uses

Permitted uses within the *Trafalgar Road Redevelopment Area* are limited to:

- a) medium density residential uses, including townhouses and low-rise apartment dwellings, subject to Sections D1.3.2.1 and D1.3.2.2 of this Plan; and,
- b) high density residential uses, including apartment dwellings, long-term care facilities and retirement homes, subject to Sections D1.3.3.1 and D1.3.3.2 of this Plan.

##### D1.6.5.2 Development and Redevelopment Policies

The redevelopment of lands designated as *Trafalgar Road Redevelopment Area* and identified as Residential Special Policy Area 5, as shown on Schedule A3, shall require an amendment to the implementing Zoning By-law. Prior to considering such a re-zoning, a Comprehensive Development Plan (CDP) for lands designated as *Trafalgar Road Redevelopment Area* shall be prepared to the satisfaction of Council.

The CDP shall include information on:

- a) proposed built form elements including type, height, massing and location of all main buildings and structures;
- b) the proposed location of appropriate access points along Trafalgar Road;
- c) the proposed location of parking areas and driveways;
- d) how the development can be designed in an innovative manner that addresses the site location adjacent to a major arterial road and a railway line;
- e) proposed streetscape components and the location of proposed landscaping;
- f) potential pedestrian areas and linkages to the residential areas to the north; and,
- g) means by which the protection of the Black Creek tributary will be accomplished in accordance with the Watercourse policies of this Plan.

The CDP shall generally conform to the policies set out in Section F2 (Urban Design) of this Plan. The CDP shall be implemented through development applications that are in general conformity with that Plan. To the extent possible, efforts should be made to coordinate the planning for this area with the CDP for the adjacent *Civic Centre Area* designation.

In order to implement this policy, the implementing Zoning By-law shall only recognize development existing at the time the by-law comes into effect pursuant to the Planning Act.

#### D1.6.6 RESIDENTIAL SPECIAL POLICY AREA 6

The development of single and semi-detached dwellings or other forms of horizontal multiple dwellings suitable for seniors housing or any combination of these dwelling types may be permitted within a condominium development on lands identified as Residential Special Policy Area 6, as shown on Schedule A3 of this Plan. Notwithstanding Section D1.3.1.2, the maximum permitted density shall not exceed 25 units per net residential hectare. Through the site plan and condominium approval processes, the street and block layout on the subject lands shall provide for an attractive appearance from 10 Side Road through the use of measures such as service roads and/or the location of private open space elements.

#### D1.6.7 RESIDENTIAL SPECIAL POLICY AREA 7

Residential Special Policy Area 7, as shown on Schedule A3, relates to lands that are within the *Low Density Residential Area* designation located on the south side of Guelph Street between the *Community Node* and *Downtown Area* designations. Given the high traffic location, the conversion of residential dwellings, in whole or in part, for office uses and limited retail uses (excluding FCTM, apparel and accessories), may be permitted subject to an amendment to the Zoning By-law and Site Plan Control and provided that Council is satisfied that:

- a) adequate on-site parking will be provided and is located in the rear and side yards;
- b) the residential character of the building is maintained;



- c) appropriate buffering such as setbacks, landscaping and fencing will be provided on-site to ensure the compatibility of the use with adjacent residential uses; and,
- d) the signage will respect the character of the area.

In order to implement this policy, the implementing Zoning By-law shall only recognize development existing at the time the by-law comes into effect pursuant to the Planning Act.

#### D1.6.8 RESIDENTIAL SPECIAL POLICY AREA 8

Residential Special Policy Area 8, as shown on Schedule A3 of this Plan, relates to lands that are within the *Low Density Residential Area* designation located on the east side of Main Street North (Hwy. 7) at the intersection with Moore Park Crescent. The property is municipally known as 79 Main Street North and is legally described as Lot 35, Registered Plan 59. Permitted uses at this location may include the following:

- a) one (1) single detached residential dwelling;
- b) day nursery; and,
- c) business and professional offices.

#### D1.6.9 RESIDENTIAL SPECIAL POLICY AREA 9

The lands identified as Residential Special Policy Area 9, as shown on Schedule A6 of this Plan, are designated as *Medium Density Residential Area* and bisected by a tributary of the Black Creek. In the absence of Regulatory Floodplain mapping for the watercourse, only existing uses are permitted. Any development proposal for the site shall include the preparation of detailed floodplain mapping, which shall establish the development limits of the area, and place the limits of the Regulatory Floodplain within the *Greenlands A* land use designation.

#### D1.6.10 RESIDENTIAL SPECIAL POLICY AREA 10

The maximum density shall not exceed 5 units per net residential hectare on lands identified as Residential Special Policy Area 10, as shown on Schedule A3 of this Plan, in recognition of the distinctive low density character of the community of Stewarttown.

#### D1.6.11 RESIDENTIAL SPECIAL POLICY AREA 11

The uses permitted on lands identified as Residential Special Policy Area 11, as shown on Schedule A3 of this Plan, are limited to a gas bar, retail store and accessory uses.

#### D1.6.12 RESIDENTIAL SPECIAL POLICY AREA 12

A seniors' residence with a maximum height of 7 storeys and a maximum of 147 suites shall be permitted within the *High Density Residential Area* designation on lands identified as Residential Special Policy Area 12, as shown on Schedule A3 of this Plan.

The main permitted land uses on lands designated as *Medium Density Residential Area* and identified as Residential Special Policy Area 13, as shown on Schedule A3 of this Plan, shall be limited to:

- i) street townhouse dwellings; and,
- ii) block townhouse dwellings.

The maximum permitted density on lands identified as Residential Special Policy Area 13 shall be 35 units per net residential hectare and the maximum height shall be 3 storeys.

Prior to any Site Plan and/or Condominium Plan Approval being granted for development on the lands identified as Residential Special Policy Area 13, any sight line issues at the intersection of Lindsay Court and Trafalgar Road shall be resolved to the satisfaction of the Town and Halton Region.

The lands designated as *Medium Density Residential Area* and identified as Residential Special Policy Area 13, as shown on Schedule A3 of this Plan, shall be developed in accordance with a Comprehensive Development Plan (CDP).

A CDP has been filed in support of the Official Plan Amendment and has formed the basis of this amendment and related Zoning By-law Amendment. The CDP shall be finalized prior to Site Plan and/or Condominium Plan Approval being granted for any development on the lands identified as Residential Special Policy Area 13 and shall update information on the following:

- a) the appropriate phasing of development;
- b) proposed built form elements including type, height, massing and location of all buildings and structures;
- c) the proposed location of an appropriate vehicular access point along Lindsay Court;
- d) the proposed emergency access provisions, to the satisfaction of MTO and the Town;
- e) the proposed location of parking areas and driveways;
- f) how development can be designed in an appropriate manner that addresses the site location in the vicinity of two major arterial roads and a railway line;
- g) the provision of parkland through a combination of cash-in-lieu of parkland and the dedication of a minimum of 0.2 hectares (0.50 acre) for local parkland, located adjacent to Lindsay Court, in accordance with Section F7 of this Plan;
- h) proposed streetscape components and the location of proposed landscaping; and,
- i) potential pedestrian connection from the lands identified as Residential Special Policy Area 13 and/or Lindsay Court to McCullough Crescent, over the Town-owned block, legally described as Block 209, Plan 20M-575, located immediately east of the subject lands.

The CDP shall generally conform to the policies set out in Section F2 (Urban Design) of this Plan. The CDP shall be implemented through development applications that are in general conformity with this Plan.

In order to implement this policy, the implementing Zoning By-law shall contain a Holding Provision in accordance with Section G4.2 of this Plan.

**D1.6.14 RESIDENTIAL SPECIAL POLICY AREA 14**

A seniors residence with a maximum height of 6 storeys and a maximum of 115 units (suites) shall be permitted within the High Density Residential Area designation on lands identified as Residential Special Policy Area 14, as shown on Schedule A3 of this Plan.

**D1.6.15 RESIDENTIAL SPECIAL POLICY AREA 15**

A maximum of 16 residential townhouses, with a maximum height of 1.5-storeys, shall be permitted within the Medium Density Residential Area designation on lands identified as Residential Special Policy Area 15, as shown on Schedule A6 of this Plan.

**D1.6.18 RESIDENTIAL SPECIAL POLICY AREA 18**

A maximum of 9 residential townhouses, with a maximum height of 2-storeys, shall be permitted within the Medium Density Residential Area designation on lands identified as Residential Special Policy Area 18, as shown on Schedule A6 of this Plan

**D1.6.19 RESIDENTIAL SPECIAL POLICY AREA 19**

A maximum of 12 affordable housing units with a maximum height of 2 storeys shall be permitted within the Medium Density Residential Area designation on lands identified as Residential Special Policy Area 19, as shown on Schedule A6 of this Plan.

## D2 COMMUNITY AREAS

### D2.1 OBJECTIVES

It is the objective of this Plan to:

- a) encourage commercial development that will provide a full range of goods and services, at appropriate locations, to meet the needs of the Town's residents, employees and businesses;
- b) anticipate and accommodate new trends in retailing at appropriate locations in the Town;
- c) improve the look, feel and vibrancy associated with the built form and public realm in the *Downtown Area*, *Community Node* and the *Corridor Commercial Area*;
- d) encourage and promote development that combines commercial, residential and other land uses to facilitate the more efficient use of urban land and the establishment of a human-scale pedestrian environment;
- e) promote the efficient use of existing and planned infrastructure by creating the opportunity for various forms of commercial and residential intensification, where appropriate;
- f) require a high standard of urban design for development and redevelopment in Community Areas;
- g) establish a comprehensive set of design policies that foster the establishment of an urban environment that is safe, functional and attractive;
- h) ensure that all new development in Community Areas is compatible with adjacent development; and,
- i) ensure that a full range of institutional uses is available to meet the social, cultural and educational needs of Town residents.

### D2.2 LOCATION

The Community Area designation applies to lands that serve primarily as community focal points in Acton and Georgetown. The Community Area is divided into the following designations:

- *Downtown Area*;
- *Community Node*;
- *Secondary Node*; Georgetown Go Station/Mill Street Corridor Area;
- *Corridor Commercial Area*;
- Local Commercial Area;

- *Major Institutional Area; and*
- *Civic Centre Area.*

### D2.3 NEED FOR MARKET IMPACT STUDIES

If an Amendment to this Plan is required to permit the development of new commercial uses, Council shall require the preparation of a market impact study where:

- a commercial use with a gross leasable floor area for Department Store Type Merchandise (DSTM) and/or Food and Convenience Type Merchandise (FCTM) in excess of 4,645 square metres is proposed in the Georgetown Urban Area;
- a commercial use with a gross leasable floor area for DSTM and/or FCTM in excess of 2,322 square metres is proposed in the Acton Urban Area; and,
- a new supermarket or department store is proposed within the Georgetown and Acton Urban Areas.

In addition to the foregoing, Zoning By-law amendment applications that propose the expansion of existing supermarkets shall be accompanied by a market impact study, which may be scoped as appropriate.

The purpose of the market impact study shall be to determine whether a proposal can proceed on the basis of market demand without having a negative impact on the planned function of the commercial designations contained in this Plan. It would not be the intent of the study to assess the impacts of any proposal on the market share of an individual business or interfere with normal market competition.

In order to implement this policy, the implementing Zoning By-law shall restrict the size of commercial uses in each of the appropriate commercial zones, as required.

Council shall consult with the Region and other local municipalities in Halton prior to approving a commercial development whose primary trade area may extend into other local municipalities in Halton.

### D2.4 URBAN DESIGN

It is the intent of this Plan to improve the vibrancy and aesthetics of all Community Areas in the Town. On this basis, the approval of any development proposal pursuant to the Planning Act will be dependent on Council being satisfied that:

- improvements to the aesthetics of the public realm (roads, parks, and sidewalks) will occur as a condition of development, or will be made possible as a result of the development, as appropriate;
- the siting, scale and massing of buildings contributes to a safe and attractive pedestrian environment and streetscape;
- parking facilities are designed to not dominate the streetscape; and,

- d) landscaping is used to buffer adjacent uses and improve the overall aesthetics of the development.

All new development requiring a Planning Act approval shall conform to Section F2 (Urban Design) of this Plan as appropriate.

In addition to the above, the implementing Zoning By-law shall contain provisions relating to building siting, location and massing to implement these same urban design policies.

## D2.5 DESIGNATIONS

### D2.5.1 DOWNTOWN AREA

#### D2.5.1.1 Objectives

It is the objective of this designation to:

- a) maintain and promote Downtown Georgetown and Downtown Acton as focal points for commerce, tourism and pedestrian-scale activity in the Town;
- b) encourage tourism and development in the *Downtown Area* that capitalizes on expected growth in the Region and beyond;
- c) encourage the development of a mix of uses to enhance the character of the *Downtown Area*;
- d) encourage streetscape and façade improvements that revitalize the cultural and historic character of the *Downtown Area*;
- e) further enhance the *Downtown Areas* so they continue to be a source of pride in the Region and beyond;
- f) establish a definitive boundary for the *Downtown Area* within which a mixed use, pedestrian-oriented environment can be enhanced and fostered;
- g) reinforce the importance of the *Downtown Area* by identifying and promoting a complementary area for transitional commercial development; and,
- h) protect the residential character of the area adjacent to the *Downtown Area*.

#### D2.5.1.2 Location

The *Downtown Area* designation as shown on Schedules A3 and A6 to this plan applies to the traditional downtown commercial districts and adjacent areas that contain a mix of residential, institutional and commercial uses in the communities of Georgetown and Acton. The Georgetown Downtown Area designation is governed by the land use and built form policies of the Downtown Georgetown Secondary Plan, contained in Section H7 of this Plan. Within the Acton *Downtown Area* there are four primary sub-areas – *Downtown Core Sub-Area*, *Downtown Residential/Office Sub-Area*, *Downtown Redevelopment Sub-Area* and the *Downtown Medium Density Residential/Commercial Sub-Area*. The sub-areas are shown on Schedules A7 to this Plan.

#### D2.5.1.3 General Policies

In the event of a conflict between the policies of this section and the policies of the Downtown Georgetown Secondary Plan, contained in Section H7 of this Plan, the policies of the Downtown Georgetown Secondary Plan shall prevail.

##### D2.5.1.3.1 New Lots Created by Consent

The creation of new lots by consent to sever may be permitted provided the consent does not limit opportunities for development and/or redevelopment in the future. Consents that will result in the demolition of buildings that are historically or culturally significant will not be permitted. The creation of new lots shall also comply with Section F1.2 (New Lots by Consent) of this Plan.

##### D2.5.1.3.2 Parking

Where a new use cannot accommodate the Town's parking requirement, Council may accept cash-in-lieu where the parking can be reasonably accommodated on the street or off-site, either in municipally owned and/or operated lots or on other private land.

##### D2.5.1.3.3 Drive-Throughs

In keeping with a pedestrian and street-oriented environment, this Plan and the implementing Zoning By-law shall prohibit the establishment of drive-throughs as a principal use or as an accessory use in the *Downtown Area* designation.

##### D2.5.1.3.4 Public Lands

Public lands in the *Downtown Area* include streets, sidewalks, parking lots, parks and administrative or public buildings. It is a policy of this Plan that the Town will partner with the Downtown BIAs and/or other stakeholders to maintain public space in a manner that is complementary to the Downtowns and is consistent with the objectives of this Plan. Specific projects aimed to revitalize public lands and buildings may include the:

- a) provision of streetscape landscaping such as trees, shrubs and flowers;
- b) provision of street furniture, lighting, landscape planters, art and monuments in strategic, pedestrian-oriented locations; and/or,
- c) renovations and/or façade improvements to existing public buildings.

#### D2.5.1.4 Downtown Acton Core Sub-Area

##### D2.5.1.4.1 The Vision for the Acton Downtown Core Sub-Area

It is the intent of this Plan that the *Acton Downtown Core Sub-Area* continue to function as focal points for commerce and hospitality in the Town, accommodating a diverse mix of commercial, residential, cultural and social uses and opportunities.

It is the intent of this Plan that the scale and location of new development in the *Acton Downtown Core Sub-Area* maintain and/or enhance the existing character of the Downtowns. This will be accomplished by encouraging:

- a) the development of diverse land uses in close proximity to each other;
- b) the maximum use of existing buildings in the *Acton Downtown Core Sub-Area* to accommodate a wide range of uses, with an emphasis on using upper level space for offices, residential and accommodation uses;
- c) the establishment and maintenance of a streetscape that is geared to the pedestrian;
- d) the preservation and enhancement of the cultural and historic features that exist in the *Acton Downtown Core Sub-Area*; and,
- e) partnerships and collaboration between the Town and service delivery groups such as the Downtown Acton BIA, the Chamber of Commerce and Heritage Halton Hills.

#### D2.5.1.4.2 Permitted Uses

Permitted uses in the *Acton Downtown Core Sub-Area* designation are limited to:

- a) retail and service commercial uses;
- b) medical offices;
- c) hotels and bed and breakfast establishments;
- d) places of entertainment;
- e) day nurseries;
- f) private and commercial schools;
- g) places of worship and other institutional uses;
- h) private recreational uses, such as banquet halls and private clubs;
- i) restaurants;
- j) libraries, museums and art galleries;
- k) adult specialty stores;
- l) small-scale supermarkets and specialty food stores not exceeding 1,393 square metres of gross leasable floor area;
- m) residential uses including long term care facilities and retirement homes; and,
- n) home occupation uses in accordance with Section D1.3.1.4 of this Plan.

#### D2.5.1.4.3 New Development and Redevelopment Policies

The following policies are intended to guide proposals for new development or redevelopment in the *Acton Downtown Core Sub-Area* designation:

- a) Land assembly is encouraged to create larger, more viable development parcels.



- b) In order to maintain a consistent façade, the maximum height of buildings fronting on Main Street and Mill Street in Acton shall be four storeys. Building heights greater than four storeys may be permitted on the rear portions of buildings, or at intersections subject to the approval of a zoning by-law amendment.
- c) All development proposals requiring a Planning Act approval may be required to restore or enhance their building façade in order to maintain the architectural character and identity of the *Acton Downtown Core Sub-Area* in accordance with the following guidelines:
  - i) Original architectural details and features should be restored.
  - ii) Where an existing building lacks significant architectural detail or a new building is to be built on a vacant lot, the façade should be representative of or consistent with adjacent architectural styles.
  - iii) Building materials that are not in keeping with the architectural character of the Downtowns will be discouraged.
  - iv) Traditional signage and lighting is preferred over fluorescent sign boxes and corporate signage.
  - v) The façade should incorporate broad window treatments at street level to maintain an open, pedestrian friendly environment.

#### D2.5.1.4.4 Residential Uses

The following policies apply to residential uses:

- a) Main Street and Mill Street in Acton are the main streets in the *Acton Downtown Area*. These areas are intended to be the focal point for commercial and business activity in the Acton Downtown and this Plan will protect these areas for street level commercial uses. New residential uses, including apartments as well as small-scale inns and bed and breakfast establishments are generally directed to the upper floor of buildings on these streets.
- b) New free-standing residential buildings, long-term care facilities and retirement homes, apartments as well as small-scale inns and bed and breakfast establishments are permitted at street level on side streets within the *Acton Downtown Core Sub-Area* designation. These uses are permitted subject to an amendment to the implementing zoning by-law and provided Council is satisfied that:
  - i) the proposed use will enhance urban life;
  - ii) parking areas for the use are not located at the front of the building;
  - iii) the proposed use is pedestrian in scale; and,
  - iv) the building is designed to blend in and complement adjacent buildings.
- c) The maximum permitted density for townhouse, multiple and apartment dwellings on a lot shall not exceed 100 units per net residential hectare based on the proportion of the overall site dedicated to the residential use and the maximum building height shall not exceed six storeys in Downtown Acton. The development of new townhouse, multiple and apartment dwellings and

long-term care facilities and retirement homes shall require an amendment to the implementing Zoning By-law. Prior to approving such an amendment, Council shall be satisfied that:

- i) the lot is suitable for the proposed building;
- ii) the built form respects the character of and can be suitably integrated with adjacent residential neighbourhoods, in terms of height and massing;
- iii) the use will not cause traffic hazards or an unacceptable level of congestion on surrounding roads;
- iv) the development can easily be accessed by public transit (if available);
- v) the development is located on a site that has adequate land area to incorporate required parking, recreational facilities, landscaping and buffering on-site;
- vi) municipal water and wastewater services are adequate and available;
- vii) the development provides additional housing choices in the Town; and,
- viii) the *Acton Downtown Area* will benefit from the increase in housing in the area.

The maximum height and/or density of residential development may be increased subject to Section G4.3 of this Plan.

#### D2.5.1.4.5 Implementing Zoning By-law

All lands that are used for commercial purposes shall be placed in an appropriate commercial zone in the implementing Zoning By-law. All other uses shall be placed in appropriate zones. The zoning for individual sites may not allow for the full range of permitted uses or the maximum densities and height specified under the *Acton Downtown Core Sub-Area* and the *Historic Main Street Area*. The zoning by-law shall also limit the location of adult specialty stores to:

- a) lands with frontage or flankage on Main Street between Church Street and Cross Street in the Community of Georgetown;
- b) lands with frontage or flankage on Mill Street generally between Main Street and John Street in the Community of Acton; and,
- c) lands with frontage or flankage on Main Street between Church Street and Knox Street and St. Alban's Drive in the Community of Acton.

#### D2.5.1.5 Downtown Acton Complementary Sub-Area

##### D2.5.1.5.1 The Vision for the Downtown Complementary Sub-Area

It is the intent of this Plan that the *Acton Downtown Complementary Sub-Area* functions as a transitional area between the lands within the *Acton Downtown Core Sub-Area* and established and stable residential neighbourhoods. The intent of this designation is to accommodate small-scale, commercial and residential uses that can utilize existing house form buildings to maintain the character of the area and be complementary to the mixed-use, pedestrian oriented focus of the Downtowns. Shopping centres,

automotive commercial uses, and commercial uses requiring extensive sites and/or outdoor storage are not permitted within this designation.

#### D2.5.1.5.2 Permitted Uses

Permitted uses in the *Acton Downtown Complementary Sub-Area* designation are limited to:

- a) residential uses including special needs housing in accordance with Section D1.3.1.8 of this Plan;
- b) home occupations in accordance with Section D1.3.1.4 of this Plan;
- c) medical offices;
- d) inns and bed and breakfasts;
- e) small-scale restaurants;
- f) retail and service commercial uses;
- g) libraries, museums and art galleries;
- h) day nurseries;
- i) private and commercial schools; and,
- j) places of worship and other institutional uses.

#### D2.5.1.5.3 Residential Uses

Land assembly is encouraged to create more viable development parcels. The maximum permitted density for new townhouse, multiple, apartment dwellings and special needs housing on a lot shall not exceed 30 units per net residential hectare based on the proportion of the overall site dedicated to the residential use and the maximum building height shall not exceed three storeys. The development of new townhouse, multiple and apartment dwellings shall require an amendment to the implementing Zoning By-law, and have appropriate regard for the Urban Design Guidelines contained in Appendix X4 to this Plan. Prior to approving such an amendment, Council shall be satisfied that:

- a) the existing building form is utilized to the greatest degree possible;
- b) the scale of the use does not detract from the residential character of the area;
- c) the use complements other uses in the *Downtown Area*;
- d) the building and/or the general appearance of the site will be improved from an urban design perspective;
- e) adequate on-site parking is provided;
- f) the use will not cause traffic hazards or an unacceptable level of congestion on surrounding roads; and,
- g) municipal water and wastewater services are adequate and available.

#### D2.5.1.5.4 Commercial Uses

The development of new commercial uses shall require an amendment to the implementing Zoning By-law, and have appropriate regard for the Urban Design Guidelines contained in Appendix X4 to this Plan. Prior to approving such an amendment, Council shall be satisfied that:

- a) the residential character of the building is maintained;
- b) the scale and type of use does not detract from the residential character of the area;
- c) the use complements other uses in the *Downtown Area*;
- d) the use will not cause traffic hazards;
- e) appropriate buffering such as setbacks, landscaping and fencing will be provided on-site to ensure the compatibility of the use with adjacent residential uses;
- f) the signage will respect the residential character of the area;
- g) an appropriate combination of on-site and off-site parking is available to service the proposed use, with all on-site parking on the lot being in the interior side and rear yards; and,
- h) municipal water and wastewater services are adequate and available.

#### D2.5.1.5.5 Implementing Zoning By-law

All lands that are used for commercial purposes shall be placed in an appropriate commercial zone in the implementing Zoning By-law. Other uses shall be placed in appropriate zones as required. The zoning for individual sites may not allow for the full range of permitted uses or the maximum densities and height specified under the *Downtown Complementary Sub-Area*.

#### D2.5.1.6 Downtown Residential/Office Sub-Area

##### D2.5.1.6.1 The Vision for the Downtown Residential/Office Sub-Area

It is the intent of this Plan that the *Downtown Residential/Office Sub-Area* function primarily as a residential area, while providing opportunities to accommodate complementary office uses that benefit from proximity to the Downtown Core. The intent of this designation is to accommodate business offices as well as residential uses within the existing house form buildings, to maintain the character of the area and be complementary to the mixed-use, pedestrian oriented focus of the Downtown.

##### D2.5.1.6.2 Permitted Uses

Permitted uses in the *Downtown Residential/Office Sub-Area* designation are limited to:

- a) low density residential uses in accordance with Sections D1.3.1.1 and D1.3.1.2;
- b) special needs housing in accordance with Section D1.3.1.8 of this Plan;
- c) home occupations in accordance with Section D.1.3.1.4 of this Plan;
- d) business offices;

- e) bed and breakfasts;
- f) day nurseries;
- g) existing places of worship; and,
- h) existing retail and service commercial uses.

#### D2.5.1.6.3 Business Offices

The development of new business office uses shall require an amendment to the implementing Zoning By-law, and have appropriate regard for the applicable urban design policies of this Plan. Prior to approving such an amendment, Council shall be satisfied that:

- a) the residential character of the building is maintained;
- b) the scale of business office does not detract from the residential character of the area;
- c) the use will not cause traffic hazards;
- d) appropriate buffering such as setbacks, landscaping and fencing will be provided on-site to ensure the compatibility of the use with adjacent residential uses;
- e) the signage will respect the residential character of the area;
- f) adequate on-site parking is available to service the proposed use, with all on-site parking being provided in the interior side and rear yards; and,
- g) municipal water and wastewater services are adequate and available.

#### D2.5.1.6.4 Implementing Zoning By-law

All lands that are currently used for commercial purposes shall be placed in an appropriate commercial zone in the implementing Zoning By-law. Any new business office shall require an amendment to the implementing Zoning By-law.

#### D2.5.1.7 Downtown Medium Density Residential/Commercial Sub-Area

##### D2.5.1.7.1 The Vision for the Downtown Medium Density Residential/Commercial Sub-Area

To recognize the potential for the development of the lots fronting onto the *Downtown Redevelopment Sub-Area* for medium density residential and/or commercial uses to complement the adjacent high density area. The intent of this designation is to be complementary to the mixed-use, pedestrian oriented focus of the Downtown and the GO Station.

##### D2.5.1.7.2 Permitted Uses

Permitted uses in the Downtown Medium Density Residential/Commercial Sub-Area designation are limited to:

- a) existing single detached dwellings;
- b) duplex dwellings;

- c) triplex dwellings;
- d) quattroplex dwellings;
- e) multiple dwellings;
- f) street townhouse dwellings;
- g) commercial/residential or office use in a single detached dwelling or street townhouse dwelling;
- h) office use in a permitted dwelling; and,
- i) existing restaurant uses.

#### D2.5.1.7.3 Residential Uses

The permitted density range shall be 21-50 units per net residential hectare based on the proportion of the overall site dedicated to the residential use and the maximum building height shall not exceed three storeys. The development of new residential dwellings shall require an amendment to the implementing Zoning By-law, and have appropriate regard for the applicable urban design policies of this Plan. Prior to approving such an amendment, Council shall be satisfied that:

- a) the use complements other uses in the *Downtown Area*;
- b) the building and/or the general appearance of the site will be improved from an urban design perspective;
- c) adequate on-site parking is provided;
- d) the use will not cause traffic hazards or an unacceptable level of congestion on surrounding roads; and,
- e) municipal water and wastewater services are adequate and available.

#### D2.5.1.7.4 Commercial Uses

The development of new commercial uses shall require an amendment to the implementing Zoning By-law, and have appropriate regard for the Urban Design Guidelines contained in Appendix X4 to this Plan. Prior to approving such an amendment, Council shall be satisfied that:

- a) the use will contribute to the vibrancy of the area;
- b) the use can be easily integrated with other uses on lands within the *Downtown Medium Density Residential/Commercial Sub-Area* designation and the *Downtown Residential/Office Sub-Area*;
- c) the use complement other uses in the *Downtown Area*;
- d) appropriate buffering such as setbacks, landscaping and fencing will be provided on-site to ensure the compatibility of the use with adjacent residential uses;
- e) the use will not cause traffic hazards or an unacceptable level of congestion on surrounding roads;

- f) municipal water and wastewater services are adequate and available; and,
- g) adequate on-site parking is available to service the proposed use, with all on-site parking being provided in the interior side and rear yards.

#### D2.5.1.7.5 Implementing Zoning By-law

The implementing Zoning By-law shall recognize the existing restaurant use. Any new residential or commercial use shall require an amendment to the implementing Zoning By-law.

#### D2.5.1.8 Acton Downtown Redevelopment Sub-Area

##### D2.5.1.8.1 The Vision for the Acton *Downtown Redevelopment Sub-Area*

It is the intent of this Plan that the Acton *Downtown Redevelopment Sub-Area* become the focus for higher density residential uses in the Acton *Downtown Area*, together with the complementary commercial or institutional uses in proximity to the *Downtown Core Sub-Area* and the future Acton GO Station.

##### D2.5.1.8.2 Density and Height

The density range shall be 75 to 100 units per net residential hectare based on the portion of the overall site dedicated to the residential use. The building height range for high density residential uses shall be four to six storeys, with the minimum height for individual development sites determined by the Comprehensive Development Plan, subject to section D2.5.1.8.5. The minimum density for developments consisting solely of street or block townhouses shall be 21 to 50 units per net hectare. The maximum density and/or height may be increased subject to Section G4.3 of this Plan.

##### D2.5.1.8.3 Main Permitted Uses

Permitted uses in the Acton *Downtown Redevelopment Sub-Area* designation are limited to:

- a) multiple and apartment dwellings;
- b) long-term care facilities and retirement homes;
- c) street or block townhouse dwellings; and,
- d) retail uses in existing buildings.

##### D2.5.1.8.4 Complementary Uses

Complementary uses that may be permitted on the first floor of a residential building include the following:

- a) retail and service commercial uses;
- b) medical offices;
- c) day nurseries;
- d) places of worship and other institutional uses; and,

- e) restaurants.

#### D2.5.1.8.5 New Development and Redevelopment Policies

The following policies are intended to guide proposals for new development or redevelopment in the Acton *Downtown Redevelopment Sub-Area*:

- a) Land assembly is encouraged to create larger, more viable development parcels;
- b) The development of any new building or redevelopment of any parcel of land within the Acton *Downtown Redevelopment Sub-Area* designation shall be subject to an amendment to the implementing Zoning By-law. Prior to considering such an application, a Comprehensive Development Plan (CDP) for all contiguous lands within the designation shall be prepared and approved. Matters to consider in preparing the CDP include:
  - i) the nature of the proposed uses;
  - ii) the location of all main buildings and structures;
  - iii) the location of parking areas and driveways;
  - iv) the extent to which the proposed uses complement the uses in the *Downtown Core Sub-Area*;
  - v) the relationship of buildings in close proximity to the rail corridor to minimize noise and vibration;
  - vi) consideration of the topography of the area in determining appropriate building height, design and massing, by ensuring that building height increases correspond with elevation increases, and that building heights respect the skyline of the area;
  - vii) consideration of the relationship between the proposed new development and existing, adjacent *Low Density Residential Areas* and the *Downtown Medium Density Residential/Commercial Sub-Area* to minimize potential height and massing impacts and to protect privacy;
  - viii) consideration of appropriate conservation options for any heritage buildings that are determined to have heritage value;
  - ix) the uses will not cause traffic hazards or an unacceptable level of congestion on surrounding roads;
  - x) the means by which the uses will be functionally integrated and supportive of the planned function of the *Downtown Core Sub-Area*; and,
  - xi) the location of proposed landscaping.

The CDP shall generally conform to the policies set out in Section F2 (Urban Design) of this Plan and have appropriate regard for the Urban Design Guidelines contained in Appendix X4 of this Plan. The CDP shall be implemented through development applications that are in general conformity with the CDP.



- c) Council may waive the requirement for a CDP provided that it is demonstrated that the proposed development is in keeping with the vision for the Acton *Downtown Redevelopment Sub-Area* and subject to the following criteria:
  - i) the development will not compromise the planned function of the designation;
  - ii) the uses will contribute to the vibrancy of the area;
  - iii) the uses can be easily integrated with other uses on lands within the Acton *Downtown Redevelopment Sub-Area* designation;
  - iv) the development will generally conform with Section F2 (Urban Design) of this Plan and have appropriate regard for the Urban Design Guidelines contained in Appendix X4 of this Plan as set out in an Urban Design Brief submitted to the Town in support of the development application;
  - v) elements of the public realm will be improved as a condition of development/redevelopment;
  - vi) the uses can be easily accessed by public transit if available;
  - vii) the uses will not cause traffic hazards or an unacceptable level of congestion on surrounding roads; and,
  - viii) municipal water and wastewater services are adequate and available.
- d) Prior to the approval of the CDP by the Town, or a decision to waive the requirement for a CDP in accordance with the criteria set out in sub-section (c), the implementing Zoning By-law shall only recognize development existing at the time the by-law comes into effect pursuant to the Planning Act. Following approval of the CDP, uses shall be placed in appropriate zones as required. The zoning for individual sites may not allow for the full range of permitted uses or the maximum densities and height specified under the Acton *Downtown Redevelopment Sub-Area*.

#### D2.5.1.9 Special Policy Areas

Special Policy Areas apply to those lands within the *Downtown Area* that are subject to a land use policy that is specific to a property or area. All other relevant policies of this Plan are applicable unless otherwise modified or exempted by the provisions applicable to each Special Policy Area.

##### D2.5.1.9.1 Special Policy Area 1

In addition to the uses permitted by Section D2.5.1.4.2 of this Plan, a medical clinic, including medical and dental offices, clinic suites, associated medical uses, a pharmacy and a café, are permitted on lands designated as *Downtown Core Sub-Area* and identified as Special Policy Area 1, as shown on Schedule A7 of this Plan. The development shall generally conform to the policies set out in Section F2 (Urban Design) and have appropriate regard for the Urban Design Guidelines contained in Appendix X4 to this Plan.

##### D2.5.1.9.2 Special Policy Area 2

In addition to the uses permitted by Section D2.5.1.8.2 of this Plan, one semi-detached dwelling is permitted on lands designated *Downtown Medium Density Residential/Commercial Sub-Area* and identified as Special Policy Area 2, as shown on Schedule A7 of this Plan. The development shall generally

conform to the policies set out in Section F2 (Urban Design) and have appropriate regard for the Urban Design Guidelines contained in Appendix X4 to this Plan.

#### D2.5.1.9.3 Special Policy Area 3

The maximum permitted residential density shall not exceed 40 units per net hectare.

### D2.5.2 COMMUNITY NODE

#### D2.5.2.1 Objectives

It is the objective of this designation to:

- a) maintain and enhance the function of this area as the major multi-use, multi-purpose commercial area of the Town;
- b) foster development and redevelopment that provides a diverse range of retail, service, community, institutional and recreational uses serving all or a major part of the Town;
- c) encourage the consolidation, intensification and expansion of major commercial uses in this area;
- d) encourage the redevelopment of existing major commercial uses to foster a more pedestrian-oriented environment; and,
- e) encourage the development of complementary residential and institutional uses to capitalize on the location of the area and existing infrastructure.

#### D2.5.2.2 Location

The *Community Node* designation as shown on Schedules A3 and A5 to this Plan applies to lands that are centred on the Guelph Street/Mountainview Road intersection in Georgetown. Within the *Community Node* designation are three commercial sub-areas – *Major Commercial Sub-Area*, *Secondary Commercial Sub-Area* and *Mixed-Use Sub-Area*. Specific land use policies are incorporated under this section of the Plan for these sub-areas.

Other sub-areas within the *Community Node* include the *Major Institutional Sub-Area*, Medium Density Residential Sub-Area, High Density Residential Sub-Area and Major Parks and Open Space Sub-Area. Development within these sub-areas shall be in accordance with the objectives of this section and the other policies of this Plan.

Permitted uses and development policies for the *Major Institutional Sub-Area* shall be in accordance with Section D2.5.6 of the Plan. Permitted uses and density and height requirements for the Medium Density and High Density Residential Sub-Areas shall be subject to Sections D1.3.2 and D1.3.3 respectively of this Plan. Permitted uses and development policies for the Major Parks and Open Space Sub-Area shall be in accordance with Section B2 of this Plan.

#### D2.5.2.3 Major Commercial Sub-Area

##### D2.5.2.3.1 The Vision for the *Major Commercial Sub-Area*

It is the intent of this Plan that the lands within the *Major Commercial Sub-Area* continue to be the main focus of retail activity in the Town. As the Town continues to grow, it is expected that there will be pressure to develop new retail and service uses and expand and intensify existing retail and service uses. In addition, it is the intent of this Plan to encourage the development of complementary medium and higher density residential uses in this area to capitalize on the visibility and function of the area and to efficiently use existing infrastructure.

Given the prime location and visibility of the lands, it is the intent of this Plan that any development and redevelopment in this area be carefully planned to improve the aesthetics, vibrancy and human-scale of the area. On this basis, it is the intent of this Plan that:

- a) new buildings, particularly smaller buildings, be located close to the street to better define the street edge;
- b) parking areas not dominate the landscape;
- c) taller buildings be encouraged to locate at or near the intersection of Mountainview Road and Guelph Street to highlight the importance of the intersection and provide a focal point;
- d) improvements to the public realm, in the form of landscaping, boulevard treatments and lighting be implemented to improve the aesthetics of Guelph Street; and,
- e) development and redevelopment have appropriate regard for the Urban Design Guidelines contain in Appendix X4 to this Plan.

#### D2.5.2.3.2 Permitted Uses

Permitted uses within the *Major Commercial Sub-Area* designation are limited to:

- a) retail and service commercial uses;
- b) supermarkets and specialty food stores;
- c) department stores;
- d) medical offices;
- e) hotels and convention centres;
- f) places of entertainment;
- g) day nurseries;
- h) private and commercial schools;
- i) commercial fitness centres;
- j) private recreational uses, such as banquet halls and private clubs;
- k) restaurants;
- l) adult specialty stores;

- m) complementary multiple and apartment dwellings including long-term care facilities and retirement homes subject to D2.5.2.3.3; and,
- n) home occupations in accordance with Section D1.3.1.4 of this Plan.

#### D2.5.2.3.3 Development and Redevelopment Policies

Complementary multiple and apartment dwellings, long-term care facilities and retirement homes and institutional and office uses in free-standing buildings that are in keeping with Section D2.5.2.3.1 may be permitted subject to an amendment to the implementing Zoning By-law. Prior to considering such an application, a Comprehensive Development Plan (CDP) shall be prepared to the satisfaction of Council. The CDP shall generally conform to the policies set out in Section F2 (Urban Design) of this Plan and be implemented through development applications that are in general conformity with that Plan. The CDP shall include:

- a) a detailed overall land use plan, identifying the location of all of the proposed uses;
- b) a detailed phasing plan that describes the sequencing of development and the timing of any infrastructure improvements;
- c) a plan showing the location of any public lands that may be dedicated to the Town as part of the approval process;
- d) the proposed built form elements including type, height, massing and location of all main buildings and structures;
- e) the location of appropriate access points onto the abutting road network;
- f) the location of pedestrian, vehicular and service circulation and access and parking areas in the context of an overall parking management strategy; and,
- g) urban design guidelines, proposed streetscape components and the location of proposed landscaping.

Council may waive the requirement for a CDP for proposals that are minor in nature provided that it is demonstrated that long term development options for the property are not precluded. Prior to approving any application for non-commercial uses, Council shall be satisfied that:

- a) the development will not unduly compromise the planned function of the designation;
- b) the uses will contribute to the vibrancy of the area;
- c) the uses can be easily integrated with other uses on lands within the *Major Commercial Sub-Area* designation;
- d) the development will generally conform with Section F2 (Urban Design) of this Plan;
- e) elements of the public realm will be improved as a condition of development/redevelopment;
- f) the uses are designed and located to minimize impacts on adjacent residential areas;
- g) the uses can easily be accessed by public transit if available;

- h) the uses will not cause traffic hazards or an unacceptable level of congestion on surrounding roads; and,
- i) municipal water and wastewater services are adequate and available.

The maximum density for residential development shall be 100 units per net residential hectare based on the proportion of the overall site dedicated to the residential use and the maximum height of buildings shall be 8 storeys. The maximum height and/or density of residential development may be increased subject to Section G4.3 of this Plan.

#### D2.5.2.3.4 Implementing Zoning By-law

All lands that are used for commercial purposes shall be placed in an appropriate commercial zone in the implementing Zoning By-law. The zoning for individual sites may not allow for the full range of permitted uses or the maximum densities and height specified under the *Major Commercial Sub-Area*.

#### D2.5.2.4 Secondary Commercial Sub-Area

##### D2.5.2.4.1 The Vision for the Secondary Commercial Sub-Area

At the time this Plan was adopted, the lands designated *Secondary Commercial Sub-Area* were the site of a number of free-standing commercial uses and small plazas on relatively small lots. Given the location of many of these properties at one of the prime intersections in the Georgetown Urban Area, it is the long-term intent of this Plan to encourage the comprehensive redevelopment of the lands within this designation to provide additional opportunities for retail and complementary residential and institutional development. It is further the intent of this Plan that development and redevelopment in this area be carefully planned to improve the aesthetics, vibrancy and pedestrian-scale of the area.

##### D2.5.2.4.2 Permitted Uses

Permitted uses within the *Secondary Commercial Sub-Area* designation are limited to:

- a) retail and service commercial uses;
- b) medical offices;
- c) hotels and convention centres;
- d) places of entertainment;
- e) day nurseries;
- f) private and commercial schools;
- g) supermarkets existing at the date of the adoption of this Plan;
- h) specialty food stores;
- i) health care facilities;
- j) private recreational uses, such as banquet halls and private clubs;
- k) commercial fitness centres;

- l) automotive commercial uses excluding motor vehicle body shops, motor vehicle wreckers and motor vehicle service stations;
- m) restaurants;
- n) adult specialty stores;
- o) multiple and apartment dwellings; and,
- p) long-term care facilities and retirement homes.

#### D2.5.2.4.3 Development and Redevelopment Policies

The development of any new building or the redevelopment of any parcel of land within the *Secondary Commercial Sub-Area* designation shall be subject to an amendment to the implementing Zoning By-law Amendment. Prior to considering such an application, a Comprehensive Development Plan (CDP) for all contiguous lands in the *Secondary Commercial Sub-Area* designation shall be prepared to the satisfaction of Council. The CDP shall include information on:

- a) the nature of the proposed uses;
- b) the proposed built form elements including type, height, massing and location of all main buildings and structures;
- c) the location of appropriate access points along Guelph Street and Mountainview Road;
- d) the proposed location of parking areas and driveways; and,
- e) proposed streetscape components and the location of proposed landscaping.

The CDP shall generally conform to the policies set out in Section F2 (Urban Design) of this Plan and have appropriate regard for the Urban Design Guidelines contained in Appendix X4 to this Plan. The CDP shall be implemented through development applications that are in general conformity with that Plan.

Council may waive the requirement for a CDP provided that it is demonstrated that the proposed development is in keeping with the vision for the *Secondary Commercial Sub-Area* and subject to the following criteria:

- a) the development will not compromise the planned function of the designation;
- b) the uses will contribute to the vibrancy of the area;
- c) the uses can be easily integrated with other uses on lands within the *Secondary Commercial Sub-Area* designation;
- d) the development will generally conform with Section F2 (Urban Design) of this Plan and have appropriate regard for the Urban Design Guidelines contained in Appendix X4 to this Plan as set out in an Urban Design Brief submitted to the Town in support of the development application;
- e) elements of the public realm will be improved as a condition of development/redevelopment;
- f) the uses can be easily accessed by public transit if available;

- g) the uses will not cause traffic hazards or an unacceptable level of congestion on surrounding roads; and,
- h) municipal water and wastewater services are adequate and available.

#### D2.5.2.4.4 Residential Uses

The development of multiple and apartment dwellings and long-term care facilities and retirement homes shall require an amendment to the implementing Zoning By-law. Prior to considering such an amendment, Council shall be satisfied that the criteria set out in Section D2.5.2.3.3 have been met.

The maximum density for residential development shall be 100 units per net residential hectare based on the proportion of the overall site dedicated to residential use and the maximum height of buildings shall be 8 storeys. The maximum height and/or density of residential development may be increased subject to Section G4.3 of this Plan.

#### D2.5.2.4.5 Implementing Zoning By-law

Prior to the approval of the CDP by the Town or a decision to waive the requirement for a CDP in accordance with the criteria set out in Section D2.5.2.4.3, the implementing Zoning By-law shall only recognize development that exists at the time the by-law comes into effect pursuant to the Planning Act. Following approval of the CDP, lands that are used for commercial purposes shall be placed in an appropriate commercial zone in the implementing Zoning By-law. Other uses shall be placed in appropriate zones as required. The zoning for individual sites may not allow for the full range of permitted uses or the maximum densities and height specified under the *Secondary Commercial Sub-Area*.

#### D2.5.2.5 Mixed-Use Sub-Area

##### D2.5.2.5.1 The Vision for the Mixed-Use Sub-Area

At the time this Plan was adopted, the lands designated *Mixed-Use Sub-Area* were the site of a number of free-standing commercial uses and small plazas on relatively small lots. These same lots back on to an established residential area and have depths that may have an impact on development and redevelopment options.

As a result of the development on the lands on the north side of Guelph Street (across from the lands designated *Mixed-Use Sub-Area*) it is anticipated that there will be pressure to upgrade many of the uses and properties in the area in the future. Given the prime location of this area, it is the intent of this Plan that the redevelopment of these lands be encouraged and that residential and institutional uses be introduced into the area in the form of mixed-use developments. Any redevelopment should be designed to have a cohesive and unified streetscape in the area and have a minimal impact on the established residential neighbourhood in the area.

##### D2.5.2.5.2 Permitted Uses

Permitted uses within the *Mixed-Use Sub-Area* designation are limited to:

- a) retail and service commercial uses;
- b) specialty food stores;

- c) medical offices;
- d) small-scale places of entertainment;
- e) day nurseries;
- f) private and commercial schools;
- g) restaurants;
- h) adult specialty stores;
- i) multiple and apartment dwellings; and,
- j) long-term care facilities and retirement homes.

#### D2.5.2.5.3 Development and Redevelopment Policies

The development of any new building or the redevelopment of any parcel of land within the *Mixed-Use Sub-Area* designation shall be subject an amendment to the implementing Zoning By-law. Prior to considering such an application, a Comprehensive Development Plan (CDP) for all contiguous lands in the *Mixed-Use Sub-Area* designation shall be prepared to the satisfaction of Council. Matters to consider in preparing the CDP include:

- a) the nature of the proposed uses;
- b) the proposed built form elements including type, height, massing and location of all main buildings and structures;
- c) the location of appropriate access points along Guelph Street and Mountainview Road;
- d) the proposed location of parking areas and driveways; and,
- e) the location of proposed streetscape components and proposed landscaping.

The CDP shall generally conform to the policies set out in Section F2 (Urban Design) of this Plan and have appropriate regard for the Urban Design Guidelines contained in Appendix X4 to this Plan. The CDP shall be implemented through development applications that are in general conformity with that Plan.

Council may waive the requirement for a CDP provided that it is demonstrated that the proposed development is in keeping with the vision for the *Mixed Use Sub-Area* and subject to the following criteria:

- a) the development will not compromise the planned function of the designation;
- b) the uses will contribute to the vibrancy of the area;
- c) the uses can be easily integrated with other uses on lands within the *Mixed Use Sub-Area* designation;
- d) the development will generally conform with Section F2 (Urban Design) of this Plan and have appropriate regard for the Urban Design Guidelines contained in Appendix X4 to this Plan as set out in an Urban Design Brief submitted to the Town in support of the development application;



- e) elements of the public realm will be improved as a condition of development/ redevelopment;
- f) the uses are designed to minimize impacts on adjacent residential areas;
- g) the uses can be easily accessed by public transit if available;
- h) the uses will not cause traffic hazards or an unacceptable level of congestion on surrounding roads; and,
- i) municipal water and wastewater services are adequate and available.

#### D2.5.2.5.4 Residential Uses

The development of multiple and apartment dwellings and long-term care facilities and retirement homes shall require an amendment to the implementing Zoning By-law. Prior to considering such an amendment, Council shall be satisfied that the criteria set out in Section D2.5.2.3.3 have been met.

The maximum density for residential development shall be 50 units per net residential hectare based on the proportion of the overall site dedicated to the residential use and the maximum height of buildings shall be 4 storeys.

#### D2.5.2.5.5 Implementing Zoning By-law

Prior to the approval of the CDP by the Town or a decision to waive the requirement for a CDP in accordance with the criteria set out in Section D2.5.2.5.3, the implementing Zoning By-law shall only recognize development that exists at the time the by-law comes into effect pursuant to the Planning Act. Following approval of the CDP, lands that are used for commercial purposes shall be placed in an appropriate commercial zone in the implementing Zoning By-law. Other uses shall be placed in appropriate zones as required. The zoning for individual sites may not allow for the full range of permitted uses or the maximum densities and height specified under the *Mixed Use Sub-Area*.

### D2.5.3 SECONDARY NODE

#### D2.5.3.1 Objectives

It is the objective of this designation to:

- a) encourage the development of a central shopping area for residents in the Georgetown South area;
- b) ensure that the lands are comprehensively planned and developed;
- c) encourage the establishment of aesthetically-pleasing and pedestrian-oriented development; and,
- d) ensure that the uses in the *Secondary Node* designation do not detract from the planned function of the *Community Node* designation.

#### D2.5.3.2 Location

The *Secondary Node* designation as shown on Schedule A3 to this Plan applies to lands that are located on the east and west side of Mountainview Road, on the south side of Argyll Road in Georgetown South.

#### D2.5.3.3 The Vision for the Secondary Node

It is the intent of this Plan that the lands included within the *Secondary Node* designation evolve into a central area where goods and services are provided to nearby residents and in which social, recreational and institutional facilities are located for neighbourhood use. This designation also supports multiple unit residential and mixed use development. It is also the intent of this Plan that every effort be made to integrate uses within this designation with surrounding residential areas and are designed to serve as an aesthetically pleasing focal point for the Georgetown South community.

#### D2.5.3.4 Permitted Uses

Permitted uses within the *Secondary Node* designation are limited to:

- a) retail and service commercial uses;
- b) specialty food stores;
- c) medical offices;
- d) commercial fitness centres;
- e) places of entertainment;
- f) day nurseries;
- g) private and commercial schools;
- h) restaurants;
- i) museums, libraries and art galleries;
- j) places of worship;
- k) parks and public recreational uses;
- l) community facilities and centres;
- m) adult specialty stores;
- n) residential uses subject to Section D2.5.3.5 of this Plan;
- o) long-term care facilities and retirement homes; and,
- p) one supermarket.

In addition to the above, interim uses such as parks and playing fields may also be permitted, subject to compatibility with surrounding uses.

#### D2.5.3.5 Residential Uses in the Secondary Node

Residential uses in the *Secondary Node* designation, including long term care facilities and retirement homes are subject to the following policies, in addition to the applicable policies of Section D2.5.3.7 of this Plan:

- a) Permitted residential building forms are limited to multiple unit development such as townhouses, apartments or other innovative housing forms;
- b) The maximum density of residential development shall not exceed 75 units per net residential hectare, based on the proportion of the overall site dedicated to residential use; and,
- c) The maximum building height shall not exceed four storeys, with the highest elements located away from residential areas north of Argyll Road. The maximum height of buildings adjacent to areas designated *Low Density Residential Area* shall generally be stepped down to a lower height or separated by distance or buffers.

#### D2.5.3.6 Commercial Uses in the Secondary Node

Commercial uses in the *Secondary Node* designation are subject to the following policies, in addition to the applicable policies of Section D2.5.3.7 of this Plan:

- a) The maximum gross leasable floor area of retail and service commercial uses shall not exceed 10,700 square metres;
- b) Council may pass by-laws allowing the maximum gross leasable floor area to be exceeded, without amendment to this Plan, providing the use is in keeping with the objectives and vision of the *Secondary Node*, and is supported by a market impact study as described in Section D2.3 of this Plan that demonstrates there will be no negative impact on the planned function of the *Downtown Area* or the *Community Node*, and any other appropriate studies, to the satisfaction of the Town of Halton Hills; and,
- c) New retail uses in which the primary merchandise sold is apparel and accessories shall not exceed 279 square metres of gross leasable floor area. Council may pass zoning by-laws to permit an apparel and accessories store over 279 square metres on land designated *Secondary Node*, subject to the submission of a market impact study as described in Section D2.3 of this Plan, which demonstrates that there will be no negative impact on the planned function of the *Downtown Area* or the *Community Node*.

#### D2.5.3.7 General Development and Redevelopment Policies

The following general development and redevelopment policies apply to lands within the *Secondary Node* designation:

- a) Council shall have regard to Section F2 (Urban Design) of this Plan and any approved Urban Design Guidelines in considering any application pursuant to the Planning Act in this designation.
- b) Prior to the approval of rezoning and site plan applications within the *Secondary Node* designation, a Comprehensive Development Plan (CDP) shall be prepared to establish further

direction on land use and the physical character and form of development. Such a plan shall contain information on the:

- i) proposed land use and density distribution on the overall site;
- ii) relationship between uses and integration of mixed uses;
- iii) nature of the major structural elements including layout of buildings, parking and landscaped areas;
- iv) integration of land uses;
- v) built form elements including type, height and location of buildings;
- vi) street and block layout;
- vii) access to adjacent roads; and,
- viii) streetscape components, pedestrian areas and linkages and connections to the open space system.

Detailed urban design guidelines shall accompany the CDP. The CDP shall be implemented through development applications that are in general conformity with that Plan.

#### D2.5.3.8 Implementing Zoning By-law

Prior to the approval of the CDP by the Town, the implementing Zoning By-law shall only recognize development that exists at the time the by-law comes into effect pursuant to the Planning Act. Following approval of the CDP, lands that are used for commercial purposes shall be placed in an appropriate commercial zone in the implementing Zoning By-law. Other uses shall be placed in appropriate zones as required. The zoning for individual sites may not allow for the full range of permitted uses or the maximum densities and height specified under the *Secondary Node* designation.

#### D2.5.4 **CORRIDOR COMMERCIAL AREA**

##### D2.5.4.1 Objectives

It is the objective of this designation to:

- a) recognize existing linear commercial development that has occurred along Queen Street in Acton and Guelph Street in Georgetown;
- b) encourage the consolidation, intensification and expansion of certain commercial uses in this area; and,
- c) encourage the redevelopment of existing commercial uses to foster a more pedestrian-oriented environment.

##### D2.5.4.2 Location

The *Corridor Commercial Area* designation as shown on Schedules A3 and A6 to this Plan applies to lands on both Queen Street in Acton and Guelph Street in Georgetown.

#### D2.5.4.3 The Vision for the Corridor Commercial Area

It is the intent of this Plan that the *Corridor Commercial Area* on Queen Street in Acton continue to support and complement the planned function of the *Downtown Area* in providing goods and services to the community. Given that growth in the Acton Urban Area is constrained as a result of servicing issues, it is anticipated that only a limited amount of additional commercial development will be required to service the existing and future population. However, there may be pressure to develop a number of new smaller uses and intensify some of the existing uses to respond to consumer demand in the Acton area. Given the location of the *Corridor Commercial Area* at the eastern entrance into Acton, development and redevelopment in this area shall be carefully planned to improve the aesthetics and vibrancy.

It is the intent of this Plan that the *Corridor Commercial Area* on Guelph Street continue to support and complement the planned function of the *Community Node*. In this regard, only uses that are appropriate in a *Corridor Commercial Area* setting shall be permitted on Guelph Street. It is also the intent of this Plan to improve the aesthetics of Guelph Street and in this regard, Council will make every effort through the development and redevelopment process to ensure that development is designed to have a cohesive and unified streetscape in the area.

#### D2.5.4.4 Permitted Uses

Permitted uses within the *Corridor Commercial Area* designation are limited to:

- a) retail and service commercial uses;
- b) medical offices;
- c) hotels and convention centres;
- d) places of entertainment;
- e) day nurseries;
- f) private and commercial schools;
- g) places of worship and other institutional uses;
- h) private recreational uses, such as banquet halls and private clubs;
- i) commercial fitness centres;
- j) supermarkets existing at the date of the adoption of this Plan;
- k) specialty food stores;
- l) automotive commercial uses excluding motor vehicle body shops and motor vehicle wreckers;
- m) restaurants;
- n) adult video stores; and,
- o) adult specialty stores.

#### D2.5.4.5 Floor Space Restrictions on Certain Uses

New retail uses in which the primary merchandise sold is apparel and accessories shall not exceed 279 square metres of gross leasable floor area. Council may pass zoning by-laws to permit an apparel and accessories store over 279 square metres on land designated *Corridor Commercial Area* subject to the submission of a market impact study as described in Section D2.3 of this Plan which demonstrates that there will be no negative impact on the planned function of the *Downtown Area* and the *Community Node*.

#### D2.5.4.6 General Development and Redevelopment Policies

Prior to considering any application for development or redevelopment requiring a Planning Act approval, Council shall be satisfied that:

- a) the proposed uses will contribute to the vibrancy of the area;
- b) the uses can be easily integrated with other uses on lands within the *Corridor Commercial Area* designation;
- c) the development will generally conform with Section F2 (Urban Design) of this Plan;
- d) elements of the public realm will be improved as a condition of development/redevelopment;
- e) the uses are designed and located to minimize impacts on adjacent residential areas; and,
- f) the application has appropriate regard for the Urban Design Guidelines contained in Appendix X4 to this Plan.

#### D2.5.4.7 Implementing Zoning By-law

Lands that are used for commercial purposes shall be placed in an appropriate commercial zone in the implementing Zoning By-law. Other uses shall be placed in appropriate zones as required. The zoning for individual sites may not allow for the full range of permitted uses as specified under the *Corridor Commercial Area* designation.

#### D2.5.4.8 Special Policy Areas

Special Policy Areas apply to those lands within the *Corridor Commercial Area* that are subject to a land use policy that is specific to a property or area. All other relevant policies of this Plan are applicable unless otherwise modified or exempted by the provisions applicable to each Special Policy Area.

##### D2.5.4.8.1 Special Policy Area 1

The uses permitted are limited to a supermarket not exceeding 2,650m<sup>2</sup> of gross leasable floor area on the lands designated as *Corridor Commercial Area* and identified as Special Policy Area 1, as shown on Schedule A6 of this Plan.

### D2.5.5 LOCAL COMMERCIAL AREA

#### D2.5.5.1 Objectives

It is the objective of this designation to:

- a) provide opportunities for the development of commercial uses that serve neighbouring residential areas; and,
- b) ensure that the size and scale of local commercial uses is compatible with surrounding uses and activities.

#### D2.5.5.2 Location

The Local Commercial Area designation applies to small concentrations of retail and service uses that are located on lands within the Living Area designation. As a result, lands designated Local Commercial Area are not shown on the Schedules to this Plan, but are intended to be identified within the implementing Zoning By-law. However, such areas may be identified conceptually on the schedules to this Plan as part of a Secondary Plan process for newly developing urban areas.

#### D2.5.5.3 The Vision for the Local Commercial Area

It is the intent of this Plan that lands within the Local Commercial Area designation function as areas where neighbouring residents and employees can obtain convenience goods and services without having to travel long distances. Local Commercial Areas shall be designed to blend in and complement adjacent residential uses and be at a scale that does not have an impact on the planned function of other commercial areas in the Town. In addition, such uses shall be designed to provide convenient access to pedestrians.

#### D2.5.5.4 Permitted Uses

Permitted uses within the Local Commercial Area designation are limited to:

- a) retail and service commercial uses;
- b) medical offices;
- c) day nurseries;
- d) private and commercial schools;
- e) small-scale supermarkets not exceeding 1,393 square metres of gross leasable floor area;
- f) specialty food stores;
- g) small scale commercial fitness centres; and,
- h) restaurants.

#### D2.5.5.5 Floor Space Restrictions on Certain Uses

The maximum gross leasable floor area for a Local Commercial Area development shall be 2,000 square metres. No single commercial use shall exceed 500 square metres unless otherwise identified by this Plan. Any proposal to develop uses in excess of the above requirements shall not be considered pursuant to the policies of this section of the Plan.

#### D2.5.5.6 General Development and Redevelopment Policies

Prior to considering any application for establishment of new Local Commercial Areas or the redevelopment of an existing Local Commercial Area, Council shall be satisfied that:

- a) the development is located at the intersection of an Arterial or Collector Road with a Local Road as shown on Schedule B1 to this Plan;
- b) the development will not cause any traffic hazards or an unacceptable level of congestion on surrounding roads;
- c) the development will be planned to provide maximum pedestrian accessibility to surrounding areas;
- d) the development is centrally located within the neighbourhood the development is intended to serve;
- e) the development will generally conform with Section F2 (Urban Design) of this Plan;
- f) elements of the public realm will be improved as a condition of development/redevelopment; and,
- g) the uses are designed and located to minimize impacts on adjacent residential areas through buffering and landscaping treatments.

#### D2.5.5.7 Implementing Zoning By-law

Lands that are used for commercial purposes shall be placed in an appropriate commercial zone in the implementing Zoning By-law. The zoning for individual sites may not allow for the full range of permitted uses as specified under the Local Commercial Area designation.

### D2.5.6 MAJOR INSTITUTIONAL AREA

#### D2.5.6.1 Objectives

It is the objective of this designation to:

- a) recognize and permit major institutional uses which serve the needs of the community; and,
- b) ensure that major institutional uses are properly planned and located to complement adjacent land uses.

#### D2.5.6.2 Location

The *Major Institutional Area* designation, as shown on the Schedules to this Plan, applies to larger institutional uses that were in existence or approved on the date this Plan was adopted by Council. These areas are primarily located within the Acton and Georgetown Urban Areas.

#### D2.5.6.3 Permitted Uses

Permitted uses in the *Major Institutional Area* designation include secondary schools, hospitals, places of worship which have a regional draw, government buildings and similar uses. Residential uses such as



retirement residences, long term care facilities and other complementary accommodation may also be permitted, but only in conjunction with a major institutional use.

#### D2.5.6.4 Development Policies

The development of any new Major Institutional use shall require an Amendment to the Official Plan and Zoning By-law and shall be subject to Site Plan Control. Expansions to existing uses on lands that are already within the *Major Institutional Area* designation may require an amendment to the implementing Zoning By-law and shall be subject to Site Plan Control.

Before considering an Amendment to the Official Plan (if required) and Zoning By-law, Council shall be satisfied that:

- a) the development is located on and accessed by an Arterial or Collector Road as shown on Schedule B1 to this Plan;
- b) the development will not cause any traffic hazards or an unacceptable level of congestion on surrounding roads;
- c) the site is large enough to accommodate the building, landscaping, on-site parking and appropriate buffering; and,
- d) the development is compatible with surrounding land uses.

#### D2.5.6.5 Zoning By-law Implementation

All lands in the *Major Institutional Area* designation shall be placed in an appropriate Institutional Zone in the implementing Zoning By-law. The implementing zoning may limit the size of existing institutional uses to that which existed on the date the by-law comes into effect, to ensure that the criteria set out in Section D2.5.6.4 can be considered by Council if an expansion is proposed.

### D2.5.7 **CIVIC CENTRE AREA**

#### D2.5.7.1 Objectives

It is the objective of this designation to:

- a) recognize the Halton Hills Civic Centre as a key community focal point;
- b) ensure that the lands are comprehensively planned and developed;
- c) encourage the development of a mix of uses to enhance the character of the *Civic Centre Area*;
- d) encourage the provision of multiple family and special needs housing; and,
- e) encourage the establishment of an aesthetically pleasing and pedestrian-oriented development.

#### D2.5.7.2 Location

The *Civic Centre Area* designation as shown on Schedule A3 to this Plan applies to lands bounded by the CNR line, Princess Anne Drive, the *Greenlands* designations associated with a Black Creek tributary and Maple Avenue.

#### D2.5.7.3 The Vision for the Civic Centre Area

At the time this Plan was adopted, other than the Halton Hills Civic Centre, the lands designated *Civic Centre Area* were undeveloped. The vacant lands, scenic attributes and the gateway location present a unique opportunity to encourage the development of a mix of institutional and multiple family residential uses. It is the intent of this Plan to encourage the establishment of aesthetically pleasing and pedestrian oriented development that serves as a community focal point and efficiently uses existing and proposed infrastructure.

#### D2.5.7.4 Permitted Uses

Permitted uses within the *Civic Centre Area* designation are limited to:

- a) Administrative offices for the Town of Halton Hills;
- b) Museums, libraries and art galleries;
- c) Community facilities and centres;
- d) Local parkland and public recreational facilities;
- e) Limited office uses;
- f) Multiple and apartment dwellings; and,
- g) Long term care facilities and retirement homes.

#### D2.5.7.5 Residential Uses

Residential uses in the *Civic Centre Area* designation, including long term care facilities and retirement homes are subject to the following policies, in addition to the applicable policies of Section D2.5.7.6 of this Plan:

- a) Permitted residential building forms are limited to multiple unit development such as townhouses and apartment dwellings or other innovative housing forms and special needs housing. The development of single and semi-detached dwellings is not permitted;
- b) The maximum density and height of Medium Density Residential development shall be subject to Section D1.3.2.2 of this Plan;
- c) The maximum density and height of High Density Residential development shall be subject to Section D1.3.3.2 of this Plan.

#### D2.5.7.6 Development and Redevelopment Policies

Prior to the approval of development applications pursuant to the Planning Act, a Comprehensive Development Plan (CDP) shall be prepared for all of the lands subject to the *Civic Centre Area* designation to the satisfaction of Council. In recognition of the Objectives and Vision for the *Civic Centre Area* as set out in Sections D2.5.7.1 and D2.5.7.3 respectively of this Plan, the CDP process shall include a significant public consultation program. All benefiting landowners shall be encouraged to participate in the establishment of appropriate Terms of Reference and the funding of the CDP. The CDP shall generally

conform to the policies set out in Section F2 (Urban Design) and be implemented through appropriate Planning Act applications that are in general conformity with the CDP. The CDP shall include:

- a) A detailed overall land use plan, identifying the location of all proposed uses;
- b) A detailed phasing plan that describes the sequencing of development and the timing of infrastructure improvements;
- c) A plan showing the location of any public lands that may be dedicated to the Town as part of the approval process;
- d) The proposed built form elements including type, height, massing and location of all main buildings and structures;
- e) overall road pattern for the area; and,
- f) the urban design guidelines.

In the event the benefiting landowners within the *Civic Centre Area* designation are unable to reach appropriate agreements pertaining to the funding and commencement of a CDP within two years of the date of the approval of this Plan by the Region, an individual landowner may proceed with site specific development applications pursuant to the Planning Act subject to the submission of appropriate studies which address Section D2.5.7.6 a) – f) for its site, together with addressing conceptually any off-site matters detailed as part of the requirements of Section D2.5.7.6 a) – f). As part of the consideration of any site specific development applications, the proponent shall demonstrate that i) the development concept shall not preclude the orderly development of adjacent lands within the designation and ii) is in keeping with the Objectives and Vision for the *Civic Centre Area* as set out in Section D2.5.7.1 and D2.5.7.3 respectively of this Plan.

Notwithstanding the foregoing, expansions to the Administrative offices for the Town of Halton Hills as permitted under Section D2.5.7.4 (a); other public uses as permitted under Section D2.5.7.4 (b), (c), and (d); together with any public office uses as permitted under Section D2.5.7.4 (e), may be permitted prior to the preparation of the Comprehensive Development Plan.

#### D2.5.7.7 Zoning By-law Implementation

Prior to the approval of the CDP by the Town, the implementing Zoning By-law shall only recognize development that exists, or has been previously approved through a site-specific by-law prior to the adoption of this Plan, at the time the by-law comes into effect pursuant to the Planning Act. Other lands shall be placed in a Development (D) Zone. Following the approval of CDP, lands that are to be developed for residential purposes shall be placed in appropriate residential zones in the implementing Zoning By-law which shall specify both minimum and maximum densities in accordance with the provisions of the Planning Act. Other uses shall be placed in appropriate zones as required. The zoning of individual sites may not allow for the full range of permitted uses or the maximum densities and height specified under the *Civic Centre Area* designation.

#### D2.5.7.8 Special Policy Areas

Special Policy Areas apply to those lands designated as *Civic Centre Area* that are subject to a land use policy that is specific to a property or area. All other relevant policies of this Plan are applicable unless otherwise modified or exempted by the provisions to each Special Policy Area.

#### D2.5.7.8.1 Special Policy Area 1

The lands designated as *Civic Centre Area* and identified as Special Policy Area 1, as shown on Schedule A3 of this Plan, shall be developed for high density uses in accordance with the following criteria:

- a) The main permitted uses are limited to apartments and special needs housing for seniors and/or the physically disabled. In addition, limited ancillary community and commercial uses such as offices, seniors centre or social club, seniors' day care, convenience commercial and similar uses shall also be permitted;
- b) The maximum permitted density of all uses shall not exceed 38,000 square metres of which a maximum of 20,000 square metres shall be for apartment uses;
- c) The maximum building height shall not exceed 12 storeys. Maximum building heights shall be directed towards those portions of the site which are of lower topographic elevation than surrounding residential lands as further directed by the implementing zoning by-law;
- d) The development may proceed in phases; and,
- e) The lands shall be subject to a (H) Holding Provision in the implementing zoning by-law. Council shall not remove the (H) Holding provision until such time as servicing capacity is identified as being available for all or part of the lands and a development agreement has been entered into with the Town including, among other matters, provisions with respect to the public ownership of the environmentally sensitive valley portion of land owned by the applicant, construction of the collector road, stormwater management and site plan approval.

## D3 EMPLOYMENT AREAS

### D3.1 OBJECTIVES

It is the objective of this Plan to:

- a) ensure that there are sufficient lands available for the creation of diverse employment opportunities at strategic locations;
- b) ensure that the Town is positioned to accommodate new and expanded business activities that provide jobs to local residents;
- c) ensure that new industrial development occurs in an orderly manner and in conjunction with the establishment of appropriate sewage and water services;
- d) streamline the approval process for the development and expansion of employment uses; and,
- e) promote intensification and increased densities in new and existing employment areas, by facilitating compact, transit-supportive built form and minimization of surface parking.

### D3.2 LOCATION

The Employment Area designation recognizes the existing industrial areas in Acton, Georgetown and Mansewood, as well as the emerging Premier Gateway Employment Area, which will continue to be developed for prestige industrial uses to the 2031 planning horizon.

### D3.3 SUPPLY OF LAND

It is the intent of this Plan that an adequate supply of employment land is available at all times in the Town. At the time this Plan was adopted by Council, almost all of the employment land in Georgetown was developed. In Acton, additional development was feasible, but the amount of development that could occur was limited by servicing constraints. The only area that could accommodate large-scale employment growth is located within the Premier Gateway Employment Area.

#### D3.3.1 EMPLOYMENT LAND CONVERSIONS

It is the policy of this Plan to prohibit the conversion of lands within the Employment Area designation to non-employment uses, including major retail uses, unless through a municipal comprehensive review where it has been demonstrated that:

- a) there is a need for the conversion;
- b) the conversion will not compromise the ability of the Town to meet the employment targets contained in Table A1A of this Plan;
- c) the conversion will not adversely affect the overall viability of the Employment Area and achievement of the Built Boundary total intensification target contained in Table D5.1 and the density targets contained in Section D6.2 of this Plan;
- d) the conversion will not compromise the ability of the Town to meet the Regional Phasing specified in Section F10.4 of this Plan;

- e) adequate infrastructure exists or is planned to accommodate the proposed conversion;
- f) the conversion will not negatively impact the suitability of adjacent lands to be used or continue to be used for employment purposes;
- g) the lands are not required in the long-term for employment purposes; and,
- h) cross-jurisdictional issues have been addressed.

### D3.3.2 NON-EMPLOYMENT USES IN EMPLOYMENT AREAS

It is the policy of this Plan to prohibit residential and other non-employment uses, including major retail uses, in the Employment Area designation, except:

- a) to recognize permitted uses within the *Prestige Industrial Area* and *Gateway Area* designations within Phases 1A and 2A as shown on Schedule A8 of this Plan;
- b) for institutional uses as a result of a detailed study that sets limits on, and establishes criteria for, such uses based on the following principles:
  - i) the use is small scale, and collectively such uses within the Employment Area designation do not change the character of that designation;
  - ii) the location and design of the use addresses land use compatibility in a manner based upon Regional Guidelines;
  - iii) the use is located at the periphery of the Employment Area designation; and,
  - iv) such uses do not collectively displace employment from the Employment Area designation to an extent that the supply of land within the Employment Area designation is insufficient to meet the employment target contained in Tables A1A and F10 of this Plan.

## D3.4 DESIGNATIONS

### D3.4.1 GENERAL EMPLOYMENT AREA

#### D3.4.1.1 Objectives

It is the objective of this designation to:

- a) ensure that existing businesses in Acton and Georgetown are retained;
- b) encourage and facilitate the expansion and redevelopment of existing employment uses;
- c) ensure that the *General Employment Area* remains as one of the key components of the Acton and Georgetown Urban Area;
- d) provide opportunities for a diversified economic base which supports a wide-range of economic activities and which takes into account the needs of existing and future businesses;

- e) protect the *General Employment Area* from incompatible uses that may limit the use and/or expansion of existing businesses in the future; and,
- f) provide for the development of appropriate employment uses on private or partial services in the Community of Acton.

#### D3.4.1.2 Location

The *General Employment Area* designation applies to:

- a) lands located to the north of Guelph Street and to the east of Mountainview Road in the Georgetown Urban Area as shown on Schedule A3 to this Plan. Additional lands are located on the south side of Guelph Street between Sinclair Avenue and Delrex Boulevard and at the southeast corner of River Drive and Mountainview Road as shown on Schedule A3 to this Plan;
- b) lands located in the northern portion of the Acton Urban Area on both sides of Main Street North as shown on Schedule A6 to this Plan. Additional lands are located at the corner of Wallace Street and Perth Street and on the east side of Main Street South and Vimy Street as shown on Schedule A6 to this Plan; and,
- c) land located at the northeast corner of Regional Road 25 and 5 Side Road, as shown on Schedule A1 of this Plan. Development within this area is to be phased over the 2021 to 2031 planning period.

#### D3.4.1.3 Main Permitted Uses

The main permitted uses on lands designated *General Employment Area* are limited to the following:

- a) industrial uses within wholly enclosed buildings, such as manufacturing, assembling, processing, fabricating, warehousing, wholesaling and distribution facilities;
- b) industrial uses that involve outdoor storage and/or processing such as concrete batching plants, aggregate transfer stations and contractors' establishments, subject to Section D3.4.1.4.2;
- c) research and training establishments;
- d) computer, electronics and data processing facilities;
- e) printing and associated service establishments;
- f) industrial malls, containing one or more of the uses permitted in this designation; and
- g) cannabis cultivation and processing subject to Section D3.4.1.4.8.

No use that causes, or is likely to cause air pollution, offensive odours, ground or surface water pollution, or noise in excess of Provincial regulations or guidelines shall be permitted.

#### D3.4.1.4 Land Use Policies

##### D3.4.1.4.1 Accessory Outdoor Storage

It is recognized that many businesses are required to store raw materials, finished products and/or equipment outdoors. However, it is recognized that the location and type of outdoor storage may have impacts on the aesthetics of an area and on adjacent land uses. On this basis, the establishment of a new accessory outdoor storage area on a lot shall be subject to Site Plan Control. In considering such an application, Council shall be satisfied that the outdoor storage is:

- a) not located in the front yard;
- b) clearly accessory to the main use on the property;
- c) is set back an appropriate distance from the side and rear lot lines, having regard to the nature of adjacent land uses; and,
- d) is completely enclosed and/or screened by landscaping, berms and/or fencing that functions year-round.

The implementing Zoning By-law may contain additional provisions regarding the location of accessory outdoor storage on a lot, and the locations in which outdoor storage shall only be permitted as an accessory use.

##### D3.4.1.4.2 Outdoor Storage Uses

The establishment of uses that have significant outdoor storage and/or outdoor processing components shall be subject to Site Plan Control. In considering such an application, Council shall be satisfied that the outdoor storage:

- e) is located on a lot that is the site of a building that meets the minimum requirements for such a building as set out in the implementing Zoning By-law;
- f) is not located in the front yard;
- g) is set back an appropriate distance from the side and rear lot lines, having regard to the nature of adjacent land uses; and,
- h) is completely enclosed and/or screened by landscaping, berms and/or fencing that functions year round.

The implementing Zoning By-law may contain additional provisions regarding the location of outdoor storage on a lot, as well as requirements for minimum lot coverage or building ground floor area.

In addition, the implementing Zoning By-law shall contain provisions that prohibit outdoor storage uses on lots that abut Guelph Street, Mountainview Road, Maple Avenue and River Drive in Georgetown and on properties in Acton, as specified in the implementing By-law.

##### D3.4.1.4.3 Asphalt Plants

Asphalt plants are not permitted in the General Employment Area designation and the establishment of a new asphalt plant shall require an amendment to this Plan. In considering an application to establish an



asphalt plant, Council shall be satisfied that the proposed use is appropriately located, designed, buffered and/or separated from sensitive land uses such as residential uses to prevent adverse effects from odour, noise and other contaminants, and minimize risk to public health and safety. In addition, such a use shall not be permitted to locate on properties that front on major roads that also serve as the boundary between the General Employment Area designation and another designation. In order to make an informed decision on such an application, supporting studies that review the potential adverse effects will be required.

#### D3.4.1.4.4 Secondary Uses

The following uses may be permitted in the *General Employment Area* designation subject to Section D3.4.1.4.5:

- a) commercial self-storage establishments;
- b) free-standing office buildings;
- c) commercial fitness centres;
- d) restaurants;
- e) day nurseries subject to Section D3.3.2b of this Plan;
- f) private and commercial schools subject to Section D3.3.2b of this Plan; and,
- g) motor vehicle repair establishments and motor vehicle body shops.

#### D3.4.1.4.5 Conditions Under Which Secondary Uses may be Permitted

Secondary uses listed in Section D3.4.1.4.4 may be permitted subject to an amendment to the implementing zoning by-law and shall be subject to Site Plan Control. Before considering such applications, Council shall be satisfied that the use:

- a) will not have a negative impact on the operation of industrial uses on abutting lands;
- b) will not compromise the ability of adjacent industrial uses to expand in the future;
- c) will complement and support the employment uses in the area;
- d) is supported by adequate parking and loading facilities on the site;
- e) is designed to blend in with its surroundings and with other buildings in the area;
- f) incorporates landscaping to enhance the site and surrounding area; and,
- g) if it abuts or is in close proximity to an existing residential use, fencing, landscaping, berming or a combination of these features are utilized to ensure that there is adequate screening between the uses.

#### D3.4.1.4.6 Adult Entertainment Uses

Adult entertainment parlours and body rub parlours may be permitted in the *General Employment Area* subject to the following criteria:

- a) Both adult entertainment parlours and body rub parlours are required to be set back a minimum of 100 metres from the edge of the Guelph Street and Main Street North right-of-ways. This setback is increased to 200 metres from Mountainview Road and 200 metres from Wallace Street. Notwithstanding these setbacks, both uses are required to be set back a minimum of 200 metres from any residential zone boundary.
- b) The establishment of an adult entertainment parlour shall be subject to an amendment to the implementing zoning by-law. An application shall only be approved if Council is satisfied that:
  - i) the adult entertainment parlour is the only use on the lot;
  - ii) the adult entertainment parlour shall be located on a lot of sufficient size so as to minimize the impacts of the use on adjacent land uses;
  - iii) the traffic from the use can be accommodated on area roads and will not conflict with truck traffic;
  - iv) the use is set back an appropriate distance from residential areas, institutional uses and day care centres;
  - v) the proposed use is located a minimum of 800 metres from another adult entertainment establishment; and,
  - vi) an appropriate amount of parking and landscaping is located on site.
- c) Body rub parlours will be permitted as of right within the implementing zoning by-law, subject to the following criteria:
  - i) a minimum separation distance of 500 metres between body rub parlours shall be maintained;
  - ii) body rub parlours are only permitted within a multiple unit building to minimize the visual impact of such a use;
  - iii) body rub parlours are permitted to only occupy a maximum of 15 percent of the floor area of the multiple unit building or 150 square metres whichever is less.

The implementing Zoning By-law shall contain specific provisions regarding adult entertainment parlours and body rub parlours. Performance standards for each type of use are also to be contained within the By-law. The By-law will also require that such uses be separated an appropriate distance from each other.

One of the primary tools of the Town is intended to be a Licensing By-law prepared in accordance with the Municipal Act. The Licensing By-law shall contain provisions that require each adult entertainment parlours and body rub parlour be licensed by the Town of Halton Hills. In addition, the licensing By-law shall contain specific provisions regarding signage to minimize the visual impact of each type of adult entertainment use.

#### D3.4.1.4.7 Acton Servicing Policy

Employment uses shall generally occur on the basis of full urban services. Employment uses requiring low volumes of water based on partial services may be permitted on lands located on the west side of

Regional Road No. 25 in accordance with the Acton Industrial Implementation Study or its successors. The design and approval of private sewage disposal systems shall be in accordance with Region of Halton by-laws, standards and guidelines and with Provincial requirements. When wastewater services are available as determined by the Region, the affected landowner shall be required to connect and pay the applicable fees for the connection.

#### D3.4.1.4.8 Cannabis Cultivation and Processing

Cannabis cultivation and processing is permitted within an enclosed building provided the lot on which it is located is set back a minimum of 150 metres from a lot that is the site of a child care centre (formerly known as a day nursery), a private or public school, a place of worship or other institutional use, a residential use, a long term care facility, a retirement home or a public park. Cannabis cultivation and processing shall also be subject to Site Plan Control in accordance with Section G8 of this Plan.

#### D3.4.1.5 Zoning By-law Implementation

Lands within the *General Employment Area* designation shall be placed in appropriate employment zones in the implementing Zoning By-law.

### D3.5 PREMIER GATEWAY EMPLOYMENT AREA PHASE 1

The Premier Gateway Employment Area Phase 1 represents an important employment area within the Town of Halton Hills and is intended to be developed with a range of industrial, office, commercial and institutional uses on full municipal services.

It is the intent of this Plan that development will be comprised of visually attractive buildings in aesthetically pleasing and sustainable environments. Future development shall be shaped around the three main tributaries of the Sixteen Mile Creek watershed system that cross the Premier Gateway Employment Area Phase 1. These natural systems are a central element in helping to define the Corridor.

#### D3.5.1 OBJECTIVES

It is the objective of this designation to:

- a) provide for the development of a major new employment area that takes advantage of the area's accessibility and visibility in the Town, Region and the Greater Toronto Area;
- b) ensure that the design of new development in the area is representative of the area's location as one of the gateways into the Town of Halton Hills and is sensitive to the character of the area;
- c) ensure that new development occurs in an orderly manner; and,
- d) ensure that all new development is provided with appropriate water and wastewater services and other infrastructure.

#### D3.5.2 LOCATION

The Premier Gateway Employment Area Phase 1 is generally located on both the north and south sides of Steeles Avenue between the Town of Milton boundary and Eighth Line as shown on Schedule A8 of

this Plan. Lands within the Premier Gateway Employment Area Phase 1 are divided into six land use designations, as set out below:

- *Prestige Industrial Area;*
- *Gateway Area;*
- *Greenlands;*
- *Major Parks and Open Space Area;*
- *Private Open Space Area; and,*
- *Phase 1B Employment Area.*

The *Prestige Industrial Area* designation applies to the majority of the lands within the Premier Gateway Employment Area Phase 1A. The *Gateway Area* designation applies to the key entrances into the employment area. The *Greenlands* designation is based on the 401 Corridor Scoped Subwatershed Plan and applies to the lands adjacent to the watercourses in the area. The *Major Parks and Open Space Area* applies to Hornby Park. The *Private Open Space Area* applies to lands within the westerly portion of the Corridor north of Steeles Avenue. The land use designations are shown on Schedule A8 to this Plan. The policies applying to the *Major Parks and Open Space Area* and the *Private Open Space Area* are contained in Sections B2 and B3 of this Plan. The *Phase 1B Employment Area* designation applies to the lands north of Steeles Avenue between the Greenbelt Plan Area and Eighth Line, which are subject to comprehensive planning, pursuant to Section D3.5.4.4.3 of this Plan.

### D3.5.3 GENERAL DEVELOPMENT POLICIES

#### D3.5.3.1 Public Utilities and Development Phasing

All development shall proceed on the basis of full municipal services. The Region of Halton is responsible for the extension of municipal water and wastewater services.

Development within the Premier Gateway Employment Area Phase 1 to the 2021 planning horizon shall occur in two sub-phases as illustrated on Schedule A8 to this Plan. It is the intent of this Plan that all development proceeds in a rational and orderly manner. Development of the Phase 1A Employment Area shall occur as market demands dictate and the Town and Region of Halton are satisfied that a sustainable financial strategy is in place. Development of the Phase 1B Employment Area shall occur in accordance with Sections D3.5.4.4.3 and F10.4 of this Plan.

Schedule A8 shows the approximate location of stormwater management ponds. It is recognized that interim stormwater management ponds may be required to facilitate development. For both interim and permanent facilities, the exact location and design will be subject to the precise nature of the development proposed and the approval of the Town and Conservation Halton. The detailed design of these facilities will also be subject to the criteria contained in the 401 Scoped Subwatershed Plan and Section C7 (Storm Water Management) of this Plan.

#### D3.5.3.2 Urban Design

A high standard of building and subdivision design and site planning will be required for all development, in accordance with the 401 Corridor Urban Design Guidelines and the following:

- a) during the processing of development proposals within the Premier Gateway Employment Area Phase 1, Council shall apply site plan controls pursuant to the Planning Act;
- b) where feasible, existing natural and heritage features shall be incorporated into the building and landscaping design to enhance their visual and environmental character; and,
- c) where development is proposed adjacent to areas designated as *Greenlands*, reference should be made to the Scoped Subwatershed Plan for direction on the restoration of valleylands.

#### D3.5.3.3 Transportation

Future access to Steeles Avenue shall require the approval of the Region of Halton in consultation with the Town and is subject to the following criteria:

- a) the access will not cause traffic hazards or an unacceptable level of congestion;
- b) the access will not limit the ability for the logical development of adjacent parcels; and,
- c) conformity with other relevant provisions of this Plan.

Reference should also be made to the Premier Gateway Employment Area Urban Design Guidelines for direction on treatment of lands within and adjacent to the right-of-way with respect to landscaping, sidewalks, cycling trails and lighting.

No direct access will be permitted from Fifth Line except as generally identified on Schedule B1 without an Amendment to this Plan. Development adjacent to Highways 401 and 407 shall be set back no less than 25 metres from the right-of-way.

#### D3.5.3.4 Cultural and Natural Heritage

Schedule A8 to this Plan identifies buildings that have historic significance. These structures provide important reference points to the local history of the Town. In any proposal to redevelop these properties, consideration should be given to incorporating these buildings and the surrounding landscaping into the new development as well as providing opportunities for the public to interpret their former role and function in the Corridor.

Woodlots and tree rows within the Corridor perform not only environmental functions but are also important physical links to the agricultural heritage of the area. It is an important objective of this Plan to retain these features wherever possible.

#### D3.5.3.5 Protection and Restoration of Valleylands and Natural Features

Valleylands that have been identified as flood susceptible, adjacent woodlots and unstable slope areas are designated as *Greenlands* on Schedule A8 to this Plan. These areas are recognized as unsuitable for development given their importance to the ecosystem as well as in terms of protecting human health and property from flooding.

It is a policy of this Plan to provide for the protection and enhancement of these features through:

- a) limitations on permitted uses;
- b) the provision of lot line and building setback requirements; and,

- c) the application of restoration techniques for watercourses, wetlands, riparian areas, unstable slopes, and other natural areas discussed within the Scoped Subwatershed Plan for the 401 Corridor.

Where development is proposed adjacent to lands identified on Schedule A8 to this Plan as “Potentially Unstable Slopes” a geotechnical study will be required to define the erosion hazards limit.

Lands not designated *Greenlands* may still have valuable natural features that should be considered for protection and enhancement. This is especially true for the wooded areas throughout the Corridor. These wooded areas perform important ecological functions both individually and as a whole that assists in erosion control, mitigates the impacts of wind and dust, provides habitat for local fish and wildlife and improves the area's aesthetics. Applications for Site Plan and Subdivision Approval must therefore be accompanied by a strategy to address the protection and compensation of the wooded areas identified within the Scoped Subwatershed Plan.

#### D3.5.4 LAND USE DESIGNATIONS

##### D3.5.4.1 Prestige Industrial Area

###### D3.5.4.1.1 Permitted Uses

Where lands are identified as *Prestige Industrial Area* on Schedule A8 to this Plan, the predominant use of the land will be for employment uses, located in well-designed buildings and structures established on landscaped lots in a visually attractive environment. Permitted uses within the *Prestige Industrial Area* shall be limited to:

- a) business and professional offices in free-standing buildings;
- b) industrial uses within wholly enclosed buildings, such as manufacturing, assembling, processing, fabricating, warehousing, wholesaling and distribution facilities;
- c) computer, electronics and data processing facilities;
- d) research and development facilities excluding those that produce biomedical wastes;
- e) printing and associated service establishments; and,
- f) industrial malls, containing one or more of the uses permitted in this designation.
- g) cannabis cultivation and processing subject to Section D3.5.4.1.6

No use that causes, or is likely to cause air pollution, offensive odours, ground or surface water pollution, or noise in excess of Provincial regulations or guidelines shall be permitted.

###### D3.5.4.1.2 Permitted Secondary Uses

The following secondary uses may be permitted in the *Prestige Industrial Area*:

- a) administrative office uses accessory to, and located on the same lot as a permitted industrial use;

- b) limited commercial uses which are ancillary to the main permitted industrial uses, such as take-out or full-service restaurants, financial institutions, convenience stores, and commercial fitness centres;
- c) commercial or trade school facilities;
- d) public parks and recreational facilities, in accordance with Section F7 (Parkland) of this Plan;
- e) limited private parks and recreational facilities;
- f) limited retail sales of goods and materials manufactured, or substantially assembled, on the premises;
- g) post-secondary education and other institutional facilities;
- h) motor vehicle service stations; and,
- i) uses that legally existed at the date of adoption of this Plan and expansions or alterations thereto which conform to the implementing Zoning By-law for the Premier Gateway Employment Area Phases 1A and 2A.

#### D3.5.4.1.3 Development Policy for Secondary Commercial Uses

The development of new secondary commercial uses is subject to the following:

- a) A high quality of building and landscaping design will be required for all commercial uses to ensure compatibility with adjacent development.
- b) Full-service restaurants may be integrated into industrial malls and/or free-standing office buildings, or may be developed as free-standing buildings provided that the building design is consistent and compatible with the overall design of the adjacent development.
- c) Take-out restaurants and convenience stores shall be integrated into industrial malls or free-standing office buildings.
- d) Two or more full-service restaurants may be developed in a “campus” development, subject to a comprehensive site development plan in order to achieve well-designed and integrated development, including:
  - i) integrated internal circulation and parking areas;
  - ii) coordinated access points, in order to minimize the number of access points to abutting roads;
  - iii) compatible building design and location;
  - iv) complementary landscaping; and,
  - v) consistent signage and lighting facilities.

#### D3.5.4.1.4 Private Parks and Recreational Uses

This Plan recognizes that many employers wish to provide private open space areas as well as facilities such as baseball diamonds and soccer fields for the exclusive use of employees. There is also a growing trend within the private sector in providing public recreation facilities for activities such as ice-skating. These types of uses are permitted in the *Prestige Industrial Area* designation in accordance with the following:

- a) the use reflects the high quality of development planned for the area; and,
- b) the development contributes to the visually attractive, landscaped setting of the area and shall be subject to site plan approval pursuant to Section 41 of the Planning Act and the 401 Corridor Urban Design Guidelines.

#### D3.5.4.1.5 Maximum Height

The height of any buildings or structures within the *Prestige Industrial Area* shall not exceed four storeys, or six storeys within 150 metres of the Highways 407 and 401. North of Steeles Avenue, building heights should not exceed two storeys. This policy does not apply to signs, utility towers or other non-habitable structures.

#### D3.5.4.1.6 Cannabis Cultivation and Processing

Cannabis cultivation and processing is permitted within an enclosed building provided the facade of any building facing Highway 401 and Steeles Avenue is clad in brick or other suitable material that does not give the impression that the building is a greenhouse. In addition, the height of any greenhouse portion of a building should not extend higher than the front facade of a building facing Highway 401 and Steeles Avenue. Cannabis cultivation and processing shall also be subject to the setback requirement identified in Section D3.4.1.4.8 of this Plan, and Site Plan Control in accordance with Section G8 of this Plan.

#### D3.5.4.2 Gateway Area

Lands within the Premier Gateway Employment Area Phases 1A and 2A in the immediate vicinity of James Snow Parkway, Winston Churchill Drive and Trafalgar Road have been designated as *Gateway Area* on Schedule A8 to this Plan. The objective of this designation is to:

- a) establish visually attractive points of entry into the Town of Halton Hills that will provide commercial services in support of the *Prestige Industrial Area*;
- b) draw the travelling public off Highway 401/407 allowing an introduction to the Town of Halton Hills and its attractions and amenities beyond the Premier Gateway Employment Area; and,
- c) provide a location for services that are important to the support of the primary industrial function of the Corridor such as hotels, financial services and opportunities for shopping.

##### D3.5.4.2.1 Permitted Uses

Permitted uses within the *Gateway Area* are limited to:

- a) full service hotels including full service hotels with conference and exhibition facilities;



- b) retail commercial uses not exceeding individual areas of 2,750 square metres in floor space and a total area of up to 10,000 square metres within the *Gateway Area*;
- c) Council may pass by-laws to permit individual retail commercial uses of individual stores over 2,750 square metres and over 10,000 square metres in total retail space within the *Gateway Area* designation providing the use meets the objectives of this Plan and:
  - i) is supported by studies addressing regional market and transportation impacts;
  - ii) is supported by an Urban Design Study outlining how the project can meet the objectives contained in this Plan and the 401 Corridor Urban Design Guidelines; and,
  - iii) is supported by any other appropriate studies requested by the Town;
- d) banks and financial institutions and services;
- e) motor vehicle service centres, but not within 200 metres of the intersection of Steeles Avenue with Trafalgar Road and James Snow Parkway;
- f) business and professional offices in free-standing buildings;
- g) computer, electronics and data processing facilities;
- h) research and development facilities excluding those that produce biomedical wastes;
- i) printing and associated service establishments;
- j) take-out restaurants providing they are incorporated into larger developments and are ancillary to the primary use;
- k) full service restaurants;
- l) theatres, cinemas and similar places of entertainment but not an adult entertainment establishment; and,
- m) uses that legally existed at the date of adoption of this Plan and expansions or alterations thereto, which conform to the implementing Zoning By-law for the Premier Gateway Employment Area Phases 1A and 2A.

No use that does, or is likely to cause air pollution, offensive odours, ground or surface water pollution, or noise in excess of Provincial regulations or guidelines shall be permitted.

#### D3.5.4.2.2 Maximum Height

Buildings within the *Gateway Area* designation are not subject to specific height restrictions. However, the height of proposed buildings will be addressed at the Site Plan Control stage of the planning approval process.

#### D3.5.4.3 Greenlands

The *Greenlands* designation within Phase 1A of the Premier Gateway Employment Area as shown on Schedule A8 of this Plan, is subject to Section B1A of this Plan.

#### D3.5.4.4 Phase 1B Employment Area

##### D3.5.4.4.1 Objectives

The objectives of the *Phase 1B Employment Area* designation are:

- a) to accommodate employment growth to the 2021 planning horizon;
- b) to provide for the continued development of the Premier Gateway Employment Area in order to enhance its development as a major employment area; and,
- c) to ensure that the urban design of the new development in the area enhances the area's location as a gateway into the Town of Halton Hills, and is sensitive to the character of the area.

##### D3.5.4.4.2 Location

The Phase 1B Employment Area designation applies to an expansion to the Premier Gateway Employment Area north of Steeles Avenue, between the Greenbelt Plan boundary and Eighth Line, as shown on Schedule A8 of this Plan.

##### D3.5.4.4.3 Comprehensive Planning

Prior to the approval of any development within this designation, the following must be completed:

- a) a Secondary Plan, according to the policies of Section G3.1 of this Plan, including a Subwatershed Study appropriately scoped to the Phase 1B Employment Area, to the satisfaction of Council;
- b) appropriate financial plans and agreements, including any necessary front ending agreements, to the satisfaction of the Region and the Town; and,
- c) appropriate development charge by-laws, to the satisfaction of Council.

The Terms of Reference for the Secondary Plan exercise shall include an evaluation of alternative locations to accommodate an additional supply of employment land to the 2021 planning horizon, which is required to complete the replacement of employment lands within Premier Gateway Employment Area Phase 2, subject to corridor protection. This supply has been accommodated by amendment to the Regional Official Plan and this Plan, on lands contiguous to the initial Phase 1B Employment Area.

##### D3.5.4.4.4 Regional Natural Heritage System

The *Greenlands* designation as shown on Schedule A8 of this Plan within Phase 1B of the Premier Gateway Employment Area, implements the Regional Natural Heritage System contained in the Regional Official Plan. Notwithstanding the policies of Section B1.2 of this Plan, the *Greenlands* designation is subject to the policies contained in Sections 115.2, 115.3, 115.4(2), 116, 116.1, 117.1, 118(1.1), 118(2), 118(3), 118(3.1), 118(4), 118(5) through 118(13), 139.11, 139.12, Map 1G, and the applicable definitions of the Regional Official Plan, which are hereby incorporated by reference into this Plan.

#### D3.5.4.4.5 Existing Rural Residential Concentrations

Existing concentrations of rural residential development, which are unlikely to be redeveloped in the short term for employment uses, are identified on Schedule A8 of this Plan.

Notwithstanding that these concentrations will be designated for employment use through the subsequent Secondary Plan process, and, provided that the adjacent employment lands are not precluded from development for employment uses due to land use compatibility issues associated with the continued presence of these non-employment uses, the following policies shall apply:

- a) subject to the Secondary Plan process, these concentrations may be zoned to permit the existing residential use and uses related to the residential use;
- b) through the Secondary Plan process, the Town may rezone the lands to permit the use of the existing residential dwellings for office or other uses that are compatible with both the adjacent residential uses, and uses permitted by the underlying employment designation;
- c) policies shall be incorporated into the Secondary Plan, which ensure the provision of landscape or other buffers between existing rural residential concentrations and future employment uses; and,
- d) through the Secondary Plan process, the development of a road network for the *Phase 1B Employment Area*, shall take into consideration the appropriateness of network solutions to address the existence of this residential concentration, including alternatives such as bypasses and cul-de-sacs.

#### D3.5.5 IMPLEMENTING ZONING BY-LAW

The implementing Zoning By-law shall place lands within the Prestige Industrial designation within a Prestige Industrial Zone, lands within the *Gateway Area* designation within a gateway zone and lands within the *Greenlands, Major Parks and Open Space Area* and *Private Open Space Area* designations within an appropriate Environmental or Open Space Zone. The boundaries of the *Greenlands* designation are delineated in a conceptual manner on Schedule A8 to this Plan. The extent and exact location of the boundaries shall be delineated by the appropriate Conservation Authority and Provincial agencies, and will not require an amendment to this Plan. The implementing Zoning By-law shall also contain specific setbacks for lot lines, buildings, structures, parking areas and other similar facilities from the *Greenlands* designation in relation to the extent and severity of the natural environmental features and ecological functions of the area. This general setback will be evaluated for specific development applications by the Town and the Region of Halton in consultation with the appropriate Conservation Authority, and as established in the Zoning By-law, to provide the appropriate setback required to protect the area from the impacts of construction. A greater setback may be required to reflect specific circumstances.

The implementing Zoning By-law may apply a Holding Provision to some or all of the lands within the Premier Gateway Employment Area Phases 1A and 2A. The holding provision shall only be lifted once Council is satisfied that all of the appropriate servicing, transportation, urban design and environmental considerations have been addressed and the appropriate agreements entered into.

#### D3.5.6 SPECIAL POLICY AREAS

Special Policy Areas apply to those lands within the Premier Gateway Employment Area Phase 1 that are subject to a land use policy that is specific to a property or area. All other relevant policies of this Plan

are applicable unless otherwise modified or exempted by the provisions applicable to each Special Policy Area.

D3.5.6.1      Special Policy Area 1

The parking and/or storing of tractor-trailers shall be permitted on lands designated as *Gateway Area* and identified as Special Policy Area 1, as shown on Schedule A8 of this Plan.

D3.5.6.2      Special Policy Area 2

A commercial operation with a maximum gross floor area of 1,861 square metres devoted to serving the horse/equestrian community, including the selling of commercial products, service, repair, and the making of related products is permitted on lands designated as *Phase 1B Employment Area* and identified as Special Policy Area 2, as shown on Schedule A8 of this Plan.

D3.5.6.3      Special Policy Area 3

Outdoor storage that is accessory to a permitted manufacturing and business office use is allowed on lands designated as *Prestige Industrial Area* and identified as Special Policy Area 3, as shown on Schedule A8 of this Plan.

## D4 SOUTH ACTON SPECIAL STUDY AREA

The following policies shall apply to those lands in the southern portion of the community of Acton as shown on Schedule A6 to this Plan.

### D4.1 OBJECTIVES

It is the objective of this designation to:

- a) identify an area of vacant land that is considered to be an integral component of the Acton Urban Area to ensure that all land use and servicing options are carefully considered prior to development occurring;
- b) ensure that development does not occur until a comprehensive review of land use and servicing options, urban design and environmental constraints is undertaken; and,
- c) ensure that all new development is integrated with and enhances existing development in the Acton Community.

### D4.2 THE NEED FOR COMPREHENSIVE PLANNING

The subject lands were formerly used by a major industrial use. As a result of servicing constraints in the community, the lands were vacant at the time this Plan was adopted by Council. Prior to the adoption of this Plan, the previous Plan permitted industrial uses on private services only.

Council recognizes that the subject lands are located in a key area of Acton. In addition to the large size of the property and its location within the urban area, a number of land uses are considered appropriate in principle for the lands. These uses include residential, industrial, public open space and recreational uses such as a golf course. In addition, if these lands are developed, an opportunity exists to establish a new east-west road connection through the community of Acton.

A review of which uses are appropriate and how they are to be sited and serviced is required. On this basis, development shall not be permitted on the subject lands until a Comprehensive Development Plan (CDP) applying to all the lands is prepared to the satisfaction of Council. The CDP shall deal with such issues as:

- a) the proposed form of servicing;
- b) the protection and enhancement of any natural heritage features and related ecological functions;
- c) the nature, location and density of all uses and the manner in which they are integrated on the subject lands and with existing development;
- d) the nature, extent and timing of any required road improvements and the overall road pattern for the subject lands;
- e) the urban design standards that are to apply; and,
- f) the remediation of any potential site contamination in accordance with Ministry of Environment guidelines.

The CDP shall form the basis of an Official Plan Amendment that will place the lands in appropriate land use designations in accordance with this Plan.

#### D4.3 CONDITIONS OF APPROVAL

The CDP required in Section D4.2 shall not be approved unless Council is satisfied that:

- a) the density and scale of development conforms to the objectives of this Plan;
- b) existing natural heritage features are integrated and enhanced with the proposed development;
- c) the lands can be appropriately serviced while having regard to the nature of the other developments in the community that may be dependent on additional and/or new servicing;
- d) the development can be integrated with existing development; and,
- e) the proposed uses will have a positive impact on the quality of life in the community.

#### D4.4 IMPLEMENTING ZONING BY-LAW

Prior to the approval of the CDP and the Official Plan Amendment by the Town, the implementing Zoning By-law shall only recognize development that exists at the time the by-law comes into effect pursuant to the Planning Act. Following approval of the CDP and the Official Plan Amendment, the lands shall be placed in zones that appropriately implement the CDP and the Official Plan Amendment.

## D5 BUILT-UP AREA AND INTENSIFICATION AREAS

### D5.1 OBJECTIVES

It is the objective of this Plan that:

- a) Intensification Areas provide the focus for the majority of intensification within the Built-up Areas of Acton and Georgetown, and cumulatively attract a significant portion of the planned population and employment growth of these Urban Areas to 2031;
- b) Intensification Areas provide an urban form that is compatible with existing development areas, promotes live-work relationships, facilitates social interaction, improves public safety, is transit-supportive and reduces reliance on the automobile, promotes active transportation, and is more environmentally sustainable;
- c) Intensification Areas receive priority for infrastructure investment, including improvements to the road network, stormwater management, and community services to facilitate development and redevelopment in these areas;
- d) consideration is given by Council to financial, planning approval, and other incentives to support development and redevelopment within Intensification Areas, including the use of Community Improvement Plans;
- e) intensification is limited, outside of Intensification Areas, but within the Built-up Area, in accordance with the policies of this Plan;
- f) identified cultural heritage resources within the Built-up Area are conserved through the process of intensification;
- g) the appropriate type and scale of development in Intensification Areas is addressed through detailed area-specific plans for each area;
- h) development or redevelopment through intensification is characterized by high quality urban design;
- i) minimum densities for development and redevelopment are achieved in Intensification Areas; and,
- j) Intensification Areas provide high quality public open spaces with site and urban design that create attractive pedestrian-friendly places for social interaction.

### D5.2 LOCATION

The Intensification Areas of the Town of Halton Hills consist of:

- a) a Major Transit Station Area, represented by the Georgetown GO Station/Mill Street Corridor Area and the Acton Downtown Redevelopment and Medium Density Residential/Commercial Sub-Areas;

- b) an Intensification Corridor, located along Guelph Street in Georgetown, and comprised of areas fronting on Guelph Street in the Georgetown *Downtown Area*, *Community Node*, and GO Station/Mill Street Corridor Area; and,
- c) other redevelopment areas, including the Civic Centre District, South Acton Special Study Area, Georgetown *Downtown Area* and Acton *Downtown Area*.

The boundaries of these Intensification Areas are as shown on Schedule A3-1 – Georgetown Built Boundary and Intensification Areas, Schedule A6-1 – Acton Built Boundary and Intensification Areas, and Schedule A7 – Acton *Downtown Area* of this Plan.

The Intensification Areas identified in this Plan correspond closely with land use designations in the Community Area designation, contained in Sections D2.5.1 and D2.5.2 of this Plan, with the exception of the Civic Centre District, which is covered by Sections D2.5.7 (*Civic Centre Area*) and D1.6.5 (Residential Special Policy Area 5). Detailed land use policies applying to the Intensification Areas are contained in these section of the Plan.

The Georgetown GO Station/Mill Street Corridor and Downtown Georgetown Intensification Areas are covered by Secondary Plans contained in Sections H3 and H7 of this Plan, respectively. Policies pertaining to the Acton Downtown Medium Density Residential Commercial Sub-Area and the Acton *Downtown Redevelopment Sub-Area*, are provided in Sections D2.5.1.7 and D2.5.1.8 respectively, of this Plan. Section D4 outlines the requirements for comprehensive planning of the South Acton Special Study Area.

### D5.3 INTENSIFICATION TARGETS

This Plan provides policies to assist in achieving the minimum intensification targets, contained in Table D5.1, for residential development within the Built Boundary of Acton and Georgetown, as shown on Schedules A3-1 and A6-1 of this Plan.

The Intensification Areas identified in Section D5.2 and Table D5.1 are intended to be the focus for intensification within the Town of Halton Hills over the planning period to 2031.

The minimum intensification targets for new residential units within the Built Boundary provide for a level of intensification that conforms to the policies and land use designations of this Plan. The number of units constructed within the Built Boundary, commencing in 2015 and each year thereafter, will be monitored to ensure conformity with the Region's requirements under the GGH Growth Plan.



TABLE D5.1: MINIMUM RESIDENTIAL INTENSIFICATION TARGETS WITHIN BUILT BOUNDARY

Intensification Area	2015-2031 Target (Units)
GO Station/Mill Street Corridor Area	1,540
Downtown Area	340
Community Node	170
Civic Centre District	520
Other Intensification	630
Georgetown TOTAL	3,200
Downtown Area	90
South Acton Special Study Area	1,240
Other Intensification	460
Acton TOTAL	1,790
Hamlets	110
Built Boundary TOTAL	5,100

Note: 'Other Intensification' refers to intensification on individual sites outside of Intensification Areas, and/or accessory apartments within permitted land use designations of this Plan.

#### D5.4 INTENSIFICATION POLICIES

Intensification within the Built-up Area identified in this Plan is subject to the following general policies:

- a) The Town shall ensure that Intensification Areas are development-ready by:
  - i) coordinating with the Regional the provision of water, wastewater, stormwater and transportation infrastructure with sufficient capacity to support the development densities planned for these areas;
  - ii) coordinating discussions with utility providers to ensure that adequate utility services are or will be in place to serve the proposed development; and,
  - iii) adopting Zoning By-laws or a Secondary Plan or Comprehensive Development Plan, within one year of the approval of the Official Plan Amendment introducing the Intensification Area;

- b) The intersections of Guelph Street and Mill Street, Guelph Street and Maple Avenue, and Guelph Street and Mountainview Road, are recognized as Activity Nodes, and are identified on Schedule A3-1 to this Plan, in which mixed-use intensification is to be focused, within the broader Intensification Areas identified in this Plan;
- c) High quality urban design for development and redevelopment through intensification shall be required, consistent with the Urban Design Guidelines contained in Appendix X4 of this Plan, and any other urban design guidelines prepared in support of Secondary Plans or Comprehensive Development Plans;
- d) The appropriate type and scale of development in Intensification Areas shall be addressed through detailed Secondary Plans or Comprehensive Development Plans for each area, including the preparation of urban design guidelines, where appropriate; and,
- e) Any site-specific Official Plan or Zoning By-law amendment to reduce development density within an Intensification Area is prohibited by this Plan, unless it is part of a municipal comprehensive review of the Official Plan or a review of the Secondary Plan for the Intensification Area.

#### D5.4.1 MAJOR TRANSIT STATION AREAS

The Georgetown GO Station/Mill Street Corridor Area is a Major Transit Station Area which is intended to be the primary focus for high density residential and/or mixed use intensification in the Town, and is subject to detailed Secondary Plan policies contained in Section H3 of this Plan.

The Acton Major Transit Station Area is located in Downtown Acton and generally corresponds to the Acton *Downtown Redevelopment Sub-Area* designation. This area is intended to accommodate moderate levels of intensification, and policies for the Acton *Downtown Redevelopment Sub-Area* and adjacent Acton Downtown Medium Density Residential Commercial Sub-Area are contained in Sections D2.5.1.7 and D2.5.1.8 of this Plan.

Planning for the Major Transit Station Area shall be guided by the following objectives:

- a) To achieve increased residential and employment densities to ensure the viability of existing and planned transit infrastructure and service; and,
- b) To provide access from various transportation modes to the transit station, including consideration of, but not limited to, pedestrians, bicycle routes and bicycle parking, commuter pick-up/drop-off areas, carpool parking, car share vehicles, and parking/recharging stations for electric vehicles.

#### D5.4.2 INTENSIFICATION OUTSIDE OF INTENSIFICATION AREAS

Lands outside of the Intensification Areas generally consist of stable residential neighbourhoods, the character of which should be protected from incompatible intensification. However, limited intensification, primarily through contextually appropriate infill, is contemplated by this Plan outside of the Intensification Areas, and may be permitted, subject to the detailed land use designations and policies, and in particular Sections D1.4.2 and D1.4.3, of this Plan. Table D5.1 identifies potential for other intensification that has been identified to 2031, including infill, redevelopment, and brownfield and greyfield sites, which conform to the policies of this Plan.

## D6 DESIGNATED GREENFIELD AREAS

### D6.1 LOCATION

The Designated Greenfield Area is comprised of all lands within the Urban Areas of Acton and Georgetown, which are situated outside the Built Boundary as shown on Schedules A3-1 and A6-1 of this Plan, and the entire Premier Gateway Employment Area as shown on Schedule A8.

### D6.2 DEVELOPMENT DENSITY

The minimum development density that must be achieved for development within the Designated Greenfield Area in the Town is 39 residents and jobs combined per gross hectare, measured over the Designated Greenfield Area as a whole.

The *Future Residential/Mixed Use Area* and Phases 1B and 2B Employment Areas will be planned at a density designed to achieve the minimum development density over the Designated Greenfield Area as a whole.

In order to achieve this target, the minimum development density for the *Future Residential/Mixed Use Area* shall be within a range of 55 to 60 residents and jobs combined per gross hectare.

### D6.3 FUTURE RESIDENTIAL/MIXED USE AREA

#### D6.3.1 OBJECTIVES

It is the objective of this Plan to:

- a) accommodate moderate population and population-related employment growth for the 2021 to 2031 planning period, as specified in Section A1A of this Plan and in accordance with Section F10.4 of this Plan;
- b) ensure that the new development area is integrated into the fabric of the existing community of Georgetown;
- c) ensure that the new development area is developed as a complete community, with compact pedestrian-friendly neighbourhoods, a mix of housing types, community facilities, commercial centres, and open spaces;
- d) ensure a high standard of urban design; and,
- e) adhere to the community design principles for new communities contained in Section F3 of this Plan.

#### D6.3.2 LOCATION

The *Future Residential/Mixed Use Area* designation applies to three areas that have been added to the Georgetown Urban Area, as shown on Schedule A3 to this Plan:

- Southeast Georgetown, bounded by Tenth Line, Tenth Side Road, and the Hamlet of Norval; and,

- An expansion to the Stewarttown community, bounded by the existing Stewarttown community, the CN railway line, and the Black Creek within the *Protected Countryside Area* of the Greenbelt Plan.

#### D6.3.3 COMPREHENSIVE PLANNING

Prior to the approval of any development within this designation, the following must be completed:

- a) a Joint Infrastructure Staging Plan, in accordance with the policies of Section F10.4 of this Plan;
- b) a Secondary Plan, in accordance with the policies of Section G3.1 of this Plan and including the establishment of the local phasing of development to the 2031 planning horizon according to the policies of Section F10.4 of this Plan, to the satisfaction of Council;
- c) a Block Plan according to the policies of Section G3.2 of this Plan, to the satisfaction of Council;
- d) appropriate financial plans and agreements, including any necessary front ending agreements, to the satisfaction of the Region and the Town; and,
- e) appropriate development charge by-laws, to the satisfaction of Council.

#### D6.3.4 REGIONAL NATURAL HERITAGE SYSTEM

The *Greenlands* designation within the *Future Residential/Mixed Use Area* as shown on Schedule A3 of this Plan, implements the Regional Natural Heritage System contained in the Regional Official Plan. Notwithstanding the policies of Section B1.2 of this Plan, the *Greenlands* designation is subject to the policies contained in Sections 115.2, 115.3, 115.4(2), 116, 116.1, 117.1, 118(1.1), 118(2), 118(3), 118(3.1), 118(4), 118(5) through 118(13), 139.11, 139.12, Map 1G, and the applicable definitions of the Regional Official Plan, which are hereby incorporated by reference into this Plan.

#### D6.3.5 STEWARTTOWN SPECIAL CHARACTER AREA

It is the policy of this Plan that the expansion area to the community of Stewarttown shall be developed as a low density residential neighbourhood, in keeping with the character of the existing community of Stewarttown.

### D6.4 PREMIER GATEWAY EMPLOYMENT AREA PHASE 2

#### D6.4.1 OBJECTIVES

It is the objective of this Plan to:

- a) accommodate employment growth for the 2021 to 2031 planning period;
- b) provide for the expansion of the Premier Gateway Employment Area, in order to enhance its development as a major new employment area; and,
- c) ensure that the urban design of new development in the area enhances the area's location as a gateway into the Town of Halton Hills, and is sensitive to the character of the area.

#### D6.4.2 LOCATION

The Premier Gateway Employment Area Phase 2 is generally located on both the north and south sides of Steeles Avenue between Eighth Line and the City of Brampton boundary. Lands within the Premier Gateway Employment Area Phase 2 are divided into four land use designations, as set out below:

- *Prestige Industrial Area;*
- *Gateway Area;*
- *Greenlands; and,*
- *Phase 2B Employment Area.*

#### D6.4.3 GENERAL DEVELOPMENT POLICIES

The general policies pertaining to the development of lands within the *Prestige Industrial Area*, *Gateway Area* and *Greenlands* designations are as set out in Sections D3.5.3.2, D3.5.3.3, D3.5.3.4 and D3.5.3.5 of this Plan.

##### D6.4.3.1 Public Utilities and Development Phasing

All development shall proceed on the basis of full municipal services. The Region of Halton is responsible for the extension of municipal water and wastewater services.

Development within the Premier Gateway Employment Area Phase 2 for the 2021 to 2031 planning period shall occur in two sub-phases as illustrated on Schedule A8 to this Plan, and shall be in accordance with Section F10.4 of this Plan. It is the intent of this Plan that all development proceeds in a rational and orderly manner. Development of the *Phase 2B Employment Area* shall also occur in accordance with Section D6.4.4.4.3 of this Plan.

#### D6.4.4 LAND USE DESIGNATIONS

##### D6.4.4.1 Prestige Industrial Area

The specific policies pertaining to the development of lands within the *Prestige Industrial Area* designation are as set out in Section D3.5.4.1 of this Plan.

##### D6.4.4.2 Gateway Area

The specific policies pertaining to the development of lands within the *Gateway Area* designation are as set out in Section D3.5.4.2 of this Plan.

##### D6.4.4.3 Greenlands

The *Greenlands* designation within Phase 2A of the Premier Gateway Employment Area as shown on Schedule A8 of this Plan, is subject to Section B1A of this Plan.

##### D6.4.4.4 Phase 2B Employment Area

###### D6.4.4.4.1 Objectives

The objectives of the *Phase 2B Employment Area* designation are:

- a) to accommodate employment growth for the 2021 to 2031 planning period;
- b) to provide for the expansion of the Premier Gateway Employment Area in order to enhance its development as a major employment area; and,
- c) to ensure that the urban design of the new development in the area enhances the area's location as a gateway into the Town of Halton Hills, and is sensitive to the character of the area.

#### D6.4.4.4.2 Location

The Phase 2B Employment Area designation applies to an expansion to the Premier Gateway Employment Area north of Steeles Avenue, between Eighth Line and the City of Brampton boundary, as shown on Schedule A8 of this Plan.

#### D6.4.4.4.3 Comprehensive Planning

Prior to the approval of any development within this designation, the following must be completed:

- a) a Joint Infrastructure Staging Plan in accordance with the policies of Section F10.4 of this Plan;
- b) a Secondary Plan, according to the policies of Section G3.1 of this Plan, including:
  - i) a Subwatershed Study appropriately scoped to the *Phase 2B Employment Area*, to the satisfaction of Council; and,
  - ii) the establishment of local phasing of development to the 2031 planning horizon;
- c) appropriate financial plans and agreements, including any necessary front ending agreements, to the satisfaction of the Region and the Town; and,
- d) appropriate development charge by-laws, to the satisfaction of Council.

#### D6.4.4.4.4 Regional Natural Heritage System

The *Greenlands* designation as shown on Schedule A8 of this Plan with Phase 2B of the Premier Gateway Employment Area, implements the Regional Natural Heritage System contained in the Regional Official Plan. Notwithstanding the policies of Section B1.2 of this Plan, the *Greenlands* designation is subject to the policies contained in Sections 115.2, 115.3, 115.4(2), 116, 116.1, 117.1, 118(1.1), 118(2), 118(3), 118(3.1), 118(4), 118(5) through 118(13), 139.11, 139.12, Map 1G and the applicable definitions of the Regional Official Plan which are hereby incorporated by reference into this Plan.

#### D6.4.4.4.5 Existing Rural Residential Concentrations

Existing concentrations of rural residential development, which are unlikely to be redeveloped in the short term for employment uses, are identified on Schedule A8 of this Plan.

Notwithstanding that these concentrations will be designated for employment use through the subsequent Secondary Plan process, and, provided that the adjacent employment lands are not precluded from development for employment uses due to land use compatibility issues associated with the continued presence of these non-employment uses, the following policies shall apply:

- a) subject to the Secondary Plan process, these concentrations may be zoned to permit the existing residential use and uses related to the residential use;
- b) through the Secondary Plan process, the Town may rezone the lands to permit the use of the existing residential dwellings for office or other uses that are compatible with both the adjacent residential uses, and uses permitted by the underlying employment designation;
- c) policies shall be incorporated into the Secondary Plan, which ensure the provision of landscape or other buffers between existing rural residential concentrations and future employment uses; and,
- d) through the Secondary Plan process, the development of a road network for the *Phase 2B Employment Area*, shall take into consideration the appropriateness of network solutions to address the existence of this residential concentration, including alternatives such as by-passes and cul-de-sacs.

## D7 FUTURE STRATEGIC EMPLOYMENT AREAS

### D7.1 PURPOSE

The purpose of the Future Strategic Employment Areas, shown as an overlay on Schedule A17, is to identify and protect from incompatible uses certain lands that are strategically located with respect to major transportation facilities and existing employment areas and are best suited for employment beyond the planning horizon of this Plan. The underlying land use designations corresponding to the overlay are shown on Schedule A1 of this Plan and are governed by the policies of this Plan. Future Strategic Employment Areas are not land use designations and confer no permitted uses.

### D7.2 LOCATION

The Future Strategic Employment Areas are located adjacent to existing employment areas and existing or potential major transportation facilities, as shown on Schedule A17 of this Plan.

### D7.3 POLICIES

#### D7.3.1 RE-DESIGNATION

The re-designation of lands within the Future Strategic Employment Areas to uses that are incompatible with employment uses in the long term, especially non-farm uses such as institutional and recreational uses, is prohibited.

#### D7.3.2 SERVICING

It is the intent of this Plan that the Town will participate with the Region in an investigation of the feasibility and cost to service the Future Strategic Employment Areas, as part of the Joint Infrastructure Staging Plan specified in Section F10.4 of this Plan.

#### D7.3.3 URBAN AREA EXPANSION

The inclusion of lands within the Future Strategic Employment Areas into the Urban Area before 2031 will only be considered through a municipal comprehensive review, as part of the Region's and Town's five-year reviews of their respective Official Plans, if it can be demonstrated that the Region and Town will not be able to meet their 2031 employment targets through development within the Urban Area.

#### D7.3.4 EXISTING RURAL RESIDENTIAL CONCENTRATIONS

At such time as an area identified as Future Strategic Employment Area is proposed for re-designation to urban employment uses, through a municipal comprehensive review, existing rural residential concentrations shall be identified, and policies incorporated into this Plan, requiring an examination of ways to mitigate the impacts of urban employment development on these areas



## E1 AGRICULTURAL AREA

### E1.1 OBJECTIVES

It is the objective of this designation to:

- a) recognize agriculture as the primary activity and land use;
- b) maintain and preserve the agricultural resource base of the Town;
- c) protect land suitable for agricultural production from fragmentation, development and land uses unrelated to agriculture;
- d) promote the agricultural industry and associated activities and enhance their capacity to contribute to the economy of the Town;
- e) preserve and promote the agricultural character of the Town and the maintenance of the countryside as an open space area; and,
- f) to provide for the designation of new *Mineral Resource Extraction Areas* which can be accommodated in accordance with the policies of this Plan and by an amendment to the Region of Halton Official Plan and this Plan.

### E1.2 LOCATION

The *Agricultural Area* designation as shown on Schedule A1 to this Plan applies to lands generally located south and east of the lands within the *Protected Countryside Area* designation that are predominantly utilized for agricultural purposes and which have an agricultural character. The *Agricultural Area* primarily consists of lands that are Class 1, 2 or 3 soils according to the Canada Land Inventory. The lands within this designation are considered by this Plan to form a major component of the Town's Prime *Agricultural Area*.

### E1.3 PERMITTED USES

The principal use of land in the *Agricultural Area* designation shall be agriculture. Additional permitted uses are limited to:

- a) single detached dwellings on existing lots;
- b) accessory residential uses on farm properties subject to Section E1.4.2 of this Plan;
- c) bed and breakfast establishments subject to Section E1.4.3 of this Plan;
- d) home occupations and cottage industries subject to Section E1.4.4 of this Plan;
- e) home industries located on a commercial farm subject to Section E1.4.5 of this Plan;
- f) forest, wildlife and fisheries management;
- g) watershed management and flood and erosion control projects carried out or supervised by a public agency;

- h) existing public uses;
- i) transportation and utility facilities;
- j) commercial uses on a commercial farm subject to Section E1.4.6 of this Plan;
- k) passive non-motorized recreational uses, such as nature viewing and pedestrian trail activities, only on publicly owned lands or on the Bruce Trail;
- l) archaeological activities;
- m) farm-related tourism establishments on a commercial farm subject to Section E1.4.7 of this Plan;
- n) residential care facilities, excluding Treatment Centres, subject to E1.4.8 of this Plan;
- o) wayside pits and quarries, portable asphalt plants and portable concrete plants used on public authority contracts, subject to the requirements of the Ministry of Natural Resources and the Ministry of Environment; and
- p) accessory apartments in single detached dwellings subject to Section E1.4.10.

All uses in the *Agricultural Area* designation shall be designed, located and managed to not detract from the primary role of the *Agricultural Area* as set out in Sections A1 and E1.1 of this Plan.

## E1.4 LAND USE POLICIES

### E1.4.1 THE CREATION OF NEW LOTS

In accordance with the intent of this Plan to maintain and protect the agricultural resources and rural character of the Town, lot creation is prohibited unless specifically provided for in Section F1.2 of this Plan.

### E1.4.2 ACCESSORY RESIDENTIAL USES ON FARM PROPERTIES

The establishment of additional dwelling unit(s) on a commercial farm for bona fide farm help is permitted, provided the lands are appropriately zoned. Prior to considering an application for re-zoning, and/or site plan approval in accordance with Section G8 of this Plan, Council shall be satisfied that the second dwelling unit:

- a) is required for farm help as set out in a detailed submission addressing matters such as labour requirements related to the size and nature of the farm operation, and an assessment of the available residential accommodation on the farm;
- b) will be located within the existing farm-building cluster;
- c) can be serviced by appropriate sewage and water services; and,
- d) will be designed and/or located to be compatible or otherwise blend in with the farm operation.

Accessory residential uses on farm properties within the *Escarpment Protection Area* and the *Escarpment Rural Area* shall be in accordance with the appropriate development criteria contained in the Niagara Escarpment Plan.

#### E1.4.3 BED AND BREAKFAST ESTABLISHMENTS

Bed and breakfast establishments are permitted in a single detached dwelling subject to Site Plan Control in accordance with Section G8 of this Plan. Prior to approving a Site Plan Application, Council shall be satisfied that:

- a) the use shall not have a negative impact on the enjoyment and privacy of neighbouring properties;
- b) the use is clearly secondary to the primary residential use in terms of floor space utilization and is compatible with surrounding uses;
- c) the bed and breakfast establishment is the principal residence of the owner and operator;
- d) the character of the dwelling as a private residence is preserved;
- e) adequate parking facilities are available on the lot for the proposed use;
- f) no more than three rooms are available for guests;
- g) the use will not cause a traffic hazard;
- h) the use can be serviced with an appropriate water supply and an appropriate means of sewage disposal; and,
- i) the signage advertising the use is to be designed and located in accordance with the Town's sign by-law and where applicable the development criteria contained in the Niagara Escarpment Plan.

The implementing Zoning By-law shall further detail appropriate performance standards for bed and breakfast establishments.

#### E1.4.4 HOME OCCUPATIONS AND COTTAGE INDUSTRIES

Home occupations and cottage industries are permitted in a single detached dwelling, subject to the following criteria:

- a) the use shall not have a negative impact on the enjoyment and privacy of neighbouring properties;
- b) the use is clearly secondary to the primary residential use in terms of floor space utilization and is compatible with surrounding uses;
- c) the use is completely located in the principal residence of the person conducting the home occupation or cottage industry;
- d) the character of the dwelling as a private residence is preserved;

- e) adequate on-site parking facilities are provided for the use, in addition to the parking required for the principal use on the property, and such parking is provided in locations compatible with surrounding land uses;
- f) the use will not cause a traffic hazard;
- g) the use can be serviced with an appropriate water supply and an appropriate means of sewage disposal; and,
- h) the signage advertising the use is to be designed and located in accordance with the Town's sign by-law and where applicable the development criteria contained in the Niagara Escarpment Plan.

The implementing Zoning By-law shall further detail appropriate performance standards for home occupations and cottage industries.

Home occupations and cottage industries shall not exceed 25 percent of the gross floor area of a single detached dwelling, to a maximum gross floor area of 100 square metres.

#### E1.4.5 HOME INDUSTRIES

Home industries are permitted subject to Site Plan Control in accordance with Section G8 of this Plan. Prior to approving a Site Plan Application, Council shall be satisfied that:

- a) the use shall not have a negative impact on the enjoyment and privacy of neighbouring properties;
- b) the use is clearly secondary to the primary residential use or agricultural use on the lot and is compatible with surrounding uses;
- c) the character of the area is preserved;
- d) adequate on-site parking facilities are provided for the use, in addition to the parking required for the principal use on the property, and such parking is provided in locations compatible with surrounding land uses;
- e) the use will not cause a traffic hazard;
- f) the use can be serviced with an appropriate water supply and an appropriate means of sewage disposal;
- g) the signage advertising the use is to be designed and located in accordance with the Town's sign by-law and where applicable the development criteria contained in the Niagara Escarpment Plan;
- h) the building housing the proposed use is located within the existing farm-building cluster and shall utilize a common driveway with the principal use of the property,
- i) the use does not have a gross floor area in excess of 200 square metres in an accessory building;
- j) the proposed use is set back from all lot lines by at least 30 metres;

- k) where permitted, open storage associated with the proposed use shall be screened from view and located within a fenced compound;
- l) the use has a limited number of employees; and,
- m) retail sales shall be limited to goods or products produced on the property.

Home industries shall not exceed 200 square metres of gross floor area. Home industries within accessory buildings in the *Escarpment Protection Area* and the *Escarpment Rural Area* shall not exceed 100 square metres gross floor area in accordance with the appropriate development criteria contained in the Niagara Escarpment Plan.

The implementing Zoning By-law shall further detail appropriate performance standards for Home Industries.

#### E1.4.6 COMMERCIAL USES ON FARM PROPERTIES

Secondary commercial uses on farm properties are permitted subject to Site Plan Control in accordance with Section G8 of this Plan. Prior to approving such an application, Council shall be satisfied that:

- a) the use is clearly associated with and located on a commercial farm;
- b) the retail component has a gross floor area of no more than 500 square metres; and,
- c) the majority of the products offered for sale, in terms of monetary value, are produced or manufactured on the farm property.

The implementing Zoning By-law shall further detail appropriate performance standards for secondary commercial uses on farm properties.

#### E1.4.7 FARM RELATED TOURISM ESTABLISHMENTS

Given the proximity of the Town to growing urban areas, the Town supports the development of uses that highlight the importance and value of the agricultural economy. On this basis, uses such as farm machinery and equipment exhibitions, farm tours, petting zoos, hay rides and sleigh rides, processing demonstrations, pick your own produce, small-scale farm theme playgrounds for children and small-scale educational establishments that focus on farming instruction are permitted in the *Agricultural Area* designation as an accessory use on a commercial farm subject to Site Plan Control in accordance with Section G8 of this Plan. Prior to approving such an application, Council shall be satisfied that:

- a) the proposed use shall not have a negative impact on the enjoyment and privacy of neighbouring properties;
- b) adequate on-site parking facilities are provided for the use, in addition to the parking required for the principal use on the property, and such parking is provided in locations compatible with surrounding land uses;
- c) the proposed access to the site will not cause a traffic hazard;
- d) the proposed use can be serviced with an appropriate water supply and an appropriate means of sewage disposal;

- e) the proposed use enhances the rural and open space character of the Town through the preservation of older barns and/or the establishment of a built form that is compatible with the rural surroundings;
- f) the building housing the proposed use is located within the existing farm-building cluster where possible and shall utilize a common driveway with the principal use of the property, and,
- g) the signage advertising the use is to be designed and located in accordance with the Town's sign by-law and where applicable the development criteria contained in the Niagara Escarpment Plan.

Farm related tourism uses shall not exceed 250 square metres of gross floor area. The implementing Zoning By-law shall further detail appropriate performance standards for the farm-related tourism establishments.

#### E1.4.8 RESIDENTIAL CARE FACILITIES

Residential care facilities are permitted in single detached dwellings subject to the requirements of the implementing Zoning By-law and Section G11 of this Plan, including:

- a) a minimum separation between facilities; and,
- b) a minimum floor space per resident.

The establishment of residential care facilities in other suitable locations, which are not permitted by the implementing Zoning By-law, may be permitted through site-specific amendment to the applicable Zoning By-law provided Council is satisfied with respect to:

- a) the adequacy of the separation distance between the proposed facility and all other residential care facilities;
- b) the suitability of the dwelling size and lot size, in relation to the number of residents to be accommodated; and,
- c) the adequacy and proximity of transportation and community support services, including medical and educational services, required by the residents.

#### E1.4.9 RECREATIONAL AND OTHER NON-AGRICULTURAL USES

The development of new recreational uses and expansions to existing recreational uses, such as golf courses and driving ranges, and cemeteries is not permitted on lands designated *Agricultural Area* by this Plan since it is the intent of this Plan to protect lands which are suitable for agricultural uses for as long as possible. However, Official Plan and Zoning By-law applications to develop such uses may be considered subject to the submission of appropriate studies, including an Agricultural Impact Assessment, that demonstrates to the satisfaction of the Town and the Region of Halton that:

- a) there is a need within the planning horizon of this Plan for the proposed use;
- b) there are clearly no other reasonable alternatives that are outside of Prime *Agricultural Areas*;
- c) there are no reasonable alternative locations in Prime *Agricultural Areas* with lower priority agricultural lands;

- d) the proposed use will not be located in an area that may have an impact on the efficient and logical expansion of nearby urban areas; and,
- e) the proposed use conforms to the applicable criteria set out in Section E2.4.4 of this Plan.

Residential dwelling units and overnight accommodation uses shall not be permitted as part of a proposed recreational use.

#### E1.4.10 ACCESSORY APARTMENTS

An accessory apartment is permitted on existing lots subject to the regulations of the Zoning By-law and the following criteria:

- a) the accessory apartment shall comply with the Ontario Building and Fire Codes;
- b) adequate parking is available on the lot for both dwelling units and minimizes the loss of outdoor amenity areas or landscaping;
- c) the accessory apartment is designed and located in such a manner to not have a negative impact on the character of the surrounding residential uses and to that end any building addition shall be compatible with the massing, height, and setbacks of adjacent dwelling units; and,
- d) municipal water and wastewater or septic and well facilities are adequate and available to accommodate an additional dwelling unit.

#### E1.4.11 INDOOR CANNABIS CULTIVATION

The indoor cultivation of cannabis may be permitted subject to the passage of an amendment to the implementing zoning by-law and will if approved through such a process, be subject to Site Plan Control in accordance with Section G8 of this Plan. Prior to considering the approval of a zoning by-law amendment, Council shall be satisfied that:

- a) The use shall not have a negative impact on the enjoyment and privacy of neighbouring properties;
- b) The proposed use will not have a negative impact on adjacent agricultural uses and is compatible with normal practices as set out in an Agricultural Impact Assessment to the satisfaction of the Town and the Region;
- c) The proposed use will not cause any traffic hazards or an unacceptable level of congestion on surrounding roads;
- d) The proposed use can be designed and sited to blend in with surrounding land uses such that the rural character of the area is maintained; and where necessary the proposed use can be appropriately buffered from adjacent uses;
- e) The impact of the noise, odour and dust generated by the proposed use on adjacent land uses can be appropriately mitigated;
- f) There will be no negative impact on the quality and quantity of groundwater and surface water;

- g) Adequate parking facilities are available on the lot for the proposed use;
- h) The use can be serviced with an appropriate water supply and an appropriate means of sewage disposal;
- i) Stormwater management needs can be met on site;
- j) The signage advertising the use is to be designed and located in accordance with the Town's sign by-law; and
- k) The proposed setback from adjacent land uses is appropriate, and in this regard, the minimum setback from proposed operations should be at least 150 metres. However, if a setback that is appropriate for the site can be established and if it less than 150 metres, an Amendment to this Plan will not be required. Additionally, where proposed cultivation is to be operated under a micro cultivation licence, consideration for reduced setbacks (where required) will be given.

Notwithstanding the above, a lot on which cannabis cultivation is located shall be set back 150 metres from Urban, Hamlet, and Rural Cluster Area lots where a sensitive land use (specified in Section D3.4.1.4.8) is present.

In addition to the above, the required studies for a proposed indoor cannabis cultivation operation, that address matters identified in Section E1.4.11 of this Plan, shall take into account the impacts from other cannabis cultivation operations developing or operating in the area.

Where zoning by-laws do not apply (in the Niagara Escarpment Plan Area), the above-noted criteria will be applied in considering the suitability of the location for indoor cannabis cultivation, including setbacks from Rural Cluster Area lots where a sensitive land use (specified in section D3.4.1.4.8) is present. These criteria will be applied in addition to any other relevant municipal or provincial policy.

#### E1.4.12 THE PROCESSING OF CANNABIS

The processing of cannabis may be permitted as an agriculture-related use, operating in conjunction with a cannabis cultivation operation, subject to the passage of an amendment to the implementing zoning by-law and will if approved through such a process, be subject to Site Plan Control in accordance with Section G8 of this Plan. Prior to considering the approval of a zoning by-law amendment, Council shall be satisfied that the matters listed in Section E1.4.11 a) to k) have been satisfied.

In addition to the above, and in order for the use to qualify as an agriculture-related use, it must be demonstrated that the proposed use satisfies all of the criteria below:

- e) Is directly related to farm operations in the area;
- f) Supports agriculture;
- g) Benefits from being in close proximity to farm operations; and
- h) Provides direct products and/or services to farm operations as a primary activity.

In considering the above, regard shall be given to the Guidelines on Permitted Uses in Ontario's Prime Agricultural Areas prepared by the Ontario Ministry of Agriculture Food and Rural Affairs in 2016.



Notwithstanding the above, a lot on which cannabis processing is located shall be set back 150 metres from Urban, Hamlet, and Rural Cluster Area lots where a sensitive land use (specified in Section D3.4.1.4.8) is present.

Where zoning by-laws do not apply (in the Niagara Escarpment Plan Area), the above-noted criteria will be applied in considering the suitability of the location for cannabis processing, including setbacks from Rural Cluster Area lots where a sensitive land use (specified in section D3.4.1.4.8) is present. These criteria will be applied in addition to any other relevant municipal or provincial policy.

#### E1.4.13 OUTDOOR CANNABIS CULTIVATION

The outdoor cultivation of cannabis is required to be set back a minimum of 50 metres from adjacent lot lines.

#### E1.5 IMPLEMENTING ZONING BY-LAW

All lands in the *Agricultural Area* designation shall be placed in an Agricultural or Rural Zone in the implementing Zoning By-law.

#### E1.6 SPECIAL POLICY AREAS

Special Policy Areas apply to those lands within the *Agricultural Area* that are subject to a land use policy that is specific to a property or area. All other relevant policies of this Plan are applicable unless otherwise modified or exempted by the provisions applicable to each Special Policy Area.

##### E1.6.1 SPECIAL POLICY AREA 1

The following uses shall be permitted on lands designated as *Agricultural Area* and identified as Special Policy Area 1, as shown on Schedule A1 of this Plan:

- a) agricultural activities located on the northern and southern portions of the property;
- b) a place of worship and accessory assembly area including an accessory rectory/residence and administrative office;
- c) a private park that includes social, cultural, athletic and/or recreational activities;
- d) storage facilities necessarily incidental to the place of worship and private park; and,
- e) accessory uses excluding a cemetery, a mausoleum, a crematorium, a school, a campground and any residential use other than a rectory accessory to the place of worship.

In addition to the above, the commercial use of the private park is prohibited and the location of the non-agricultural uses shall be restricted to the central 29 hectares of the property. The site shall also be serviced with municipal water from the Region of Peel. The property is also subject to Site Plan Control.

##### E1.6.2 SPECIAL POLICY AREA 2

A day use activity centre and a commercial driving range is permitted on lands designated as *Agricultural Area* and identified as Special Policy Area 2, as shown on Schedule A1 of this Plan. In addition, permitted

uses on the eastern and western portions of the property shall be limited to agricultural uses and a permanent second non-farm residence. The precise location of the day use activity centre and the commercial driving range shall be delineated in the implementing Zoning By-law.

#### E1.6.3 SPECIAL POLICY AREA 3

A place of worship not exceeding 1250 square metres of gross floor area with accessory uses is permitted on lands designated as *Agricultural Area* and identified as Special Policy Area 3, as shown on Schedule A1 of this Plan. The place of worship shall be located in the southeast corner of the property and shall be more precisely defined in the implementing Zoning By-law. Permitted uses on the balance of the lands subject to Special Policy Area 3 shall be in accordance with Section E1.3 of this Plan.

#### E1.6.4 SPECIAL POLICY AREA 4

Permitted uses on lands designated as *Agricultural Area* and identified as Special Policy Area 4, as shown on Schedule A1 of this Plan, shall be limited to existing uses pending the completion of a secondary planning exercise for the *Phase 1B Employment Area* as specified in Section D3.5.4.4.3 of this Plan.

## E2 PROTECTED COUNTRYSIDE AREA

### E2.1 OBJECTIVES

It is the objective of this designation to:

- a) protect against the loss and fragmentation of the agricultural land base;
- b) support agriculture as the predominant land use;
- c) promote the agricultural industry and associated activities and enhance their capacity to contribute to the economy of the Town;
- d) give permanent protection to the natural heritage and water resource systems that sustain ecological and human health;
- e) provide for a diverse range of economic and social activities associated with rural communities, agricultural, tourism and recreation and resource uses;
- f) to protect, maintain and enhance natural heritage, hydrologic and landform features and functions, including protection of habitat for flora and fauna and particularly species at risk;
- g) protect, improve or restore the quality and quantity of ground and surface water and the hydrologic integrity of watersheds;
- h) preserve the open space character of the rural landscape; and,
- i) recognize the benefits of protecting renewable and non-renewable natural resources.

### E2.2 LOCATION

The *Protected Countryside Area* designation as shown on Schedule A2 to this Plan applies to lands within the Greenbelt Plan established by the Province of Ontario, excluding the *Niagara Escarpment Plan Area*, that are the site of agricultural and rural land uses. Lands within the *Protected Countryside Area* designation that are south and east of the *Niagara Escarpment Plan Area* are considered to form part of the Town's Prime *Agricultural Area*.

### E2.3 PERMITTED USES

Permitted uses in the *Protected Countryside Area* designation are limited to:

- a) agricultural operations;
- b) single detached dwellings on existing lots;
- c) bed and breakfast establishments subject to Section E1.4.3 of this Plan;
- d) home occupations and cottage industries subject to Sections E1.4.4 of this Plan;
- e) home industries subject to Section E1.4.5 of this Plan;
- f) accessory residential uses on farm properties subject to Section E1.4.2 of this Plan;

- g) veterinary clinics serving the agricultural community on lands north and west of the *Niagara Escarpment Plan Area*;
- h) commercial animal kennels on lands north and west of the *Niagara Escarpment Plan Area* subject to Section E2.4.2 of this Plan;
- i) passive non-motorized recreational uses, such as nature viewing and pedestrian trail activities, only on publicly owned lands or on the Bruce Trail;
- j) forest, wildlife and fisheries management;
- k) watershed management and flood and erosion control projects carried out or supervised by a public agency;
- l) archaeological activities;
- m) small scale public uses on lands north and west of the *Niagara Escarpment Plan Area*;
- n) cemeteries on lands north and west of the *Niagara Escarpment Plan Area* subject to Section E2.4.3 of this Plan;
- o) transportation and utility facilities;
- p) commercial uses on farm properties subject to Section E1.4.6 of this Plan;
- q) farm-related tourism establishments subject to Section E1.4.7 of this Plan;
- r) residential care facilities subject to Section E1.4.8 of this Plan;
- s) wayside pits and quarries, portable asphalt plants and portable concrete plants used on public authority contracts, subject to the requirements of the Ministry of Natural Resources and the Ministry of Environment;
- t) berming, screening, temporary stockpiling of earthen material, accessory structures and facilities normally associated with a mineral extraction operation, and facilities and servicing for, the water management and monitoring system and associated infrastructure for the quarry extension, on lands forming the buffer and setback for the Mineral Resource Extraction Area, which may be included within the license area approved under the Aggregate Resources Act, in Part of Lots 20, 21, and 22, Concession 3, in the Town of Halton Hills, Regional Municipality of Halton. Extraction shall be prohibited in this area;
- u) development related to the installation of, access to, facilities and servicing for, the water management and monitoring system within the Greenbelt Greenlands, associated with the mineral aggregate operation located in the Mineral Resource Extraction Area, in Part of Lots 20, 21, and 22, Concession 3, in the Town of Halton Hills, Regional Municipality of Halton;
- v) development related to the installation of, access to, facilities and servicing for, the water management and monitoring system within the mineral aggregate operation located in the Mineral Resource Extraction Area, in Part of Lots 20 and 21, Concession 3, in the Town of Halton Hills, Regional Municipality of Halton;
- w) cannabis cultivation subject to Section E1.4.11;

- x) cannabis processing subject to Section E1.4.12;
- y) outdoor cannabis cultivation subject to Section E1.4.13; and
- z) accessory apartments in single detached dwellings subject to Section E1.4.10.

## E2.4 LAND USE POLICIES

### E2.4.1 THE CREATION OF NEW LOTS

In accordance with the intent of this Plan to maintain and protect the agricultural resources and rural character of the Town, lot creation is prohibited unless specifically provided for in Section F1.2 of this Plan.

### E2.4.2 COMMERCIAL ANIMAL KENNELS

Commercial animal kennels may be permitted in the *Protected Countryside Area* designation subject to an amendment to the implementing Zoning By-law. Before considering such an amendment, Council shall be satisfied that:

- a) the size of the proposed animal kennel is appropriate for the area;
- b) the building housing the animal kennel and the associated animal runs is set back at least 100 metres from lot lines;
- c) the use is located at least 500 metres from lands within the Urban Area, *Rural Cluster Area*, *Hamlet Area* and *Country Residential Area* designations;
- d) the noise emanating from the kennel will not have a negative impact on adjacent properties;
- e) an appropriate animal waste management plan is put in place;
- f) an undue concentration of animal kennels does not already exist in the general vicinity of the proposed kennel; and,
- g) where applicable, the use conforms with Section E2.5 of this Plan.

A new animal kennel shall also be subject to a Site Plan Agreement in accordance with Section G8 of this Plan. Notwithstanding the foregoing, a commercial animal kennel shall not be permitted on lands within the *Protected Countryside Area* that are below the *Niagara Escarpment Plan Area*.

### E2.4.3 CEMETERIES

Cemeteries may be permitted in the *Protected Countryside Area* designation subject to an amendment to the implementing Zoning By-law. A cemetery may include, as an accessory use, a mausoleum and/or a crematorium. Before considering such an amendment, Council shall be satisfied that:

- a) the size of the cemetery and the accessory uses are appropriate for the area;
- b) no lands are available for a cemetery in the urban areas;

- c) the cemetery will not create the need to develop other uses, such as a place of worship, on the site in the future;
- d) appropriate hydrogeological studies have been completed which indicate that the use will not have a negative impact on the quantity and quality of groundwater and surface water;
- e) the proposed use can be accessed by roads that are designed to accommodate high volumes of traffic in short periods of time; and,
- f) where applicable, the use conforms with Section E2.5 of this Plan.

Notwithstanding the foregoing, a cemetery shall not be permitted on lands within the *Protected Countryside Area* that are below the *Niagara Escarpment Plan Area*.

#### E2.4.4 RECREATION USES

The development of any new recreational uses and expansions to existing recreational uses, such as golf courses and driving ranges, shall require an Amendment to the Official Plan and implementing Zoning By-law and shall be subject to Site Plan Control. Residential dwelling units and overnight accommodation uses shall not be permitted as part of a proposed recreational use. New and expanding recreational uses shall not be considered for lands below the *Niagara Escarpment Plan Area*.

Before approving an amendment to the Official Plan and Zoning By-law, Council shall be satisfied through the submission of appropriate studies that:

- a) there is a need within the planning horizon for the proposed use;
- b) the agricultural capability of the subject lands has been evaluated and it is demonstrated that the proposed use will not have a negative impact on adjacent agricultural uses and is compatible with normal farm practices;
- c) the proposed use is located on a Provincial highway or arterial road, where traffic impacts on the rural character of the area would be less;
- d) the proposed use can be designed and sited to blend in with surrounding land uses such that the rural character of the area is maintained;
- e) there will be no negative impacts on any natural heritage features and related ecological functions;
- f) where appropriate the proposed use can be appropriately buffered from adjacent uses;
- g) there will be no negative impact on the quality and quantity of groundwater and surface water; and,
- h) an appropriate vegetation enhancement plan that incorporates planning, design, landscaping and construction measures has been prepared that:
  - i) maintains or where possible enhances the amount of natural self-sustaining vegetation on the site and the connectivity between adjacent Key Natural Heritage Features or Key Hydrologic Features as defined in Section E2.5;

- ii) wherever possible, keeps intermittent stream channels and drainage swales in a free to grow, low maintenance condition;
- iii) minimizes the application and use of pesticides and fertilizers; and,
- iv) locate new natural self-sustaining vegetation in areas that maximizes the ecological value of the area.

Any application to expand or establish a golf course or private recreational use shall be accompanied by a Conservation Plan demonstrating how water use and nutrient biocide will be kept to a minimum, including the establishing and monitoring of targets.

## E2.5 NATURAL SYSTEM

### E2.5.1 BASIS

The *Protected Countryside Area* designation contains a Natural System that provides a continuous and permanent land base necessary to support human and ecological health in the Greenbelt and beyond. The Natural System policies are intended to protect areas of natural heritage, hydrologic and/or landform features, which are often functionally inter-related and which collectively support bio-diversity and overall ecological integrity. The Natural System is made up of a Natural Heritage System and a Water Resource System that often coincide given ecological linkages between terrestrial and water-based functions.

The Natural Heritage System as identified by the Province is shown as an overlay on Schedule A2 to this Plan. Included within the Natural Heritage System are Key Natural Heritage Features and Key Hydrologic Features which have been designated as *Greenbelt Greenlands* by this Plan. The Natural Heritage System may be further refined following the completion of a more detailed Greenbelt Plan implementation exercise to be undertaken by the Region of Halton, in consultation with the Town of Halton Hills.

### E2.5.2 NATURAL HERITAGE SYSTEM POLICIES

- a) New buildings or structures for agriculture, agricultural-related and secondary uses are not subject to the Natural Heritage System policies below, but are subject to the policies on Key Natural Heritage Features and Key Hydrologic Features in Section E2.5.3.
- b) New development or site alteration in the Natural Heritage System (as permitted by the policies of this Plan) shall demonstrate that:
  - i) there will be no negative impacts on Key Natural Heritage Features or Key Hydrologic Features or their functions;
  - ii) connectivity between Key Natural Heritage Features and Key Hydrologic Features is maintained, or where possible, enhanced for the movement of native plants and animals across the landscape;
  - iii) the removal of other natural features not identified as Key Natural Heritage Features and Key Hydrologic Features should be avoided. Such features should be incorporated into the planning and design of the proposed use wherever possible; and,

- iv) the disturbed area of any site does not exceed 25 percent, and the impervious surface does not exceed 10 percent, of the total developable area, except for recreational and mineral aggregate. With respect to golf courses, the disturbed area shall not exceed 40 percent of the site; and
- c) Where non-agricultural uses requiring an amendment to this Plan and/or the implementing zoning by-law or site plan approval are proposed within the Natural Heritage System, applicants shall demonstrate that:
  - i) at least 30 percent of the total developable area of the site will remain or be returned to natural self-sustaining vegetation;
  - ii) connectivity along the system and between Key Natural Heritage Features or Key Hydrologic Features located within 240 metres of each other is maintained or enhanced; and,
  - iii) buildings or structures do not occupy more than 25 percent of the total developable area and are planned to optimize the compatibility of the project with the natural surroundings.

### E2.5.3 KEY NATURAL HERITAGE FEATURES AND KEY HYDROLOGIC FEATURES POLICIES

#### E2.5.3.1 Key Natural Heritage Features include:

- significant habitat of endangered species, threatened species and special concern species;
- fish habitat;
- wetlands;
- Life Science Areas of Natural and Scientific Interest (ANSIs);
- significant valleylands;
- significant woodlands;
- significant wildlife habitat;
- sand barrens, savannahs and tall grass prairies; and,
- alvars.

Some Key Natural Heritage Features are within the *Greenbelt Greenlands* designation as shown on Schedule A2. The incorporation of additional Key Natural Heritage Features will occur following the completion of a more detailed Greenbelt Plan implementation exercise to be undertaken by the Region of Halton, in consultation with the Town of Halton Hills.

#### E2.5.3.2 Key Hydrologic Features include:

- permanent and intermittent streams;
- lakes (and their littoral zones);



- seepage areas and springs; and
- wetlands.

Some Key Hydrologic Features are within the *Greenbelt Greenlands* designation as shown on Schedule A2. The incorporation of additional Key Hydrologic Features will occur following the completion of a more detailed Greenbelt Plan implementation exercise to be undertaken by the Region of Halton, in consultation with the Town of Halton Hills.

#### E2.5.3.3 Policies

For lands within a Key Natural Heritage Feature or a Key Hydrologic Feature the following policies shall apply:

- a) Development or site alteration is not permitted in Key Hydrologic Features and Key Natural Heritage Features within the Natural Heritage System, including any associated vegetation protection zone, with the exception of:
  - i) forest, fish and wildlife management;
  - ii) conservation and flood or erosion control projects, but only if they have been demonstrated to be necessary in the public interest and after all alternatives have been considered; or
  - iii) infrastructure subject to Section E2.6 of this Plan;
  - iv) single detached dwellings and accessory uses on existing lots of record if it is demonstrated that:
    - there is no alternative and the expansion, alteration or establishment is directed away from the feature to the maximum extent possible; and,
    - the impact of the expansion or alteration on the feature and its functions is minimized to the maximum extent possible;

Notwithstanding the foregoing, single detached dwellings shall not be permitted on those portions of the lot containing provincially significant wetlands, significant habitat of endangered and threatened species and the Regulatory Floodplain;

  - v) existing agricultural operations and expansions to existing agricultural buildings and structures and accessory uses within Key Natural Heritage Features and Key Hydrologic Features if it demonstrated that:
    - there is no alternative and the expansion, alteration or establishment is directed away from the feature to the maximum extent possible; and,
    - the impact of the expansion or alteration on the feature and its functions is minimized to the maximum extent possible; and,
  - vi) mineral aggregate extraction operations subject to Section E6.5 of this Plan.

- b) Beyond the Natural Heritage System within the *Protected Countryside Area*, as shown on Schedule A2 to this Plan, Key Hydrologic Features are defined by and subject to the natural features policies of section E2.5.3.
- c) In the case of wetlands, seepage areas and springs, fish habitat, permanent and intermittent streams, lakes, and significant woodlands, the minimum vegetation protection zones shall be a minimum of 30 metres wide measured from the outside boundary of the Key Natural Heritage Feature or Key Hydrologic Feature.
- d) A proposal for new development or site alteration within 120 metres of a Key Natural Heritage Feature within the Natural Heritage System or a Key Hydrologic Feature anywhere within the *Protected Countryside Area* designation requires an Environmental Impact Study, including a natural heritage evaluation and hydrological evaluation, which identify a vegetation protection zone which:
  - i) is of sufficient width to protect the Key Natural Heritage Feature or Key Hydrologic Feature and its functions from the impacts of the proposed change and associated activities that may occur before, during and after, construction, and where possible, restore or enhance the feature and/or its function; and,
  - ii) is established to achieve, and be maintained as natural self-sustaining vegetation.
- e) Notwithstanding the natural features policies of section E2.5.3 of this Plan, new buildings and structures for agricultural uses will be required to provide a 30 metre vegetation protection zone from a Key Natural Heritage Feature or Key Hydrologic Feature, but may be exempted from the requirement of establishing a condition of natural self-sustaining vegetation if the land is, and will continue to be, used for agricultural purposes. Despite this exemption, agricultural uses should pursue best management practices to protect and/or restore Key Hydrologic Features and functions.

## E2.6 INFRASTRUCTURE

### E2.6.1 GENERAL INFRASTRUCTURE POLICIES

The following policies shall apply:

- a) All existing, expanded or new infrastructure subject to and approved under the Canadian Environmental Assessment Act, the Environmental Assessment Act, the Planning Act, the Aggregate Resources Act, the Telecommunications Act or by the National or Ontario Energy Boards, or which receives a similar environmental approval, is permitted within the *Protected Countryside Area*, subject to the policies of this section and provided it meets one of the following two objectives:
  - i) It supports agriculture, recreation and tourism, rural settlement areas, resource use or the rural economic activity that exists and is permitted within the Greenbelt; or
  - ii) It serves the significant growth and economic development expected in Southern Ontario beyond the Greenbelt by providing for the appropriate infrastructure connections among urban growth centres and between these centres and Ontario's borders.
- b) The location and construction of infrastructure and expansions, extensions, operations and maintenance of infrastructure in the *Protected Countryside Area*, are subject to the following:
  - i) Planning, design and construction practices shall minimize, wherever possible, the amount of the Greenbelt, and particularly the Natural Heritage System, traversed and/or occupied by such infrastructure;
  - ii) Planning, design and construction practices shall minimize, wherever possible, the negative impacts and disturbance of the existing landscape, including, but not limited to, impacts caused by light intrusions, noise and road salt;
  - iii) Where practicable, existing capacity and coordination with different infrastructure services is optimized so that the rural and existing character of the *Protected Countryside Area* and the overall urban structure for southern Ontario established by Greenbelt and any provincial growth management initiatives are supported and reinforced;
  - iv) New or expanding infrastructure shall avoid Key Natural Heritage Features or Key Hydrologic Features unless need has been demonstrated and it has been established that there is no reasonable alternative; and,
  - v) Where infrastructure does cross the Natural Heritage System or intrude into or result in the loss of a Key Natural Heritage Feature or Key Hydrologic Feature, including related landform features, planning, design and construction practices shall minimize negative impacts and disturbance on the features or their related functions and where reasonable, maintain or improve connectivity.
- c) Infrastructure serving the agricultural sector, such as agricultural irrigation systems, may need certain elements to be located within the vegetation protection zone of a Key Natural Heritage

Feature or Key Hydrologic Feature. In such instances, these elements of the infrastructure may be established within the feature itself or its associated vegetation protection zone but all reasonable efforts shall be made to keep such infrastructure out of Key Natural Heritage Features or Key Hydrologic Features or the vegetation protection zones.

#### E2.6.2 STORMWATER MANAGEMENT INFRASTRUCTURE POLICIES

In addition to the above general infrastructure policies the following policies apply to stormwater management infrastructure proposals:

- a) Stormwater management ponds are prohibited in Key Natural Heritage Features or Key Hydrologic Features or their vegetation protected zones, except for those portions of the *Protected Countryside Area* that define the major river valleys that connect the Niagara Escarpment to Lake Ontario. In these areas, naturalized stormwater management ponds are permitted provided they are located a minimum of 30 metres away from the edge of the river/stream and in the vegetation protection zones of any abutting Key Natural Heritage Features or Key Hydrologic Features.
- b) Applications for development and site alteration in the *Protected Countryside Area* shall be accompanied by a stormwater management plan which demonstrates that:
  - i) planning, design and construction practices will minimize vegetation removal, grading and soil compaction, sediment erosion and impervious surfaces;
  - ii) where appropriate, an integrated treatment approach shall be used to minimize stormwater management flows and structures through such measures as lot level controls and conveyance techniques such as grass swales; and,
  - iii) applicable recommendations, standards or targets within watershed plans and water budgets are complied with.
- c) The objectives of a stormwater management plan are to avoid, minimize and/or mitigate stormwater volume, contaminant loads and impacts to receiving watercourses in order to:
  - i) maintain groundwater quality and flow and stream baseflow;
  - ii) protect water quality;
  - iii) minimize the disruption of pre-existing (natural) drainage patterns wherever possible;
  - iv) prevent increases in stream channel erosion;
  - v) prevent any increase in flood risk; and,
  - vi) protect aquatic species and their habitat.

#### E2.7 IMPLEMENTING ZONING BY-LAW

All lands within the *Protected Countryside Area* designation shall be placed in a Protected Countryside Zone in the implementing Zoning By-law. Key Natural Heritage Features and Key Hydrologic Features

and related vegetation protection zones shall be placed in a series of Environmental Protection Zones in the implementing Zoning By-law.

## E3 HAMLET AREA

### E3.1 OBJECTIVES

It is the objective of this designation to:

- a) recognize the Hamlets of Glen Williams and Norval as unique and historic communities that provide a transition between the Georgetown Urban Area and the surrounding agricultural and rural landscape;
- b) carefully control new residential development in the Hamlets in order to maintain the character and scale of Glen Williams and Norval;
- c) provide opportunities for small-scale commercial and tourism related uses that are compatible with the character and scale of the Hamlets; and,
- d) ensure that all development in the Hamlets is serviced by appropriate sewer and water services that reflect the location of each Hamlet, the amount of planned development in each and its character.

### E3.2 LOCATION

The *Hamlet Area* designation as shown on Schedule A1 to this Plan applies to the communities of Glen Williams and Norval, adjacent to the Georgetown Urban Area. The establishment of new Hamlets is not permitted by this Plan. The *Hamlet Area* designation is generally intended to be comprised of the following designations:

- *Hamlet Residential Area;*
- *Hamlet Community Core Area;*
- *Institutional Area;*
- *Open Space Area; and,*
- *Greenlands.*

Additional designations may be established through the preparation of Hamlet Secondary Plans.

### E3.3 LAND USE POLICIES

#### E3.3.1 SECONDARY PLAN CONFORMITY

All development within *Hamlet Areas* is subject to the policies included within approved Hamlet Secondary Plans and the other relevant policies of this Plan. In the event of a conflict between the policies contained in this Plan and those contained in Secondary Plans, the Secondary Plan policies shall prevail to the extent of the conflict within the geographic area covered by the Secondary Plan. Secondary Plans for the Hamlets of Glen Williams and Norval are contained within Section H of this Plan.

### E3.3.2 HAMLET AREA EXPANSIONS

At the time of the adoption of this Plan, the Town had completed Secondary Plans for the Hamlets of Norval and Glen Williams. As part of the Norval Secondary Plan process, it was determined that an expansion to the Hamlet of Norval was not appropriate. Subsequent to the approval of the Secondary Plan, the Hamlet of Norval was included within the Greenbelt Plan. A modest expansion to the Hamlet of Glen Williams was approved flowing from the results of the Glen Williams Integrated Planning Project. The Hamlet of Glen Williams is largely confined by the *Protected Countryside Area* of the Greenbelt Plan and the Georgetown Urban Area. On this basis, further expansions to the Hamlets of Glen Williams and Norval are prohibited by this Plan.

### E3.3.3 SERVICING

Existing and limited infill development within Hamlets is generally intended to be serviced on the basis of either private, individual on-site water services or municipal water supply and private, individual on-site sewage services. The Region of Halton has reserved a 2,600 equivalent population in the Georgetown Wastewater Pollution Control Plant to service the Hamlets when the Region, in consultation with the Town, determines it to be prudent and feasible. The Secondary Plans for the Hamlets shall provide more detailed policy direction regarding servicing.

### E3.3.4 MINIMUM LOT SIZE

The minimum lot size for new residential development serviced by either private, individual on-site water services or municipal water supply and private individual on-site sewage services shall be 1,900 square metres or larger and must meet minimum criteria set forth by the Regional Medical Officer of Health. Multi-lot residential development shall require the preparation of a hydrogeological study satisfactory to the Town and the Region of Halton. The minimum lot size for new development on full urban services shall be 929 square metres. Larger lot sizes may be established in Secondary Plans to reflect the character of established hamlet residential neighbourhoods.

### E3.3.5 NEW RESIDENTIAL LOTS BY CONSENT

The creation of new lots for a residential use by consent to sever is permitted, provided a Plan of Subdivision is not required in accordance with Section F1.1 (Preferred Means of Land Division). The creation of new lots for residential purposes shall comply with Sections E3.3.4 and F1.2 (New Lots By Consent) of this Plan.

### E3.3.6 MAXIMUM NON-RESIDENTIAL FLOOR AREA

The maximum size of a permitted non-residential use shall be 500 square metres of gross floor area and may require a hydrogeological study to the satisfaction of the Town and the Region of Halton.

### E3.3.7 RELATIONSHIP BETWEEN USES

Where a proposed non-residential use abuts or is in close proximity to an existing residential use, fencing, landscaping or berming, or a combination of these features shall be utilized to ensure that there is adequate screening between the uses.

#### E3.3.8 USE OF SITE PLAN CONTROL

The establishment of a new permitted non-residential use or an expansion to an existing non-residential use shall be subject to Site Plan Control in accordance with Section G8 of this Plan.

#### E3.4 DESIGNATIONS

##### E3.4.1 HAMLET RESIDENTIAL AREA

Permitted uses in the *Hamlet Residential Area* designation are limited to:

- a) single detached dwellings;
- b) bed and breakfast establishments subject to Section E1.4.3 of this Plan;
- c) home occupations and cottage industries subject to Section E1.4.4 of this Plan;
- d) residential care facilities meeting the definition of a Group Home Type 1, subject to the requirements of the implementing Zoning By-law and Section G11 of this Plan; and
- e) accessory apartments in single detached dwellings subject to Section E1.4.10.

##### E3.4.2 HAMLET COMMUNITY CORE AREA

Permitted uses in the *Hamlet Community Core Area* designation are limited to:

- a) retail and service commercial uses;
- b) restaurants;
- c) institutional uses;
- d) open space uses;
- e) public parking uses;
- f) single detached dwellings;
- g) bed and breakfast establishments subject to the provisions of Section E1.4.3 of this Plan;
- h) home occupations and cottage industries subject to Section E1.4.4 of this Plan;
- i) residential care facilities meeting the definition of a Group Home Type 1, subject to the requirements of the implementing Zoning By-law and Section G11 of this Plan; and
- j) accessory apartments in single detached dwellings subject to Section E1.4.10.

##### E3.4.3 INSTITUTIONAL AREA

Permitted uses in the *Institutional Area* designation are limited to public and private institutional uses including schools, place of worship, community centres, nursing homes, funeral homes, public utilities and day care centres.



#### E3.4.4 MAJOR PARKS AND OPEN SPACE AREA

Permitted uses and applicable policies in the *Major Parks and Open Space Area* designation shall be in accordance with Section B2 and Section F7 of this Plan.

#### E3.4.5 PRIVATE OPEN SPACE AREA

Permitted uses and applicable policies in the *Private Open Space Area* designation shall be in accordance with Section B3 of this Plan.

#### E.3.4.6 GREENLANDS A AND GREENLANDS B

Permitted uses and applicable policies in the *Greenlands A* and *Greenlands B* designations shall be in accordance with Section B1 of this Plan.

#### E3.5 IMPLEMENTING ZONING BY-LAW

All lands that are used for residential purposes shall be placed in a Residential Zone in the implementing Zoning By-law. Lands that are used for existing non-residential uses shall be placed in appropriate zones that recognize the use. All undeveloped land that is designated *Hamlet Residential Area* and *Hamlet Community Core Area*, may be placed in a Development Zone until a development application is approved by Council. Permitted uses in the Development Zone shall be restricted to uses that existed on the date the implementing Zoning By-law comes into effect.

## E4 RURAL CLUSTER AREA

### E4.1 OBJECTIVES

It is the objective of this designation to:

- a) provide opportunities for residential and other non-farm development in the rural area;
- b) recognize existing and identifiable communities within the Agricultural/Rural Area and the *Niagara Escarpment Plan Area*;
- c) ensure that Rural Clusters are developed in a logical and cost-effective manner; and,
- d) ensure that all new development contributes to and enhances the character of the rural area.

### E4.2 LOCATION

The *Rural Cluster Area* designation as shown on Schedules A1 and A2 applies to the communities of Ashgrove, Ballinafad, Bannockburn, Crewsons Corners, Henderson's Corners, Limehouse, Silvercreek, and Terra Cotta.

The precise boundaries of each of the Rural Clusters are shown on Schedules A9 to A16 inclusive. The expansion of the Rural Clusters beyond the boundaries established by the Plan shall not be permitted. In addition, the establishment of new Rural Clusters shall not be permitted.

### E4.3 PERMITTED USES

Permitted uses in the *Rural Cluster Area* designation are limited to:

- a) single detached dwellings;
- b) small scale commercial uses that serve the needs of the *Rural Cluster Area* and/or the surrounding rural area subject to Section E4.4.3 of this Plan;
- c) commercial uses that serve the traveling public, such as convenience stores and gas stations subject to Section E.4.4.3 of this Plan;
- d) small scale industrial operations including an accessory retail component that serve the needs of the *Rural Cluster Area* and/or the surrounding rural area subject to Section E4.4.3 of this Plan;
- e) institutional uses such as schools, places of worship, community centres, libraries and similar uses that serve the needs of the *Rural Cluster Area* and/or the surrounding rural area subject to Section E4.4.3 of this Plan;
- f) bed and breakfast establishments subject to Section E1.4.3 of this Plan;
- g) home occupations and cottage industries subject to Section E1.4.4 of this Plan;
- h) local parkland that serves the needs of the *Rural Cluster Area* and/or the surrounding rural area subject to Section F7 of this Plan;

- i) residential care facilities meeting the definition of a Group Home Type 1, subject to the requirements of the implementing Zoning By-law and Section G11 of this Plan; and
- j) accessory apartments in single detached dwellings subject to Section E1.4.10.

#### E4.4 LAND USE POLICIES

##### E4.4.1 SERVICING

All development in a *Rural Cluster Area* shall be serviced by private, individual on-site water services and private, individual on-site sewage services.

##### E4.4.2 NEW RESIDENTIAL LOTS BY CONSENT

The creation of new lots for a residential use by consent to sever is permitted, provided a Plan of Subdivision is not required in accordance with Section F1.1. The creation of new lots for residential purposes shall comply with Section F1.2 (General Consent Policies) of this Plan. The minimum lot size shall be 1,900 square metres or larger and must meet minimum criteria set forth by the Regional Medical Officer of Health.

##### E4.4.3 NEW NON-RESIDENTIAL USES

The predominant use of land within the *Rural Cluster Area* designation shall be residential. New non-residential uses may be permitted in the *Rural Cluster Area* designation subject to an amendment to the implementing Zoning By-law or, where applicable, the issuance of a development permit from the Niagara Escarpment Commission. New non-residential uses shall also be subject to Site Plan Control. Before approving an amendment to the Zoning By-law to permit any of the uses listed in Section E4.3 (Permitted Uses), Council shall be satisfied that:

- a) the proposed use is compatible with the character of the *Rural Cluster Area*;
- b) the proposed use has frontage and direct access onto an arterial road as shown on Schedule B1 to this Plan;
- c) the proposed use serves the needs of the *Rural Cluster Area* and/or the surrounding rural area;
- d) the proposed use can be serviced with an appropriate water supply and an appropriate means of sewage disposal;
- e) the proposed use shall not have a negative impact on the enjoyment and privacy of neighbouring properties;
- f) adequate parking and loading facilities and landscaping can be provided on the site;
- g) any outdoor storage is accessory to the proposed use and shall be located in the rear and side yards and screened from public view or views from adjacent properties using fencing, landscaping, berming or a combination of these features; and,
- h) where a proposed use abuts or is in close proximity to an existing residential use, fencing, landscaping, berming or a combination of these features shall be utilized to ensure that there is adequate screening between the uses.

The maximum size of a permitted non-residential use shall be 500 square metres of gross floor area and may require a hydrogeological study to the satisfaction of the Town and the Region of Halton.

#### E4.5 IMPLEMENTING ZONING BY-LAW

All lands that are used for residential purposes shall be placed in a Residential Zone in the implementing Zoning By-law. Lands that are used for existing non-residential uses shall be placed in appropriate zones that recognize the use. All undeveloped land that is designated *Rural Cluster Area* may be placed in a Development Zone. Permitted uses in the Development Zone shall be restricted to uses that existed on the date the implementing Zoning By-law comes into effect.

#### E4.6 SPECIAL POLICY AREAS

Special Policy Areas apply to those lands within the *Rural Cluster Area* that are subject to a land use policy that is specific to a property or area. All other relevant policies of this Plan are applicable unless otherwise modified or exempted by the provisions applicable to each Special Policy Area.

##### E4.6.1 SPECIAL POLICY AREA 1

A commercial automobile dealership is permitted with a maximum floor area of 2,972 square metres on lands designated as *Rural Cluster Area* and identified as Special Policy Area 1, as shown on Schedule A13 of this Plan. Enclosed self-storage, outdoor self-storage and a caretaker suite that serves the self-storage facility is also permitted on the above subject lands, subject to the following:

- a) the maximum building floor area is limited to 3,716 square metres (40,000 square feet) for all buildings and structures;
- b) all buildings and structures are subject to site plan and architectural control to maintain the character of the *Rural Cluster Area*;
- c) all outdoor storage areas are screened from views off Highway 7, including views through abutting properties; and,
- d) that outdoor storage has a maximum height of 6 metres.

##### E4.6.2 SPECIAL POLICY AREA 2

The uses identified under Section E4.3 are permitted on lands designated as *Rural Cluster Area* and identified as Special Policy Area 2, as shown on Schedule A13 of this Plan, subject to the following criteria:

- a) the uses shall serve the local community;
- b) accessory outdoor storage may be permitted provided that it is setback a minimum of 6.1 metres from any property line and/or the defined limit to this Special Policy Area and shall have a maximum height of 6.0 metres; and,
- c) the compatibility, site design and servicing requirements of any proposed use may be further reviewed and assessed through a Niagara Escarpment development permit application and may be subject to Site Plan Approval in accordance with the Planning Act.

#### E4.6.3 SPECIAL POLICY AREA 3

A commercial use is permitted within a building or structure having a maximum floor area of 929 square metres on lands designated as *Rural Cluster Area* and identified as Special Policy Area 3, as shown on Schedule A9 of this Plan.

#### E4.6.4 SPECIAL POLICY AREA 4

In addition to the uses permitted in Subsection E4.3 of the Official Plan, two (2) existing second storey apartment dwelling units are permitted within the existing building on lands designated as *Rural Cluster Area* and identified as Special Policy Area 4, as shown on Schedule A11 of this Plan.

## E5 COUNTRY RESIDENTIAL AREA

### E5.1 OBJECTIVE

It is the objective of this designation to only recognize existing estate and country residential developments in the Town.

### E5.2 LOCATION

The *Country Residential Area* designation as shown on Schedule A2 to this Plan applies to developed, Draft Approved or approved through a site-specific Official Plan Amendment prior to the adoption of this Plan for large estate lot development serviced by private, individual on-site water services and private, individual on-site sewage services.

### E5.3 PERMITTED USES

Permitted uses on lands designated *Country Residential Area* are limited to:

- a) single detached dwellings;
- b) bed and breakfast establishments subject to Section E1.4.3 of this Plan;
- c) home occupations and cottage industries in accordance with Section E1.4.4 of this Plan;
- d) transportation and utility facilities;
- e) residential care facilities meeting the definition of a Group Home Type 1, subject to the requirements of the implementing Zoning By-law and Section G11 of this Plan; and
- f) accessory apartments in single detached dwellings subject to Section E1.4.10.

### E5.4 PROHIBITED USES

The development of new country or estate residential subdivisions on lands that are not designated *Country Residential Area* on Schedule A2 is prohibited by this Plan consistent with policies contained in the Regional Official Plan that prohibit estate residential development outside of Hamlets and Rural Clusters. In addition, the creation of new lots for residential use within existing subdivisions is prohibited.

### E5.5 IMPLEMENTING ZONING BY-LAW

All lands designated *Country Residential Area* and located within a Registered Plan of Subdivision shall be placed in an appropriate Residential Zone in the implementing Zoning By-law.

### E5.6 SPECIAL POLICY AREAS

Special Policy Areas apply to those lands within the *Country Residential Area* that are subject to a land use policy that is specific to a property or area. All other relevant policies of this Plan are applicable unless otherwise modified or exempted by the provisions applicable to each Special Policy Area.

**E5.6.1 SPECIAL POLICY AREA 1**

The maximum permitted number of lots is twenty (20) with a minimum lot size of 0.787ha on lands identified as Special Policy Area 1, as shown on Schedule A2 of this Plan.

**E5.6.2 SPECIAL POLICY AREA 2**

The maximum permitted number of lots is twenty-three (23) with a minimum lot size of 0.86ha on lands identified as Special Policy Area 2, as shown on Schedule A2 of this Plan.

## E6 MINERAL RESOURCE EXTRACTION AREA

### E6.1 OBJECTIVES

It is the objective of this designation to:

- a) recognize existing pits and quarries and protect them from activities that would preclude or hinder their continued use or expansion;
- b) protect known aggregate deposits and areas of high potential mineral aggregate resources for potential future extraction;
- c) ensure that new pits and quarries will not have a negative impact on significant natural heritage features and related ecological functions in keeping with the principle of net environmental gain;
- d) ensure that the haul routes used are appropriate;
- a) ensure that extractive activities are carried out in a manner that minimizes environmental and social impacts;
- e) ensure that aggregate extraction operations are designed to minimize visual impacts on the open space character of the rural landscape; and,
- f) ensure the progressive rehabilitation of pits and quarries to an appropriate after use.

### E6.2 LOCATION

The *Mineral Resource Extraction Area* designation shown on Schedule A2 to this Plan applies to mineral aggregate operations that are licensed in accordance with the Aggregate Resources Act and for lands within Part of Lot 13 and 14, Concession 1 (Esquesing), the areas defined as the area to be extracted by a valid license pursuant to the Aggregate Resources Act as shown on Schedule A2. Once a license has been surrendered or revoked, the Town will take steps to redesignate the subject lands from the Mineral Resource Extraction Area designation to another appropriate designation.

### E6.3 PERMITTED USES

Permitted uses on lands designated *Mineral Resource Extraction Area* are limited to:

- a) mineral aggregate operations;
- b) agricultural operations;
- c) essential transportation and utility facilities;
- d) watershed management and flood and erosion control projects carried out or supervised by a public authority;
- e) forestry and resource management uses;
- f) archaeological activities;



- g) non-intensive recreation uses such as nature viewing and pedestrian trail activities;
- h) recreation uses including golf course only on lands above the escarpment brow and subject to the following:
  - i) the proposed use will not have a negative impact on adjacent agricultural uses and is compatible with normal practices as set out in an Agricultural Impact Assessment to the satisfaction of the Town and the Region;
  - ii) the proposed use will not cause any traffic hazards or an unacceptable level of congestion on surrounding roads;
  - iii) the proposed use can be designed and sited to blend in with surrounding land uses such that the rural character of the area is maintained;
  - iv) where necessary the proposed use can be appropriately buffered from adjacent uses;
  - v) there is no overnight accommodation for users or guests of the facility;
  - vi) there will be no negative impacts on any significant natural heritage features and related ecological functions;
  - vii) there will be no negative impact on the quality and quantity of groundwater and surface water; and,
  - viii) where applicable the criteria referenced in Section E2.4.4 of this Plan; and,
- i) wayside pits and quarries, portable asphalt plants and portable concrete plants used on public authority contracts in the area with the exception that in designated *Mineral Resource Extraction Areas* within lands subject to the Niagara Escarpment Plan, asphalt plants of any kind shall not be permitted.

The establishment of permanent concrete batching plants or asphalt plants on lands within the *Mineral Resource Extraction Area* designation is not permitted.

## E6.4 LAND USE POLICIES

### E6.4.1 RELATIONSHIP BETWEEN THE TOWN AND THE MINISTRY OF NATURAL RESOURCES

It is recognized that the Ministry of Natural Resources licenses and regulates mineral aggregate operations under the Aggregate Resources Act. It is the intent of this Plan to ensure that there is open and transparent consultation between the appropriate Provincial Ministries and agencies, the proponent of the mineral aggregate operation and the Town before licenses are issued or modified, in order to ensure that new mineral aggregate operations or expansions of existing operations are carried out in a manner that is consistent with the goals and objectives of this Plan.

#### E6.4.2 DEVELOPMENT ADJACENT TO LANDS IN MINERAL AGGREGATE RESOURCES DESIGNATION

When new development requiring a Planning Act approval is proposed within 300 metres of a pit or 500 metres of a quarry within the *Mineral Resource Extraction Area* designation, Council shall be satisfied that the proposed use is compatible with the current and future operation of the pit or quarry. In some cases, setbacks between the uses may be required to minimize conflicts. In order to determine if a new use is compatible, the development proponent will be required to demonstrate that the proposed use shall provide for the necessary mitigation of impacts arising from the existing pit or quarry, including future phases that are under license but are yet to be extracted, to meet applicable Provincial standards, regulations and guidelines. The Town may require the submission of noise, traffic and other studies to demonstrate that this policy is met.

#### E6.4.3 NEW MINERAL AGGREGATE OPERATIONS OR EXPANSIONS TO EXISTING OPERATIONS

##### E6.4.3.1 Introduction

All new mineral aggregate operations and/or expansions to existing mineral aggregate operations onto lands that are not designated *Mineral Resource Extraction Area* shall require an Amendment to the Official Plan and, where applicable, the Niagara Escarpment Plan. Notwithstanding the foregoing, an Official Plan Amendment will not be required for the proposed expansion of an existing sandstone quarry located on Part of Lot 21, Concession 5.

##### E6.4.3.2 Locational Criteria

It is the policy of this Plan:

- a) to direct new or expanded mineral aggregate operations to locate in the *Agricultural Area*, the *Escarpment Rural Area* and the *Protected Countryside Area*;
- b) to prohibit new or expanded mineral aggregate operations from locating in the *Escarpment Natural Area*, *Escarpment Protection Area*, *Urban Area*, *Hamlet Area* and *Rural Cluster Area*; and,
- c) to only permit new or expanded mineral aggregate operations within the Greenlands System where it can be demonstrated that there will be no negative impact on significant natural heritage features and related ecological functions in keeping with the principle of net environmental gain in accordance with Section E6.4.3.5 of this Plan.

##### E6.4.3.3 Mineral Aggregate Operations in Prime Agricultural Areas

In Prime *Agricultural Areas*, on prime agricultural land, extraction of mineral aggregates is permitted as an interim use, subject to Section E6.4.3.1 of this Plan, provided that rehabilitation of the site will be carried out whereby substantially the same areas and same average soil quality for agriculture are restored. On these prime agricultural lands, complete agricultural rehabilitation is not required if:

- a) there is a substantial quantity of mineral aggregates below the water table warranting extraction; or,
- b) the depth of planned extraction in a quarry makes restoration of pre-extraction agricultural capability unfeasible;

- c) other alternatives, including resources in areas of Canada Land Inventory Class 4 to 7 soils and resources on prime agricultural lands where rehabilitation is feasible, have been considered by the applicant and found unsuitable; and,
- d) agricultural rehabilitation in remaining areas will be maximized.

#### E6.4.3.4 Application Requirements

Any application for Amendment to the Official Plan and/or the zoning by-law shall be supported by studies that are based on predictable, measurable, objective effects on people and the environment. Such studies will be based on Provincial standards, regulations and guidelines, where they exist and will consider and identify methods of addressing the anticipated impacts in the area affected by the extractive operation. All applications shall be supported by information that address:

- a) the impact of the operation of the mineral aggregate resource use on:
  - i) the natural heritage features and ecological functions on the site and in the area;
  - ii) nearby communities;
  - iii) agricultural resources and activities;
  - iv) the quality and quantity of groundwater and surface water;
  - v) the built or cultural heritage resources in the area;
  - vi) significant geologic formations on the site and in the area;
  - vii) the groundwater recharge and discharge functions on the site and in the immediate area;
  - viii) surface water features in the area; and,
  - ix) nearby wells used for drinking water purposes;
- b) the effect of the additional truck traffic on the ability of an existing haul route to function as a safe and efficient haul route considering among other matters the following:
  - i) the types of operations proposed;
  - ii) current road standards and an assessment of the proposed haul route relative to those standards;
  - iii) anticipated type of truck traffic; and,
  - iv) increases in background traffic levels together with current levels of truck traffic and other traffic;
- c) the suitability of any new haul route. It is a policy of this Plan to encourage the establishment of new aggregate operations on established haul routes. If a new haul route is proposed, it shall only be approved if it has been demonstrated that:

- i) the new haul route is, or can be made, safe and capable of handling the volume of traffic proposed;
  - ii) the selection and design of the proposed haul route has taken into consideration and addressed impacts on existing and permitted sensitive land uses along the proposed haul route;
  - iii) the design of the new haul route has taken into consideration the existing road right-of-way characteristics including existing trees and vegetation within the road right-of-way, wood, wire, stump and stone fence lines within or adjacent to the right-of-way or other historical landscape remnants and where practical has identified means by which such features will be retained in order to minimize the impacts on the character of the area;
  - iv) the design of the new haul route has taken into consideration the physical characteristics of the potential route including road classification, load limits, road surfacing and the identification of any physical constraints to heavy truck traffic, such as vertical or horizontal curves, sight lines or shoulders and the means to address any deficiencies; and,
  - v) The design of the haul route has taken into consideration the traffic impacts (both operational and physical) resulting from the truck traffic generated by the proposed operation, including impacts on road structure, traffic flow and safety and the mitigation measures that will be employed to address these impacts;
- d) the impact of the noise, odour, dust and vibration generated by the proposed use on adjacent land uses;
  - e) how the policy of no negative impact on significant natural heritage features and related ecological functions on the site and in the area can be satisfied taking into account the net environmental gain provisions of this Plan. This may be achieved, for example, through the progressive rehabilitation or design of a pit or quarry;
  - f) how the impacts from the proposed pit or quarry on adjacent uses will be mitigated in order to lessen those impacts;
  - g) how the site will be progressively rehabilitated to accommodate subsequent land uses after the extraction is completed; and,
  - h) how the visual impacts of the proposed aggregate extraction operation will be minimized through the preparation of appropriate studies that considers potential visual impacts from adjacent roads and from those properties that may be potentially impacted.

Some of the above information shall be contained in an Environmental Impact Study that is prepared in accordance with Section C2 of this Plan.

#### E6.4.3.5 Assessment of Impacts

Where a policy in this Plan requires that mineral aggregate extraction not have a negative impact on a significant natural heritage feature or related ecological function in accordance with the principle of net environmental gain, the Town shall consider compensation and enhancements to be initiated by the

proponent prior to and/or during extraction and the proposed progressive and final rehabilitation of the site.

In addition to the above, the required studies for a proposed pit or quarry, that address the matters identified in Section E6.4.3.4 of this Plan shall take into account the impacts from pits or quarries that are operating or undergoing rehabilitation in the area.

In considering the added impact of a new pit or quarry licence to existing impacts, Council shall ensure that mitigation measures intended to lessen the added impact are reviewed and applied as required.

#### E6.4.4 CRITERIA FOR APPROVAL

Prior to the approval of an Official Plan Amendment and/or Zoning By-law for a new or expanded mineral aggregate operation the applicant shall demonstrate that:

- a) the quality of sensitive groundwater and sensitive surface water features in the area will be protected, improved or restored taking into account any mitigative measures initiated by the proponent prior to and/or during extraction and the proposed rehabilitation plan, in accordance with the recommendations of appropriate studies;
- b) the quantity of water available for other uses in the area and as base flow for sensitive surface water features will be protected, improved or restored taking into account any mitigative measures that are initiated by the proponent prior to and/or during extraction and the proposed rehabilitation plan, in accordance with the recommendations of appropriate studies;
- c) there is no negative impact on significant natural heritage features or related ecological functions in accordance with the principle of net environmental gain taking into account any compensation and enhancements that are recommended in an EIS and initiated by the proponent prior to and/or during extraction and the proposed progressive and final rehabilitation of the site;
- d) as much of the site as possible will be rehabilitated by establishing or restoring natural self-sustaining vegetation; and,
- e) other environmental and social impacts such as noise, dust, odour and visual impacts are minimized.

#### E6.4.5 HIGH POTENTIAL MINERAL AGGREGATE RESOURCE AREAS

High potential mineral aggregate resource areas, include selected bedrock/shale resource areas and primary and secondary sand and gravel resource areas, exclusive of the Urban Areas, *Hamlet Areas*, *Rural Cluster Areas*, and the *Escarpment Natural Areas* and *Escarpment Protection Areas* and provincially significant wetlands. These high potential mineral aggregate resource areas are shown on Appendix X2 to this Plan for information purposes only.

It is the policy of this Plan that the high potential mineral aggregate resource areas be protected from uses and/or activities that may preclude or hinder the effective and/or economical extraction of aggregate in the future. To this end, it is the policy of this Plan to require that Official Plan Amendment, Zoning By-law Amendment and consent applications on lands that are wholly or partially within 300 metres of a sand and gravel deposit or 500 metres of a selected bedrock and shale resource, and that has the potential to preclude or hinder continued extraction or expansion to existing operations or the

establishment of new operations or access to mineral resources, be accompanied by appropriate studies that demonstrate to the satisfaction of Council that:

- a) resource use would not be feasible;
- b) the proposed land uses or development serves a greater long term public interest; and,
- c) issues of public health, public safety and environmental impact have been addressed.

#### E6.4.6 AFTER USES

It is intended that the Official Plan will be amended after a license has been surrendered or revoked to provide for an after-use that is compatible with and has minimal impacts on the surrounding natural environment, vistas and views and existing uses.

The determination of the appropriate designation of the lands for an after use will be made at the time an application is submitted. Factors to consider include:

- a) the use and designation of the land before the extraction commenced;
- b) the land uses and designations on adjacent properties;
- c) the proximity of the lands to agricultural lands and activities;
- d) the character of the area;
- e) the accessibility of the property;
- f) the recreational opportunities that the site may afford;
- g) the opportunities that may be available to enhance natural heritage features and functions in the area;
- h) the nature and cost of any long-term monitoring required on the site;
- i) if the lands are subject to the Niagara Escarpment Plan, as determined through the criteria for designation contained in the Niagara Escarpment Plan; and,
- j) opportunities for public ownership.

#### E6.4.7 REHABILITATION

It is the policy of this Plan to encourage the progressive rehabilitation of all pits and quarries within the Town. Council will work with pit and quarry operators and the Ministry of Natural Resources to ensure that all new licenses have appropriate progressive rehabilitation plans. If the site to be rehabilitated is in the area of the *Niagara Escarpment Plan Area*, rehabilitation shall take place in accordance with the objectives of the applicable re-designation of the Niagara Escarpment Plan. The rehabilitation measures taken will be compatible with, and have minimal impact upon, the surrounding natural and visual environment and existing uses.

If a site is to be rehabilitated to a natural state, it is the policy of this Plan that natural self-sustaining vegetation and, where possible, hydrologic features be established and restored. The rehabilitation of prime agricultural lands shall be in accordance with Section E6.4.3 of this Plan.

## E6.5 EXTRACTION IN THE PROTECTED COUNTRYSIDE AREA

### E6.5.1 GENERAL POLICIES

Notwithstanding any provisions of this Plan to the contrary, within the Natural Heritage System, the relationship between mineral aggregate operations, and Key Natural Heritage Features and Key Hydrologic Features is as follows:

- a) No new mineral aggregate operation and no wayside pits and quarries, or any ancillary or accessory use thereto will be permitted in the following Key Natural Heritage Features and Key Hydrologic Features:
  - i) Significant wetlands;
  - ii) Significant habitat of endangered species and threatened species; and,
  - iii) Significant woodlands unless the woodland is occupied by young plantation or early successional habitat (as defined by the Ministry of Natural Resources). In this case, the application must demonstrate that the specific provisions of Sections E6.5.3 (c), (d), and E6.5.4 (c) have been addressed, and that they will be met by the operation.
- b) An application for a new mineral aggregate operation or new wayside pits and quarries may only be permitted in other Key Natural Heritage Features and Key Hydrologic Features not identified in Section E6.5.1 (a) and any vegetation protection zone associated with such other feature where the application demonstrates:
  - i) How the Water Resources System will be protected or enhanced; and,
  - ii) That the specific provisions in Section E6.5.3 (c), (d) and E6.5.4 (c) have been addressed, and that they will be met by the operation.
- c) Any application for a new mineral aggregate operation, or the expansion of an existing mineral aggregate operation shall be required to demonstrate:
  - i) How the connectivity between Key Natural Heritage Features and Key Hydrologic Features will be maintained before, during and after the extraction of mineral aggregates;
  - ii) How the operator could immediately replace any habitat that would be lost from the site with equivalent habitat on another part of the site or on adjacent lands; and,
  - iii) How the Water Resource System will be protected or enhanced.
- d) An application for the expansion of an existing mineral aggregate operation may be permitted in the Natural Heritage System, including Key Natural Heritage Features and Key Hydrologic Features, and in any associated vegetation protection zone only if the related decision is consistent with the Provincial Policy Statement.

### E6.5.2 CONDITIONS OF APPROVAL

The Ministry of Natural Resources will pursue the following under the Aggregate Resources Act, for all mineral aggregate operations, including wayside pits and quarries within the *Protected Countryside Area*:

- a) Rehabilitated area will be maximized and disturbed area minimized on an ongoing basis during the life cycle of an operation;
- b) Progressive and final rehabilitation efforts will contribute to the goals of the Greenbelt Plan;
- c) The Ministry of Natural Resources will determine the maximum allowable disturbed area of each mineral aggregate operation. Any excess disturbed area above the maximum will be required to be rehabilitated. For existing operations this shall be completed within 10 years of the date of approval of the Greenbelt Plan, and 50% completed within six years. For new operations, including expansions, the total disturbed area shall not exceed an established maximum allowable disturbed area; and,
- d) An application for a mineral aggregate operation or wayside pits and quarries may be permitted only where the applicant demonstrates that the quantity and quality of groundwater and surface water will be maintained as per Provincial Standards under the Aggregate Resources Act.

#### E6.5.3 REHABILITATION

When operators are undertaking rehabilitation of mineral aggregate operation sites in the *Protected Countryside Area*, the following provisions apply:

- a) the aggregate industry will work with the Ministry of Natural Resources to consider the development and implementation of comprehensive rehabilitation plans in areas of high concentration of mineral aggregate operations;
- b) The disturbed area of a site will be rehabilitated to a state of equal or greater ecological value, and for the entire site, long-term ecological integrity will be maintained or restored, and to the extent possible, improved;
- c) If there are Key Natural Heritage Features or Key Hydrologic Features on the site, or if such features existed on the site at the time of application:
  - i) The health, diversity and size of these Key Natural Heritage Features and Key Hydrologic Features will be maintained or restored and, to the extent possible, improved to promote a net gain of ecological health; and,
  - iii) Any permitted extraction of mineral aggregated that occurs in a feature will be completed, and the area will be rehabilitated, as early as possible in the life of the operation;
- e) Aquatic areas remaining after extraction are to be rehabilitated to aquatic enhancement, which shall be representative of the natural ecosystem in that particular setting or eco-district, and the combined terrestrial and aquatic rehabilitation shall meet the intent of Section E6.5.3 (c); and,
- f) Outside the Natural Heritage System and except as provided in Section E6.5.3 (b), (c) and (d), final rehabilitation will appropriately reflect the long-term land use of the general area, taking into account applicable policies of this Plan and, to the extent permitted under this Plan, existing municipal and provincial policies.

#### E6.5.4 FINAL REHABILITATION



Final rehabilitation in the Natural Heritage System will meet these additional provisions:

- a) Where there is no underwater extraction, an amount of land equal to that under natural vegetated cover prior to extraction and no less than 35% of each license, is to be rehabilitated to forest cover, which shall be representative of the natural ecosystem in that particular setting or ecodistrict;
- b) Where there is underwater extraction, no less than 35% of the non-aquatic lands of each license is to be rehabilitated to forest cover, which shall be representative of the natural ecosystem in that particular setting or ecodistrict; and,
- c) Rehabilitation will be implemented so that the connectivity of the Key Natural Heritage Features and the Key Hydrologic Features on the site and on adjacent lands will be maintained or restored, and to the extent possible, improved.

#### E6.5.5 OTHER CRITERIA

All other relevant criteria in Sections E6.4.3, E6.4.4 and E6.4.5 shall also be considered unless specifically modified by the policies of Section E6.5, when considering an application for a new or expanded pit or quarry.

#### E6.6 IMPLEMENTING ZONING BY-LAW

The implementing Zoning By-law shall place all existing mineral aggregate operations in a zone that permits quarries, sand and gravel extraction operations and wayside pits and portable asphalt plants for road works in the area. The implementing Zoning By-law shall only permit mineral aggregate operations as defined by this Plan on lands that are zoned to implement this section of the Official Plan. The implementing Zoning By-law shall also clearly indicate that aggregate related uses are not permitted on a site once a license has been surrendered or revoked in accordance with the Aggregate Resources Act.

## E7 MAJOR INSTITUTIONAL AREA

### E7.1 OBJECTIVE

It is the objective of this designation to recognize an existing major institutional complex adjacent to the Georgetown Urban Area.

### E7.2 LOCATION

The *Major Institutional Area* designation as shown on Schedule A1 to this Plan applies to a major institutional complex located in Part of Lot 21, Concession 8 adjacent to the Georgetown Urban Area.

### E7.3 PERMITTED USES

Permitted uses on lands designated *Major Institutional Area* are limited to:

- a) a building or buildings used for educational/religious instruction and administrative offices;
- b) a building or buildings for the preparation, production, distribution and storage of literature and programs; and,
- c) complementary uses limited to:
  - i) a place of worship;
  - ii) residential accommodation for persons carrying out a permitted use;
  - iii) manufacturing, assembly and servicing of goods and equipment to carry out a permitted use;
  - iv) accessory uses; and,
  - v) utility facilities.

### E7.4 LAND USE POLICIES

The development of additional *Major Institutional Areas* outside of the Urban Area designation is not permitted by this Plan.

### E7.5 IMPLEMENTING ZONING BY-LAW

Lands within the existing *Major Institutional Area* designation shown on Schedule A1 to this Plan shall be zoned in accordance with existing uses in the implementing Zoning By-law.

## F1 SUBDIVISION OF LAND

This section contains policies that are to be considered with every application to subdivide land in the Town. Regard shall also be had to the specific policies dealing with lot creation in each land use designation in addition to other policies in the Plan and the Niagara Escarpment Plan that may be applicable to a particular application.

### F1.1 PREFERRED MEANS OF LAND DIVISION

Land division by Plan of Subdivision, rather than by consent, shall generally be required if:

- a) the extension of an existing public road or the development of a new public road is required to access the proposed lots;
- b) the area that is proposed to be developed is not considered to be infilling;
- c) a Plan of Subdivision is required to ensure that the entire land holding or area is developed in an orderly and efficient manner; or,
- d) more than four lots including the retained lands are being created and/or the owner is retaining sufficient lands for the development of additional lots.

### F1.2 NEW LOTS BY CONSENT

#### F1.2.1 GENERAL CRITERIA

Prior to issuing provisional consent for a new lot for any purpose, the Committee of Adjustment shall be satisfied that the lot to be retained and the lot to be severed:

- a) front on and will be directly accessed by a public road that is maintained on a year-round basis;
- b) will not cause a traffic hazard;
- c) has adequate size and frontage for the proposed use in accordance with the implementing Zoning By-law and is compatible with adjacent uses;
- d) can be serviced with an appropriate water supply and means of sewage disposal;
- e) will not have a negative impact on the drainage patterns in the area;
- f) will not restrict the development of the retained lands or other parcels of land, particularly as it relates to the provision of access, if they are designated for development by this Plan; and,
- g) will not have a negative impact on natural heritage features and related ecological functions in the area.

Provisional consent may be granted subject to appropriate conditions of approval for the severed and/or retained lot in accordance with the Planning Act.

#### F1.2.2 LOT CREATION OUTSIDE OF SETTLEMENT AREAS

The creation of new lots outside of the Urban Area, *Hamlet Area* and *Rural Cluster Area* designations is prohibited unless specifically provided for in Section F1.2.5 and Section F1.2.6 of this Plan.

#### F1.2.3 LOT LINE ADJUSTMENTS

A consent may be permitted for the purpose of correcting conveyances and reconfiguring lot lines provided no new building lot is created. In reviewing such applications, the Committee of Adjustment shall be satisfied that:

- a) the use of the properties subject to the application will not be negatively impacted;
- b) any agricultural parcels subject to the application will not be negatively impacted; and,
- c) there is no increased fragmentation of significant and Key Natural Heritage Features and Key Hydrologic Features.

#### F1.2.4 FARM CONSOLIDATIONS

Farm consolidations may be considered where the effect of the boundary adjustment or consolidation is to improve the viability of a farm operation provided:

- a) no new lot is created; and,
- b) the viability of using the lands affected by the application for agricultural uses is not adversely impacted if the application is approved.

#### F1.2.5 LOTS FOR UTILITIES

The creation of new lots for public utilities, communication utilities and water and sewer infrastructure may be permitted provided:

- a) the use cannot be accommodated through the use of easements or rights-of-way;
- b) the area of the proposed lot is minimized and reflects what is required for the use; and,
- c) the implementing Zoning By-law, as a condition of Provisional Consent, only permits uses that are related to the utility on the lot.

#### F1.2.6 NEW LOTS FOR PUBLIC PURPOSES

New lots may be created for acquisition by a public body.

#### F1.3 SUBDIVISION DEVELOPMENT POLICIES

This section is intended to contain general Plan of Subdivision policies that are to be considered with every application for Plan of Subdivision. Regard shall also be had to the specific policies dealing with lot creation in each land use designation.

Prior to the consideration of an application for Plan of Subdivision, Council shall be satisfied that:

- a) the approval of the development is not premature and is in the public interest;

- b) the lands will be appropriately serviced with infrastructure, schools, parkland and open space, community facilities and other amenities;
- c) the density of the development is appropriate for the area;
- d) the subdivision, when developed, will be easily integrated with other development in the area;
- e) the subdivision conforms with the environmental protection and management policies of this Plan; and,
- f) the proposal conforms to Section 51 (24) of the Planning Act, as amended.

Prior to the registration of any Plan of Subdivision, a Subdivision Agreement between the landowner and the Town will be required.

## F2 URBAN DESIGN

### F2.1 OBJECTIVES

It is anticipated that the Town's urban population and the range of services offered will continue to grow over the planning period. This growth will occur within the existing Built-up Area through redevelopment and intensification and in new greenfield areas. In order to ensure that the urban areas evolve in a manner that enhances the quality and vibrancy of urban life, it is the desire of Council to create and encourage a high quality of built form. On this basis, it is the intent of this Plan to:

- a) improve the aesthetic quality of the Town's built form, and promote development which is based on good design principles and standards that reflect the goals, objectives, and policies of this Plan;
- b) enhance the unique character of the Town's settlement areas by encouraging high quality design that is complementary and compatible with existing development, the Town's cultural and natural heritage, and which fosters a strong sense of civic identity and pride;
- c) enhance gateways into the urban areas, strengthen the Georgetown and Acton downtown communities, refine development in, and connections to, the Georgetown GO Station, and revitalize the Guelph Street corridor;
- d) ensure high quality design is employed in the development of all public works and that these public works contribute to an improved urban environment;
- e) exercise appropriate municipal development control in order to achieve a consistently high quality of site, building and landscape design; and,
- f) ensure the safety and security of public and publicly accessible places through urban design.

### F2.2 URBAN DESIGN POLICIES

The following policies shall apply, as appropriate, to all developments requiring a Planning Act approval within the Urban Areas. Additional policies for certain areas of the Town, such as the *Downtown Area*, are contained within the appropriate sections of the Plan containing the land use designations.

#### F2.2.1 PUBLIC REALM

The look, feel and treatment of public areas such as roads, parks, and public open spaces are a key component of what makes up the character of the community. Therefore, high quality design in the public realm shall be encouraged in the development of all public parks and open spaces, roads, buildings and engineering projects. The development of new public buildings shall be in accordance with relevant provisions of Sections F2.2.2 of this Plan. The following additional policies shall apply to the design of these public areas and to the public works carried out in these areas.

##### F2.2.1.1 Streetscape

- a) An integrated design and treatment of streetscape features shall be promoted throughout the municipality. Specialized streetscape designs and treatments may be adopted for particular

areas of the Town in accordance with area, or site-specific, Council-adopted Urban Design Guidelines.

- b) Streetscape features located within public rights-of-way, such as lighting fixtures, directional and street signs, parking meters, transit shelters, and street furniture shall be complementary in their design and located in an integrated manner, so as to avoid visual clutter.
- c) Gateway features shall be established at strategic locations within the municipality, as identified in Council-adopted Secondary Plan policies and/or Urban Design Guidelines and may include specialized boulevards, landscape medians, decorative street lightings, and/or decorative signage treatments.
- d) Planned road reconstruction shall be encouraged to include improvements to the existing streetscape consistent with the policies of this Plan or any Council-adopted Urban Design Guidelines.

#### F2.2.1.2 Roads

Road designs shall include well-designed streetscape features, incorporating, among other things: street tree planting, street lighting and furnishings, sidewalk and boulevard treatments, a variety of paving materials, and, where appropriate, bicycle lanes, community mailboxes and future transit shelters.

Road designs may incorporate traffic calming techniques such as narrower rights-of-way, traffic circles, and speed control devices, where appropriate, to promote a safer pedestrian environment and/or to maintain vehicles within designated speed limits. The Town will consider alternative standards for public road rights-of-way in order to achieve urban design objectives in certain areas of the municipality.

The design of roads shall incorporate a high quality of urban design standards. On this basis:

- a) road rights-of-way shall be designed to secure a separation of vehicles and pedestrians and should provide an appropriate sidewalk for pedestrian use;
- b) on collector and arterial roads within Urban Areas and *Hamlet Areas*, a suitable boulevard shall be provided to separate the road curb from the sidewalk and such boulevard shall include hard and/or soft landscape materials, street trees and pedestrian-level street lights, where appropriate;
- c) where medians are provided within the road rights-of-way, such medians shall be encouraged to include hard and/or soft landscape materials, where appropriate;
- d) a regularized pattern of street tree planting shall be encouraged along roads within Urban Areas and *Hamlet Areas*, and the Town shall establish minimum planting standards and species types in the Site Plan Manual and/or Council-adopted Urban Design Guidelines;
- e) the number and location of access points onto the public road system shall be minimized by encouraging common access points to be shared by adjacent development;
- f) street lighting shall, where appropriate, incorporate pedestrian-level lighting to maintain pedestrian safety; and;

- g) street lighting that reduces energy consumption and directs light away from the night sky and adjacent uses shall be encouraged.

#### F2.2.1.3 Services and Utilities

- a) Services and utilities shall be encouraged to locate underground in a common trench, where possible, in order to maintain a pleasant visual environment along public roads.
- b) Above-ground utility service providers shall be encouraged to co-operate with the Town in identifying locations which minimize the visual impacts of such equipment and facilities where located within the public road right-of-way.
- c) All proposed development adjacent to utility corridors shall ensure that appropriate safety and environmental protection measures such as setbacks and fencing are provided to the satisfaction of the Region and/or the Ministry of Environment.

#### F2.2.2 PRIVATE REALM

A high quality of design of the private realm shall be required in all new development and redevelopment.

##### F2.2.2.1 Site Design

- a) Site design incorporates the built form of structures, landscaping, services and the layout of all amenities. Site design shall promote an appropriate design relationship between the public realm, adjacent land uses, on-site operations and visual aesthetics, in order to promote an environment that is pleasant and attractive to the community.
- b) Site design shall address compatibility between differing adjacent land uses in context of density, height and massing through appropriate site layout, building locations and landscape treatments.
- c) The design of sites adjacent to parks, woodlots and watercourses shall be sensitive to these features. In these instances, appropriate setbacks shall be maintained between buildings and sensitive natural areas, while on-site landscaping shall be well integrated with natural areas.
- d) Continuous, highly visible, well-articulated and landscaped connections between building(s) and the street should be provided to establish appropriate pedestrian linkages between the sidewalk and building entrances, and generally improve access for public transit users.
- e) Site design along major roads should also consider the integration of future public transit access.
- f) Along collector and arterial roads within Urban Areas and *Hamlet Areas*, reverse frontage residential lots shall be minimized through techniques such as window streets and where reverse frontage lots are provided, shall incorporate a substantial landscape buffer to improve the visual amenity of such areas.
- g) On large sites, efforts shall be made to encourage pedestrian linkages between uses and adjacent sites.



Building Design and Siting

- a) The design of new buildings should achieve a complementary design relationship to existing buildings, while accommodating a diversity of architectural styles, building materials and colours, energy conservation techniques and innovative built forms.
- b) The design of all buildings shall have regard to pedestrian safety and direct street access. Buildings should be massed to recognize pedestrian scale and provide an appropriate street wall height at the street line and be architecturally articulated to provide visual variety and interest, yet be sensitive to high wind speeds, and long periods of shadowing. Generally, building articulation features such as canopies, cornice lines and varying façade materials should be used to reinforce a pedestrian scale.
- c) Buildings shall be encouraged to present their principal building facades with an appropriate building design and fenestration to the public street. The design of corner buildings shall take into account exposure to multiple street frontages and high public exposure; as well as incorporate elements such as increased height, fenestration and roof features, and well-articulated entrances.
- d) Buildings located at major vista terminations in their urban setting, or on view corridors within the development site, shall be given special treatment through the use of massing and building articulation strategies, such as added height, special roof treatments, and use of special cladding materials.
- e) The protrusion of garages on residential buildings into the front yard, beyond the main front building wall, shall be discouraged.
- f) Building functions that do not directly serve the public, such as loading areas, shall not face a public street and should be located away from noise sensitive land uses, such as residential areas, and buffered as necessary.
- g) Buildings should employ devices such as awnings, canopies, building cantilevers / overhangs to minimize uncomfortable high winds which may be associated with the height or placement of buildings, and generally improve the level of pedestrian comfort. Sheltered building entrances should be provided at primary building entrances to high-density residential, public, recreational, industrial, office and commercial buildings, where necessary.
- h) When a development is located adjacent to existing, or planned residential areas, sufficient building setbacks should be provided to minimize potential height and massing impacts such as overlook, shadowing and high wind speeds. Massing strategies such as stepping down towards buildings of lower height should be employed to minimize impacts. Site Plan applications may be required to submit wind and/or shadow studies to address such potential conditions.
- i) Roofscapes shall be an integral part of the design of a building and harmonize with the design of the rest of the building. On this basis, roof top mechanical units shall be organized and screened with complementary materials, colours and finishes as necessary to provide a skyline with desirable visual attributes.

- j) Freestanding buildings for retail, restaurants and services, as well as office and residential buildings, shall be located at the street edge where possible, to encourage their use by pedestrians.
- k) Buildings on corner lots shall be located in close proximity to the street rights of way. Corner lots should emphasize their important urban presence by employing appropriate strategies for major landscape treatments as well as building massing and articulation that emphasize the corner condition.
- l) Building entrances shall be located to be visible from the adjoining street(s) and, where possible, directly linked to the sidewalks through appropriately articulated walkways.

#### F2.2.2.3 Drive-Throughs

Drive-through service facilities are subject to urban design objectives and policies of this Plan. In general, drive-through service facility development applications will be discouraged when they are determined not to be compatible and sensitive to surrounding land uses, and areas of particular urban form, cultural significance and special identity, and streetscape patterns. Drive-through service facility applications shall be contextually appropriate, support a positive pedestrian environment, and integrate well with other on-site facilities, features and amenities. The following additional matters shall be considered when reviewing drive through proposals:

- a) Drive-through service facilities shall be incorporated into a larger building form, where possible.
- b) Drive-through service facilities shall be designed and located to minimize visual impact on the streetscape. In this regard, drive through stacking and pick up lane should not be located between the front face of the building and the public sidewalk.
- c) Drive-through service facilities should be designed to minimize visual, traffic and noise impacts on adjacent residential development.

#### F2.2.2.4 Landscaping

- a) Landscaping is a major contributor to a vibrant streetscape. A high quality of landscape design shall be required to enhance the visual aesthetics of development and to enhance the site and land use compatibility.
- b) Landscaping within private lands shall be complementary to streetscape design and materials within the public realm.
- c) Where appropriate, planted landscaping strips and fencing shall be used to buffer development from adjacent uses and mitigate on-site operational activities such as loading and waste storage facilities;
- d) Landscape materials shall be selected for their aesthetic, ecological, disease-tolerance and maintenance characteristics.
- e) Hard and soft landscaping shall be used for the spaces between the street line and buildings to enhance the streetscape, as well as provide a buffering function when on-site parking is placed close to the street or in side yards. To help create a strong landscape presence that also screens

surface parking, a generously sized area and/or low decorative fencing should be provided along the street line to allow for an aesthetically pleasing view from the street into the site.

- f) Vehicular entrances often present opportunities for landscaping that highlight entry points into the site. Therefore, appropriate landscaping shall be provided on either side of driveway entrances, particularly at the main entrances.
- g) The use of berms along public street frontages shall generally be avoided due to their tendency to isolate buildings from the street.
- h) Landscaping can play an important role in delineating a site's side yards and often provides a visual break in large asphalted areas. In this regard, landscape strips planted with trees and/or shrubs and flowers shall be used to separate each development and the associated parking areas. The presence of significant trees on a development site shall be determined through a tree survey and, where appropriate, preserved, maintained and integrated into the new landscape design.
- i) Native, non-invasive species shall be used on lands that are adjacent to the Greenlands System.

#### F2.2.2.5 Parking

- a) The location of parking is a major determinant for the layout of a development that is pedestrian friendly and transit supportive. Where appropriate, the Town shall encourage the provision of surface parking areas in locations not visible from the public street, such as in rear yards and/or well-landscaped side yards.
- b) Where surface parking areas are situated adjacent to a public street in the front yard, their layout should be subdivided into smaller areas to avoid large monotonous asphalt surfaces. In these cases, a certain percentage of the frontage should be reserved for landscaping between the buildings and the street line. The parking areas may be partially buffered and/or screened from the street through the use of landscaping, tree planting, pedestrian facilities, lighting, fencing and/or other landscape elements in order to enhance the visual aesthetics of, and pedestrian activity within, such parking areas.
- c) Clearly defined pedestrian accesses between parking and adjacent buildings and entrances should be provided with well-delineated walkways using decorative paving surfaces.
- d) Surface parking lots shall be linked to the streets and other public areas with well-delineated walkways, utilizing decorative paving treatments.

#### F2.2.2.6 Signage, Display Areas and Lighting

- a) All signs shall be in accordance with the Town's Sign By-law and designed as an integral element of the site layout and/or building design that does not dominate the overall development character. Signs on designated heritage properties or within Heritage Conservation Districts pursuant to the Ontario Heritage Act are encouraged to be compatible with the architecture and character of the property or district.
- b) Some land uses require outside display areas such as sales outlets for vehicles and garden supply stores. Display areas should be designed to make a positive contribution to the

streetscape and the overall site development. Generally, there shall be a limited percentage of site frontage devoted to outside display areas. Architectural and/or landscaping components shall be used as appropriate, to provide well-defined display areas that work in harmony with adjacent buildings and display areas. Some of these treatments include extensions of building facades, colonnades and canopies, planting, pergolas and decorative walls.

- c) All lighting shall be internally oriented so as not to cause glare on adjacent properties or public roads. Outdoor lighting fixtures that reduce energy consumption and direct light away from the night sky shall be encouraged.

#### F2.2.2.7 Services, Utilities, Outside Processing and Storage

- a) Site and building services and utilities such as waste storage facilities, loading, air handling equipment, hydro and telephone transformers and switching gears and metering equipment, shall be located and/or screened from public streets and adjacent residential areas or other sensitive land uses, in order to buffer their visual and operational effects. Waste storage areas should be integrated into the main building on the lot. Waste storage areas external to the main building shall be enclosed and shall not face a public street.
- b) Site access, service areas and loading areas shall be located away from streets so as to minimize disruption or conflicts with adjacent land uses, sidewalks and both on-site, and off-site, pedestrian routes and shall be visually screened as necessary from public views. Screening should be designed to use landscaping and/or solid fencing. Loading and service areas should be buffered for noise impacts, particularly when located against residential areas. Buffering strategies include berms, tree and shrub planting and opaque noise walls and fences.
- c) It is recognized that in some developments, there will be a need to accommodate outside processing and storage areas, particularly for uses such as building supply centres and some industrial operations. These storage areas should be organized and placed to reduce their potential negative impacts on the streetscape. Open processing and storage areas should be located in the rear or side yards and screened from public view, or from views from adjacent properties, using fencing and/or landscaping.
- d) Storage areas should be paved with hard surfaces such as asphalt, concrete or interlocking pavers where possible to reduce dust.

#### F2.2.2.8 Access and Circulation

- a) The number of vehicular access points into a site and their width could potentially have detrimental effects on street frontages by reducing available areas for landscaping and by creating large expanses of asphalt. On this basis, joint access driveways shall be considered on adjacent sites.
- b) To ensure safety and promote their priority over vehicular traffic, major pedestrian routes on the site should be identified and delineated with paving materials that differentiate them from the driving surfaces. Pedestrian walkways should be made continuous across driving aisles as well as across driveway entrances at the street. The use of soft landscaping is also encouraged along major pedestrian routes.

- c) Generously sized walkways shall be provided along buildings, particularly in areas with large pedestrian traffic. These walkways should be connected to other pedestrian routes on the site and linked to major pedestrian entry points at the street, and where appropriate to adjacent developments.

#### F2.2.3 NATURAL HERITAGE

Development shall be designed to be compatible with, and complementary to, existing natural heritage features.

#### F2.2.4 CULTURAL HERITAGE

Development shall be designed to incorporate, conserve and enhance identified cultural heritage resources as distinct elements and/or focal points, and incorporate these features into the overall site and building design.

#### F2.2.5 SAFETY

Personal safety for individuals for new development shall be promoted including the provision of:

- a) appropriate lighting, visibility and opportunities for public surveillance for parking lots, walkways, parking garages and open space area;
- b) unobstructed views into parks and open spaces from adjoining streets;
- c) design and siting of new buildings shall provide opportunities for visual overlook and ease of public access to adjacent streets, parks and open space;
- d) views into and out of publicly accessible buildings shall be encouraged;
- e) landscaping that maintains views for safety and surveillance; and,
- f) clear and appropriately located signage, which precludes entrapment or the perception of entrapment.

#### F2.2.6 BARRIER-FREE ACCESS

- a) Barrier-free access for persons using walking or mobility aids shall be provided in all public and publicly-accessible buildings and facilities and along major pedestrian routes. Such barrier-free access features may include level surfaces, ramps and curb cuts, railings, automatic door openers and rest areas.
- b) Barrier free features shall be integrated with the functional and design components of the site and/or buildings.

#### F2.2.7 PUBLIC ART

- a) The creation of public art in public and private spaces which fosters community identity by interpreting local history, traditions and culture shall be encouraged.

- b) The integration of art into new development shall be encouraged, particularly within the *Downtown Area* and *Community Node* designations.
- c) Consideration shall be given to incorporating public art into new public buildings, bridges, parks, and noise barriers where appropriate and feasible.

#### F2.2.8 VIEWS AND VISTAS

- a) The preservation, enhancement and/or creation of significant views and vistas shall be encouraged as part of comprehensive planning studies, such as Secondary Plans and during the review of development applications. Examples of significant views include the Niagara Escarpment, Fairy Lake, the Downtowns, particularly Mill Street in Acton and Main Street in Georgetown, important public or historic buildings and natural heritage features and open space.
- b) Public and institutional buildings shall be encouraged to locate:
  - i) at the termination of a street or view corridor;
  - ii) at street intersections; and,
  - iii) on Main Street in the Georgetown *Downtown Area*, on Mill Street in the Acton *Downtown Area* or along Guelph Street and Mountainview Road within the *Community Node*.

#### F2.3 IMPLEMENTATION

The Town shall employ all relevant municipal development controls in order to achieve a consistently high standard of site, building and landscape design.

The urban design policies of this Plan shall be implemented by:

- a) ensuring that the implementing Zoning By-law is regularly reviewed and amended to include standards that reflect the objectives and policies of this Plan;
- b) preparing specific Urban Design Guidelines for key focal points in the Town, such as the Guelph and Queen Street corridors and the *Downtown Area*;
- c) ensuring that the Town's engineering standards are regularly refined to reflect the objectives and policies of this Plan and any Council-adopted Urban Design Guidelines;
- d) ensuring that the Town's Site Plan Manual sets out the minimum requirements of the Town in a clear and concise manner;
- e) ensuring that all Public Works decisions generally conform with this Plan and Town approved Urban Design Guidelines;
- f) utilizing Architectural Control in Greenfield areas, where appropriate, to detail proposed building designs and materials, and in order to avoid repetitive building forms along residential subdivision streets; and,

- g) requiring the preparation of Urban Design Guidelines in accordance with section F2.3.1 which are intended to be complementary to the general policies of this Plan.

#### F2.3.1 URBAN DESIGN GUIDELINES

The Town may require the preparation of detailed Urban Design Guidelines for selected areas within the municipality, where appropriate, and shall require Urban Design Guidelines and/or Architectural Control Guidelines for those areas identified in Sections F2.3.1 a) and F2.3.1 b). In addition, Urban Design Guidelines shall be prepared where new, or reviews of existing, local Secondary Plans are undertaken.

Urban Design Guidelines shall be adopted by Council and then shall be employed in the design and construction of all public projects and the evaluation of all development applications.

In areas where Urban Design Guidelines have been adopted by Council, all development applications shall be evaluated to determine the extent to which the application achieves the Urban Design policies of this Plan and the relevant Council-adopted Urban Design Guidelines.

Council may require the preparation of area-specific Urban Design Guidelines as part of the consideration of major development proposals or the preparation of area-specific studies.

- a) For residential subdivision/condominium plans in excess of 200 units, the Town shall require, prior to draft plan of subdivision approval, the preparation of Community Urban Design Guidelines for the subdivision plan, to establish the area's vision, structure and design principles and to guide the overall character of the proposed subdivision through design features such as street light design, sidewalk materials and locations, perimeter fencing, community mailbox locations and/or facilities, gateway or entrance features, street tree planting in the public right-of-way, and park and public open space design.
- b) For residential subdivision/condominium plans in excess of 25 units, the Town shall require, as a condition of draft plan of subdivision approval, the preparation of Architectural Control Guidelines to guide: the desired siting of the building, architectural design elements and materials; the design of identified special lots; and the design review process. The Town shall also require, as a condition of draft plan of subdivision approval or site plan approval, design review by a control architect approved by the Town prior to the issuance of construction permits for each building to ensure that the intent of the Architectural Control Guidelines is secured.
- c) Where applicable, Urban Design Guidelines shall incorporate the principles listed in Section F3 of this Plan.

#### F2.3.2 DEVELOPMENT APPROVALS

- a) The urban design policies of this Plan, and any Council-adopted Urban Design Guidelines, shall be employed in the evaluation of all development applications within the Town.
- b) The Town shall exercise its powers of zoning and site plan approval to encourage a high quality of building and landscape design and shall review and amend its Site Plan Manual and Zoning By-law in order to implement the Urban Design policies of this Plan and any Council-adopted Urban Design Guidelines.

- c) For development requiring site plan approval, the Town may request the applicant to prepare design guidelines in conjunction with the submission of site plan application, where the context of the development area is sensitive in nature.
- d) The Town shall exercise control over signs and fences in accordance with applicable legislation and shall ensure that its municipal by-laws are regularly reviewed and amended to reflect the Urban Design policies of this Plan and any Council-adopted Urban Design Guidelines.

#### F2.3.3 OTHER PROGRAMS

The Town may undertake studies or participate in programs, including community improvement plans in accordance with Section G7 of this Plan, for the aesthetic improvement of existing areas.

#### F2.3.4 MUNICIPAL STANDARDS BY-LAW

The Town shall monitor and, where necessary, apply the regulations of the Municipal property standards by-law, in order to maintain a high quality of site development throughout the municipality.



The following community design principles apply to development in greenfield areas:

- a) Residential development shall include a combination of housing types, with a range of densities that implement the housing objectives and policies of this Plan;
- b) High density housing shall be located on arterial and collector roads to ultimately facilitate the establishment of public transit and a pedestrian-oriented environment;
- c) New development areas shall be integrated with existing Built-up Areas;
- d) New subdivision streets should align in a grid pattern to create appropriately sized development blocks and to promote traffic permeability and street connectivity;
- e) The development of reverse-frontage residential lots shall be minimized through techniques such as window streets and where reverse frontage lots are provided, shall incorporate a substantial landscape buffer to improve the visual amenity of such areas;
- f) Open space and parkland areas shall integrate with adjacent development areas and provide a range of active and passive recreational opportunities;
- g) School sites shall be centrally located to be integrated with parkland in a manner that supports their development as community hubs and/or neighbourhood gathering places which are the site of a full range of uses. The number and location of school sites shall be very carefully chosen and take into account the location of existing schools and programs;
- h) New buildings shall be designed and oriented to the street and to street corners to encourage a pedestrian-oriented streetscape;
- i) Where appropriate, employment lands shall be buffered from residential development by a variety of measures such as roads, landscaping, natural heritage areas, and parkland and community facilities;
- j) Non-residential uses shall address the policies in section F2.2.2;
- k) Above ground utilities shall be located to minimize visual and environmental impacts; and,
- l) Collector roads shall be provided approximately mid-block between arterial roads to promote traffic connectivity, and ultimately the establishment of public transit, when feasible.

It is the intent of Council to only permit development outside of Urban Areas, *Hamlet Areas* and *Rural Cluster Areas* that is compatible with the character, role and function of the rural landscape since the existing character of the rural area greatly contributes to the quality of life enjoyed by the Town's residents. The rolling hills and undulating topography, the rivers and streams and the open and natural setting of the rural landscape are all components that define this character. It is the intent of this Plan to protect the natural and rural character of the rural landscape in accordance with the policies of this Plan.

On this basis, Council shall ensure, as a condition of any Planning Act approval for development located outside of Urban Areas, *Hamlet Areas* and *Rural Cluster Areas* that:

- a) uses be designed to blend in with the existing topography and vegetation wherever possible;
- b) existing trees are maintained wherever possible;
- c) new buildings on farm properties are sited in existing building clusters only;
- d) existing hedgerows and original farm fences along property lines are maintained wherever possible;
- e) non-residential uses are set back far from the road;
- f) all signage, if required, reflects the rural and natural character of the area;
- g) all lighting, if required, is subdued and appropriate for the use; and,
- h) existing buildings, structures, barns and other agricultural buildings are upgraded and/or restored wherever possible.

## F5

## CULTURAL HERITAGE RESOURCES

It is the intent of this Plan that the Town's cultural heritage resources be identified, conserved and enhanced whenever practical and that all new development occur in a manner that respects the Town's rich cultural heritage. The heritage resources of the Town generally include:

- a) built heritage, such as buildings, structures, monuments or remains of historical, cultural and/or architectural value;
- b) cultural heritage landscapes, such as rural, hamlet and urban areas that are of historic and scenic interest; and,
- c) archaeological resources.

### F5.1

### GENERAL POLICIES

#### F5.1.1

#### CULTURAL HERITAGE MASTER PLAN

In order to implement the objectives of this Plan, Council may prepare a Cultural Heritage Master Plan (CHMP). Such a Master Plan would survey, inventory, examine and study the Town's cultural heritage resources. The purpose of the CHMP is to make recommendations on how the cultural heritage resources of the Town should be enhanced and protected in accordance with the goals and objectives of this Plan. In addition, the CHMP shall make recommendations on:

- a) the need for the designation of Heritage Conservation Districts in accordance with the Ontario Heritage Act and as described in Sections F5.2.4 and F5.2.5 of this Plan; and,
- b) the need for area-specific Official Plan policies and/or zoning by-law regulations for cultural heritage areas as described in Section F5.2.6 of this Plan.

#### F5.1.2

#### CULTURAL HERITAGE IMPACT STATEMENTS

Council shall require the submission of a Cultural Heritage Impact Statement (CHIS) to support an application for development if the affected lands are the site of an identified or significant cultural heritage resource or are located in close proximity to a significant cultural heritage resource or are on adjacent lands to a significant cultural heritage resource. The purpose of the CHIS is to determine what impacts the development will have on the resource and whether the application for development will conform to the goals, objectives and policies of this Plan and/or the area-specific policies or zoning regulations described in Section F5.1.1 of this Plan.

Development or site alteration may be permitted on adjacent lands to a protected heritage property where a CHIS has demonstrated that the heritage attributes of the protected heritage property will be conserved, including the use of mitigation measures and/or alternative development approaches.

The CHIS shall be in the form of a report and contain a description of:

- a) the proposed development;

- b) the cultural heritage resource(s) to be affected by the development;
- c) the impacts upon the cultural heritage resource(s) of the proposed development;
- d) the measures necessary to mitigate the negative impacts of the development upon the cultural heritage resource(s);
- e) how the proposed development will relate, in terms of height, bulk, massing and presence with identified heritage buildings on the property and in the area; and,
- f) how the policies of the CHMP have been incorporated or satisfied, where one has been prepared and the recommendations have been incorporated into this Plan.

#### F5.1.3 PUBLIC WORKS

Public authorities have the ability to make decisions affecting the public realm that can have a positive impact on cultural heritage resources. On this basis, the carrying out of any public work by any Public Authority shall have regard to the retention and protection of significant cultural heritage resources in accordance with the goals and objectives of this Plan and the CHMP. Pursuant to the Ontario Heritage Act, all public works within Heritage Conservation Districts shall conform to the applicable Heritage Conservation District Plan.

#### F5.1.4 MITIGATION OF IMPACTS ON CULTURAL HERITAGE RESOURCES

Council may impose as a condition of any development approval the retention and conservation of cultural heritage resources identified in a CHIS or the CHMP, or the implementation of appropriate mitigation measures, to minimize the impact of the development on the cultural heritage resource.

#### F5.1.5 HERITAGE HALTON HILLS (MUNICIPAL HERITAGE COMMITTEE)

Heritage Halton Hills will serve as the Municipal Heritage Committee pursuant to the Ontario Heritage Act or any successor legislation to advise and assist Council in all matters related to cultural heritage resource conservation, including public awareness of heritage, heritage planning and designations under the Ontario Heritage Act within the Town.

#### F5.1.6 RESTORATION AND/OR REHABILITATION OF SIGNIFICANT CULTURAL HERITAGE RESOURCES

It is the intent of this Plan to encourage the restoration or rehabilitation of significant cultural heritage resources by assisting with funding applications, establishing grant programs and creating special taxation districts. Council may also encourage the restoration and retention of heritage properties through the use of bonusing and density transfers and other means as permitted by the Planning Act. Council may lead by example by restoring, rehabilitating, enhancing and maintaining municipally owned cultural heritage resources, through appropriate heritage stewardship practices.

#### F5.1.7 PIONEER HERITAGE CEMETERIES

Council shall discourage the closure and relocation of pioneer cemeteries since they contribute to the character of the Town and are an excellent representation of the Town's history and identity.

#### F5.1.8 REHABILITATION OF MINERAL RESOURCE EXTRACTION AREAS OF CULTURAL HERITAGE INTEREST

Council will encourage the rehabilitation of historic mineral resource extraction areas and will ensure that the cultural heritage attributes of such areas are considered in any program of rehabilitation. Where a historic resource extraction area forms part of a cultural heritage landscape, the area and its component features will be conserved.

#### F5.2 BUILT HERITAGE AND CULTURAL LANDSCAPE RESOURCES

##### F5.2.1 BUILT HERITAGE INVENTORY

An inventory of heritage buildings and structures within the Town shall be maintained. Inventoried heritage resources may be considered for designation under the Ontario Heritage Act and/or conservation through the review of any proposed development, subject to all relevant legislation. The inventory may be included within the CHMP described in Section F5.1 of this Plan.

##### F5.2.2 CULTURAL HERITAGE LANDSCAPE INVENTORY

Council may prepare an inventory of cultural heritage landscapes. This inventory may form a component of a Cultural Heritage Master Plan for the Town. Landscapes such as existing rural and agricultural areas, historic hamlets, and heritage roads will be identified in the inventory. A cultural heritage landscape is a defined geographical area of heritage significance that has been modified by human activities. Such an area is valued by a community and is of significance to the understanding of the history of a people or place.

##### F5.2.3 DESIGNATION UNDER THE ONTARIO HERITAGE ACT

Council may by by-law designate cultural heritage resources, such as individual properties and conservation districts pursuant to the Ontario Heritage Act and the policies of this Section. Prior to the passage of such a by-law, Council shall be satisfied that:

- a) the building or property is strongly associated with the life of a person who played an integral role in the development of the Town and/or is well-known locally, nationally or internationally;
- b) the building or property is the location of, or is associated in a significant way, with a significant local, national or international event;
- c) the building has an architectural style that is distinctive and representative of a period of history and/or is the work of a recognized architect;
- d) the building or property is considered to be an easily recognizable landmark in the Town and contributes to the character of the community; and/or,
- e) the neighbourhood contains a collection of buildings and properties described in Sections a), b), c) and d) above and which collectively contribute to the character of the Town.

#### F5.2.4 HERITAGE CONSERVATION DISTRICTS

Where merited by the concentration and significance of cultural heritage resources in accordance with Section F5.2.3 e) of this Plan, Council may consider the establishment of a Heritage Conservation District to conserve an area's heritage character.

Prior to designating a Heritage Conservation District pursuant to the Ontario Heritage Act, Council shall:

- a) pass a by-law to define an area to be studied for possible future designation;
- b) consider passing a by-law to impose interim control of alteration, demolition or removal of cultural heritage resources in the area defined as under study for possible future designation pursuant to the Ontario Heritage Act;
- c) prepare and adopt a Heritage Conservation District Plan; and,
- d) establish a District Committee to advise Council on matters pertaining to the designated district.

Within designated Heritage Conservation Districts, property owners, in consultation with the appropriate District Committee, will be encouraged to maintain and repair heritage buildings and seek government grants and loans for eligible conservation work.

#### F5.2.5 CONTENTS OF HERITAGE CONSERVATION DISTRICT PLAN

The general principles and process for the administration of a Heritage Conservation District will be outlined in a Heritage Conservation District Plan. The Heritage Conservation District Plan will:

- a) delineate boundaries of the designated area and reasons for the designation;
- b) inventory cultural heritage resources;
- c) prescribe policies, conservation and design guidelines, and other pertinent material relating to the sound and prudent management of the district's unique character;
- d) be adopted by Council after consultation with affected property owners and other interested agencies as considered appropriate; and,
- e) be administered by municipal review of heritage permit applications for changes and alterations to individual buildings and structures within the designated district.

In reviewing proposals for the construction, demolition or removal of buildings or structures, or the alteration of buildings within a Heritage Conservation District, Council shall be guided by the applicable Heritage Conservation District Plan.

#### F5.2.6 AREA-SPECIFIC OFFICIAL PLAN POLICY AND ZONING BY-LAW PROVISIONS

Council may, by Amendment to this Plan or incorporation into a Secondary Plan, include policies that are intended to provide guidance on how buildings and properties can be developed/redeveloped in an area where a concentration of significant cultural heritage resources exists. The intent of the policies would

be to conserve and enhance the cultural heritage of an area, in accordance with the goals and objectives of this Plan. These policies shall:

- a) describe the historical development context of the area;
- b) review the existence and significance of cultural heritage resources in the area;
- c) identify the conservation priorities for significant cultural heritage resources;
- d) establish how cultural heritage resources should be considered through a redevelopment process; and,
- e) identify and describe the architectural design and streetscape guidelines that will guide development in a defined area.

The policies may be implemented in the implementing Zoning By-law through the creation of a heritage overlay zone or an area-specific heritage area zone, as appropriate.

#### **F5.2.7 RETENTION/RELOCATION OF BUILT HERITAGE STRUCTURES**

Council shall encourage the retention of buildings of significant cultural heritage and protected heritage structures in their original locations whenever possible. All options for on-site retention shall be considered before approval is given for relocation to another site. These options include: integration within new development areas, adaptive re-use of the building in its original location (e.g. use as a community centre within a residential subdivision), and relocation of the building on the development site.

#### **F5.2.8 PREVENTION OF DEMOLITION OF BUILT HERITAGE STRUCTURES**

Pursuant to the Ontario Heritage Act, and as part of an overall strategy to conserve built heritage resources, Council may refuse to permit the demolition of heritage buildings or structures that have been designated under the Ontario Heritage Act.

#### **F5.3 ARCHAEOLOGICAL RESOURCES**

##### **F5.3.1 ARCHAEOLOGICAL ASSESSMENT REQUIREMENTS**

Council recognizes that there are archaeological remnants of prehistoric and early historic habitation as well as areas of archaeological potential within the Town. Archaeological sites and resources contained within these areas can be negatively impacted by any future development.

Council shall therefore require archaeological impact assessments and the preservation or excavation of significant archaeological resources in accordance with Provincial requirements and the Halton Region Archaeological Master Plan.

Archaeological assessment reports by licensed archaeologists are to be in compliance with guidelines set out by the Ministry of Citizenship, Culture and Recreation, as well as licensing requirements referenced under the Ontario Heritage Act.

Council may conserve the integrity of archaeological resources by adopting zoning by-laws under Section 34 of the Planning Act, to prohibit land uses on sites where an identified significant archaeological resource exists.

#### F5.3.2 ARCHAEOLOGICAL CONTINGENCY PLANNING

In accordance with the Halton Region Archaeological Master Plan, the Town shall prepare, with the advice of a licensed archaeologist and/or the appropriate Provincial ministry, and adopt by by-law a Contingency Plan for the Protection of Archaeological Resources in Urgent Situations. This plan shall provide guidelines for immediate action where accidental discoveries or imminent threats of damage to archaeological sites occur.

Council shall consult appropriate government agencies, including the Ministry of Citizenship, Culture and Recreation and the Ministry of Consumer Relations, where an identified human cemetery, marked or unmarked human burial is affected by land development. The provisions under the Ontario Heritage Act and the Cemeteries Act shall apply.



## F6 TRANSPORTATION

### F6.1 OBJECTIVES

It is the objective of this Plan to:

- a) facilitate the safe and efficient movement of people and goods within the Town's communities and to and from adjacent municipalities;
- b) establish an integrated transportation system that safely and efficiently accommodates various modes of transportation including trains, automobiles, trucks, public transit, cycling and walking;
- c) promote public transit, cycling and walking as energy efficient, affordable and accessible forms of travel;
- d) protect transportation corridors to facilitate the development of a transportation system that is compatible with and supportive of existing and future land uses;
- e) ensure that new roads in urban development areas are constructed safely, designed in a grid-oriented street network to help distribute car and truck traffic evenly and provide access for the future operation of an efficient public transit system;
- f) encourage the location of school sites on roads that can accommodate cycling, walking and all forms of motor vehicle traffic including school buses;
- g) ensure that appropriate right-of-way widths for all existing and proposed roads are provided in accordance with the Planning Act;
- h) encourage the use of alternative development standards for roads, where appropriate;
- i) encourage the efficient use of land along transportation corridors to maximize the use of public transit; and,
- j) restrict development on private roads.

### F6.2 PEDESTRIAN AND CYCLING ROUTES AND FACILITIES

Council shall develop an interconnected system of cycling and walking routes providing access to major activity and employment areas and to future public transit. In this regard, Council shall refer to the Trails and Cycling Master Plan to provide the basis for the establishment of a pedestrian and cycling network in the Town.

In addition to the Master Plan, and in order to plan for and encourage walking and cycling, Council shall:

- a) consider the provision of safe and convenient cycling and walking routes in the review of all development applications;
- b) require the provision of sidewalks in Urban Areas and *Hamlet Areas*, where appropriate;

- c) require the integration of cycling and walking routes with the location of school sites in existing and new neighbourhoods;
- d) investigate and provide for bicycle lanes wherever possible in the construction or reconstruction of roads and bridges;
- e) encourage and support measures which will provide for barrier-free design of pedestrian facilities;
- f) ensure that lands for bicycle/pedestrian paths are included with the land requirements for roads;
- g) ensure that the rights and privacy of adjacent property owners are factored into the design process for pedestrian and cycling routes; and,
- h) ensure that all pedestrian and cycling routes are designed to be safe.

### F6.3 PUBLIC TRANSIT

At the time of adoption of this Plan, a public transit system did not exist in the Town, however, the use and expansion of existing special transit service for the physically disabled (Acti-van) shall be encouraged and promoted.

Council shall actively promote transit-supportive land uses in Nodes, Corridors and new development areas. Council shall also review the need for a municipal transit system, as permitted by its financial capability, and if and when provided, integrate and support other transit systems and co-ordinate transportation planning efforts with Regional, Provincial and Federal transportation initiatives. Council shall encourage continuous improvements to the Provincial GO transit system.

### F6.4 ROAD NETWORK

#### F6.4.1 GENERAL POLICIES

##### F6.4.1.1 Road Classification System

For the purposes of this Plan, all roads in the Town are classified as follows:

- Provincial Freeway;
- Provincial Highway;
- Major Arterial Road;
- Minor Arterial Road;
- Multi-Purpose Arterial Road;
- Collector Road;
- Employment Road;

- Local Road; and,
- Private Road / Condominium Road/Private Laneway.

The types of roads described in this section of the Plan are shown on Schedule B1. Planned right-of-way widths are shown on Schedule B2 to this Plan. New roads and re-constructed roads under the Town's jurisdiction shall be developed to comply with the classification, function and general design requirements outlined in Table F6 – Function of Transportation Facilities and the planned right-of-way widths shown on Schedule B2 to this Plan. Highways and roads under the jurisdiction of the Province of Ontario or the Region of Halton are shown on Schedules B1 and B2 for information purposes only.

Any road transferred from the Province of Ontario to the Town shall be considered a Major Arterial Road for the purposes of this Plan. Any road transferred from the Region of Halton to the Town will be classified in accordance with the current and intended function of the road and will be subject to Section F6.4.1.2 of this Plan.

The location of any future Arterial and Collector Roads shown on Schedule B1 are approximate. The exact alignment of future Arterial and Collector Roads shall be determined either through municipal studies or during the consideration of development applications. An Amendment to the Official Plan will not be required to modify the locations of future roads, provided their function and location will continue to generally conform with the intent of the Official Plan.

#### F6.4.1.2 Changes to the Road Classification System

An Official Plan Amendment is required to change the classification of a road on Schedule B1.

#### F6.4.1.3 Right-of-Way Widths and Road Widenings

Right-of-way widths for every type of road are set out in Table F6. The right-of-way width for any public road may allow for the placement of travel lanes, turning lanes, utilities, infrastructure, high occupancy vehicle lanes, sidewalks, paths, bicycle lanes, medians, streetscaping and landscaped boulevards, where appropriate.

In addition to the road right-of-way widths set out in Table F6, the Town may, without the need for an amendment to the Official Plan, require the dedication of lands to be used for daylight triangles, to provide sufficient sight distances and turning lanes and/or traffic control devices, to provide safe and appropriate access to major generators or attractors of traffic. A greater right-of-way width may be required in cases where design solutions require additional lands for road improvements.

As a condition of a development approval, land for road widenings shall be conveyed at no expense to the Town in accordance with the provisions of the Planning Act. As a general principle, required road widenings will be taken equally from both sides of the right-of-way. Unequal road widenings may be considered by the Town where:

- a) the area is the site of a topographic feature which is difficult to overcome or costly to develop for road purposes;
- b) the location of an identified cultural heritage resource limits design options;
- c) the presence of a significant natural heritage feature limits design options; and/or,

- d) the location of mature trees contributes to the character of an area.

Notwithstanding the policies set out in this Plan, the Town recognizes that the reconstruction of roads to approved minimum standards in some existing developed areas may not be appropriate from a right-of-way acquisition or community design perspective, or economically or physically feasible. Any attempt to reconstruct such roads to minimize deficiencies shall only be undertaken after a study to determine a right-of-way which will result in a streetscape which minimizes impacts on abutting properties and is appropriate to the character of the area, while serving anticipated traffic volumes. No amendment to the Plan shall be required to implement such a modification to the right-of-way.

It is the policy of this Plan that an environmental assessment for any Arterial Road widening project shall address whether there are other transportation alternatives and how the project would implement the transportation goals, objectives and policies of this Plan.

F6.4.1.4 Financing of Road Construction

Construction of any part of the road network shall be in accordance with the ability of the authority having jurisdiction to finance such infrastructure. In addition, road construction under the jurisdiction of the Town shall be in accordance with the approved Capital Budget and/or the Capital Forecast.

F6.4.1.5 Road Design Standards

Council shall endorse a road standard design manual that reflects the Town's urban design policies and which will serve as the basis for the construction and design of roads that are under the jurisdiction of the Town. Council may consider alternative design standards to provide for the more efficient use of land in newly developing areas of the Town. In addition, the Town shall upgrade roads, sidewalks and related facilities to provide better access to the physically challenged. Changes to standards and design criteria for such roads and facilities may be permitted without an amendment to this Plan.

Local Urban Roads shall generally be designed on the basis of a grid street pattern and may be modified in certain circumstances to accommodate special locational or development circumstances.

F6.4.1.6 Road Closures

Council may stop up and close existing local roads and road related facilities, subject to the provisions of the Municipal Act, 2001, as amended without the need to amend the Official Plan.

F6.4.1.7 Traffic Calming

The Town may investigate traffic calming measures to be implemented in certain locations within the Town and/or as a requirement of a development approval to promote pedestrian safety and mitigate the effects of automobile traffic within the Town. Traffic calming features may be permitted subject to an evaluation by the Town of functional, operational, servicing and financial issues associated with their use.

F6.4.1.8 Roads in the Niagara Escarpment Plan Area

Proposed roads or roads to be re-constructed in the *Niagara Escarpment Plan Area* shall be subject to the provisions of the relevant land use designation and development criteria and the policies of the Niagara Escarpment Plan.

#### F6.4.1.9 Use of Town Roads by Trucks

Truck traffic is expected to use Arterial Roads and may be discouraged from using Collector and/or Local Roads.

#### F6.4.1.10 Special Situations

Any consideration of the extension of Queen Street in Acton, westerly to Main Street, as shown on Schedules B1 and B2 of this Plan, is conceptual only and will require a specific study in accordance with the requirements of the Municipal Class Environmental Assessment. Once an Environmental Assessment is completed, road widenings may be taken in accordance with the recommendations of the assessment.

### F6.4.2 PRIVATE ROADS

Private roads are lanes, driveways, roads or right-of-ways maintained by private individuals or Condominium Corporations. It is the policy of this Plan to restrict new development on private roads to a Plan of Condominium.

The creation of a new lot for any purpose on a private road outside of a Plan of Condominium is not permitted.

If the Town is to assume any private road, it must be brought up to a municipal standard. The cost of bringing such a road up to municipal standards shall not be borne by the Town.

Development on vacant lots of record existing at the date of the adoption of this Plan may be subject to Site Plan Control. It is not the intent of this Plan to control the siting of building or structures on the lot, unless it is deemed by the Town that issues relating to buffering, building placement and/or drainage should be dealt with in the context of a Site Plan Agreement.

The Site Plan Agreement shall contain wording that indicates that:

- a) the owner acknowledges and agrees that the lot in question does not front on an improved public road;
- b) the owner acknowledges and agrees that the Town does not, or is not, required to maintain or snowplow the said road or street;
- c) the owner acknowledges and agrees that the Town will not take over or assume a private road or street as a Town public road or street unless it has been built according to an appropriate road standard; and,
- d) the owner acknowledges and agrees that the Town is not liable for any injuries, losses or damages as a consequence of the Town issuing a building permit.

### F6.4.3 LANEWAYS

Private laneways shall only be permitted as part of a plan of condominium. Public water and wastewater services are generally prohibited by the Region in private laneways, except as permitted by the Region's Urban and Rural Servicing Guidelines.

TABLE F6: FUNCTION OF TRANSPORTATION FACILITIES

Facility Type	Function	General Design Guidelines
Provincial Freeway	<ul style="list-style-type: none"> <li>Serve mainly inter-regional travel demands</li> <li>Accommodate truck traffic</li> <li>Accommodate rapid transit services and high-occupancy vehicle lanes</li> <li>Carry high volumes of traffic</li> <li>Connect urban areas or nodes in different regions</li> </ul>	<ul style="list-style-type: none"> <li>Under the jurisdiction of MTO</li> <li>Minimum of 4 travel lanes</li> <li>Access is fully controlled (under the jurisdiction of MTO)</li> <li>Noise sensitive land uses to be discouraged along right-of-way</li> <li>Adjacent development will be subject to the minimum safety and geometric requirements of MTO. MTO permits required prior to any construction and/or grading being undertaken.</li> </ul>
Provincial Highways	<ul style="list-style-type: none"> <li>Serve mainly inter-regional travel demands</li> <li>Accommodate truck traffic</li> <li>Accommodate rapid transit services and high occupancy vehicle lanes</li> <li>Carry high volumes of traffic</li> <li>Connect urban areas or nodes in different municipalities</li> </ul>	<ul style="list-style-type: none"> <li>Right-of-way width up to 50m</li> <li>Access is restricted (under the jurisdiction of MTO)</li> <li>Transit-supportive land uses to be encouraged along right-of-way within urban areas</li> <li>Adjacent development will be subject to the minimum safety and geometric requirements of MTO. MTO permits required prior to any construction and/or grading being undertaken.</li> </ul>

Facility Type	Function	General Design Guidelines
Major Arterials	<ul style="list-style-type: none"> <li>Serve mainly inter-regional and regional travel demands</li> <li>Accommodate truck traffic</li> <li>Accommodate rapid transit services and high occupancy vehicle lanes</li> <li>Connect urban areas or nodes in different municipalities</li> <li>Carry high volumes of traffic</li> <li>Distribute traffic to and from Provincial Highways and Freeways</li> </ul>	<ul style="list-style-type: none"> <li>Right-of-way width up to 50m</li> <li>Transit supportive land uses to be encouraged along right-of-way within urban areas</li> <li>On-street parking not permitted</li> <li>Access is restricted with access points consolidated where possible</li> </ul>
Minor Arterials	<ul style="list-style-type: none"> <li>Serve mainly local travel demands</li> <li>Connect urban areas or nodes within the same municipality</li> <li>Accommodate local truck traffic</li> <li>Carry moderate to high volumes of traffic</li> <li>Distribute traffic to and from Major and Multi-Purpose Arterials</li> </ul>	<ul style="list-style-type: none"> <li>Right-of-way width up to 35m</li> <li>Up to 4 travel lanes</li> <li>On-street parking may be permitted where appropriate</li> <li>Access is partially controlled</li> <li>Consolidated access points where possible</li> </ul>
Multi-Purpose Arterials	<ul style="list-style-type: none"> <li>Serve a mix of functions of Major Arterials and Minor Arterials</li> </ul>	<ul style="list-style-type: none"> <li>Right-of-way width up to 50m</li> <li>Up to 4 travel lanes</li> <li>Access is restricted with consolidated access points where possible</li> </ul>

Facility Type	Function	General Design Guidelines
Collectors	<ul style="list-style-type: none"> <li>Connect neighbourhoods</li> <li>Distribute traffic to and from arterials</li> <li>Provide access to adjacent land uses</li> </ul>	<ul style="list-style-type: none"> <li>Right-of-way width up to 26m</li> <li>2 to 4 travel lanes</li> <li>On-street parking generally permitted</li> <li>Access is partially controlled</li> </ul>
Employment Road	<ul style="list-style-type: none"> <li>Serve mainly business parks, employment districts and areas with industrial type land uses</li> </ul>	<ul style="list-style-type: none"> <li>Right-of-way width up to 26m</li> <li>2 travel lanes</li> <li>On-street parking generally permitted</li> <li>Access is partially controlled</li> </ul>
Local Road	<ul style="list-style-type: none"> <li>Provide access to urban land uses</li> <li>Connect individual properties to collectors and arterials</li> <li>Carry comparatively low volumes of traffic</li> </ul>	<ul style="list-style-type: none"> <li>Right-of-way width up to 20m in urban areas</li> <li>Right-of-way width up to 26m in rural areas</li> <li>2 travel lanes</li> <li>Convenient linkages to collector roads and arterial roads</li> <li>Parking in rural areas is generally restricted</li> <li>Parking in urban areas may be allowed on both sides depending on pavement widths</li> <li>Access control not required</li> </ul>
Rapid Transit and Commuter Rail Corridors	<ul style="list-style-type: none"> <li>Serve inter-municipal and inter-regional travel demand</li> <li>Accommodate rapid transit services</li> <li>Connect Nodes</li> </ul>	<ul style="list-style-type: none"> <li>Exclusive right-of-way for transit vehicles as possible</li> <li>Transit-supportive land uses to be encouraged around stations</li> </ul>



Facility Type	Function	General Design Guidelines
Rail Lines	<ul style="list-style-type: none"> <li>Serve all types of people and goods movement by rail at the regional or national scale</li> <li>Accommodate commuter rail movement to major urban centres in the Greater Toronto Area and surrounding region</li> </ul>	<ul style="list-style-type: none"> <li>Grade separations at intersections with other major transportation facilities</li> <li>Noise or vibration-sensitive land uses to be discouraged along right-of-way</li> <li>Transit-supportive land uses to be encouraged around major transit stations</li> </ul>
Bicycle Paths	<ul style="list-style-type: none"> <li>Serve both local transportation and recreational travel demands</li> <li>Connect communities and activity areas throughout the Town</li> </ul>	<ul style="list-style-type: none"> <li>Bicycle paths may be located off-street or within road rights-of-way</li> </ul>

\*Right-of-way widths in some cases may need to be wider to accommodate design features, noise walls, turning lanes, bike paths, and utilities etc. Final right-of-way requirements along roads will be determined through functional designs and subdivision approvals.

## F6.5 INTER-MUNICIPAL TRANSPORTATION STUDIES

The Town of Halton Hills shall participate in a study, led by the Regions of Halton and Peel, and the Province where necessary, to identify the long-term transportation and transit network requirements and other transportation solutions along the Halton Hills/Peel boundary. It is intended that the study be completed on a timely basis, and may lead to amendments to this Plan and other appropriate implementation mechanisms.

## F6.6 HPBATS/GTA WEST CORRIDOR PROTECTION AREA

### F6.6.1 OBJECTIVE

It is the objective of this Plan to protect the lands within the HPBATS/GTA West Corridor Protection Area from development that could preclude or negatively affect their future use for the purposes for which they are identified and being planned, until the completion of the applicable Environmental Assessments, including the Greater Toronto Area (GTA) West Environmental Assessment (EA) and environmental assessments for Halton Peel Boundary Area Transportation Study (HPBATS) transportation improvements, including the East-West connection.

#### F6.6.2 LOCATION

The HPBATS/GTA West Corridor Protection Area as shown on Schedules A1, A3, A8 and B1 to this Plan correspond with the Route Planning Study Area as identified by the Ministry of Transportation (MTO) through Phase 1 of the GTA West EA process, as well as lands in the vicinity of Tenth Line and Ten Side Road, including the Southeast Georgetown lands which the HPBATS 2031 Recommended Road Network identifies as required for the East-West connection/Norval West By-Pass.

#### F6.6.3 LAND USE POLICIES

It is a policy of this Plan to prohibit the development of urban lands, within the HPBATS/GTA West Corridor Protection Area until the completion of the appropriate Environmental Assessments and by amendment to this Plan.

#### F6.7 OFF-STREET PARKING

The Town shall require that, as a condition of development or redevelopment, adequate off-street parking and loading facilities are provided. In addition, points of ingress/egress to parking areas shall be limited in number and the sharing of access points with adjacent similar uses shall be encouraged. The design and layout of parking facilities are addressed through the Town's Urban Design policies contained in Section F2 of this Plan.

- a) The Town will endeavour to provide off-street parking to serve the Georgetown and Acton *Downtown Areas*. In this regard, Council may:
  - i) operate municipal parking lots or structures on properties the Town owns, acquires and/or leases, and provide direction for establishing new lots and structures;
  - ii) establish areas where payment of cash-in-lieu of required parking may be accepted. A reserve fund may be established to be used for the improvement or expansion of public parking facilities; and,
  - iii) use, or authorize the use of vacant lands for parking on a temporary basis, where such parking is needed and desirable.
- b) The Town shall review off-street and on-street parking regulations to reflect evolving patterns of automobile use at the time of the preparation of the implementing Zoning By-law. Reduced parking requirements may be considered where sufficient public off-street and on-street parking facilities exist. In addition, parking requirements may be reduced if the uses on the lot each require parking at different times of the day. Opportunities for the sharing of parking in mixed use development will be considered during the review of a development application.

#### F6.8 RAIL NETWORK

The following policies apply to the rail network in the Town:

- a) The Town shall work with the Railway Corporations, Federal, Provincial and Regional governments to establish grade separated railway crossings on major roads wherever possible. Improvements to existing at-grade crossings shall also be encouraged to improve safety.

- b) Industrial uses that are adjacent to existing rail lines will be encouraged to use rail transport for the distribution of goods in order to reduce the burden on the existing highway system and to better utilize existing infrastructure. In addition, aggregate extraction operators will be encouraged to utilize rail wherever possible to transport aggregate to market and thereby minimize the impact to truck travel on residents, businesses and the natural environment.
- c) All proposed development adjacent to or in proximity to railway corridors shall be based on the implementation of the recommendations of the required noise and vibration studies as set out in C15 of this Plan. This shall include the provision of appropriate safety measures such as setbacks, berms and security fencing, mitigating measures, and notices on title to the satisfaction of the Town in consultation with the appropriate rail authority.
- d) The Town shall work with the appropriate agencies to develop appropriate strategies to deal with the movement of dangerous goods through the Town.

## F7 PUBLIC PARKLAND

### F7.1 OBJECTIVES

It is the objective of this Plan to:

- a) establish and maintain a system of public open space, parkland and recreational facilities that meets the needs of present and future residents;
- b) enhance existing parkland areas wherever possible to respond to changing public needs and preferences;
- c) ensure that appropriate amounts and types of parkland are acquired by the Town through the development process;
- d) encourage the dedication and donation of environmentally sensitive lands into public ownership to ensure their continued protection;
- e) protect and enhance the public open space and parkland areas in a manner that is consistent with the 'environment-first' objectives of this Plan, while accommodating appropriate levels of public use;
- f) promote the establishment of a continuous linear open space system connecting natural, cultural and recreational land uses within the Town and to surrounding municipalities;
- g) coordinate with other public and private agencies in the provision of open space, recreational and cultural facilities; and,
- h) encourage the development of a walking and cycling trail system within the open space system that is accessible to the public utilizing trails, paths, streets and other public open spaces.

### F7.2 GENERAL POLICIES APPLYING TO ALL PUBLIC PARKLAND

#### F7.2.1 RECREATION AND PARKS STRATEGIC ACTION PLAN

The Recreation and Parks Strategic Action Plan adopted by Council in 2007 serves as the basis for the policies in this section of the Plan. The Strategic Action Plan is intended to serve as a guide for the development of parks and recreation facilities and services. The policies of this Official Plan are intended to complement the Recreation and Parks Strategic Action Plan. The Recreation and Parks Strategic Action Plan shall be updated, as required, to respond to changing needs and circumstances.

#### F7.2.2 TRAILS AND CYCLING MASTER PLAN

The Trails and Cycling Master Plan adopted by Council in 1999 together with the Cycling Master Plan adopted by Council in 2010 provide the basis for trail development and linkages. The policies of this Official Plan are intended to complement the Trails and Cycling Master Plan and Cycling Master Plan, and shall be updated, as required, to respond to changing needs and circumstances. The development of trails within the Greenlands System shall be subject to Section B1.2.6 of this Plan.

#### F7.2.3 OVERALL PUBLIC PARKLAND STANDARD

Council shall encourage the provision of public parkland to achieve a minimum ratio of 1.2 hectares of local parkland per 1,000 residents and 2.5 hectares of non-local parkland per 1,000 residents.

#### F7.2.4 INTEGRATION OF OTHER PUBLIC USES WITH THE PUBLIC PARKLAND SYSTEM

Where a public parkland area is to be integrated with an educational or major recreational facility, it is the policy of this Plan that:

- a) no physical barriers be introduced to separate those facilities that fulfill a joint function;
- b) a separate identity be maintained for the park and school or recreational facility components through signage and landscaping; and
- c) the facilities are designed to complement each other.

#### F7.2.5 OTHER SOURCES OF PARKLAND AND FACILITIES

Where appropriate, Council may:

- a) encourage other agencies to provide open space and amenity areas for public use;
- b) enter into joint use/management agreements respecting the development of specific recreational facilities that are available to the general public;
- c) provide linkages between municipal open space areas and the facilities provided by other agencies or private organizations; and,
- d) ensure the adequate provision of private indoor/outdoor amenity spaces through regulations pertaining to development applications.

#### F7.2.6 DEDICATION OF LAND THROUGH THE DEVELOPMENT PROCESS

The Town shall require five percent of the gross residential land area within a Draft Plan of Subdivision in the *Low Density Residential Area* designation to be dedicated to the Town as parkland. The Town shall also require the dedication of 1.0 hectare of land per 300 dwelling units for development within all other Urban Living Area designations and wherever residential uses are permitted uses in the Community Area designations in the Urban Areas. Two percent of the land within industrial/commercial development shall be dedicated as parkland. For uses other than those listed above, the Town shall require a dedication equal to five per cent of the land value. In lieu of the above requirements, Council may require cash-in-lieu of parkland instead, as deemed appropriate.

Lands designated as *Greenlands* or required for stormwater management facilities shall not be considered as any part of the required parkland calculation. To the extent possible, stormwater facilities will be incorporated into the adjacent parkland and designed in a manner to enhance the aesthetic appeal of the overall development.

Where applicable, lands to be reserved for future public open space and trail linkages will be identified as part of a development proposal.

All lands dedicated to the Town shall be conveyed in a physical condition satisfactory to the Town.

#### F7.2.7 USE OF DEDICATED LANDS

Lands dedicated for park purposes are intended to fulfill the requirements for Local Parkland as outlined in Section F7.3.4 of this Plan. In special cases, as determined by Council, such land may be utilized to fulfill some Non-Local Parkland requirements, as outlined in Section F7.3.5 of this Plan.

#### F7.2.8 USE OF MONIES RECEIVED THROUGH THE CASH-IN-LIEU PROCESS

All monies received under the provisions of Section F7.2.6 shall be used for the sole purpose of developing and acquiring public parkland and/or developing recreational facilities in accordance with the Planning Act. These monies may be used by Council for the:

- a) acquisition of additional land to expand existing parks where appropriate;
- b) acquisition of vacant infill sites to create new small parks;
- c) acquisition of lands that are declared surplus by a Board of Education or other public agencies;
- d) improvement of park design and development within existing parks; and/or,
- e) establishing priorities for acquisition through Council approval of updated Recreation Master Plans.

#### F7.2.9 BASIS FOR ACCEPTING CASH-IN-LIEU OF PARKLAND

Cash-in-lieu of parkland may be required where:

- a) the application of the rate of parkland dedication would render the remaining portion of the site unsuitable or impractical for development;
- b) existing municipal parkland is available in sufficient quantity and quality to accommodate further development in a particular area; and/or,
- c) more suitable parcels of land are available for municipal park purposes on other locations.

#### F7.2.10 PARKLAND DEDICATION BY-LAW

Council shall enact a Parkland Dedication By-law that establishes:

- a) the lands to which the by-law is applicable;
- b) the rate of parkland dedication in accordance with Section F7.2.6 of this Plan;
- c) the development applications which are subject to parkland dedication requirements; and,
- d) land uses that are exempt from parkland dedication requirements.

### F7.3 PARKLAND DEVELOPMENT POLICIES

#### F7.3.1 PARKLAND SITING AND DESIGN

All public parkland shall:

- a) have as much street frontage as possible and be open to view on as many sides as possible to maximize visibility from adjacent streets and promote safety;
- b) maximize public safety through park block size, visibility, configuration and location of park fixtures and facilities;
- c) have direct and safe pedestrian access from adjacent residential areas or adjacent greenlands where appropriate;
- d) be designed to minimize any potential negative impacts on adjacent residential areas through the use of such measures as planting, fencing and the provision of appropriate access, parking and buffers to active recreational facilities;
- e) incorporate natural heritage features wherever possible into the design of the parkland;
- f) be integrated into the fabric of the adjacent neighbourhood by promoting open space or walkway linkages to adjacent facilities, neighbourhoods and natural features;
- g) be connected, wherever possible, to trail systems, cycling routes, walkways, natural heritage corridors, utility corridors and drainage systems; and,
- h) incorporate multi-purpose community/recreational centers using the campus approach that may include schools, public, cultural facilities, libraries, cultural heritage resources, day nurseries and recreational facilities.

#### F7.3.2 PARKLAND DEVELOPMENT STANDARDS

It is the policy of this Plan that a Parkland Development Manual be prepared and approved by Council. The Parkland Manual shall deal with criteria such as planting size and species requirements, seeding and/or sodding requirements, drainage, park servicing requirements, facility material and quality requirements, parking requirements including quantities per facility and surfacing requirements and standard sizes for the layout of sports fields and park amenities.

#### F7.3.3 PARKLAND CLASSIFICATION SYSTEM

There are two types of public parkland in the Town. The Local Parkland classification applies to lands that are intended to be used by residents in the immediate area. The Non-Local Parkland classification applies to lands that are intended to be utilized on a community wide or Town basis. Policies for both types of parkland are contained within this section of the Plan.

#### F7.3.4 LOCAL PARKLAND

Local Parkland is intended to fulfill the needs and interests of residents in the surrounding residential area. The major components of Local Parkland include:

- a) Parkettes; and,
- b) Neighbourhood Parks.

Parkettes and Neighbourhood Parks are a permitted use in the Living Area (Low, Medium and *High Density Residential Area* designations) and do not require a separate land use designation. The conceptual locations of existing and planned Neighbourhood Parks are shown by symbol on the Land Use Schedules of this Plan. New Secondary Plans shall conceptually delineate the location of Local Parkland. Council will encourage the provision of Local Parkland at a ratio of 1.2 hectares per 1,000 residents. Council recognizes that factors, such as the amount and location of available vacant land, the proximity of lands within the Greenlands System and the location of schools, places of worship and Non-Local Parkland may influence the amount and location of Local Parkland provided in each area of the Town.

#### F7.3.4.1 Parkettes

Parkettes shall:

- a) generally service lands within a 0.2 to 0.4 kilometre radius, depending on population density;
- b) generally range from 0.2 to 0.6 hectares in size;
- c) be required when site conditions or neighbourhood design restrict access to other Town facilities;
- d) be required for housing developments that provide smaller lots with reduced opportunities for amenity areas;
- e) be centrally located within the neighbourhood it is intended to serve; and,
- f) provide a range of opportunities for active and passive activities, particularly for young children and older adults seeking close to home activities.

#### F7.3.4.2 Neighbourhood Parks

Neighbourhood Parks shall:

- a) generally service lands within a 0.4 to 0.8 kilometre radius, depending on population density;
- b) generally range from 1.5 to 2.5 hectares in size;
- c) be comprised mostly of tableland and be configured to support their intended use;
- d) be centrally located within the neighbourhood or neighbourhoods it is intended to serve;
- e) have frontage on a Collector Road that is adequate for the provision of on-street parking and site visibility;
- f) be located adjacent to an elementary school, other community facilities where feasible, other open space lands or storm water detention areas to complement existing facilities and/or provide a neighbourhood focal point; and,
- g) where adjacent to an elementary school have complementary facilities such as sports fields, hard surface play areas and components, and play apparatus.

#### F7.3.5 NON-LOCAL PARKLAND



Non-Local Parkland is intended to fulfill the needs and interests of all Town residents and visitors to the Town. The major components of Non-Local Parkland include:

- a) Community Parks; and,
- b) Town Wide Parks.

Council will encourage the provision of Non-Local Parkland at a ratio of 2.5 hectares per 1,000 residents. Council will encourage the co-ordinated development of Non-Local Parkland with secondary schools and other major institutional uses, and natural heritage features that form part of the Greenlands System. Non-Local Parkland in the Town is included within the *Major Parks and Open Space Area* designation and is also subject to the policies of Section B2 of this Plan.

#### F7.3.5.1 Community Parks

Community Parks shall:

- a) generally service a Secondary Plan Area or a specific Urban Area or *Hamlet Area*;
- b) have a minimum site area of 6.0 hectares of primarily tableland;
- c) be located adjacent to a secondary school or community facility such as a community centre, arena, pool or library where feasible, or other major open space lands to complement other neighbourhood facilities and provide a community focal point;
- d) have frontage on an Arterial or Collector Road;
- e) where adjacent to a secondary school, have complementary facilities such as major sports fields, hard surface play areas and other active park uses; and,
- f) be the site of primarily outdoor recreational uses with broader community facilities such as water play areas, gardens or special event staging areas with associated washrooms, pavilions or service areas.

#### F7.3.5.2 Town Wide Parks

Town Wide Parks shall:

- a) generally service the entire Town;
- b) have a minimum area of 11.0 hectares of primarily tableland;
- c) be located on an Arterial Road or Provincial Highway;
- d) provide major athletic facilities or standard sports fields, hard surface play areas, special event staging areas, multi-use trails, and other major recreation facilities with associated play, water play, washrooms, pavilions or service areas; and,
- e) where appropriate, integrate public cemeteries and other complementary public uses and community facilities.

Public cemeteries and related facilities are considered part of the Town open space system since cemeteries are largely an open space use. Public cemeteries are only permitted in the *Major Parks and Open Space Area* designation and are subject to the policies of Section B2 of this Plan.

## F8 COMMUNITY FACILITIES AND SERVICES

### F8.1 COMMUNITY FACILITIES

Community facilities include schools, libraries, community centres, and other recreational facilities that are designed to meet the educational, social, recreational and cultural needs of Town residents. It is the intent of this Plan to ensure that:

- a) schools are designed and built to adequately serve the surrounding neighbourhood and/or catchment area;
- b) new schools are planned and sites are reserved as part of the planning process for the establishment of new neighbourhoods;
- c) the Town continues to be well served by community centres and other recreational facilities in appropriate locations;
- d) adequate library services are provided to Town residents; and,
- e) adequate emergency access is provided throughout the Town by police and fire stations and by police, fire and other emergency vehicles.

In order to ensure that municipal services are provided in a manner that meets the needs of Town residents, Facility Strategies that deal with service areas shall be prepared to serve as a guide for the provision of services to a growing and changing population. In addition, Council shall ensure that the School Boards are an integral part of the planning process so that the provision of schools keeps pace with projected demand.

#### F8.1.1 ELEMENTARY AND SECONDARY SCHOOL SITES

In the interest of preserving elementary and secondary school sites for their intended educational purposes and due to their importance as community hubs and/or neighbourhood gathering places, the Town of Halton Hills encourages the retention of all such schools that existed on the date this Plan was approved. In addition, where a school site has been identified and reserved in a newly developing area, the Town encourages the school boards to implement their request initially to reserve the site when the planning for the area was undertaken. Once such a school site has been identified, the Town will require that a sign be placed on the property advising residents that a school is planned for the site and that the timing and need for the site is at the absolute discretion of the school board involved.

Where all or part of a site which has been identified for an elementary or secondary school is not required, or where an existing school is proposed to be closed, alternative uses shall be permitted in order of priority as follows:

- a) Other public institutional uses, parks or other open space uses that would continue to fulfill the role of the site as a focus of community activity and neighbourhood interaction particularly in areas where there is a parkland deficiency;
- b) Private institutional uses such as places of worship, private educational facilities, day nurseries, and long term care facilities and retirement homes in accordance with Sections D1.3.1.8.1 and D1.3.1.9 as appropriate; and,

- c) Low Density Residential uses in accordance with the policies of Section D1.3.1 of this Plan and/or commercial uses, but only if commercial uses on the site would be permitted in accordance with Section D2.5.5 of this Plan.

In order to implement the Town's objectives in this regard, communication between the school boards and the Town is required and the Town will work cooperatively with the school boards on finding solutions regarding the long term use of existing and planned school sites.

## F8.2 MUNICIPAL WATER AND WASTEWATER SERVICES

Municipal water and wastewater services are the responsibility of the Region of Halton as set out in the Regional Plan. It is the policy of this Plan that:

- a) All development in the Urban Area shall be connected to municipal water and wastewater systems unless exempted by the policies of this Plan and the Regional Official Plan;
- b) Development be limited in the Urban Area to the ability and financial capability of the Region to provide municipal water and wastewater services in accordance with its approved Development Charges Bylaws;
- c) The extension of municipal water and wastewater services across Urban Area boundaries is prohibited, unless the services are being provided to lands within the *Hamlet Area* designation or other exceptions specifically identified in the Regional Plan; and,
- d) The allocation of municipal water and wastewater system capacities through the development approvals process will be based upon a program developed in consultation with the Region, and implemented through reports to Council when necessary, which ensures the timely and efficient use of these services.

Public and quasi-public uses are permitted in land use designations within the Urban Area, *Hamlet Area* and *Rural Cluster Area*, subject to any regulatory requirements such as the provisions of the Environmental Assessment Act. These uses include municipal water and wastewater infrastructure, stormwater management facilities, roads, railways lines, natural gas, telecommunications infrastructure and similar uses. The implementing Zoning By-law shall contain regulations that control the siting of all public buildings and structures, accessory uses and outdoor storage associated with any public use.

Public and quasi-public uses are permitted in the Environmental and Open Space Area and the various designations outside of the Urban Area, *Hamlet Area* and *Rural Cluster Area* if specifically listed as a permitted use in the relevant land use designation. Specific policies regarding infrastructure within the lands subject to the Greenbelt Plan are contained in Section E2.6 of this Plan.

Where companies subject to federal or provincial control propose a new wireless communication facility, it is the policy of this Plan to encourage where feasible and appropriate:

- a) the screening of antennas and towers from view from roads or scenic vistas through landscaping, fencing or other architectural screening;
- b) the use of innovative design measures such as the integration of such uses with existing buildings or among existing uses;
- c) collocation with other service providers;
- d) locations on existing infrastructure such as water towers or utility poles; and,
- e) locations away from sensitive land uses.

The Town shall work with pipeline companies to ensure that the location and design and construction standards used for major oil and gas transmission pipelines have regard to environmental and safety matters. To this end, a minimum 7metre setback shall be maintained from the limits of the TransCanada Pipeline right-of-way located within the Premier Gateway Employment Area for all permanent structures and excavations. A reduction in the setback may be considered if it is demonstrated to the satisfaction of TransCanada Pipelines Limited that it will not compromise the safety and integrity of the pipeline.

## F10 DEVELOPMENT PHASING STRATEGIES

### F10.1 OBJECTIVES

It is the intent of this Plan:

- a) to ensure that development occurs in an orderly, well-planned manner;
- b) to ensure that the provision of community services and facilities is coordinated with the development of new communities;
- c) to give priority to the development of employment lands and Intensification Areas, infilling and completion of existing neighbourhoods and communities; and
- d) to maintain an acceptable balance between residential and non-residential assessment.

### F10.2 GENERAL PHASING CRITERIA

Development in the Urban Area shall be phased in accordance with the following criteria:

- a) the proposal contributes to the protection and enhancement of natural heritage features and ecological functions and provides opportunities for public ownership of these lands;
- b) the proposal represents the logical completion or extension of the existing urbanized area, roads and other infrastructure to avoid scattered development patterns;
- c) the proposal can be adequately and economically serviced by municipal water and wastewater services and storm drainage facilities;
- d) the proposal will provide park sites, school sites and portions of a community wide trail system;
- e) the proposal provides an opportunity for both employment and residential uses in order to encourage improved live/work relationships and to attract non-residential assessment;
- f) where appropriate, proposals for residential development provide a mix and range of housing to accommodate the Town's housing needs as set out in the Municipal Housing Statement;
- g) the proposal can be accommodated within the financial capabilities of the Town to provide the necessary services without decreasing the level of service to existing development;
- h) the proposal participates in local or Regional servicing and financial agreements pursuant to the Development Charges Act; and,
- i) the proposal contributes to achieving the density targets of this Plan and the Regional Phasing set out in Section F10.4.

More detailed phasing strategies shall be included in Secondary Plans and where appropriate, Block Plans.

### F10.3

#### GEORGETOWN SOUTH EXPANSION LANDS - SPECIAL PHASING POLICIES

Within the Georgetown South Expansion Lands, identified as Parts of Lots 11 and 12, Concessions 9 and 10, the phasing of development shall occur generally in accordance with the phases shown on Schedule A3 to this Plan. The Schedule anticipates the development will proceed within three stages, labelled as Phase 1, Phase 2A and Phase 2B generally in that order. Each phase shall contain a mix and range of housing types. Prior to any development within any phase, water and wastewater capacity must be available for lots to be registered within that subsequent phase.

The Town in consultation with the Region and landowners, may make adjustments to the boundaries of the Phases on Schedule A2 without amendment to this Plan, provided that the Phases correspond to the description set out below.

Phase 1 includes the completed Catholic elementary school site and an extension of the eastern section of Barber Drive. Also included in Phase 1 is the extension of the east and west “legs” of Barber Drive to 10 Side Road, which can be provided with sanitary sewer services by gravity drainage to the sewers in the existing Georgetown South Community to the north, with the exception of the westerly portion which will require the construction of a new sewage pumping station in the vicinity of the southwest corner of Phase 1. Phase 2A will also drain to this new pumping station. Phase 1 and 2A will each require the construction of a separate stormwater management facility. As part of any development in Phase 2A, the collector road and a separate pedestrian connection across the Sixteen Mile Creek will be required. Phase 2B requires the construction of a new sewage pumping station in the vicinity of the southeast corner of Phase 2B.

Prior to the registration of plans of subdivision in Phase 2, at least 65 percent of the residential units in Phase 1 must be serviced, which shall include the lands immediately adjacent to Mountainview Road, excepting residential units within the *Secondary Node*.

In the event that plans of subdivision are approved for only a portion of the Phase 1 lands, lands designated *Secondary Node* that are outside of the boundaries of the approved plans will be placed in a zoning category which restricts development until such time as servicing capacity is available.

### F10.4

#### GENERAL PHASING POLICIES

#### F10.4.1

##### REGIONAL PHASING OF THE URBAN AREA

The phasing of development shall be in accordance with the Regional Phasing identified in Table F10 and subject to other relevant policies of this Plan and the Regional Official Plan.

Development within the *Future Residential/Mixed Use Area* shown on Schedule A3, the Premier Gateway Employment Area Phase 2 shown on Schedule A8, and the Mansewood area shown on Schedule A1 of this Plan is to be phased over the 2021 to 2031 period in accordance with Table F10.

TABLE F10: REGIONAL PHASING

	2012- 2016	2017- 2021	2022- 2026	2027- 2031
Units in Designated Greenfield Area	672	847	3,936	3,773
Low Density Units	623	751	2,205	2,402
Medium and High Density Units	49	96	1,731	1,371
Units Inside the Built Boundary	517	917	2,056	2,087
Employment	787	2,192	9,420	9,606

#### F10.4.2 JOINT INFRASTRUCTURE STAGING PLAN

It is the intent of this Plan that the Town participate with the Region, the School Boards, and Provincial human service agencies, in the preparation of a Joint Infrastructure Staging Plan based on the population and employment forecast contained in Section A1A of this Plan to ensure infrastructure and human services to support growth is planned and financing is in place before it is required.



The implementation section contains policies pertaining to the administration and implementation of the Official Plan. The Planning Act contains a number of tools that are intended to be used by municipalities to administer and implement an Official Plan. This section of the Plan contains the policies that set out how these tools are to be utilized by Council to meet the goals and objectives of this Plan.

## G2 OFFICIAL PLAN ADMINISTRATION

This section of the Plan describes how the Official Plan will be administered, updated and amended, as required.

### G2.1 AMENDMENTS TO THE PLAN

It is the intent of this Plan to serve as the basis for managing change in the Town until 2031.

It is therefore a policy of this Plan that this Plan should only be amended when the policies of this Plan have been found not to address issues, or alternatively, issues have been raised with respect to site-specific proposals that must be addressed in a comprehensive manner. However, where Official Plan Amendments are contemplated by the policies of this Plan, Council shall consider them.

Any Amendment shall conform to the overall intent of the Official Plan as set out in the:

- a) community vision, goals and strategic objectives;
- b) other relevant objectives and policies of this Plan; and,
- c) 'environment-first' philosophy that is enshrined within this Plan.

### G2.2 OFFICIAL PLAN REVIEW PROCESS

The assumptions, objectives and policies of this Plan shall be reviewed at least once every five years at a meeting of Council, which shall be advertised in accordance with the Planning Act, as amended.

The five-year review shall consist of an assessment of:

- a) the effectiveness of the Plan in meeting the 'environment-first' objectives of the Plan;
- b) the effectiveness of the Plan in protecting water quality, heritage resources, natural resources and habitat and the general environment within the Town;
- c) the continuing relevance of the community vision that forms the basis of all policies found in this Plan;
- d) the degree to which the goal and objectives of this Plan have been met;
- e) the amount and location of lands available for urban development;
- f) whether the Town has realized a desirable balance of commercial and industrial assessment in relation to residential assessment;
- g) the Town's role within Halton Region and the Greater Toronto Area and its relationship with other municipalities;
- h) development trends in Halton Region and the Greater Toronto Area and their effect on development in the Town;

- i) any revisions to the Region of Halton Official Plan, the Niagara Escarpment Plan and the Provincial Policy Statement and their implications for the Town; and,
- j) the nature of any Greater Toronto Area – wide planning initiatives and their implications for the Town.

## G2.3

### GROWTH MANAGEMENT

As noted in Section A1 (The Community Vision) of this Plan, Council recognizes that the small town character and rural nature of the Town set it apart from other communities in the Region. It is the intent of this Plan to preserve and enhance that character and distinctiveness, while providing for growth and change in key locations.

In keeping with the Places to Grow Act (2005), municipal official plans must be brought into conformity with the Growth Plan for the Greater Golden Horseshoe within three years of the final approval of that Plan (June 16, 2009). Between 2006 and 2009, the Region of Halton completed a multi-year work program, entitled Sustainable Halton, to achieve Growth Plan conformity, as well as conformity with the Greenbelt Plan and 2005 Provincial Policy Statement. This planning exercise culminated in the selection of a Preferred Growth Option in June 2009, and the adoption of Regional Official Plan Amendment No. 38 in December 2009, implementing the Preferred Growth Option, as well as a number of other policy changes arising from the multi-faceted work program. In July 2011, the Region subsequently adopted Regional Official Plan Amendment No. 39, establishing Regional phasing of residential and employment development by Local Municipality within the Built Boundary and the Designated Greenfield Area to the 2031 planning horizon.

The Preferred Growth Option in the context of Halton Hills consisted of 20,000 population growth, 370 hectares of residential/mixed use area contiguous to the Georgetown Urban Area, and 340 hectares of employment land contiguous to the Premier Gateway Employment Area. In addition, a minimum intensification requirement of 5,100 units between 2015 and 2031 within the Built-up Areas of Acton and Georgetown was established. Through a series of amendments to the Halton Hills Official Plan, the Town has worked to achieve conformity with the Provincial Growth Plan and the Regional Official Plan, as amended by Amendment No. 38.

In addition to the requirements of the Regional Official Plan, the expansion of any urban boundary may only be considered provided that:

- a) the expansion area serves as a logical extension to the existing built up area;
- b) the expansion area can be easily integrated with the fabric of the existing built up area;
- c) the expansion area can be appropriately serviced;
- d) an appropriate housing mix, as determined by the Municipal Housing Statement, is provided for on the lands;
- e) new employment lands are an integral component of the expansion;
- f) the lands can be easily accessed by existing arterial roads and will not contribute to traffic congestion within the existing community;

- g) prime agricultural lands shall only be included if no reasonable alternative exists;
- h) the expansion area shall conform with the 'environment-first' objectives of this Plan;
- i) the lands are not located within the *Protected Countryside Area* designation; and,
- j) the scale of the expansion is in keeping with the Community Vision, Goals and Strategic Objectives of this Plan.

Notwithstanding the foregoing, an expansion to the Community of Acton shall not be considered until the ten-year review of the Province's Greenbelt Plan.

Expansions to the Hamlets of Glen Williams and Norval are prohibited.

Expansions to the Rural Clusters shall not be permitted in accordance with Section E4.2 of this Plan.

## G2.4 PUBLIC PARTICIPATION

It is policy of this Plan that public participation be an integral component of any land use planning process. On this basis, before making any planning decisions, Council shall be satisfied that:

- a) Adequate public notice has been given;
- b) Enough information to enable a person to reasonably understand the nature of the proposal and its impact is available prior to any public meeting;
- c) All public and agency comments have been assessed and analyzed by staff; and,
- d) Their decision will appropriately balance the overall public interest against the private interest expressed in the application.

Proponents shall be encouraged to pre-consult with neighbouring landowners to obtain their views before a formal application is submitted.

Public Open Houses shall be required for major municipally-initiated planning programs and studies, such as Secondary Plans and Zoning By-law reviews.

Council may eliminate notice to the public and a public meeting for a minor Official Plan or Zoning By-law Amendment that:

- a) Changes the numbers of sections or the order of sections in the Plan or By-law, but does not add or delete sections;
- b) Consolidates previously approved Official Plan or Zoning By-law Amendments in a new document without altering any approved policies, provisions or maps;
- c) Corrects grammatical or typographical errors in the Plan or By-law that do not affect the intent of the policies, provisions, or maps; and,

- d) Rewords policies or provisions or re-illustrates mapping to clarify the intent and purpose of the Plan or By-law, without affecting the intent or purpose of the policies, provisions, or maps.

In all other instances, notification to the residents of the Town of public meetings held by Council shall be given a minimum of 20 days prior to the public meeting, unless additional notice is required at the discretion of the Town, in accordance with the following procedures:

- a) Notice of any privately initiated application requiring notice shall be provided through on-site signage, which is to be provided along each frontage of the subject lands so that it is visible from the street and is in a location that ensures safe sight lines. If posting a sign on the property is impractical, posting will be required at a nearby location chosen by the clerk of the municipality;
- b) Notice of any privately initiated application requiring notice shall be provided by prepaid mail to every owner of land, or condominium owner, and to those people who have requested notice, within 120 metres of the subject lands;
- c) Notice of any publicly initiated application requiring notice shall be provided through digital and online channels as appropriate, or other means as deemed appropriate by Town staff.



The purpose of any Secondary Plan is to establish a detailed development concept for infrastructure and land use in specific areas of the Town, such as new communities or the redevelopment of an existing community. More specifically, it shall be the intent of any Secondary Plan to:

- a) establish clear and appropriate boundaries for the new community or the redevelopment of an existing community;
- b) establish population, housing unit and employment capacity targets, including targets for affordable housing;
- c) establish the most appropriate mix of land uses that recognizes the location of the area and the goals and objectives of this Plan;
- d) ensure that the environment-first objectives of this Plan are met, including policies for the protection and enhancement of natural heritage features and ecological functions;
- e) ensure that the urban design objectives and policies of this Plan are met;
- f) ensure that adequate transportation networks are/or will be established and that the transportation network is adequately supported by the proposed development pattern;
- g) ensure that adequate water and wastewater servicing is established to serve the anticipated development and that they can be phased in a way that is cost-effective and efficient;
- h) ensure that appropriate Secondary Plan policies, including phasing, servicing and financing policies, are in place to clearly and effectively guide future development within the Secondary Plan Area;
- i) incorporate appropriate policies pertaining to the provision and location of utilities;
- j) establish land use patterns that promote mixed-use, compact, transit-supportive, walkable communities, including identifying the locations for social, cultural, recreational, educational and religious facilities;
- k) establish the location, types and density of residential and employment lands that contributes to the creation of healthy communities through: the appropriate mix and density of housing; strengthening live-work relationships through the balance of residential and employment land uses; the provision of local parks and open space; and promoting active transportation and the use of public transit;
- l) address land use compatibility in accordance with Regional and Ministry of Environment guidelines; and,
- m) establish overall development density for the area, and for areas within the Designated Greenfield Area, how this density will contribute to achieving the minimum development density specified in Section D6.2 and the Regional Phasing in Section F10.4 of this Plan.

Secondary Plans shall be adopted by an Amendment to this Plan and shall generally conform with and implement the intent of this Plan as set out in the Goals, Strategic Objectives and other relevant objectives and policies of this Plan. In the event of a conflict between the policies in this Plan and those contained in Secondary Plans, the Secondary Plan policies shall prevail to the extent of the conflict within the geographic area covered by the Secondary Plan.

New Secondary Plans shall be based on the following studies:

- a) a Subwatershed Study in accordance with Section C7 of this Plan;
- b) a Functional Servicing Plan that demonstrates the means by which the new community will be appropriately serviced;
- c) Urban Design Guidelines in accordance with Section F2 of this Plan;
- d) a detailed Transportation Study that determines the impact of the development on the surrounding road network and recommends necessary improvements to the transportation network consistent with the goals, objectives and policies of this Plan;
- e) a Fiscal Impact Analysis that demonstrates development shall not negatively impact the Town's financial position;
- f) a Market Impact Assessment that determines the need for additional commercial floorspace, having regard for the planned function of existing commercial designations established by this Plan;
- g) an Air Quality Impact Assessment based on Regional guidelines;
- h) a Community Infrastructure Plan, based on Regional guidelines, describing planning for the provision of public services for health, education, recreation, social and cultural activities, safety, security and the provision of affordable housing; and,
- i) an Agricultural Impact Assessment of the potential impact of urban development on existing agricultural operations, based on Regional guidelines including the requirement for compliance with the Minimum Distance Separation formulae where an agricultural operation is outside the Urban Area.

Additional studies may be identified as part of the Terms of Reference for a new Secondary Plan.

### G3.2

#### BLOCK PLANS

Block Plans are detailed development plans for a defined area that are intended to further coordinate development for a defined area, such as but not limited to, a Secondary Plan area prior to the granting of zoning and subdivision approvals. Such an area may be in one, or a limited number of ownerships, where it is proposed to phase development over a period of time. Council may require the preparation of a Block Plan, in accordance with defined Terms of References and studies, to show:

- a) the various land uses and densities;
- b) the location, dimensions and intersection details of all existing and proposed roads;



- c) the location of existing and proposed water and wastewater servicing, stormwater management ponds and utilities;
- d) the location and the anticipated timing of any schools and community centres;
- e) location, dimensions and areas of lands to be conveyed to the Town for parks and open space purposes; and,
- f) the phasing of development.

Where Council has determined that a Block Plan is required to coordinate the development for a defined area, the approval of draft plans of subdivision and implementing zoning by-laws shall be considered premature. Prior to the completion of a Block Plan to the satisfaction of the Town in consultation with the relevant agencies, lands within the area under consideration may be placed in a Development Zone.

### G3.3

#### COMPREHENSIVE DEVELOPMENT PLANS

Comprehensive Development Plans are detailed plans for a defined area within existing urban communities that are intended to provide an appropriate context for the consideration of infill, intensification and redevelopment proposals. Comprehensive Development Plans are intended to address matters such as appropriate distribution of uses, built form, urban design, access, parking, and streetscaping. These Plans may relate to one or a number of ownerships as appropriate and are to be approved by Council prior to the consideration of development applications pursuant to the Planning Act. More specific policies regarding Comprehensive Development Plans are found under the relevant designations set out in this Plan.

## G4

## ZONING BY-LAWS

Once this Official Plan is approved and in effect, no zoning by-law can be passed by Council that does not conform with this Plan. In order to ensure that the Official Plan is implemented appropriately, a new Comprehensive Zoning By-law that will apply to the entire Town, save and except for those lands subject to the Niagara Escarpment Development Control Area pursuant to Ontario Regulation 826/90, shall be prepared to implement this Plan. The intent of the new by-law will be to effectively control and regulate development in accordance with the intent of this Official Plan.

It is not the intent of this Plan to zone all lands in accordance with the land use designations until such time as the policies of this Plan can be met. However, lands may be pre-zoned in accordance with the land use designations provided that Council is satisfied that it will further the goals and strategic objectives, and other relevant objectives and policies of this Plan.

### G4.1

### TEMPORARY USE BY-LAWS

#### G4.1.1

#### PURPOSE OF A TEMPORARY USE BY-LAW

Council may pass by-laws permitting the temporary use of lands, buildings or structures, which may not conform to this Plan, subject to the criteria contained in Section G4.1.2. These temporary uses may be authorized for a specific time period up to three years and should be applied where it is considered inappropriate by the Town to permit the proposed use on a permanent or continuing basis and where alternatives such as relocation are not practical. A garden suite may be permitted on a property for up to 10 years. Subsequent by-laws granting extensions of up to three years (or 10 years for a garden suite) may be passed. However, once the by-law has lapsed, the use must cease or otherwise will be viewed as contravening the implementing Zoning By-law.

#### G4.1.2

#### CRITERIA

Prior to the approval of a temporary use Zoning By-law, Council shall be satisfied that:

- a) the proposed use is of a temporary nature and shall not entail any major construction or investment on the part of the owner so that the owner will not experience undue hardship in reverting to the original use upon the termination of the temporary use;
- b) the proposed use will not prejudice the long term intent of or the orderly development contemplated by the provisions and land use designations contained in this Plan;
- c) the proposed use is compatible with adjacent land uses and the character of the surrounding neighbourhood;
- d) the proposed use will not require the extension or expansion of existing municipal services;
- e) the proposed use will not cause traffic hazards or an unacceptable level of congestion on surrounding roads;
- f) parking facilities required by the proposed use will be provided entirely on-site; and,
- g) the proposed use shall generally be beneficial to the neighbourhood or the community as a whole.

## G4.2

### HOLDING PROVISIONS

Council may pass a Zoning By-law that identifies a use of land, but prohibits the actual development of the land until a later date when identified conditions have been met. These conditions are set out in the policies applying to the land use designations in this Plan. The objective of utilizing a Holding Provision is to ensure that:

- a) the appropriate phasing of development or redevelopment occurs;
- b) development does not proceed until community services and facilities such as water and wastewater services, stormwater management and utilities are available to service the development;
- c) development does not proceed until adequate transportation facilities and/or access arrangements are available; and,
- d) agreements respecting the design of the proposed development and/or the services that are to be provided by Council and/or the Region are entered into.

Council, at any time, may designate any Zone or part of a Zone as a Holding Zone by placing an “H” in conjunction with the Zone symbol in order to meet one of the above-mentioned objectives. During the interim period, when the Holding Zone is in place, uses permitted on the affected lands are limited to existing uses only.

## G4.3

### HEIGHT AND DENSITY BONUSING

#### G4.3.1

#### BONUSING TRIGGERS

Council may pass a by-law pursuant to the Planning Act that increases the maximum density and/or height permitted by this Official Plan for medium and high-density residential development if the increase will result in the provision of a significant public benefit that would have not otherwise been realized. A significant public benefit could include the:

- a) provision of long-term care facilities or retirement homes;
- b) provision of needed community facilities;
- c) provision of parkland over and above what would be required by this Plan;
- d) dedication of lands that are the site of a natural heritage feature and related buffers;
- e) provision of public transit facilities;
- f) conservation/enhancement/restoration of a significant built heritage feature;
- g) provision of affordable or special needs housing;
- h) provision of public art in a key location; or,

- i) any other public benefit that has been identified in a Council-initiated study.

#### G4.3.2 CRITERIA

It is the intent of this Plan that there has to be a clear and measurable public interest served by the granting of a height and/or density bonus. Prior to granting a bonus, Council shall be satisfied that:

- a) the proposed development will conform with the development policies of the applicable designation of this Plan;
- b) the built form will contribute to the urban design policies of this Plan, as set out in Section F2;
- c) the use will be a positive addition to the urban landscape and contribute to the vibrancy of the urban area; and,
- d) the development of the use will potentially facilitate the development or establishment of other uses in the area, thereby furthering the objectives of this Plan.

It is the intent of this Plan that each request for a bonus shall be carefully considered on a case-by-case basis. The approval of one request by Council shall not be considered a precedent for other requests.

#### G4.3.3 IMPLEMENTATION

A site-specific zoning by-law passed by Council shall establish detailed development standards that would apply when a bonus is awarded and the relationship between these standards and the conditions that must be met.

In the granting of a height and density bonus, the Town may require the owner to enter into one or more agreements with the municipality dealing with facilities, services or other matters to be provided.

The maximum residential density and height permitted through the bonus provisions may exceed that which is permitted in the general Official Plan policies. Notwithstanding the foregoing, no bonus may be granted that:

- a) increases the density of development in the *Medium Density Residential Area* by more than 20 percent;
- b) increases building height in the *Medium Density Residential Area* by more than two storeys;
- c) increases the density of development in the *High Density Residential Area* by more than 50 percent; and,
- d) increases building height in the *High Density Residential Area* by more than four storeys.

In considering any bonusing request, Council shall have regard for the policies in Section D1 and D2 of this Plan.

#### G4.4 INTERIM CONTROL BY-LAWS

Council may pass interim control by-laws to restrict the use of land in a defined area for a period of up to one year in order to provide Council with the time to study a particular land use planning issue. The passage of such a By-law shall be preceded by the passage of a Council Resolution that identifies the land use planning issue and authorizes an appropriate review of the land use planning issue.

In the event that the review or study related to the Interim Control By-law has not been finalized within one year, Council may amend the Interim Control By-law in order to extend the period of time for which it is in effect, provided that the total period of time it is in effect does not exceed two years from the date of passing of the Interim Control By-law.

#### G4.5 DELEGATED AUTHORITY

- a) Council may, by by-law, delegate the authority to pass by-laws under Section 34 of the Planning Act, that are of a minor nature, to an individual who is an officer or employee of the Town (i.e., Commissioner of Planning & Development or designate).
- b) Delegation of authority to pass by-laws under Section 34 of the Planning Act shall be limited to:
  - i. a by-law to remove a holding “H” symbol;
  - ii. a by-law to authorize the temporary use of land, buildings, or structures subject to the criteria contained in Section G4.1.2 of this Plan; and
  - iii. minor zoning by-law amendments.
- c) The delegation of authority to pass a by-law to authorize the temporary use of land, buildings, or structures and to pass minor zoning by-law amendments is subject to the following criteria:
  - i. an Official Plan Amendment is not required, and the proposal maintains the general intent and purpose of the Town’s Official Plan, including its vision, goals, objectives, and policies;
  - ii. a Draft Plan of Subdivision is not required in accordance with Section F1.1 of the Town’s Official Plan; and
  - iii. any concerns raised by the public and/or staff during the application review and consultation process are resolved prior to the passing of the by-law.

## G5 LEGAL NON-CONFORMING USES

### G5.1 INTENT OF THE OFFICIAL PLAN

As a general rule, existing uses that do not conform with the designations and policies of this Plan should gradually be phased out so that the affected land use may change to a use that is in conformity with the Official Plan and the intent of the implementing Zoning By-law. Uses that were legally existing at the date of the adoption of this Plan, which do not conform to the designations and policies of this Plan, may be recognized in the implementing Zoning By-law. In some instances, it may also be appropriate and practical to allow the replacement, extension or enlargement of non-conforming uses in order to avoid unnecessary hardship. Council shall, therefore, have regard for the following principles:

- a) the feasibility of acquiring the property for holding, sale, lease or development by the Town for a more appropriate permitted use; and,
- b) the possibility of relocating the non-conforming use to another site.

If the property cannot be acquired or a building relocated, the Council may, without an amendment to this Plan, consider passing a Zoning By-law Amendment pursuant to the Planning Act to allow for an extension to a non-conforming use. Prior to such approval, Council shall be satisfied that:

- a) the size of the extension or enlargement of the established use is in proportion to the size of the use as it existed at the date of the enactment of the implementing Zoning By-law;
- b) the proposed extension is compatible with the character of the surrounding area in relation to noise, vibration, fumes, dust, smoke, odours, lighting and traffic generation;
- c) the proposed extension will not prejudice the long term intent of or the orderly development contemplated by the provisions and designations contained in this Plan;
- d) site planning and design will minimize the impact of the proposed extension on neighbouring conforming uses and includes, where appropriate, measures such as fencing, landscaping, and setbacks through the use of Site Plan Control; and,
- e) adequate water and wastewater servicing is available.

### G5.2 ROLE OF THE COMMITTEE OF ADJUSTMENT

When considering applications to expand or extend a legal non-conforming use pursuant to the Planning Act, the Committee of Adjustment shall be satisfied that the application conforms to the criteria identified in Section G5.1 of this Plan.

When considering applications for minor variances, the Committee of Adjustment shall be satisfied that the:

- a) the proposed variance meets the general intent and purpose of Official Plan;

- b) the proposed variance meets the general intent and purpose of the implementing Zoning By-law;
- c) the proposed variance will result in development or the use of land, buildings or structures that is appropriate; and,
- d) the proposed variance is minor in nature.

In determining whether a minor variance is appropriate, the Committee shall also consider whether:

- a) alternative building designs that would conform to the implementing Zoning By-law are not feasible or appropriate; and,
- b) an undesirable precedent will be established by the approval of the application.

**NON-COMPLYING BUILDINGS, STRUCTURES OR LOTS**

A non-complying building, structure or lot is such that it does not comply with the regulations of the implementing Zoning By-law. A non-complying building or structure may be enlarged, repaired or renovated provided that the enlargement, repair or renovation:

- a) does not further increase a situation of non-compliance;
- b) complies with all other applicable provisions of this Plan and the implementing Zoning By-law;
- c) does not increase the amount of floor area in a required yard or setback area;
- d) will not pose a threat to public health or safety; and,
- e) complies with the applicable policies and provisions of the relevant Conservation Authority, if located within an area regulated by the Conservation Authority.

A non-complying lot in existence prior to the effective date of the implementing Zoning By-law that does not meet the lot area and/or lot frontage requirements contained within the implementing Zoning By-law, may be used and buildings thereon may be erected, enlarged, repaired or renovated provided the use conforms with the applicable policies of this Plan and the implementing Zoning By-law, and the buildings or structures comply with all of the other provisions of the implementing Zoning By-law.



## G7 COMMUNITY IMPROVEMENT PLANS

### G7.1 PURPOSE OF COMMUNITY IMPROVEMENT PLANS

Community Improvement Plans identify the specific projects that need to be carried out in a particular area to improve the quality of life and the built environment in an area. Community Improvement Project Areas may be established by Council and designated by by-law, in accordance with the provisions of the Planning Act if:

- a) a number of the non-residential land uses conflict with residential uses in a predominantly residential area; or,
- b) a number of incompatible land uses conflict with commercial or industrial uses in a commercial or industrial area; or,
- c) the area contains a number of buildings in need of maintenance, repair or rehabilitation; or,
- d) there are deficiencies in the sanitary sewer, water or stormwater systems in the area; or,
- e) there are deficiencies in the road network and associated infrastructure in the area; or,
- f) there is a lack of appropriate parkland and other recreational facilities within the area; or,
- g) the potential exists to achieve economic growth in an area as a result of building improvement, repair and/or replacement; or,
- h) a number of environmental problems such as soil contamination exist in the area; or,
- i) there are a number of screening, buffering or landscaping deficiencies in the area; or,
- j) there are cultural heritage resources in an area warranting protection and/or enhancement; or,
- k) the area contains a number of vacant lots, vacant retail space or underutilized properties.

Based on the criteria outlined above, Council can designate a portion or the entire municipality as a Community Improvement Project Area.

### G7.2 COMMUNITY IMPROVEMENT PLAN AREAS

On the basis of Section G7.1, and in accordance with the Town of Halton Hills Community Improvement Plan, this Plan establishes the entirety of the Town of Halton Hills as a Community Improvement Project Area, with the following eight Community Improvement Project Sub-Areas:

- Georgetown Community Node Sub-Area as shown on Schedule A5 to this Plan;
- Georgetown Downtown Sub-Area which includes lands within the Downtown Georgetown boundary on Schedule A3 of this Plan;
- Acton Downtown Sub-Area as shown on Schedule A7 to this Plan;
- GO Station Lands Sub-Area which includes lands within the GO Station Study Area Boundary on Schedule A3 to this Plan;

- South Acton Sub-Area which includes the lands designated as South Acton Special Study Area on Schedule A6 to this Plan;
- Agricultural Sub-Area which includes lands designated as *Agricultural Area*, *Niagara Escarpment Plan Area*, and *Protected Countryside Area* on Schedule A1 to this Plan;
- Acton Industrial Park Sub-Area which includes lands designated as *General Employment Area* on Schedule A6 to this Plan; and,
- Georgetown Industrial Park Sub-Area which includes lands designated as *General Employment Area* on Schedule A3 to this Plan.

Council may establish other Community Improvement Project Sub-Areas, as appropriate, in accordance with Section G7.1 of this Plan.

### G7.3

#### COMMUNITY IMPROVEMENT PLAN IMPLEMENTATION

Council will utilize Community Improvement Plan programs at the appropriate time and circumstances to implement the policies of this Plan. In implementing the Town of Halton Hills Community Improvement Plan, Council will seek participation of the Region of Halton.

## G8 SITE PLAN CONTROL

All areas of the Town are designated as proposed Site Plan Control areas under the provisions of the Planning Act. Specific uses subject to site plan control shall be identified in the Town's Site Plan Control By-law passed under the provisions of the Planning Act. Reference shall be made to the Town's Site Plan Manual and Development Manual for guidance on the site plan standards of the Town. Prior to the consideration of an application for Site Plan approval, the Commissioner of Planning & Development or designate shall be satisfied that the proposal conforms to Section 41(4) of the Planning Act, as amended" The intent of a Site Plan Agreement is to ensure that any proposed development is designed to be compatible with adjacent development, appropriately serviced and accessed and otherwise in conformity with the intent of this Plan. Any required site plan agreement shall deal with the following, as appropriate:

- a) road widenings;
- b) location of vehicular access points;
- c) loading, parking and driveway locations;
- d) the surfacing of loading, parking and driveway areas;
- e) the location and design of walkways and walkway ramps,
- f) the location, massing and conceptual design of any buildings and structures;
- g) the location and type of lighting and landscaping;
- h) the location and type of garbage storage;
- i) the location and nature of easements;
- j) the grade and elevation of the land;
- k) the type and location of storm, surface and wastewater disposal facilities;
- l) the location and type of snow removal facilities; and,
- m) the location of any natural heritage features and hazardous lands.

Council shall prepare a Capital Works Plan that shall be in conformity with this Plan. The intent of the Capital Works Plan will be to determine what short term and long term capital works expenditures will be required to implement the goals and objectives of this Plan.

## **G10 MAINTENANCE AND OCCUPANCY BY-LAWS (PROPERTY STANDARDS)**

### **G10.1 MINIMUM STANDARDS**

Council shall update or enact a Property Standards By-law in accordance with the Ontario Building Code Act, regarding minimum standards for the following:

- a) the physical condition of buildings and structures;
- b) the physical condition of lands;
- c) the adequacy of sanitation; and,
- d) the fitness of buildings and structures for occupancy.

### **G10.2 PROPERTY STANDARDS BY-LAW**

The By-law may require that substandard properties be repaired and maintained to comply with the standards, prohibit the use of substandard property, and require the demolition and clearing of such property which the owner does not intend to repair and maintain. Upon passing a Property Standards By-law, Council shall appoint a Property Standards Officer who will be responsible for administering and enforcing the By-law.

### **G10.3 PROPERTY STANDARDS COMMITTEE**

Council shall also appoint a Property Standards Committee for the purpose of hearing appeals against an order issued by the Property Standards Officer.

Council shall require the registration with the Town of residential care facilities and dwelling units with an accessory apartment pursuant to by-laws passed under the Municipal Act.

In the event that a Residential Care Facility undergoes a change of use from one category of facility to another, re-registration of the facility shall be required.

## G12 PRE-CONSULTATION AND COMPLETE APPLICATION REQUIREMENTS

### G12.1 PRE-CONSULTATION

Prior to the submission of an Official Plan Amendment, Zoning By-law Amendment, Plan of Subdivision, Consent or Site Plan application, applicants, landowners or agents are required to pre-consult with the Town. Pre-consultation is encouraged for all other applications requiring Planning Act approval. The Region of Halton is encouraged to participate in the Town's pre-consultation process as appropriate.

The objective of pre-consultation is:

- a) for the Town, in consultation with the Region, other applicable agencies, and the proponent to determine the scale and scope of any required information or material necessary to ensure the submission of a complete application. The scale and scope are dependent on the nature of the proposal, its relationship to adjacent land uses, and the type of planning approval required; and
- b) for the Town, in consultation with the Region, and other applicable agencies to provide a proponent with preliminary comments on a pre-consultation proposal in order to inform the nature of any future Planning Act application submission so that that Council, or staff as delegated, is best positioned to make a decision on an application within the statutory timeframes set out under the Planning Act.

For Zoning By-law Amendment applications, including those filed in conjunction with an Official Plan Amendment and/or Plan of Subdivision application, the applicant is strongly encouraged to hold a public consultation meeting to obtain preliminary feedback from the community regarding the proposal in advance of filing the formal application.

### G12.2 COMPLETE APPLICATIONS

Official Plan Amendment, Zoning By-law Amendment, Plan of Subdivision, Consent and Site Plan applications shall be supported by a complete application as set out in Section G12 of this Plan. The purpose of requiring a complete application is to ensure that Council, or staff as delegated, has the necessary information to make informed decisions on the aforementioned applications.

Prior to being processed, applications must be deemed to be complete in accordance with Section G12 of this Plan. The date on which an application is deemed to be complete is the date on which the processing time frames set out in the Planning Act begin. Incomplete applications will not be accepted or processed.

To be considered complete under the Planning Act, Official Plan Amendment, Zoning By-law Amendment, Plan of Subdivision, Consent and Site Plan applications must be accompanied by the following:

- a) a completed application form(s);
- b) any information or material prescribed by the Planning Act and relevant Ontario Regulations;
- c) prescribed application fee(s);

- d) a completed pre-consultation form setting out the applicable information requirements. For Zoning By-law Amendment applications, including those filed in conjunction with an Official Plan Amendment and/or Plan of Subdivision application, the completed pre-consultation form should also include documentation demonstrating that a public consultation meeting was held by the applicant prior to submission of the Zoning By-law Amendment application, if such meeting was held; and,
- e) the applicable information requirements set out in Section G12.3 of this Plan.

### G12.3 SUPPLEMENTARY INFORMATION REQUIREMENTS

In addition to the requirements specified in Section G12.2 of this Plan, this section sets out the supplementary information or material in the form of studies that is required in support of an application for Official Plan Amendment, Zoning By-law Amendment, Plan of Subdivision, Consent or Site Plan.

The supplementary information requirements shall include the following, unless otherwise determined not to be required by the Commissioner of Planning & Development or designate:

- a) Land Use Planning Report. The intent of such a report would be to describe the proposal in detail and provide an opinion on how the proposal will conform to the Community Vision, Goals and Strategic Objectives, and the relevant objectives and policies of this Plan. The report shall also provide an opinion on how the proposal conforms to the applicable Provincial Plans and Regional Official Plan and is consistent with the Provincial Policy Statement.
- b) Market Impact Study. The intent of such a report is set out in Section D2.3 of this Plan.
- c) Agricultural Impact Assessment (AIA). The intent of such an assessment is set out in Sections E1.4.9 and E2.4.4 of this Plan.
- d) Environmental Impact Study (EIS). The purpose, intent, and content of such a study is set out in Section C2 of this Plan.
- e) Hydrogeological Assessment. Such an assessment will be required to support development on private or partial services. Policies regarding hydrogeological issues are contained within Section C5 and C6 of this Plan.
- f) Stormwater Management Report. The study requirements for such a report are contained within Section C8 of this Plan.
- g) Servicing Study. The purpose of such a study is to identify specific sanitary and water supply needs for individual development proposals.
- h) Traffic Impact Assessment. Such an assessment may be required to determine what impact a development proposal will have on roads adjacent to a proposed development and roads in the general area.
- i) Archaeological Assessment. The requirements for carrying out such an assessment are contained within Section F5 of this Plan.



- j) Cultural Heritage Impact Statement. The requirements for preparing such a statement are contained within Section F5 of this Plan.
- k) Tree Preservation Plan/Study. The intent of such a Plan/Study is to inventory trees on a development site and make recommendations on how trees can be retained and/or replanted as a condition of development.
- l) Environmental Site Assessment (Phase I and II). The intent of a Phase I study is to determine whether or not a property is contaminated. In the event that a Phase I investigation shows evidence of contamination, a Phase II investigation that includes matters such as surface and subsurface soil sampling and groundwater and surface water sampling is required.
- m) Land Use Compatibility Assessment. The intent of such an assessment is to describe and review the potential impacts of proposed sensitive land uses on existing industrial uses or the impacts of existing industrial uses on proposed sensitive land uses, in terms of noise, dust, odour and similar items in accordance with Ministry of Environment Guidelines. Such an assessment may include a Noise Impact and Vibration Study. The requirements for carrying out such an assessment are contained within Section C14 of this Plan.
- n) Noise Impact and Vibration Study. The requirements for preparing such a study are contained within Section C15 of this Plan.
- o) Green Development Standards Checklist. The intent is to demonstrate that the development meets the goals of the Official Plan to provide for sustainable building and development and to be consistent with Section C18 of this Plan.
- p) Subwatershed Impact Study/Environmental Implementation Report. The purpose, intent, and content of such a study is set out in Sections C3, C7 and also partly in Section C2, of this Plan.
- q) Construction Management Plan. The intent of such a plan is to identify and document how on-site works associated with different phases of a development project, including demolition, site alteration and completion of construction, will occur in an efficient manner that minimizes and/or mitigates disruptions or impacts to municipal infrastructure, rights-of-way and neighbouring properties.
- r) Zoning By-law Amendment or Minor Variance approval. All zoning amendment or minor variance approvals identified as being required through a pre-consultation process to facilitate a proposed development shall be approved by Council or the Committee of Adjustment for the Town of Halton Hills prior to submission and acceptance of a Site Plan application.

The list of information or material specified in this section is not intended to be exhaustive. Other information or material may be required by the Town, in consultation with the Region, and other applicable agencies in response to a particular development proposal. All studies shall be:

- a) prepared in accordance with approved terms of reference, where applicable or required by the Commissioner of Planning & Development or designate, to help guide the quality of a study in order to influence the best outcome for an application; and,
- b) carried out by qualified professional consultants retained by and at the expense of the proponent; the qualified professional consultant must apply their signature or seal to any such study. The Town shall require peer reviews of the studies by an appropriate public agency or by

a professional consultant retained by the Town at the proponent's expense; or carried out by a qualified professional consultant retained by the Town at the expense of the proponent.

All study recommendations shall be implemented by the proponent to the satisfaction of the Town, the Region, and other applicable agencies.

#### G12.4

#### FLEXIBILITY

Given that applications submitted pursuant to the Planning Act can vary considerably in terms of scale, relationships to adjacent land uses, and the type of planning approval required, it is anticipated that the information required to properly evaluate development proposals will vary accordingly. The scale and scope of required information or material necessary to ensure the submission of a complete application shall be determined as part of the preconsultation process by the Town, in consultation with the Region, other applicable agencies, and the proponent.

## G13 INTERPRETATION

### G13.1 GENERAL

This Plan is a statement of policy. It sets out Council's land use policy direction, however, some flexibility in interpretation may be permitted provided that the intent is maintained. The Official Plan is more than a set of individual goals, objectives and policies. The Plan is intended to be read in its entirety and the relevant policies are to be applied to each situation. While specific policies sometimes refer to other policies, these cross-references do not take away from the need to read the Official Plan as a whole.

### G13.2 INTERPRETATION OF LAND USE DESIGNATION BOUNDARIES

The external boundaries of the Urban Areas, *Hamlet Areas* and *Rural Cluster Areas* designations as shown on the Schedules to this Plan are fixed and inflexible. Expansions to Urban Areas and *Hamlet Areas* shall only occur in accordance with the policies of this Plan.

The boundaries between land uses designated on the Schedules to this Plan are approximate except where they meet with roads, railway lines, rivers, pipeline routes, transmission lines, lot lines or other clearly defined physical features and in these cases, are not open to flexible interpretation. Where the general intent of the document is maintained, minor adjustments to boundaries will not require an amendment to this Plan.

It is recognized that the boundaries of the *Greenlands A* and *B* designations may be imprecise and subject to change. The Town shall determine the extent of the environmental areas on a site-by-site basis when considering development proposals, in consultation with the appropriate agencies. Any minor refinement to the *Greenlands A* and *B* designations shall not require an Amendment to this Plan.

Where a lot is within more than one designation on the Schedules to this Plan, each portion of the lot shall be used in accordance with the applicable policies of that designation.

### G13.3 STATED MEASUREMENTS AND QUANTITIES

The interpretation of numerical figures, including densities, can be varied by 5 percent of the figures in the text and such minor variation will not require an amendment to this Plan provided that the intent of the Plan is maintained.

### G13.4 ROAD LOCATIONS

The location of the roads as indicated on Schedule B1 and B2 shall be considered as approximate. Amendments to this Plan will not be required in order to make minor adjustments or deviations to the locations of roads provided that the general intent of the Plan is maintained.

### G13.5 LEGISLATION

Where this Plan makes reference to a Provincial Act or Ontario Regulation, to the minimum distance separation formulae or the Provincial Policy Statement, such reference shall include any subsequent amendments or replacements.

#### G13.6 ACCESSORY USES

Whenever a use is permitted in a land use designation, it is intended that uses, buildings or structures normally incidental, and accessory to that use are also permitted.

#### G13.7 GLOSSARY

The following terms are defined to assist in understanding the intent of this Plan.

##### ACCESSORY APARTMENT

Means a self-contained apartment within a single detached, semi-detached, or townhouse dwelling unit.

##### ACCESSORY BUILDING OR STRUCTURE

Means a detached building or structure, the use of which is naturally and normally incidental to, subordinate to, or exclusively devoted to the principal use or building, and located on the same lot and that is not used for human habitation.

##### ACCESSORY USE

Means a use, customarily and normally subordinate to, incidental to and exclusively devoted to the principal use and located on the same lot.

##### ACTIVE TRANSPORTATION

Means non-motorized or lightly-motorized travel, including walking, cycling, roller-blading and movements with mobility devices. The active transportation network includes sidewalks, crosswalks, designated road lanes and off-road trails to accommodate active transportation.

##### ADJACENT LANDS

Means:

- a) those lands, contiguous to a specific natural heritage feature or area where it is likely that development or site alteration would have a negative impact on the feature or area; or,
- b) those lands contiguous to a protected heritage property.

##### ADULT ENTERTAINMENT USE

Means a use that includes an adult entertainment parlour, adult video store, adult specialty store, and/or body rub parlour, whether such use is a primary or accessory use.

##### ADULT ENTERTAINMENT PARLOUR

Means a building or structure or part thereof in which is provided, in pursuance of a trade, calling, business or occupation, services or entertainment appealing to or designed to appeal to erotic or sexual appetites or inclinations.

## ADULT SPECIALTY STORE

Means a building or structure or part thereof specializing in the sale of materials and products, such as clothing and accessories, appealing to, or designed to appeal to, erotic or sexual appetites or inclinations. An adult specialty store may, as an accessory use, sell or rent pre-recorded video tapes, video discs, films and or slides appealing to, or designed to appeal to, erotic or sexual appetites or inclinations.

## ADULT VIDEO STORE

Means a building or structure or part thereof where pre-recorded video tapes, video discs, films and/or slides appealing to, or designed to appeal to, erotic or sexual appetites or inclinations, or depicting sexual acts, are offered for rent or sale. An adult video store shall not include facilities for the screening or viewing of such products.

## ADVERSE EFFECTS

Means, pursuant to the Environmental Protection Act one or more of:

- a) impairment of the quality of the natural environment for any use that can be made of it;
- b) injury or damage to property or plant or animal life;
- c) harm or material discomfort to any person;
- d) an adverse effect on the health of any person;
- e) impairment of the safety of any person;
- f) rendering any property or plant or animal life unfit for human use;
- g) loss of enjoyment or normal use of property; and,
- h) interference with normal conduct of business.

Appealing to, or designed to appeal to, erotic or sexual appetites or inclinations, when used to describe services or entertainment, includes,

- a) services or entertainment of which a principal feature or characteristic is the nudity or partial nudity of any person; and,
- b) services or entertainment in respect of which the word "nude", "naked", "topless", "bottomless", "sexy", or any other word, picture, symbol or representation having like meaning or implication is used in any advertisement.

## AFFORDABLE HOUSING

Means housing with market price or rent that is affordable to households of low and moderate income spending 30 percent of their gross household income without government subsidies. Such households would be able to afford, at the low end, at least three out of ten rental properties on the market, and at the high end, ownership housing with sufficient income remaining, after housing expenses, to sustain a basic standard of living.

#### AGRICULTURAL USE OR AGRICULTURAL OPERATION

Means an activity consisting of animal husbandry, horticulture, beekeeping, dairying, field crops, fruit farming, fur farming, market gardening, pasturage, poultry keeping, mushroom farming, aquaculture, agro-forestry or any other farming activity and may include growing, raising, small scale packing and storage of produce on the building, structure or part thereof and other similar activities customarily carried out in the field of general agriculture.

#### AMENITY AREA

Means an interior area within a residential building or an outdoor area exterior to the residential building that is designed and intended primarily for the leisure and recreation of the occupants of the dwelling.

#### ANIMAL KENNEL

Means a building or structure or part thereof used for the raising or boarding of dogs, cats, or other household pets.

#### ARCHAEOLOGICAL RESOURCE

Includes artifacts, archaeological sites and marine archaeological sites. The identification and evaluation of such resources are based upon archaeological fieldwork in accordance with the Ontario Heritage Act.

#### AREAS OF ARCHAEOLOGICAL POTENTIAL

Means areas with the likelihood to contain archaeological resources. Archaeological potential is confirmed through archaeological fieldwork in accordance with the Ontario Heritage Act.

#### AREA OF NATURAL AND SCIENTIFIC INTEREST

Means an area of land and water containing natural landscapes or features that has been identified as having earth or life science values related to protection, scientific study or education. See Provincially Significant Area of Natural and Scientific Interest and Regionally Significant Area of Natural and Scientific Interest.

#### ASPHALT PLANT

Means a facility which produces and/or recycles asphalt or similar coated road stone and has equipment designed to heat and dry aggregate and to mix mineral aggregate with bitumen and/or tar, and includes the stockpiling and storage of bulk materials used in the process or finished product(s) manufactured on the premises and the storage and maintenance of equipment.

#### ASSISTED HOUSING

Means housing that is available to low and moderate income households for rent or purchase where part of the housing cost is subsidized through a government program.

## AUTOMOTIVE COMMERCIAL

Means retail uses involving the sale of automobiles and service commercial uses involving the repair and maintenance of automobiles and the sale of gasoline or similar products and includes motor vehicle dealerships, repair garages, car washes and motor vehicle service stations.

## BED AND BREAKFAST ESTABLISHMENT

Means a single detached dwelling in which a maximum of three rooms are provided with or without meals for hire or pay for the traveling public.

## BODY RUB

Means the kneading, manipulating, rubbing, massaging, touching or stimulating, by any means, of a person's body or part thereof but does not include:

- a) medical or therapeutic treatment given by a person duly qualified, licensed or registered to do so under the laws of the Province of Ontario; and,
- b) alternative health care services, such as shiatsu and reflexology, given by a person duly qualified to provide such services through completion of formal courses and education and training in the provision of such service offered by an organization or institution that has the qualifications to provide such services.

## BODY RUB PARLOUR

Means a building or structure or part thereof where a body rub is performed, offered or solicited in pursuance of a trade, calling, business or occupation, but shall not include:

- a) any building or structure or part thereof where body rubs are performed for the purpose of medical or therapeutic treatment and are performed by persons duly qualified, licensed or registered to do so under the laws of the Province of Ontario; and,
- b) any building or structure or part thereof where body rubs are performed for the purpose of alternative health care services, such as shiatsu and reflexology, given by a person duly qualified to provide such services through completion of formal courses and education and training in the provision of such service offered by an organization or institution that has the qualifications to provide such services.

## BROWNFIELD SITES

Means undeveloped or previously developed properties that may be contaminated. These are usually, but not exclusively, former industrial or commercial properties that may be underutilized, derelict or vacant.

## BUILT BOUNDARY

Means the limits of the developed urban area as identified in the Provincial Paper, Built Boundary for the Growth Plan for the Greater Golden Horseshoe, 2006 (2008), as shown on Schedules A3 and A6 to this Plan.

## BUILT HERITAGE

Means an individual or group of significant buildings, structures, monuments, installations, or remains, which are associated with architectural, cultural, social, political, economic, or military history and identified as being important to a community. These resources may be designated or subject to a conservation easement under the Ontario Heritage Act, or listed by the federal or provincial governments or the Town.

## BUILT-UP AREA

Means all lands within the Built Boundary.

## BUSINESS OFFICE

A service commercial use where the affairs of businesses, professions, services, industries, governments, non-profit organizations or the like activities are conducted, and in which the chief product of labour is the processing and storage of information rather than the production and distribution of goods.

## CHARACTER

- a) Means the aggregate of the distinct features that work together to identify a particular area. The distinct features may include the built and natural elements of an area.
- b) Means, with respect to residential neighbourhoods, including Mature Neighbourhood Areas, the collective physical qualities and characteristics that are prevalent in a neighbourhood, and which define its distinct identity. These qualities and characteristics may include such features as street network and design, lot pattern and configuration, lot area, lot frontage, location and orientation of buildings on a lot in relation to surrounding lots, building setbacks, building height, massing, scale and design, façade articulation and materials, trees, vegetation, cultural heritage resources, and age of buildings.

## COMMERCIAL FARM

Means a farm, which is deemed to be a viable farm operation and which normally produces sufficient income to support a farm family.

## COMPATIBLE

Means the development or redevelopment of uses as well as new housing, replacement housing, additions, or alterations, which may not necessarily be the same as or similar to the existing development, but can coexist with the surrounding area without negative impact.

Means in context of the *Niagara Escarpment Plan Area* where the building, structure, activity or use blends, conforms or is harmonious with the Escarpment's ecological, physical, visual or cultural environment.

## COMPLETE COMMUNITIES

Means communities that meet people's needs for daily living throughout an entire lifetime by providing convenient access to an appropriate mix of jobs, local services, a full range of housing, and community



infrastructure including affordable housing, schools, recreation and open space for their residents. Convenient access to public transportation and options for safe, non-motorized travel is also provided.

#### CONNECTIVITY

Means the degree to which natural heritage features, including Key Natural Heritage Features or Key Hydrologic Features in lands subject to the Greenbelt Plan, are connected to one another by links such as plant and animal movement corridors, hydrological and nutrient cycling, genetic transfer, and energy flows through food webs.

#### CONSERVATION AUTHORITY

Means Credit Valley Conservation, the Grand River Conservation Authority, or the Halton Region Conservation Authority (Conservation Halton).

#### CONSERVATION USE

Means an area of land that is generally left in its natural state and which is used to preserve, protect and/or improve components of the Natural Heritage System of other lands for the benefit of man and the natural environment and which may include, as an accessory use, hiking trails and/or cross country ski trails, buildings and structures such as nature interpretation centres and public information centres.

#### CONSERVED

Means the identification, protection, use and/or management of cultural heritage resources in a manner that retains their heritage values, attributes and integrity.

#### CONTAMINATED SITE

Means property or lands that have not been rehabilitated and for reasons of public safety or environmental quality, are unsafe for use as a result of human activities, particularly those activities that have left a chemical or radioactive residue.

#### CONTRACTORS ESTABLISHMENT

Means the use of land , building or structure, or parts thereof, by any general contractor or builder where equipment and materials are stored, or where a contractor and/or tradesman performs shop and assembly work, and/or offers a trade or service, including, but not limited to landscaping services, general construction services, cabinetry services, plumbing services and welding services, or other similar services but does not include any other use as defined by this by-law.

#### COTTAGE INDUSTRY

Means an activity conducted as an accessory use within a single detached dwelling by one or more of its residents. A cottage industry may include activities such as dressmaking, upholstering, weaving, baking, ceramic making, painting, sculpting and the repair of personal effects.

#### COUNCIL

Means the Municipal Council of the Corporation of the Town of Halton Hills.

## CULTURAL HERITAGE LANDSCAPE

Means a defined geographical area of heritage significance that has been modified by human activities and is valued by a community. It involves a grouping(s) of individual heritage features such as structures, spaces, archaeological sites and natural elements, which together form a significant type of heritage form, distinctive from that of its constituent elements or parts.

## CULTURAL HERITAGE RESOURCES

Means those things left by a people of a given geographic area, and includes:

- a) built heritage, such as buildings, structures, monuments or remains of historical, cultural or architectural value, and including protected heritage property;
- b) cultural heritage landscapes, such as rural, hamlet or urban uses of historical or scenic interest; and,
- c) archaeological resources.

## DAY NURSERY

Means a building or structure or part thereof where more than 5 children are provided with temporary care and/or guidance for a continuous period, not exceeding twenty-four hours and are licensed in accordance with the Day Nurseries Act.

## DEPARTMENT STORE

Means a retail store that sells the following general lines of merchandise:

- a) family clothing and apparel – at least six commodity lines in this group, comprising at least 20 percent of the outlet's total sales;
- b) furniture appliances and home furnishings – at least four commodity lines in this group, comprising at least 10 percent of the outlet's total sales; and,
- c) all other – at least three commodity lines in this group, comprising at least 10 percent of the outlet's total sales.

## DEPARTMENT STORE TYPE MERCHANDISE (DSTM)

Means merchandise such as apparel and accessories, household furnishings, drugs and cosmetics, and durables and semi-durables that are displayed, rented or sold directly to the general public in a department store, or a retail store or part thereof. This may also include such merchandise displayed and sold to members of a warehouse membership club.

## DESIGNATED AND AVAILABLE

Means lands designated in this Plan for urban and hamlet residential use.

## DEVELOPMENT

Means the creation of a new lot, a change in land use, or the construction of buildings and structures, requiring approval under the Planning Act, but does not include:

- a) activities that create or maintain infrastructure authorized under an environmental assessment process; and,
- b) works subject to the Drainage Act.

## DEVELOPMENT DENSITY

Means the number of residents and jobs combined per gross hectare. Areas of the Greenlands System are excluded from the calculation of this density.

## DRIVE-THROUGH SERVICE FACILITY

Means a building or structure or part thereof where goods or services are offered to the public within a parked or stationary vehicle by way of a service window.

## ECOLOGICAL FEATURES

Means naturally occurring land, water and biotic features that contribute to ecological and hydrological integrity.

## ECOLOGICAL FUNCTIONS

Means the natural processes, products or services that living and non-living environments provide or perform within or between species, ecosystems and landscapes. These may include biological, physical, and socio-economic interactions.

## ECOLOGICAL VALUE

Means, in the context of lands subject to the Greenbelt Plan, the value of vegetation in maintaining the health of the Key Natural Heritage or Key Hydrologic Feature and the related ecological features and ecological functions, as measured by factors such as the diversity of species, the diversity of habitats, and the suitability and amount of habitats that are available for rare, threatened and endangered species.

## ECOLOGICAL INTEGRITY

Means the condition of ecosystems in which:

- a) the structure, composition, and function of the ecosystems are protected, enhanced or restored;
- b) natural ecological processes are protected, enhanced or restored; and,
- c) the ecosystems generally evolve naturally.

## ECOSYSTEM

Means systems of plants, animals, and micro-organisms, together with the non-living components of their environment and related ecological processes, critical for the functioning of the biosphere in all its diversity.

## EMERGENCY HOUSING

Means emergency shelters or facilities that accommodate not less than three and not more than ten residents, and provide temporary lodging, board, and/or personal support services to homeless individuals in a 24-hour supervised setting, for up to 30 days.

## EMPLOYMENT AREA

Means areas designated in an official plan for clusters of business and economic activities including, but not limited to, manufacturing, warehousing, offices and associated retail and ancillary facilities.

## ENDANGERED SPECIES

Means a species that is listed or categorized an endangered species on the Ministry of Natural Resources' official species at risk list, as updated and amended from time to time.

## ENHANCE

Means, as applied to the natural heritage/environmental policies of this Plan, strengthening the components of a natural area through management measures to increase stability, biodiversity and long-term viability.

Means, in other respects, to complement and strengthen the character of the Town, community, neighbourhood, site or structure.

## ENVIRONMENTALLY SENSITIVE AREAS (ESAS)

Means land or water areas or a combination of both containing natural features or ecological functions of such significance as to warrant their protection.

## ENVIRONMENTAL IMPLEMENTATION REPORT (EIR)

Means a report prepared at the scale of a subcatchment or tributary, the purpose of which is to ensure that the goals and objectives of an approved Subwatershed Study are met, when land use change is proposed.

## EQUIVALENT POPULATION

Means a figure expressing the capacities of water and wastewater services, in terms of residential population, with a commensurate proportion of non-residential development.

## EROSION HAZARD

Means the loss of land, due to human or natural processes, that poses a threat to life and property. The erosion hazard limit is determined using considerations that include the 100-year erosion rate (the average annual rate of recession extended over an one hundred year time span), an allowance for slope stability, and an erosion/erosion access allowance.

## ESCARPMENT BROW

Means the uppermost point of the escarpment slope or face. It may be the top of a rock cliff, or where the bedrock is buried, the most obvious break in the slope associated with the underlying bedrock.

## ESSENTIAL

Means that which is deemed necessary to the public interest after all alternatives have been considered.

## FARM RELATED TOURISM ESTABLISHMENT

Means a commercial farm that provides, as an accessory use, educational and active opportunities to experience the agricultural way of life in the Town. Such activities may include farm machinery and equipment exhibitions, farm-tours, petting zoos, hay rides, sleigh rides, processing demonstrations, pick your own produce, farm theme playground for children and small scale educational establishments that focus on farming instruction. Overnight camping, amusement parks and recreational uses are not permitted.

## FISHERIES MANAGEMENT

Means the management of fish habitat and fish population for the purpose of sustaining and improving the quality and quantity of fish.

## FISH HABITAT

Means the spawning grounds and nursery, rearing, food supply, and migration areas on which fish depend directly or indirectly to carry out their life processes.

## FLOODPLAIN

Means the area, usually lowlands, adjoining the channel of a river, stream, or watercourse, which has been or may be covered by floodwater during a regional flood or a one-in-one hundred year flood, whichever is greater. See also Regulatory Floodplain.

## FOOD AND CONVENIENCE TYPE MERCHANDISE (FCTM)

Means merchandise such as groceries, baked goods, fresh fruit and vegetables, canned goods, dairy products, frozen foods, and fresh and frozen meats that are displayed and sold to the general public in a supermarket, or specialty food store. This may also include such merchandise displayed and sold to members of a warehouse membership club.

## FOREST MANAGEMENT OR FORESTRY

Means the management of woodlands, including accessory uses such as the construction and maintenance of forest access roads and maple syrup production facilities:

- a) for the production of wood and wood products, including maple syrup;
- b) to provide outdoor recreation opportunities;
- c) to maintain, and where possible improve or restore, conditions for wildlife; and,
- d) to protect water supplies.

## GARDEN SUITE

Means a self-contained detached residential structure that is accessory to a single detached dwelling unit and that is designed to be portable.

## GATEWAY

Means an entrance into the Town, Community or other specifically defined area, such as the *Downtown Area*, that includes special signage, landscaping and banners and is designed to project a positive image for the Town.

## GREYFIELD SITES

Means developed properties that are not contaminated. They are usually, but not exclusively, commercial properties that may be under-utilized, derelict or vacant.

## GROSS LEASABLE FLOOR AREA

Means the total floor area occupied by a commercial facility for its exclusive use, including basements, mezzanines, upper storey areas and integral storage areas but, in a shopping centre, not including public or common areas such as malls, corridors, stairways, elevators or machine or equipment rooms.

## GROSS RESIDENTIAL HECTARE

Means the area of land measured in hectares for residential dwelling units and consists of only the residential lots and blocks, local and collector roads, local institutional uses such as elementary schools, local commercial uses, local parkland, stormwater management ponds and walkways. This area excludes Arterial Roads and required widenings thereto and lands that are designated *Greenlands* by this Plan.

## GROUNDWATER DISCHARGE

Means water that travels from under the ground to the surface and which forms part of the base flow of rivers and streams.

## GROUNDWATER RECHARGE

Means the replenishment of subsurface water:

- a) resulting from natural processes, such as the infiltration of rainfall and snowmelt and the seepage of surface water from lakes, streams and wetlands; and,
- b) resulting from human intervention, such as the use of stormwater management systems.

#### GROUNDWATER RESOURCES

Means water from underground that is required to sustain ecological features and provide drinking water for humans and animals.

#### GROUP HOME TYPE 1

Means a single detached dwelling unit occupied by not less than six and not more than ten persons exclusive of staff and/or receiving family, who live under supervision as a single housekeeping unit and require 24-hour residential, sheltered, specialized or group care and which is licensed, approved and/or supervised by the Province of Ontario under any general or special Act.

#### GROUP HOME TYPE 2 (CORRECTIONAL)

Means a single detached dwelling unit occupied by not less than six and not more than ten persons exclusive of staff, who live under supervision as a single housekeeping unit and require 24-hour residential, sheltered, specialized or group care and which is licensed, approved, supervised or contracted by the Province of Ontario or the Federal Government under any general or special Act, and which shall be maintained and operated primarily for:

- a) persons who have been placed on probation under the Youth Criminal Justice Act, the Probation Act, the Criminal Code (Canada) as amended or any Act passed to replace the foregoing Acts;
- b) persons who have been released on parole under the Ministry of Correctional Services Act or the Parole Act (Canada) as amended or any Act passed to replace the foregoing Acts; and,
- c) persons who have been charged under the Youth Criminal Justice Act.

#### HAZARDOUS LANDS

Means property or lands that could be unsafe for development due to naturally occurring processes.

#### HERITAGE ATTRIBUTES

Means the principal features, characteristics, context and appearance that contribute to the cultural heritage significance of a protected heritage property.

#### HERITAGE CONSERVATION DISTRICT

Means an area defined by the Town to be of unique character to be conserved through a designation By-law pursuant to Part V of the Ontario Heritage Act.

#### HOME INDUSTRY

Means a small-scale industrial use, such as a carpentry shop, a metal working shop, a welding shop or an electrical shop that provides services or wares to the rural farming community and which is an accessory

use to an agricultural use or a single detached dwelling. For the purpose of this By-law, the repairing of motor vehicles or paint shop, mobile homes and/or trailers is not a home industry.

#### HOME OCCUPATION

Means an occupation that provides a service as an accessory use within a dwelling unit performed by one or more of its residents. Such activities may include services performed by an accountant, architect, auditor, dentist, medical practitioner, engineer, insurance agent, land surveyor, lawyer, realtor, planner, hairdresser or a provider of private home daycare, but shall not include an adult entertainment use.

#### HYDROLOGICAL FUNCTIONS

Means the functions of the hydrological cycle that include the occurrence, circulation, distribution, and chemical and physical properties of water on the surface of the land, in the soil and underlying rocks, and in the atmosphere, and water's interaction with the environment including its relation to living things.

#### HYDROLOGICAL INTEGRITY

Means the condition of ecosystems in which hydrological features and hydrological functions are protected, enhanced or restored.

#### HYDROLOGICALLY SENSITIVE FEATURE

Means a hydrological feature that is potentially sensitive to changes in surface and groundwater flow that may result from development.

#### INDUSTRIAL USE

Means a building or structure or part thereof used primarily for the purpose of manufacturing, processing, fabrication, assembly, treatment, packaging, and incidental storage of goods and materials and may include accessory sales and distribution of such products.

#### INFILL

Means development on vacant lots or through redevelopment to create additional new residential units.

#### INFRASTRUCTURE

Means physical structures that form the foundation for development. Infrastructure includes sewage and water works, waste management systems, electric power generation and transmission, communications/telecommunications, transit and transportation corridors and facilities, oil and gas pipelines and associated facilities.

#### INSTITUTIONAL USE

Means a use that caters to the social, educational and/or religious needs of humans.



## INTENSIFICATION

Means the development of a property, site or area at a higher density than currently exists through:

- a) redevelopment, including the reuse of brownfield sites;
- b) the development of vacant and/or under-utilized lots within previously developed areas;
- c) infill development; or,
- d) the expansion or conversion of existing buildings.

## INTENSIFICATION AREAS

Means lands identified in this Plan within the Urban Area that are to be the focus for accommodating intensification, including Major Transit Station Areas, and Intensification Corridors.

## INTENSIFICATION CORRIDOR

Means Intensification Areas identified along major roads, arterials or higher order transit corridors that have the potential to provide a focus for higher density mixed-use development consistent with planned transit service levels.

## KEY HYDROLOGIC FEATURE

Means a Key Hydrologic Feature as identified in Section E2.5.3.2 of this Plan.

## KEY NATURAL HERITAGE FEATURE

Means a Key Natural Heritage Feature as identified in Section E2.5.3.1 of this Plan.

## LAKE

Means any inland body of standing water, usually fresh water, larger than a pool or pond or a body of water filling a depression in the earth's surface.

## LANDFILL SITE

Means a waste disposal site used for the purpose of landfilling.

## LANDFORM FEATURES

Means distinctive physical attributes of land such as slope, shape, elevation and relief.

## LIVE-WORK RELATIONSHIP

Means the place of residence relative to the place of employment.

## LONG TERM CARE FACILITY

Means a building or structure or part thereof used to provide health care under medical supervision for twenty-four or more consecutive hours, to two or more persons.

## MATURE NEIGHBOURHOOD AREAS

Means older established residential neighbourhoods within the Low Density Residential Area designation, characterized by predominantly single-detached housing stock on larger lots as identified in the implementing Zoning By-law by a Mature Neighbourhood (MN) Suffix on certain areas within the LDR1 zone.

## MAJOR TRANSIT STATION AREA

Means the area including and around any existing or planned higher order transit station within the Urban Area; or the area including and around a major bus depot in an urban core. Station areas generally are defined as the area within an approximate 500m radius of a transit station, representing about a 10-minute walk.

## MAJOR VALLEY/WATERCOURSE

Means a watercourse and its associated valley system that typically has valley walls 5 metres or greater in height.

## MEDICAL OFFICE

Means a building or structure or part thereof used for the medical, dental, surgical and/or therapeutic treatment of human beings including clinics operated by a number and/or variety of medical professionals, but does not include a public or private hospital or office located in the medical professional's residence.

## MINOR VALLEY/WATERCOURSE

Means a watercourse and its associated valley system or stream corridor that typically has valley walls less than 5 metres in height.

## MINERAL AGGREGATE RESOURCES

Means gravel, sand, clay, earth, shale, stone, limestone, dolostone, sandstone, marble, granite, rock or other material prescribed under the Aggregate Resources Act suitable for construction, industrial, manufacturing and maintenance purposes but does not include metallic ores, asbestos, graphite, granite, mica, nepheline syenite, salt, talc, wollastonite, mine tailings or other material prescribed under the Mining Act.

## MINERAL AGGREGATE OPERATION

Means:

- a) Lands under license or permit, other than for a wayside pit or quarry, issued in accordance with the Aggregate Resources Act, or successors thereto; and,
- b) Associated facilities used in extraction, transport, beneficiation, processing or recycling of mineral aggregate resources, or in the production of secondary related products.

### MINIMUM DISTANCE SEPARATION (MDS)

Means the tool to determine a recommended distance between livestock and manure storage facilities and non-agricultural uses to prevent land use conflicts and minimize nuisance complaints from odour. MDS One provides minimum distance separation for new development from existing livestock facilities. MDS Two provides minimum distance separation for new or expanding livestock facilities from existing or approved development.

### MOTOR VEHICLE BODY SHOP

Means a building or structure or part thereof used for the painting and/or repairing of the interior and/or exterior and/or the undercarriage of motor vehicle bodies. Car washes are not an accessory use.

### MOTOR VEHICLE REPAIR ESTABLISHMENT

Means a building or structure or part thereof used for the repairing of motor vehicles, but shall not include the sale of motor vehicle fuels. Car washes are not an accessory use.

### MUNICIPAL COMPREHENSIVE REVIEW

Means an official plan review, or an official plan amendment, initiated by a municipality that comprehensively applies the policies and schedules of the Provincial Growth Plan, except as it applies to Section D7 of this Plan, in which case such a review will be focused on the need for employment lands in order to achieve the employment targets of Table A1A of this Plan, as established by the Regional Official Plan.

### NATURAL HERITAGE FEATURES

Means features and areas, including significant wetlands, fish habitat, significant woodlands, significant valleylands, significant habitat of endangered and threatened species, significant wildlife habitat, and significant areas of natural and scientific interest that are important for their environmental and social values as a legacy of the natural landscapes of an area.

### NATURAL HERITAGE SYSTEM

- a) Means a system made up of natural heritage features and areas, linked by natural corridors which are necessary to maintain biological and geological diversity, natural functions, viable populations of indigenous species and ecosystems. These systems can include lands that have been restored and areas with the potential to be restored to a natural state.
- b) Within the Protected Countryside of the Greenbelt Plan, means areas with the highest concentration of the most sensitive and/or significant natural features and functions.

### NATURAL SELF-SUSTAINING VEGETATION

Means vegetation dominated by native species that can grow and persist without direct human management, protection, or tending.

## NEGATIVE IMPACT

- a) In respect to natural heritage features and areas, means degradation that threatens the health and integrity of the natural features or ecological functions for which an area is identified due to single, multiple or successive development or site alteration activities. In regard to fish habitat, the term means the harmful alteration, disruption or destruction of fish habitat, except where, in conjunction with the appropriate authorities, it has been authorized under the Fisheries Act, using the guiding principle of no net loss of productive capacity. In regard to water, degradation to the quality and quantity of water, sensitive surface water features and sensitive groundwater features, and their related hydrologic functions due to single, multiple or successive development or site alteration activities.
- b) In respect to cultural heritage resources, means but is not limited to:
  - i) destruction of any, or part of any, heritage attributes or features;
  - ii) alteration that is not sympathetic, or is incompatible, with the historic fabric and appearance;
  - iii) shadows created that alter the appearance of a heritage attribute or change the viability of a natural feature or plantings, such as a garden;
  - iv) isolation of a heritage attribute from its surrounding environment, context or a significant relationship;
  - v) direct or indirect obstruction of significant views or vistas within, from, or of built and natural features; and,
  - vi) land disturbances such as a change in grade that alters soils, and drainage patterns that adversely affect an archaeological resource.
- c) In respect of the commercial hierarchy in this Plan, means a detrimental effect on planned function that leads to or contributes to urban blight.
- d) In all other respects, means a deleterious effect or result on an adjacent use, the enjoyment of a neighbouring property or on the public realm that cannot be reasonably mitigated through the use of planning controls such as setbacks, buffering, fencing, and landscaping.

## NET RESIDENTIAL HECTARE

Means for detached, semi-detached, duplex, street townhouses and other dwelling types with individual frontages on a public street, the area of land measured in hectares for residential dwelling units and consists of only the residential lots and blocks and local roads on which the lots and blocks front.

Means for cluster or group housing and apartment blocks, the area of land measured in hectares for residential units, private internal roads and parking, landscaped areas, and other associated amenities.

## NORMAL FARM PRACTICES

Means a practice, as defined in the Farming and Food Production Protection Act, 1998, that is conducted in a manner consistent with proper and acceptable customs and standards as established and followed by similar agricultural operations under similar circumstances; or makes use of innovative technology in a manner consistent with proper advanced farm management practices. Normal farm practices shall be consistent with the Nutrient Management Act, 2002 and regulations made under that Act.

## ONE ZONE CONCEPT

Means the approach whereby the entire floodplain, as defined by the regulatory flood, is treated as one unit and all development is prohibited or restricted.

## OUTDOOR STORAGE

Means an area of land used in conjunction with a business located within a building or structure on the same lot, for the storage of goods and materials.

## PARTIAL SERVICES

Means municipal water services and individual on-site sewage services.

## PLACE OF ENTERTAINMENT

Means a motion picture or live theatre, arena, auditorium, planetarium, concert hall and other similar uses, but shall not include an adult entertainment use, any use entailing the outdoor operation or racing of animals or motorized vehicles, a casino or any other establishment accommodating or providing gambling or other gaming activities, wagering or betting, video lottery or gaming machines or any other similar type of gambling use.

## PLACE OF WORSHIP

Means a building or structure or part thereof used by a charitable religious group(s) for the practice of religious rites.

## PORTABLE ASPHALT PLANT

Means a facility:

- e) with equipment designed to heat and dry aggregate and to mix aggregate with bituminous asphalt to produce asphalt paving material, and includes stockpiling and storage of bulk materials used in the process; and,
- f) which is not of permanent construction, but which is to be dismantled at the completion of the construction project.

#### PORTABLE CONCRETE PLANT

Means a building or structure:

- g) with equipment designed to mix cementing materials, aggregate, water and admixtures to produce concrete, and includes stockpiling and storage of bulk materials used in the process; and,
- h) which is not of permanent construction, but which is designed to be dismantled at the completion of the construction project.

#### PRIME AGRICULTURAL AREA

Means an area where prime agricultural land predominates. This includes: areas of prime agricultural lands and associated Canada Land Inventory Class 4-7 soils; and additional areas where there is a local concentration of farms which exhibit characteristics of on-going agriculture.

#### PRIME AGRICULTURAL LAND

Means land that includes specialty crop lands and/or Canada Land Inventory Classes 1, 2 and 3 soils, in this order for priority protection.

#### PRIVATE HOME DAYCARE

Means the use of a dwelling unit for the temporary care and custody of not more than five children who do not live in the dwelling unit and who are under ten years of age for reward or compensation for a continuous period not exceeding twenty-four hours.

#### PROTECTED HERITAGE PROPERTY

Means designated real property and heritage conservation easement property under the Ontario Heritage Act and property that is subject to a covenant or agreement between the property owner and a conservation body or level of government, registered on title, with the primary purpose of conserving a cultural heritage resource or preventing its destruction, demolition or loss.

#### PROVINCIALY SIGNIFICANT AREA OF NATURAL AND SCIENTIFIC INTEREST

Means areas of land and water containing natural landscape features identified as provincially significant by the Ministry of Natural Resources, using evaluation procedures established by that Ministry, as amended from time to time.

#### PROVINCIALY SIGNIFICANT WETLAND

Means a wetland classified as provincially significant by the Ministry of Natural Resources based on the Ontario Wetland Evaluation System 1994 Southern Manual, as amended from time to time.

#### PUBLIC OR QUASI-PUBLIC USES

Means:

- a) uses carried out by Federal or Provincial ministries or companies subject to Federal and Provincial control;

- b) uses carried out by the Region of Halton and the Town of Halton Hills;
- c) public roads and railway lines;
- d) water supply, sewage and storm water management facilities; and,
- e) gas, telephone and cable transmission lines.

#### QUALITY AND QUANTITY OF WATER

Means as measured by indicators such as minimum base flow, oxygen levels, suspended solids, temperature, bacteria, nutrients, hazardous contaminants and hydrologic regimes.

#### REDEVELOPMENT

Means the creation of new units, uses or lots on previously developed land in existing communities, including brownfield and greyfield sites.

#### REGION OR REGIONAL

Means the Region of Halton.

#### REGIONALLY SIGNIFICANT AREA OF NATURAL AND SCIENTIFIC INTEREST

Means areas identified by the Ministry of Natural Resources, Conservation Authorities, municipalities and other agencies as having ecological or geological/ geomorphological value at the regional level.

#### REGIONALLY SIGNIFICANT WETLAND

Means a wetland classified as “Other Wetlands” by the Ontario Ministry of Natural Resources based on the Ontario Wetland Evaluation System 1994 Southern Manual, as amended from time to time.

#### REGULATORY FLOODPLAIN

Means the limit of the floodplain for regulatory purposes, defined by the application of the approved standards, a regional flood or a one-in-one hundred-year flood, used in a particular watershed.

#### RESIDENTIAL CARE FACILITIES

Residential facilities in which residents who have a range of emotional, psychiatric, physical, developmental, or social disadvantages or problems live in a 24-hour supervised setting, and receive both room and board and support with daily living, including:

- a) Group Home Type 1;
- b) Group Home Type 2;
- c) Second Level Lodging Homes; and,
- d) Treatment Centres.

## RESIDENTIAL INTENSIFICATION

Means the development of a property or site that results in a net increase in residential units. It includes redevelopment or development within existing communities where demolition of the previous structures is to take place or has taken place, including the development of brownfield and greyfield sites; infill development or development on vacant lots or redevelopment within a built up area; conversion, or the change of use of an existing industrial, commercial or institutional structure or land use to residential use; and the creation of accessory apartments or rooming, boarding and lodging accommodation in houses.

## RETAIL USE

Means a use conducted in a building or structure or part thereof in which goods, wares, merchandise, substances, articles or things are displayed, rented or sold directly to the general public but does not include a supermarket, a department store, a specialty food store, an automotive commercial use, an adult specialty store or an adult video store.

## RETIREMENT HOME

Means a building or structure or part thereof providing accommodation primarily for retired persons or couples where each private bedroom or living unit has a separate private bathroom and separate entrance from a common hall but where common facilities for the preparation and consumption of food are provided, and common lounges, recreation rooms and medical care facilities may also be provided.

## REVERSE FRONTAGE RESIDENTIAL LOTS

Means residential lots adjacent to a major arterial, minor arterial, multi-purpose arterial or collector road that front onto an internal street, while the rear yard faces onto and is typically fenced from the arterial or collector road. Road access onto the arterial or collector is prohibited.

## SCHOOL, COMMERCIAL

Means a building or structure or part thereof used as a school conducted for gain, including a studio of a dancing teacher or a music teacher, an art school, a golf school, school of calisthenics, business or trade school and any other such specialized school.

## SCHOOL, PRIVATE

Means a building or structure or part thereof used as an academic school which secures the major part of its funding from sources other than government agencies.

## SECONDARY USES

Means uses secondary to the principal use of the property, including home occupations, home industries and uses that produce value-added agricultural products from the farm operation on the property.

Means, in reference to employment areas, uses secondary to the main permitted uses provided for under the applicable designation. Secondary uses may be permitted either as a standalone use on a lot or in conjunction with a principal use, subject to criteria established by this Plan.



## SECOND LEVEL LODGING HOME

Means a single detached dwelling which has been converted as a lodging or rooming house and is occupied by not less than eleven persons and not more than twenty persons exclusive of staff, who live under supervision as a single housekeeping unit and require 24-hour residential, sheltered, specialized or group care.

## SELECTED BEDROCK RESOURCE AREA

Means an area identified by the Ontario Geological Survey in the Aggregate Resource Inventory Paper, 1996 in which a major bedrock resource is known to exist and is of primary significance for potential extraction.

## SELECTED SAND AND GRAVEL RESOURCE AREA

Means an area identified by the Ontario Geological Survey in the Aggregate Resource Inventory Paper, 1996 in which a major sand and gravel resource is known to exist and is of primary or secondary significance for potential extraction.

## SENIORS HOUSING

Means housing designed and targeted to seniors, and includes privately owned developments marketed to seniors and assisted housing for seniors, as well as special needs housing such as retirement homes and long-term care facilities. See also Special Needs Housing.

## SENSITIVE LAND USE

Means buildings or structures or parts thereof, amenity areas or outdoor spaces where routine or normal activities occurring at reasonably expected times would experience one or more adverse effects from contaminant discharges generated by a major nearby facility. Sensitive land uses may be part of the natural or built environment. Examples include residences, day nurseries and educational and health facilities.

## SERVICE COMMERCIAL USE

Means a building or structure or part thereof where clerical, business, professional and administrative services such as banking, accounting, insurance, investment and financial planning, land use planning, engineering or architectural services or the services of other consultants are provided and may include personal service uses such as florists, dry cleaning, tailors, travel agents, hair salons, diet centres, day spas, tanning parlours, shoe repair and video stores, but shall not include any adult entertainment use.

## SERVICES OR ENTERTAINMENT

Means activities, facilities, performances, exhibitions, viewings and encounters but does not include the exhibition of film approved under the Theatres Act.

## SHOPPING CENTRE

Means a commercial development, designed and functioning as a unit with common parking facilities and access points. The predominant use shall be retail, including department stores, specialty food stores and supermarkets where permitted by the policies of this Plan, and service commercial within one

building unit, however, free standing buildings may also be developed in conjunction with the centre, provided that they are compatible with the design and accessibility of the centre itself.

## SIGNIFICANT

Means:

- a) in regards to wetlands and areas of natural and scientific interest, an area identified as provincially significant by the Ontario Ministry of Natural Resources using evaluation procedures established by the Province, as amended from time to time;
- b) in regards to the habitat of endangered species and threatened species, means the habitat, as approved by the Ontario Ministry of Natural Resources, that is necessary for the maintenance, survival, and/or the recovery of naturally occurring or reintroduced populations of endangered and threatened species, and where those areas of occurrence are occupied or habitually occupied by the species during all or any part(s) of its life cycle;
- c) in regards to other features and areas identified in Section B1.3.2 of this Plan, excluding woodlands, ecologically important in terms of features, functions, representation or amount, and contributing to the quality and diversity of an identifiable geographic area or Natural Heritage System. Criteria for determining significance may be recommended by the Province, but municipal approaches that achieve the same objective may also be used;
- d) in regard to cultural heritage, resources that are valued for the important contribution they make to our understanding of the history of a place, an event, or a people; and,
- e) in regard to other matters, important in terms of amount, content, representation or effect.

Deferred, as it relates to lands identified as D4, D5, and D6 on Schedule A2

## SIGNIFICANT WOODLAND

Means a woodland 0.5 ha or larger, determined through a Watershed Management Plan, a Subwatershed Study or a site-specific Environmental Impact Study, to meet one or more of the following four criteria:

- a) the woodland contains forest patches over 99 years old;
- b) the patch size of the woodland is 2 hectares or larger if it is located in an Urban Area, 4 hectares or larger if located outside an Urban Area but below the escarpment brow, or 10 hectares or larger if located outside an Urban Area but above the escarpment brow;
- c) the woodland has an interior core area of 4 hectares or larger, measured 100 metres from the edge of the woodland; or
- d) the woodland is wholly or partially within 50 metres of a major creek or certain headwater creek, or within 150 metres of the escarpment brow.

#### SITE ALTERATION

Means activities, such as the placement of fill, grading and excavation that would change the landform and natural vegetative characteristics of a site.

#### SPECIAL CONCERN SPECIES

Means a species that is listed or categorized as a special concern species on the Ontario Ministry of Natural Resources' official species at risk list, as updated and amended from time to time.

#### SPECIAL NEEDS HOUSING

Means any housing, including dedicated facilities, that is used by people who have specific needs beyond economic needs, including but not limited to mobility requirements or support functions required for daily living, and includes housing for persons with disabilities such as physical, sensory or mental health disabilities, long term care facilities and retirement homes, where varying degrees of support services are provided including meal preparation, laundry, housekeeping, respite care and attendant services; emergency housing; and residential care facilities.

#### SPECIALTY FOOD STORE

Means a building or structure or part thereof specializing in a specific type or class of foods such as an appetizer store, bakery, butcher, delicatessen, fish, gourmet and similar foods.

#### STABLE TOP OF BANK

Means the line which defines the transition between tableland and sloping ground at the crest of a slope associated with a watercourse, as determined by an approved Geotechnical Study and/or by the appropriate Conservation Authority.

#### STREAM CORRIDOR

Means an area of land on both sides of a stream that contribute to the ecological functions of the stream.

#### SUPERMARKET

Means a building or structure or part thereof, containing a complete departmentalized food store selling a comprehensive line of groceries, baked goods, fresh fruit and vegetables, canned goods, dairy products, frozen foods, and fresh and frozen meats. Other accessory lines may include confectionary, drugs and cosmetics, household supplies, hardware, commercial service uses such as dry cleaning and financial services, and a seasonal garden centre.

#### THEATRE

Means a building or structure or part thereof that is used for the showing of motion pictures or for dramatic, musical or live performances. For the purposes of this definition, an adult entertainment parlour or an adult video store are not considered to be a theatre.

## THREATENED SPECIES

Means a species that is listed or categorized as a threatened species on the Ontario Ministry of Natural Resources' official species at risk list, as updated and amended from time to time.

## TOTAL DEVELOPABLE AREA

Means the total area of the property less the area occupied by Key Natural Heritage Features and Key Hydrologic Features, including any related vegetation protection zone.

## TRANSPORT TERMINAL

Means the use of land for the purpose of storing, servicing, washing, repairing or loading of trucks and/or transport trailers with materials or goods that are not manufactured, assembled, warehoused, or processed on the same lot.

## TREATMENT CENTRE

Means a single detached dwelling which is occupied by not more than 20 persons exclusive of staff, who live under supervision as a single housekeeping unit, and require 24-hour residential, sheltered, specialized or group care, and treatment and rehabilitation for addiction to drugs or alcohol.

A Treatment Centre shall be limited to not less than three and not more than ten persons in the Urban Area, *Hamlet Area*, *Rural Cluster Area* and the *Niagara Escarpment Plan Area*, and not more than twenty persons in the *Agricultural Area* and *Protected Countryside Area*, exclusive of staff.

## UNIVERSAL PHYSICAL ACCESS

Means housing designs that incorporate the following principles and features:

- a) universality – same means for all users;
- b) flexibility – providing choice in methods of use and adaptable to the needs of the user;
- c) low physical effort – usable with a minimum of effort and fatigue;
- d) accessibility – providing for adequate sight lines, reach, hand grip and use of helpful devices;
- e) safety – minimal consequences of errors with fail-safe features;
- f) simplicity – minimal complexity and accommodating a wide range of skills and abilities; and,
- g) perceptible information – communicating necessary information effectively and in various ways.

## VALLEY OR VALLEYLANDS

Means a natural area that occurs in a landform depression that has water flowing through or standing for some period of the year and is defined by the primary top of bank. See also Major Valley/Watercourse and Minor Valley/Watercourse.

## VEGETATION PROTECTION ZONE

Means a vegetated buffer area surrounding a Key Natural Heritage Feature or Key Hydrologic Feature within which only those land uses permitted within the feature itself are permitted. The width of the vegetation protection zone is to be determined when new development or site alteration occurs within 120 metres of a Key Natural Heritage Feature or Key Hydrologic Feature, and is of sufficient size to protect the feature and its functions from the impacts of the proposed change and associated activities that will occur before, during, and after, construction, and where possible, restore or enhance the feature and/or its function.

## WASTE DISPOSAL SITE

Means any land upon, into or through which, a building or structure in which, waste is deposited, disposed of, handled, stored, transferred, treated or processed and includes an operation carried out or machinery or equipment used in connection with the depositing, disposal, handling, storage, transfer, treatment or processing of waste.

## WATERCOURSE

Means a natural depression in which is situated a continuous or intermittent flow of water sufficient to form and maintain a defined channel, with bed and banks; but generally not including wetland flow or modified/constructed drainage features such as swales, agricultural drains or roadside ditches. See also Major Valley/Watercourse and Minor Valley/Watercourse.

## WAYSIDE PIT OR QUARRY

Means a temporary pit or quarry opened and used by or for a public authority solely for purpose of a particular project or contract of road construction and which is not located within the right-of-way of a public street.

## WETLAND

Means lands that are seasonally or permanently covered by shallow water, as well as lands where the water table is close to or at the surface. In either case the presence of abundant water has caused the formation of hydric soils and has favoured the dominance of either hydrophytic plants or water-tolerant plants. The four major types of wetlands are swamps, marshes, bogs and fens. See also Provincially Significant Wetland and Regionally Significant Wetland.

Periodically soaked or wetlands being used for agricultural purposes which no longer exhibit wetland characteristics are not considered to be wetlands for purposes of this definition.

## WILDLIFE HABITAT

Means areas where plants, animals and other organisms live and find adequate amounts of food, water, shelter and space to sustain their populations. Specific wildlife habitats of concern, may include areas where a species concentrate at a vulnerable point in their annual or life cycle and an area that is important to a migratory or non-migratory species.

## WILDLIFE MANAGEMENT

Means management of wildlife habitats for the purposes of sustaining the quantity and quality of wildlife.

## WOODLAND

Means land with at least 1000 trees of any size per ha, or 750 trees over 5 cm in diameter per ha, or 500 trees over 12 cm in diameter per ha, or 250 trees over 20 cm in diameter per ha, but does not include an active cultivated fruit or nut orchard, a Christmas tree plantation, a plantation certified by the Region, a tree nursery, or a narrow linear strip of trees that defines a laneway or a boundary between fields. For the purpose of this definition, all measurements of the trees are to be taken at 1.37 m from the ground, and trees in regenerating fields must have achieved that height to counted.



## SECONDARY PLANS INTRODUCTION

In accordance with the provisions of Section G3 of this Plan, Secondary Plans may be prepared to allow for more detailed area or issue-based planning in newly developing areas or other areas where specific issues and concerns are identified.

Part H of the Official Plan includes Secondary Plans prepared for specific areas of the Town. These Secondary Plans are:

- the Hamlet of Norval Secondary Plan;
- the Georgetown GO Station Area Secondary Plan;
- the Hamlet of Glen Williams Secondary Plan.
- Premier Gateway Employment Area Phase 1B Secondary Plan
- Vision Georgetown Secondary Plan
- Downtown Georgetown Secondary Plan



## H2 HAMLET OF NORVAL SECONDARY PLAN

### H2.1 OVERALL GOAL OF THE SECONDARY PLAN

The overall goal of the Norval Secondary Plan is to ensure the retention and enhancement of the natural, cultural and heritage resources of the Hamlet and to guide change so that it contributes to and does not detract from the character of the Hamlet, in an environmentally protective and cost effective manner. The plan provides for a limited amount of growth through infilling and the promotion of existing businesses focused on Guelph Street and small-scale tourist/retail related activities.

### H2.2 OBJECTIVES

The objectives of the Hamlet of Norval Secondary Plan are as follows:

- To reduce traffic congestion and heavy truck traffic within the Hamlet to the extent feasible and to fully support the further evaluation of bypass opportunities;
- To recognize that the boundary of Norval has been fixed through the Greenbelt Plan and to continue to ensure the preservation of hamlet character by addressing the interface between the *Hamlet Area*, surrounding land uses, the Georgetown urban area and lands in the City of Brampton;
- To maintain and enhance the existing character of the Hamlet and to recognize the existing street and lot pattern which contribute to the character;
- To improve the visual aesthetics through the use of Hamlet Design Principles and Hamlet Design Guidelines;
- To strengthen Norval's tourism presence within Halton Hills and the Greater Toronto Area;
- To provide for increased tourist-pedestrian related amenities and facilities;
- To enhance the vitality of the commercial core through the support of small, independent businesses focused on Guelph Street and maintain the core as a focal point for the community;
- To maintain, enhance and restore the health of the natural environment;
- To provide an environmental framework which serves both the existing and future community, which is formed by linking existing open spaces, natural features, and parks and the developed areas of the Hamlet;
- To recognize the constraints of the floodplain and to balance the need to protect public safety with the need to recognize historic development within the Hamlet;
- To recognize the heritage resources within the Hamlet which form an integral component of its character and ensure that new buildings are in keeping with the heritage character including height, built form, massing, scale and setbacks; and,
- To encourage active transportation and to provide facilities within the core which connect to Georgetown and the surrounding communities.

## H2.3 GENERAL POLICIES

Schedule H2 to this Plan identifies land use designations that apply to Norval and should be reviewed in conjunction with the following policies.

### H2.3.1 SERVICING

A municipal water system and storm sewers currently service the Hamlet of Norval. Individual, private septic beds accommodate sanitary sewage disposal. Halton Region is responsible for the provision of municipal water and wastewater services. The Region of Halton has reserved a limited capacity in the Georgetown Wastewater Pollution Control Plant to service the Hamlets when the Region, in consultation with the Town, determines it to be prudent and feasible.

Where feasible, development shall occur on the basis of municipal water supply and private wastewater treatment systems. In certain situations a municipal water connection may not be feasible or necessary. In such cases the policies of this Section and Sections H2.3.2, and H2.3.3 shall apply.

All new lots on partial municipal services or private services within the Hamlet shall meet the requirements of the Town of Halton Hills and the Regional Municipality of Halton. No new lot shall be smaller than 1,900 square metres in area.

The Town of Halton Hills does not currently envision and the Regional Municipality of Halton has not currently budgeted for the extension of municipal sanitary sewage services to the Hamlet. The introduction of this service could have a significant impact on the formation of new lots and the density of development.

Therefore, any proposal to provide municipal sanitary sewage services to the Hamlet will require the approval of the Town of Halton Hills, the Regional Municipality of Halton, and where appropriate Credit Valley Conservation. Such approval shall require all associated supporting studies deemed necessary by approval agencies.

The minimum lot size for new development on full urban services (municipal water and sewer) shall be 929 square metres.

### H2.3.2 GROUNDWATER PROTECTION

Prior to any development on private or partial services a hydrogeological study may be required to the satisfaction of the Town of Halton Hills and Regional Municipality of Halton in consultation with Credit Valley Conservation to ensure that groundwater resources will not be negatively impacted. Any residential proposal containing three lots or more shall be subject to a hydrogeological study that will address the protection of the existing groundwater supply.

Reference should be made to the Region of Halton's Hydrogeological Studies and Best Practices Guidelines when preparing a hydrogeological study in support of a development application.

### H2.3.3 NON-RESIDENTIAL USES

Existing non-residential uses can occupy up to 500 square metres of gross floor area. New non-residential uses can occupy up to 250 square metres of gross floor area. New non-residential uses proposing to exceed the 250 square metre limit, but less than 500 square metres of gross floor area

must also satisfy the criteria in Section H2.4.2 of this Plan. Prior to any development on private or partial services a hydrogeological study may be required in accordance with Section H2.3.2 of this Plan.

A “new” non-residential use refers to:

- the introduction of a use which is not currently permitted on a property; or.
- the re-designation and/or rezoning of a property from one land use to another.

#### H2.3.4 STORMWATER MANAGEMENT

All commercial, industrial, institutional, recreational and residential development shall be supported by a Stormwater Management (SWM) report unless waived by the Town through a pre-consultation process. The report shall be prepared to the satisfaction of the Town of Halton Hills in consultation with the appropriate agencies in accordance with the policies in Section C8 of the Halton Hills Official Plan.

In order to encourage more sustainable development, stormwater best management practices will be encouraged. This may include Low Impact Development (LID) stormwater management techniques with the objective to address stormwater at the source rather than solely in traditional end of pipe measures (e.g. stormwater management ponds). Stormwater best management practices such as LID techniques may include the use of measures such as bioretention facilities, permeable pavers, vegetated filter strips, rainwater harvesting, and disconnected downspouts.

In addition, a report on the impact on fisheries within the Credit River and Silver Creek may also be required. A Stormwater Management report may be required for a lot severance, if determined to be necessary by the Town of Halton Hills in consultation with the Credit Valley Conservation Authority.

#### H2.3.5 FLOODPLAIN, VALLEYLANDS AND NATURAL FEATURES

The Hamlet of Norval has historically developed within the valley of the Credit River. Development is normally prohibited within valleylands given the potential for impacts on the watershed. However, the long-term establishment of a community in this area makes the prohibition of development undesirable due to the impact of such a prohibition on the viability of the community and the potential loss of important cultural assets. A balance has been sought between the protection of the valleylands and the allowance of limited development to ensure the continued viability of the Hamlet.

The Regulatory Floodplain has been illustrated on Schedule H2 in order to identify all areas that are susceptible to flooding. Schedule H2 further designates areas of environmental importance as *Greenlands*. Section H2.12 of this Plan defines and provides planning policies for the *Greenlands*.

Areas that have been identified as wetlands, significant woodlands, Environmentally Sensitive Areas (ESAs) or steep slopes, have been designated as *Greenlands* on Schedule H2. Lands which are located within the floodplain have only been included in the *Greenlands* designation if they are not developed or if they have other natural heritage features listed above which also applies to them. Developed lands whose only environmental constraint is the floodplain have been given an underlying land use designation with a floodplain overlay designation as outlined in Section H2.13 of this Plan.

In accordance with the Provincial Policy Statement and Credit Valley Conservation’s Regulation 160/06 (Regulation of Development, Interference with Wetlands and Alterations to Shorelines and Watercourses), no new or expanded nursing homes, day care facilities, group homes, seniors homes,

schools, fire, police or ambulance stations, or other sensitive uses shall be permitted within the regulatory floodline established by Credit Valley Conservation.

Areas that are designated *Greenlands* are recognized as generally unsuitable for development given their importance to:

- human and ecological health;
- the protection of property from damage due to all natural hazards including erosion and slope instability as well as flooding; and,
- human safety.

The Georgetown Credit River Valley and Hungry Hollow Ravine ESAs are located partially within and adjacent to the Norval Secondary Plan area.

It is a policy of this plan to provide for the protection and enhancement of features within the *Greenlands* designation through the limitation of permitted uses, the provision of building setback requirements and other development requirements, such as the need for more detailed studies. The protection and enhancement of natural features can also be achieved through the restoration of watercourse corridor or riparian zones, and landowner stewardship.

An Environmental Impact Study may be required for development applications adjacent to the *Greenlands* to the satisfaction of the Town in consultation with the Region of Halton and Credit Valley Conservation Authority in accordance with Section C2 of the Town's Official Plan. Reference should be made to the Region of Halton's Environmental Impact Assessment Guidelines when preparing terms of reference for the study and when undertaking and completing the study.

Lands not designated as *Greenlands* may still have valuable natural features that should be considered for protection and enhancement. This is especially true for woodlands in the Hamlet which are not deemed significant woodlands. These woodlands perform important ecological functions both individually and as a whole that assist in erosion control, mitigating the impacts of wind and dust, providing habitat for local and migratory wildlife and improving aesthetics. Applicants for Site Plan approval, Subdivision approval or severance, may be required to submit a tree inventory and preservation plan and a proposed planting plan to the satisfaction of the Town. The tree inventory and preservation plan shall include tree protection, enhancement and replacement measures.

#### H2.3.6 EXPANSION OF REPLACEMENT OF EXISTING BUILDINGS WITHIN THE REGULATORY FLOODPLAIN

A portion of Norval's historical core area is within the regulatory flood line established by Credit Valley Conservation. Generally, new development and/or the creation of new lots is prohibited by the Credit Valley Conservation Authority within the floodplain given the potential danger to life and property and impacts on the Credit River Watershed. However, it is recognized that the existing buildings provide the homes and businesses that make up the community and are also important from a tourism and heritage preservation perspective.

In balancing these issues a limited amount of expansion or, under certain conditions, replacement of buildings will be permitted subject to approval by the Credit Valley Conservation Authority. The following Credit Valley Conservation requirements as may be amended from time to time are applicable:

- a) Expansions of existing buildings are limited to a ground floor addition of 50% or less of the original habitable ground floor area to a maximum of 100 square metres and does not include a basement;
- b) Expansions of existing buildings are flood proofed, to the regulatory flood level to the satisfaction of Credit Valley Conservation and the policies of this plan;
- c) Buildings destroyed by fire or other means will be permitted on the existing footprint provided that the reconstruction occurs within two years of the buildings destruction and that the building is designed so that no structural damage will occur from the regulatory flood and the building is flood proofed to the satisfaction of Credit Valley Conservation;
- d) That no habitable rooms are established below the regulatory flood level and there is no increase in the number of dwelling units;
- e) That safe access is provided for pedestrian and vehicular access and evacuation routes as determined by the Town of Halton Hills and Credit Valley Conservation;
- f) No new sensitive land uses including nursing homes, day care facilities, group homes, seniors homes, schools, fire, police or ambulance stations, or other similar uses shall be permitted, and any existing sensitive uses shall not be allowed to expand or be replaced in the event of destruction; and,
- g) Notwithstanding any policies of this Plan no uses associated with the storage or use of any hazardous or toxic materials shall be permitted to be reconstructed or enlarged.

#### H2.3.7 PROTECTION OF FISH HABITAT

The reach of the Credit River through Norval is classified by Credit Valley Conservation as a “mixed cool/cold” water fisheries community. This classification requires a 30 metre development setback from the bank of the watercourse. The exact boundaries of the 30 metre buffer zone should be established in consultation with Credit Valley Conservation.

No new developments are permitted within the 30 metre buffer zone without the approval of Credit Valley Conservation. However, there is extensive historical development within the buffer zone and a prohibition on improvements, replacements or minor additions to buildings is not desirable. It is therefore a policy of this plan that Credit Valley Conservation must be consulted to evaluate impacts and propose mitigation strategies to achieve no negative impact as part of any application for development within this area.

#### H2.3.8 HAMLET BOUNDARIES AND BUFFERS

A rational Hamlet boundary has been established based upon municipal boundaries and identifiable features such as valley top-of-bank, watercourses and roads.

In the case of Norval, the Silver Creek valley, Willow Park, and the Greenbelt Plan Area form an urban separator between Norval and the Georgetown Urban Area.

If the Hamlet of Norval is to retain its character, these separators are viewed as important elements. To this end, the following policies will apply:

- a) Wherever possible, environmental areas that represent a logical buffer between the Hamlet and both future and existing urban development will be used as an “urban separator”;
- b) Wherever possible, buffers will be included within the boundaries of the Hamlet;
- c) The Town of Halton Hills will engage in discussions with the City of Brampton and the Region of Peel to examine ways that a distinct separation between the Hamlet of Norval and the City of Brampton can be preserved; and,
- d) Planning for the Southeast Georgetown new growth area shall consider the interface with the Hamlet of Norval.

#### H2.3.9 NOXIOUS USES

No use, which does, or is likely to cause air pollution, offensive odours, ground or surface water pollution, or noise in excess of Provincial regulations or guidelines, shall be permitted.

#### H2.3.10 ADULT ENTERTAINMENT USES

Adult entertainment uses shall be prohibited in all land use designations in the Hamlet.

#### H2.3.11 TRANSPORTATION

Highway 7 is a Provincial Highway with access restrictions. New entry points to Highway 7 are subject to the approval of the Ministry of Transportation.

The Halton-Peel Boundary Area Transportation Study (HPBATS) was undertaken to look at accommodating future travel demands generated by growth in Brampton and Halton Hills. Highway 7 and Adamson Street (Winston Churchill Boulevard) through Norval were identified as “bottlenecks”. Means of addressing congestion were discussed in the Study and included shifting traffic outside of peak hours (using travel demand management), shifting modes of travel (to carpooling, transit, cycling, or walking), reducing demand (typically addressed through employer-based programs such as telecommute), or shifting traffic elsewhere (i.e., a bypass). The HPBAT Study indicated that the roadways through Norval should remain two lanes serving local traffic and restricting capacity which will help to ensure the long term preservation of the Hamlet. It recommended additional capacity be provided via bypasses, new arterials, or new freeway options which included the Norval Bypass, the Adamson Street North Bypass and the Winston Churchill Blvd Bypass.

Environmental Assessments are required in order to undertake the bypass options. The Town will request as part of any Environmental Assessments that specific attention to north-south and east-west connections be made to determine how the connections will be achieved through either a combination of road connections or a new corridor. Possible conflicts between these connections and the existing housing and environmental features in the Hamlet must be given careful consideration in order to ensure that the river valley setting and the current built form character of the community is maintained. The Town will also request that truck traffic be restricted in the hamlet core.

The Town of Halton Hills and the Region of Halton will implement corridor protection for future north-south transportation facilities and related connections (East/West Connection bypass or Norval West Bypass). Schedule H2 of the Secondary Plan has identified a *Corridor Protection Area* within the Hamlet boundaries.

The HPBAT Study indicated that no bypass option will bring the volume-to-capacity ratio through Norval to less than one (functional capacity). As a result, there will continue to be on-going traffic congestion. Traffic Calming measures have therefore been initiated within Norval on streets under the jurisdiction of the Town in order to slow traffic. If traffic calming is to be provided on Highway 7 and Adamson Streets, yet still maintain function and capacity, the streets should be made visually narrower. On-street parking, street trees, landscaping, and street furniture should be investigated to make the road feel narrower and convey the message to slow down. Additional traffic calming measures which do not place any form of structure within the right of way such as pavement markings and pavement colouring should also be considered. As Guelph Street is under the jurisdiction of the Ministry of Transportation (MTO), Provincial approval will be required for any changes within the Guelph Street right of way.

Active transportation will be encouraged by improving pedestrian connections including movements into and out of the Hamlet and the expansion and enhancement of the trail systems, and by facilitating the addition of bicycle lanes as identified in the Town's Cycling Master Plan.

## **H2.3.12 TOURISM DEVELOPMENT**

Given both the cultural and natural heritage of the area, Norval has the potential to offer specialized tourism opportunities.

A significant opportunity within Norval stems from the artifacts associated with former resident, Lucy Maud Montgomery. Considered one of Canada's best-known writers, and recognized worldwide for many of her works, Lucy Maud Montgomery lived in Norval for a period of nine years dating from 1926 to 1935. Six of her most popular books were written in her home, the Norval Presbyterian Manse. Other former residents important to the Hamlet include Peter Adamson and Robert Noble whose names have been given to local streets.

The Willow Park Ecology Centre is an educational and tourist attraction, drawing upon the natural environment of the area to provide environmental education to visitors.

Norval is attracting tourists as people visit the town in which Montgomery lived. The parks and natural amenities that are associated with and surround the Credit River system through Norval will, if properly managed and protected, also contribute to the hamlet character and the attraction of tourists. Towards the maintenance and enhancement of Norval's tourism base the following policies shall apply:

- a) Consistent with the Hamlet Design and Heritage Protection Guidelines contained in Appendix X5, there will be safe, efficient and deliberate movement of pedestrians between tourist attractions within the Hamlet and connection to the core commercial area via both trails and public streets;
- b) The community core, consistent with the Hamlet Design and Heritage Protection Guidelines contained in Appendix X5 shall be the focus for commercial activities in the Hamlet;
- c) This plan encourages the production of materials such as self-guided historic and natural feature interpretation oriented walking tours to increase public knowledge and awareness of existing heritage resources;
- d) Interpretation signs, consistent with the Hamlet Design and Heritage Protection Guidelines contained in Appendix X5, will be located within or adjacent to areas of natural, heritage and community significance, along any trail system and at major gateways;

- e) Land uses within the Hamlet shall be designated to support the required amenities to accommodate the visitors generated by the tourism development initiatives; and,
- f) It is a policy of this plan to encourage the tourism potential of events such as the Montgomery Christmas Festival and Riverfest, with themes relating to Lucy Maud Montgomery and the protection of the Credit River as a vital natural resource.

#### H2.3.13 ARCHAEOLOGICAL RESOURCES

Development that impacts on areas that have information or evidence of potential for archaeological significance will require an archaeological impact assessment in accordance with the Halton Hills Official Plan to determine the presence of archaeological resources and, if required, provide mitigative recommendations.

Development and site alteration may be permitted on land containing archaeological resources or areas of archaeological potential, if significant archaeological resources have been conserved by removal and documentation, or preservation on site. Where significant archaeological resources must be preserved on site, only development and site alteration, which maintains the heritage integrity of the site, will be permitted.

#### H2.3.14 HAMLET DESIGN AND HERITAGE PROTECTION

The character of the Hamlet of Norval is largely defined by the heritage buildings which shape the built form. These buildings help create an environment that is distinctive and lays the foundation for not only a cohesive community but also for tourism development initiatives. This is especially true for initiatives that leverage Norval's place in history as the residence of Lucy Maud Montgomery, as well as the recreational opportunities associated with the Credit River.

In order to maintain the character of the Hamlet, all development will recognize the existing built form context and protect the character of the heritage buildings and landscapes. Development will respect and reinforce:

- a) the height, massing, and scale of nearby buildings;
- b) the average setbacks of buildings from the street;
- c) the prevailing side and rear yard setbacks;
- d) the landscaped open space patterns;
- e) the size and configuration of lots; and,
- f) the existing street pattern.

All development must be in keeping with the physical character of the Hamlet and no approvals will be granted under the Planning Act for development which does not respect the existing built form.

Further details regarding the appropriate built form and the protection and enhancement of the community features are established in the Hamlet Design and Heritage Protection Guidelines. These are attached as Appendix X5 to this document and are designed to help the Town and the community guide new development.



The approval process for all planning applications within the Hamlet will include the application of the Hamlet Design and Heritage Protection Guidelines. An architectural and urban design brief/study may be required in support of an application indicating how the proposed development conforms with the Hamlet Design and Heritage Protection guidelines to the satisfaction of the Town. The Town may require a third party “peer review” of the proposed development.

It is a policy of this Plan that the cost of a peer review and implementation of on and off-site improvements consistent with the Hamlet Design and Heritage Protection Guidelines will be the responsibility of the proponent to the satisfaction of the Town.

#### H2.3.15 HAMLET BOUNDARIES

Under the policies of the Greenbelt Plan, the boundaries of the Hamlets are delineated by the local Official Plan. Minor rounding out to a hamlet’s boundary is only permitted through an Official Plan Conformity exercise with the Greenbelt Plan. The Town of Halton Hills has undertaken a Greenbelt Plan conformity exercise and therefore no further expansion of the Hamlet boundaries is permitted.

#### H2.3.16 TRAIL SYSTEMS

The development of improved trail systems which encourage walking and bicycling has been identified by the Town of Halton Hills through the Cycling Master Plan and the earlier Trails and Cycling Master Plan.

The existing trails and conceptual alignments for future bicycle and pedestrian trails/routes are shown on Schedule H2. Reference should be made to the Cycling Master Plan and the earlier Trails and Cycling Master Plan for details on how and when future trails are expected to be developed. The actual implementation of any trail system will be the subject of further assessment and additional public consultation will occur during that period. All trails will be developed to municipal standards and with regard to potential environmental impacts. Additional information on trail development is also provided in the Hamlet Design and Heritage Protection Guidelines.

### H2.4 HAMLET COMMUNITY CORE AREA

#### H2.4.1 PURPOSE

The *Hamlet Community Core Area* designation identifies the central portion of the Hamlet where the greatest concentration of commercial activities and heritage features are located. It is an objective of the *Hamlet Community Core Area* designation to define and strengthen the character of the Hamlet through the protection of its architectural style and natural heritage while at the same time supporting and improving the commercial viability of the existing businesses and tourism based activities.

This designation serves to allow for the concentration of the primary commercial and tourism functions within the historic core area towards the creation of a vibrant centre of activity. Residential uses are also encouraged that are consistent with the policies of the Hamlet Design and Heritage Protection Guidelines contained in Appendix X5.

#### H2.4.2 PERMITTED USES

Uses permitted within the *Hamlet Community Core Area* designation include tourism and cultural uses, retail services, residential uses and office space. Existing non-residential uses can occupy up to 500

square metres of gross floor area. New non-residential uses can occupy up to 250 square metres of gross floor area. New non-residential uses proposing to exceed the 250 square metre limit, but less than 500 square metres of gross floor area must satisfy the following criteria:

- a) A design study must be provided that clearly demonstrates that the development meets the criteria of Section H2.3.14 and the Hamlet Design and Heritage Protection Guidelines and that the built form respects and reinforces the character of the community, is visually consistent with surrounding uses and is sensitive to existing vegetation and topography;
- b) The development is compatible with adjacent uses;
- c) A traffic impact study must be provided that addresses traffic impacts, mitigation measures and ensures that there is acceptable vehicular and pedestrian access to and within the site;
- d) Adequate Servicing must be provided for the proposed use in accordance with Section H2.3.1 of this Plan; and,
- e) Any adverse noise impacts can be appropriately mitigated.

The permitted uses include:

- a) animal clinics;
- b) antique and furniture stores;
- c) artist studios and art galleries;
- d) bakeries;
- e) banks;
- f) bed and breakfast establishments;
- g) business or professional offices;
- h) cafes;
- i) community centres;
- j) craft shops;
- k) custom workshops;
- l) day care centres;
- m) dwelling unit located in a non-residential building;
- n) general stores;
- o) home occupations and cottage industries;
- p) ice cream parlours;
- q) professional medical or dental offices;

- r) museums;
- s) personal service uses such as hair stylist and shoe repair;
- t) private parks;
- u) public parks;
- v) public parking areas;
- w) recreational uses;
- x) residential care facilities meeting the definition of a Group Home Type 1 subject to the requirements of the implementing Zoning By-law and the Official Plan;
- y) restaurants, but not a drive through restaurant;
- z) retail showrooms;
- aa) single detached dwellings;
- bb) tourist attractions; and
- cc) accessory apartments in single detached dwellings subject to Section E1.4.10.

In addition, small scale woodworking or blacksmith or other similar operations that have a tourism function in terms of providing opportunities for visitors to view historic or unique manufacturing practices, shall be permitted providing they conform to Section H2.3.3 of this Plan.

Within the area identified as *Hamlet Community Core Area*, automotive uses that legally existed on the day of the passing of the revised Secondary Plan (Official Plan Amendment 20) will be recognized and permitted to continue their existing operation and function in their current location. No additional automotive services or uses will be permitted with the exception of limited motor vehicle sales accessory to the main permitted automotive use at 488/490/492 Guelph St.

No new automotive uses will be permitted within the *Hamlet Community Core Area* designation.

Notwithstanding the permitted uses listed above, no new or expanded nursing homes, day care facilities, group homes, seniors' homes, schools, fire, police or ambulance stations, or other sensitive uses shall be permitted within the regulatory floodline established by Credit Valley Conservation.

## H2.4.3 LAND USE POLICIES

Development within the *Hamlet Community Core Area* designation shall be consistent with the following land use policies:

- a) Where commercial and residential uses abut, site planning to achieve adequate buffering in the form of landscaping or architectural screening shall be required;
- b) Adequate off street parking will be required. Parking facilities will be oriented to the side or rear of all commercial buildings;

- c) Adequate servicing for the proposed use must be provided in accordance with Section H2.3.1 of this Plan;
- d) The subdivision of land will only be permitted as infilling where the lots meet the minimum criteria specified in Section H2.3.1 of this Plan;
- e) New commercial uses shall generally be compatible with the character and scale of the Hamlet;
- f) Home occupations and cottage industries may be permitted as a secondary use to the primary residential use subject to criteria in accordance with the Halton Hills Official Plan;
- g) Outdoor storage is not permitted within the *Hamlet Community Core Area* designation;
- h) The development of office space will be encouraged to occur on second storeys above retail uses;
- i) All development will be consistent with the policies in section H2.3.14 and the Hamlet Design and Heritage Protection Guidelines contained in Appendix X5, and shall not exceed two storeys;
- j) No drive through operations will be permitted within the *Hamlet Community Core Area* designation; and,
- k) Any development adjacent to Highway 7 is subject to approval from the Ministry of Transportation for new or changes to access on to Highway 7.

## H-H2.5 HAMLET COMMERCIAL AREA

### H2.5.1 PURPOSE

The *Hamlet Commercial Area* designation applies to existing automotive uses located on Highway 7 or existing commercial uses which are located outside of the *Hamlet Community Core Area*. The purpose of the designation is to recognize the existing uses and to permit limited additional commercial uses which are compatible with surrounding residential uses.

### H2.5.2 PERMITTED USES

Existing non-residential uses can occupy up to 500 square metres of gross floor area. New non-residential uses can occupy up to 250 square metres of gross floor area. New non-residential uses proposing to exceed the 250 square metre limit, but less than 500 square metres of gross floor area must satisfy the criteria in Section H2.4.2 of this Plan in addition to the land use policies of this designation.

The uses permitted within the *Hamlet Commercial Area* designation include:

- a) automotive uses that legally existed on the day of the passing of the bylaw adopting this Plan;
- b) commercial uses that legally existed on the day of the passing of the bylaw adopting this Plan;
- c) day care centres;
- d) single detached dwellings;

- e) bed and breakfast establishments;
- f) home occupations and cottage industries;
- g) animal clinics;
- h) business or professional offices;
- i) custom workshops;
- j) personal service uses such as hair stylists and shoe repair;
- k) retail stores; and
- l) accessory apartments in single detached dwellings subject to Section E1.4.10.

The existing commercial self-storage facility use at 16 Adamson Street North is permitted. No outdoor storage is permitted in the *Hamlet Commercial Area* designation in association with this use or any other use.

### H2.5.3 LAND USE POLICIES

Development within the *Hamlet Commercial Area* designation shall be consistent with the following land use policies:

- a) Notwithstanding the permitted uses listed above, no new or expanded nursing homes, day care facilities, group homes, seniors homes, schools, fire, police or ambulance stations, or other sensitive uses shall be permitted within the regulatory floodline established by Credit Valley Conservation;
- b) Within the area identified as *Hamlet Commercial Area*, automotive uses that legally existed on the day of the passing of the revised Secondary Plan (Official Plan Amendment 20) will be recognized and permitted to continue their existing operation and function in their current location. No additional automotive services or uses will be permitted with the exception of limited motor vehicle sales accessory to the main permitted automotive use at 546/548 Guelph St.;
- c) No new automotive uses are permitted in the *Hamlet Commercial Area* designation;
- d) Outdoor storage is not permitted in the *Hamlet Commercial Area* designation;
- e) Where commercial and residential uses abut, site planning to achieve adequate buffering in the form of landscaping, or architectural screening shall be required;
- f) All development will be consistent with the policies in section H2.3.14 and the Hamlet Design and Heritage Protection Guidelines contained in Appendix X5, and shall not exceed two storeys;
- g) Adequate off street parking will be required. Parking facilities should be oriented to the side or rear of all commercial buildings;
- h) Adequate servicing for the proposed use must be provided in accordance with Section H2.3.1 of this Plan;

- i) New commercial uses shall generally be compatible with the character and scale of the Hamlet;
- j) Home occupations and cottage industries may be permitted as a secondary use to a primary residential use subject to criteria in accordance with the Halton Hills Official Plan;
- k) No drive through operations will be permitted within the *Hamlet Commercial Area* designation; and,
- l) Any development adjacent to Highway 7 is subject to approval from the Ministry of Transportation for new or changes to access on to Highway 7.

## H2.6 HAMLET COMMERCIAL SPECIAL AREA

### H2.6.1 PURPOSE

The *Hamlet Commercial Special Area* designation applies to a portion of 16 Adamson Street North. The purpose of the designation is to recognize the existing commercial self-storage facility on the rear of the property.

### H2.6.2 PERMITTED USES

Permitted uses are limited to the existing commercial self-storage facility on the property at 16 Adamson Street North. As outlined in Section H2.3.3 Non-Residential Uses of this Plan, existing non-residential uses can occupy up to 500 square metres of gross floor area.

### H2.6.3 LAND USE POLICIES

The following land use policies apply within the *Hamlet Commercial Special Area* designation:

- a) Accessory uses are limited to the temporary indoor and/or outdoor storage of recreational vehicles including boats and trailers, and antique/classic motor vehicles. The storage, use, and/or parking of shipping containers, truck/van/coach bodies, rail cars, dismantled and/or inoperable motor vehicles, commercial motor vehicles including transport trucks and/or transport trailers, and the outdoor storage of other materials or goods shall not be permitted;
- b) The implementing zoning by-law shall include provisions and setbacks for the accessory temporary or outdoor storage of recreational vehicles including boats and trailers, and antique/classic motor vehicles;
- c) The use of the property as a transport terminal, contractors yard/establishment, salvage yard or other outdoor storage use shall not be permitted;
- d) Where the commercial self-storage facility and accessory temporary outdoor storage of recreational vehicles including boats and trailers, and antique/classic motor vehicles is proposed abutting an existing residential use, site planning is required to achieve adequate buffering in the form of landscaping, or architectural screening;
- e) All development will be consistent with the policies in Section H2.3.14 and the Hamlet Design and Heritage Protection Guidelines contained in Appendix X5, and shall not exceed two storeys;

- f) Adequate off street customer parking will be required. Parking facilities should be oriented to the side or rear of all commercial buildings; and,
- g) Adequate servicing for the use must be provided in accordance with Section H2.3.1 Servicing of this Plan.

## H2.7 HAMLET RESIDENTIAL AREA

### H2.7.1 PURPOSE

Lands designated within the *Hamlet Residential Area* designation recognize the predominant existing use of lands as residential. The designation allows for minor expansion of residential development within the Hamlet subject to the policies of this Plan. The purpose of this designation is to recognize the future use of these lands for residential purposes.

### H2.7.2 PERMITTED USES

The uses permitted within the *Hamlet Residential Area* designation include:

- h) single detached dwellings;
- i) bed and breakfast establishments;
- j) home occupations and cottage industries; and,
- k) residential care facilities meeting the definition of a Group Home Type 1 subject to the requirements of the implementing Zoning By-law and the Official Plan.
- l) accessory apartments in single detached dwellings subject to Section E1.4.10.

### H2.7.3 LAND USE POLICIES

Development within the *Hamlet Residential Area* designation shall be consistent with the following land use policies:

- a) Expansion will only occur through minor infilling;
- b) The subdivision of land will only be permitted as infilling where the lots meet the minimum criteria specified in Section H2.3.1 of this Plan and the Zoning By-law;
- c) Any development adjacent to Highway 7 is subject to approval from the Ministry of Transportation for new or changes to access on to Highway 7;
- d) Home occupations and cottage industries may be permitted as a secondary use to the primary residential use subject to criteria in accordance with the Halton Hills Official Plan;
- e) All development will be consistent with the policies in section H2.3.14 and the Hamlet Design and Heritage Protection Guidelines contained in Appendix X5, and shall not exceed two storeys; and,

- f) Adequate servicing for the proposed use must be provided in accordance with Section H2.3.1 of this Plan.

## H2.8 HAMLET RESIDENTIAL/OFFICE AREA

### H2.8.1 PURPOSE

Lands that are designated *Hamlet Residential/Office Area* are intended to encourage the retention of the existing built form in those areas so that it is consistent and compatible with the adjacent residential and commercial uses.

### H2.8.2 PERMITTED USES

New non-residential uses can occupy up to 250 square metres of gross floor area.

The uses permitted within the *Hamlet Residential/Office Area* designation include:

- a) single detached dwellings;
- b) bed and breakfast establishments;
- c) home occupations and cottage industries;
- d) business or professional offices; and,
- e) dwelling unit located in a non-residential building.
- f) accessory apartments in single detached dwellings subject to Section E1.4.10.

### H2.8.3 LAND USE POLICIES

Development within the *Hamlet Residential/Office Area* designation shall be consistent with the following land use policies:

- a) Where office and residential uses abut, site planning to achieve adequate buffering in the form of landscaping, or architectural screening shall be required;
- b) Adequate on-site parking will be required. Parking facilities should be oriented to the side or rear of all office buildings;
- c) All development will be consistent with the policies in section H2.3.14 and the Hamlet Design and Heritage Protection Guidelines contained in Appendix X5, and shall not exceed two storeys;
- d) Adequate servicing for the proposed use must be provided in accordance with Section H2.3.1 of this Plan;
- e) The subdivision of land will only be permitted as infilling where the lots meet the minimum criteria specified in Section H2.3.1 of this Plan;
- f) New office uses shall be compatible with the character and scale of the Hamlet;



- g) Home occupations and cottage industries may be permitted as a secondary use to the primary residential use subject to criteria in accordance with the Halton Hills Official Plan;
- h) Any development adjacent to Highway 7 is subject to approval from the Ministry of Transportation for new or changes to access on to Highway 7; and,
- i) Outdoor storage is not permitted within the *Hamlet Residential/Office Area* designation.

## H2.9 CORRIDOR PROTECTION AREA

### H2.9.1 PURPOSE

The purpose of the *Corridor Protection Area* designation is to recognize this area will be the subject of future environmental assessments to determine an appropriate location for a transportation bypass corridor.

### H2.9.2 PERMITTED USES

The uses permitted within the *Corridor Protection Area* are limited to:

- a) uses existing that legally existed on the day of the passing of the bylaw adopting Official Plan Amendment 20 (updating this Plan);
- b) existing agricultural operations;
- c) non-intensive recreation uses such as nature viewing and pedestrian trail activities; and,
- d) public uses.

### H2.9.3 LAND USE POLICIES

The erection of new buildings and structures shall be prohibited within lands designated *Corridor Protection Area*.

## H2.10 INSTITUTIONAL AREA

### H2.10.1 PURPOSE

Institutional activities that presently exist in the Hamlet include churches (St. Paul's Anglican Church, Norval Presbyterian Church, and Norval United Church), and the Norval Community Centre.

The purpose of the institutional designation is to recognize these uses and their role in the community.

### H2.10.2 PERMITTED USES

The predominant use of land designated *Institutional Area* shall be for purposes such as:

- a) public and private institutional activities;
- b) public utilities;
- c) community centres;

- d) day care centres;
- e) nursing homes;
- f) funeral homes;
- g) cultural, educational, religious and fraternal buildings; and,
- h) ancillary uses, which may be permitted on the same lots as the primary use may include a residence of caretakers or essential personnel.

Notwithstanding the permitted uses listed above, no new or expanded nursing homes, day care facilities, group homes, seniors' homes, schools, fire, police or ambulance stations, or other sensitive uses shall be permitted within the regulatory floodline established by Credit Valley Conservation.

### H2.10.3 LAND USE POLICIES

Development within the *Institutional Area* designation shall be consistent with the following land use policies:

- a) Adequate off street parking for institutional uses shall be provided on site; the parking standard to be applied shall be based on the anticipated number of vehicles to be generated by the proposed use and/or the standards contained in the zoning by-law as amended;
- b) Adequate landscaping, tree planting and buffering shall be provided;
- c) Institutional uses, where possible, shall be connected to a municipal water service and sanitary sewage service if such service becomes available. Studies may be required to determine the suitability of the use on private systems; and,
- d) All development will be consistent with the policies in section H2.3.14 and the Hamlet Design and Heritage Protection Guidelines contained in Appendix X5.

## H2.11 OPEN SPACE AREA

### H2.11.1 PURPOSE

Open space uses that presently exist in the Hamlet include: Norval Park, McNab Park, Hillcrest Cemetery, McNab Pioneer Cemetery, and St. Paul's Anglican Pioneer Cemetery. The purpose of the *Open Space Area* designation is to recognize these uses and their role in the community.

### H2.11.2 PERMITTED USES

The uses permitted within the *Open Space Area* designation include public and private open space uses such as:

- a) parks;
- b) cemeteries; and,
- c) public uses.

### H2.11.3

### LAND USE POLICIES

All parks within the Hamlet exhibit characteristics of both Community and Neighbourhood Parks as defined in the Halton Hills Official Plan. Norval Park serves the local area, as well as acting as a trailhead for the Town-wide network of trails. Development of parkland in the Hamlet will be consistent with the parkland policies of the Halton Hills Official Plan.

Development within the *Open Space Area* designation shall be consistent with the following land use policies:

- a) Adequate off street parking for open space uses shall be provided on site; the parking standard to be applied shall be based on the anticipated number of vehicles to be generated by the proposed use and/or the standards contained in the Zoning By-law as amended; and,
- b) Adequate landscaping, tree planting and buffering shall be provided.

### H2.12

### GREENLANDS

#### H2.12.1

#### PURPOSE

*Greenlands* are designated on Schedule H2 of the Secondary Plan. The purpose of the *Greenlands* designation is:

- a) to identify undeveloped lands which are flood susceptible for the protection of life and property;
- b) to protect the diversity of fauna and flora, ecosystems, plant communities, and significant landforms of Halton Hills;
- c) to maintain the water quality and natural flow regulation of rivers, streams and wetlands within the rural areas of Halton Hills;
- d) to provide opportunities, where appropriate, for passive outdoor recreational activities;
- e) to contribute to a continuous natural open space system, to provide a visual separation of communities and to provide continuous corridors between ecosystems;
- f) to protect significant scenic and heritage resources;
- g) to achieve no loss of function or area of provincially significant wetlands; and,
- h) to maintain or enhance fish and wildlife habitats.

#### H2.12.2

#### CRITERIA FOR DESIGNATION

The *Greenlands* designation includes only land and water areas that meet one or more of the following criteria:

- a) Areas included in the Regulatory Floodplains, as determined and mapped by Credit Valley Conservation, and refined from time to time, which are not currently developed, as shown on Schedule H2 of this Plan;

- b) Valleylands or significant portions of the habitat of endangered and threatened species, as determined by the Town, the Region, Credit Valley Conservation and the Ministry of Natural Resources, as refined from time to time;
- c) Environmentally Sensitive Areas and wetlands;
- d) Significant woodlands; and,
- e) Unstable or steep slopes.

### H2.12.3 SETBACKS

All new development in the Hamlet shall meet the following setbacks:

- a) a 10 metre building setback from the stable top and bottom of the bank of valley;
- b) a 10 metre building setback from any identified erosion allowance associated with a watercourse;
- c) a 10 metre building setback from the regulatory floodline is required for all new development;
- d) in non-valley situations a 30 metre building setback from the bank of the watercourse, or 10 metres from the flood line, whichever is greater unless the setback is within the *Greenlands* designation;
- e) a 10 metre building setback from Environmentally Sensitive Areas and wetlands;
- f) a 30 metre building setback from provincially significant wetlands; and,
- g) where lands are being subdivided or severed, a setback of 10 metres is required from the lot line to the greater of any flood hazard, erosion hazards or significant natural features.

These setback requirements must be met for all new development unless more appropriate setbacks are recommended in accordance with an approved report or study (e.g. subwatershed study, environmental impact study, geotechnical study) to the satisfaction of the Town in consultation with the Region of Halton and Credit Valley Conservation. For proposals minor in scale, study or report requirements may be scoped or waived by the Town in consultation with the Region of Halton and the Credit Valley Conservation Authority.

### H2.12.4 PERMITTED USES

The following uses may be permitted within the *Greenlands* designation shown on Schedule H2, subject to the policies of this Plan:

- a) Existing agricultural operations;
- b) Non-intensive recreation uses such as nature viewing and pedestrian trail activities on publicly owned lands or privately owned lands if access can be negotiated with the property owner;
- c) Forest, wildlife and fisheries management;
- d) Archaeological activities;

- e) Essential transmission and utility facilities;
- f) Essential watershed management and flood and erosion control projects carried out or supervised by a public authority; and,
- g) Uses that legally existed on the day of the passing of the bylaw adopting this Plan.

#### H2.12.5 LAND USE POLICIES

The erection of buildings and structures or the placing or removal of fill of any kind or any alteration to a watercourse or valley shall be prohibited within lands designated *Greenlands*.

It is the policy of this plan to require that any expansion or replacement of existing uses or permitted buildings within the *Greenlands* shall only be considered for approval by the Town, in consultation with the Region of Halton and Credit Valley Conservation on the basis of the following:

- a) Any environmental and/or physical hazards which affect the site can be mitigated in a manner consistent with accepted engineering techniques and resource management practices; and,
- b) Such buildings, structures and works will not locate in significant natural areas and are designed in a manner which:
  - i) recognizes natural ecological systems and processes and ensures they are maintained and enhanced;
  - ii) minimizes disruption to existing landforms and landscape features including vegetation, wetlands, steep slopes and groundwater discharge areas through such approaches as clustering works on less sensitive parts of the site;
  - iii) will result in no changes to the natural quality and quantity of ground and surface water resources;
  - iv) will result in the maintenance and/or enhancement of existing aquatic ecosystem functions;
  - v) will result in the maintenance and/or enhancement of existing terrestrial ecosystem functions; and,
  - vi) the required setbacks established in accordance with the policies of subsection H2.12.3 of this Plan can be met.

#### H2.12.6 DEVELOPMENT EVALUATION CRITERIA

The Town in consultation with the Region of Halton and Credit Valley Conservation may require as a basis for the evaluation of any proposed development within or adjacent to lands designated *Greenlands* the submission of:

- a) appropriate engineering studies conducted by the applicant to the satisfaction of the Town, in consultation with the Region of Halton, and Credit Valley Conservation, which address:
  - i) the existing environmental and/or physical hazards which affect the site;

- ii) the potential impact of such hazards;
  - iii) the potential impact of the proposed works on the lands and features in the *Greenlands* designation, particularly the natural quality and quantity of ground and surface water functions and resources; and,
  - iv) techniques and management practices so the identified impacts may be mitigated.
- b) an Environmental Impact Study prepared to the satisfaction of the Town in consultation with the Region of Halton and the Credit Valley Conservation, in accordance with Section C2 of the Halton Hills Official Plan. Reference should be made to the Region of Halton's Environmental Impact Assessment Guidelines when preparing terms of reference for the study and when undertaking and completing the study; and,
- c) a detailed site plan, and landscaping and grading plans prepared to the satisfaction of the Town which demonstrate:
  - i) how natural ecological, valleyland and erosional systems and processes will be maintained; and,
  - ii) how disruption to existing landform and landscape features will be minimized.

#### H2.12.7 ZONING

The boundaries of the *Greenlands* are delineated on Schedule H2 based on the best mapping currently available. The boundaries shall be delineated in the Zoning By-law and may be refined at the time of a development application based on studies and/or field investigations conducted by the proponent and approved by the Town, in consultation with the Region of Halton, Credit Valley Conservation and Provincial agencies. Refinement to the boundaries will not require an amendment to this Plan.

The Zoning By-law shall also incorporate general setbacks for lot lines, buildings, structures, parking areas and other similar facilities from the *Greenlands* designation in relation to the extent and severity of the natural environmental features and ecological functions of the area. This general setback will be evaluated for specific development applications by the Town in consultation with the Region of Halton and the Credit Valley Conservation, and as established in the Zoning Bylaw, to provide the appropriate setback required to protect the area from the impacts of construction.

#### H2.12.8 OWNERSHIP

Where any land designated as *Greenlands* is held under private ownership, this Plan shall not be construed as implying that such areas are free and open to the general public nor that the lands shall be purchased by the Town or other public agency; although the Region and the Town shall ensure that consideration be given to the acquisition of the critical parts of the *Greenlands* designation through the development approval process as permitted by legislation. Existing agricultural operations within the *Greenlands* designation will be recognized.

#### H2.12.9 PARKLAND DEDICATION

Where new development is proposed on a site, part of which is designated *Greenlands*, such lands shall not be considered acceptable as part of the dedication for park purposes as required by the Planning Act.

## H2.13 REGULATORY FLOODPLAIN OVERLAY

### H2.13.1 PURPOSE AND EFFECT

The purpose of the Floodplain Overlay is to identify developed properties which are located within the Regulatory Floodplain and identify that the properties are subject to the Credit Valley Conservation Authority's Regulation of Development, Interference with Wetlands and Alterations to Shorelines and Watercourses (160/06) as may be amended.

Properties subject to the Regulatory Floodplain Overlay are shown on Schedule H2 to the Secondary Plan.

### H2.14 IMPLEMENTATION

The provisions of the Halton Hills Official Plan and the Planning Act, as amended from time to time, regarding implementation, shall apply in regard to this Plan.

### INTRODUCTION

The Georgetown GO Station Secondary Plan is an integral part of the Town's planning initiatives related to the implementation of the Sustainable Halton Plan, an exercise being carried out by the Region of Halton to address the requirements of the Greater Golden Horseshoe Growth Plan. It also builds on the policy framework of the Town's Official Plan and implements the results of the Georgetown GO Station Area Land Use Study. The Secondary Plan provides detailed policy direction with respect to the redevelopment of strategic locations in the Secondary Plan area, and the maintenance and enhancement of the neighbourhood as a whole.

### STRUCTURE OF THE SECONDARY PLAN

The Introduction is considered part of the Secondary Plan. The Plan is further divided into seven sections:

#### SECTION H3.1 - VISION STATEMENT

Section H3.1 which provides a clear vision for the Secondary Plan area in the context of the Town's overall Community Vision, Goals and Strategic Objectives.

#### SECTION H3.2 - GOALS

Section H3.2 outlines overall goals applicable to the Secondary Plan area.

#### SECTION H3.3 - GENERAL POLICIES

Section H3.3 provides general policies related to matters such as transportation, urban design and sustainability applicable to the Secondary Plan area.

#### SECTION H3.4 - GO SOUTH PRECINCT OBJECTIVES AND POLICIES

Section H3.4 contains objectives and policies specific to the South Precinct of the Secondary Plan area.

#### SECTION H3.5 - GO NORTH PRECINCT OBJECTIVES AND POLICIES

Section H3.5 contains objectives and policies specific to the North Precinct of the Secondary Plan area.

#### SECTION H3.6 - MILL STREET CORRIDOR PRECINCT OBJECTIVES AND POLICIES

Section H3.6 contains objectives and policies specific to the Mill Street Corridor Precinct of the Secondary Plan area.

#### SECTION H3.7 - MAJOR TRANSIT STATION PRECINCT

Section H3.7 contains objectives and policies specific to the Major Transit Station Precinct of the Secondary Plan area.



## SECTION H3.8 - IMPLEMENTATION AND ADMINISTRATION

Section H3.8 describes how the Secondary Plan will be implemented and administered.

### SCHEDULES

In addition, Schedule H3 (Georgetown GO Station Area Land Use Plan) is to be read in conjunction with the Secondary Plan and constitute an operative part of the Secondary Plan.

### LOCATION

The Secondary Plan affects all the lands in the area identified on Schedule H3 which is generally bounded by:

- a) Northeast: Mountainview Road North;
- b) Southeast: Maple Avenue;
- c) South and Southwest: Guelph Street (Highway 7); and,
- d) Northwest: John Street and CNR Rail.

### H3.1 VISION STATEMENT

The GO Station/Mill Street Corridor Area will provide opportunities for redevelopment and intensification at strategic locations, and enhanced connections to other areas of the community, particularly the *Downtown Area*, where such connections will not be to the detriment of the existing community. This redevelopment and intensification will have a positive impact on the quality of life in Halton Hills, including support for the Town's economic and housing objectives, while still ensuring that the character of these existing, well established residential areas are maintained and enhanced, including improvements which will mitigate the impact of traffic on the community.

### H3.2 GOALS

- a) To maintain and enhance existing residential areas, with intensification focused primarily on strategic redevelopment sites.
- b) To protect and enhance the environmental function of Silver Creek and its related valleylands, while providing improved access, where appropriate, for residents through the introduction of trails and other features.
- c) To introduce an improved circulation system for pedestrians and cyclists, as well as improvements in accessibility for vehicular traffic, both within the Secondary Plan area and to other parts of Georgetown.
- d) To ensure the highest quality urban design forms the basis for new development in both the public and private realms of the Secondary Plan area.
- e) To ensure that the necessary capacity is available in the Town's infrastructure for any proposed redevelopment, recognizing the need for stormwater management controls which are appropriate for the proposed redevelopment and intensification.

### H3.3

#### GENERAL POLICIES

The general policies of the Official Plan shall apply to the Secondary Plan area, particularly:

- a) Section B1A *Greenlands* within existing Urban Areas;
- b) Part C Environmental Management Policies;
- c) Part F General Development Policies, particularly F2 Urban Design, F6 Transportation, F7 Public Parkland, F8 Community Facilities and Services, and F10 Development Phasing Strategies; and,
- d) Part G Plan Implementation and Administration, particularly G3 Secondary Plans and More Detailed Plans, G4.3 Height and Density Bonusing, G7 Community Improvement Plans, G8 Site Plan Control, and G12 Pre-Consultation and Complete Application Requirements.

Decision Withheld

Only for the population and employment targets to the year 2031.

#### H3.3.1

##### POPULATION AND EMPLOYMENT TARGETS

The population target for the Secondary Plan Area to the year 2031 is 2,800 with the majority, in the order of 2,200, being planned for the North Precinct on the lands designated *High Density Residential/Mixed Use Area I* and *High Density Residential/Mixed Use Area II*.

The employment target for the Secondary Plan area to the year 2031 is 300 jobs, all of which will be found in the North Precinct, the majority in the *High Density Residential/Mixed Use Area I* and II designations.

Any site-specific Official Plan or zoning by-law amendment to reduce development density in the Secondary Plan area is prohibited unless it is part of a municipal comprehensive review of the Official Plan or review of the Secondary Plan Area.

#### H3.3.2

##### GREENLANDS

- a) Location

The *Greenlands* designation on Schedule H3 includes the lands within the Regulatory Floodplain along Silver Creek, as determined by Credit Valley Conservation.

- b) Permitted Uses

The permitted uses shall be those in Section B1.3.1.2 of the Official Plan.

- c) Enhancement of Silver Creek Corridor

The lands designated *Greenlands* form part of a key natural system extending beyond the Secondary Plan area, and are the only significant natural resource in the area. The Town shall work with Credit Valley Conservation in the context of the Silver Creek Subwatershed Study to:

- i) implement measures to enhance the biodiversity and environmental integrity of this area;
- ii) develop a public trail system in and/or adjacent to the valley, in conformity with the policies of Sections B1.2.7 and F7 of the Official Plan; and,
- iii) consider opportunities for the creation of open space areas in either public or private ownership, adjacent to the *Greenlands* designation. These areas will assist in providing additional protection to the valley, as well as enhancing views, and potentially access, into the valley.

#### Regional Natural Heritage System

Notwithstanding the policies pertaining to the Greenlands System contained in Section 3.3.2 of this Plan, the *Greenlands* designation within the Georgetown GO Station Area is subject to the policies contained in Sections 115.2, 115.3, 115.4(2), 116, 116.1, 117.1, 118(1.1), 118(2), 118(3), 118(4), 118(5) through 118(13), 139.11, 139.12, Map 1G and the applicable definitions of the Regional Official Plan, which are hereby incorporated by reference into this Plan.

### H3.3.3 CEMETERY

The *Cemetery* designation on Schedule H3 recognizes Greenwood Cemetery, a public cemetery operated by the Town. This area is subject to the policies of the Official Plan, in particular Section B2.4.2, Public Cemeteries.

### H3.3.4 PARKS

The *Parks* designation on Schedule H3 identifies the existing local parkland. These facilities are limited and, with additional development proposed for the Secondary Plan area, the Town shall, in accordance with the policies of Section F7 of the Official Plan:

- a) develop plans for the enhancement of these existing facilities; and,
- b) as part of any proposed development:
  - i) require the dedication of land for the creation of additional parkland, particularly for lands in the High Density Residential/ Mixed Use Area I and II designations in the North Precinct; and/or,
  - ii) use cash-in-lieu of parkland to expand or improve existing parkland and/or to create new parkland, including on lands adjacent to Silver Creek to enhance that resource.

### H3.3.5 LOW DENSITY RESIDENTIAL AREA

The lands in the *Low Density Residential Area* designation on Schedule H3 shall develop in accordance with the provisions of Section D1.3.1 and D1.4.2 of the Official Plan. In addition, in evaluating applications for new development which require an amendment to, or variance from, the zoning bylaw or consent, the following additional criteria shall be considered:

- a) lotting pattern, proposed placement and design of buildings and structures, including additions and facilities such as an air conditioner, on the lot, will promote privacy on adjacent lots,

minimize noise and shadow impacts, and respect the existing pattern of setbacks in the immediate area including back yard setbacks;

- b) the Town is satisfied with the proposed grading, drainage and stormwater management including lot level controls, and, in particular, there shall be no impact related to these factors on adjacent properties;
- c) garages are located so they are not the dominant feature in the streetscape, and new buildings relate to the street; and,
- d) a new residence incorporates measures which enhance sustainability in particular construction to an ENERGY STAR standard at minimum or an equivalent alternative.

### H3.3.6 URBAN DESIGN

The Urban Design policies of Section F2 of the Official Plan provide the general framework for consideration of any proposals for changes in the public realm, as well as private development proposals. In addition, the following specific directions will be considered:

#### a) Public Realm

The primary component of the public realm in the Secondary Plan area is the street system. The Town, in accordance with the policies of Section F2.2.1.1 of the Official Plan, shall develop a specific plan for the improvement of the design of the streetscape in the Secondary Plan area, which reflects the policies of the Official Plan and this Secondary Plan. In particular, the plan will include:

- i) the recommendations of the pedestrian/cycling plan prepared in accordance with the direction in Section H3.3.7 of this Plan;
- ii) traffic calming techniques which balance the need for access to the GO Station with the creation of a safer environment for pedestrians and bicyclists;
- iii) enhancement of the tree lined boulevards;
- iv) the creation of specific gateways at key locations identified on Schedule H3 in accordance with the provisions of subsection b) of this Section;
- v) provisions for working with GO Transit, Via Rail and CN to enhance the GO Station area and its relationship with the surrounding area; and,
- vi) consideration of the character of Guelph Street and the relationship between development in the Secondary Plan area and the lands on the other side of the street in the *Downtown Area*.

#### b) Gateways

Key Gateways to the Secondary Plan area are designated on Schedule H3 at the following intersections:

- Queen Street and Guelph Street;

- King Street and Mountainview Road North;
- Queen Street and King Street;
- River Drive and Mountainview Road North; and,
- Mill Street and Guelph Street.

The Town will develop plans for the streetscape in these areas which incorporate upgraded street furniture, appropriate signage, and landscaping as well as special pavement treatments to reflect their role. In addition, consideration should be given to the introduction of non-habitable structures such as a clock tower, decorative poles with banners and/or specialized lighting to serve as a focal point at the gateway at Queen and King Streets which serves as the entrance to the GO Station in the South Precinct, and, as part of the redevelopment of the station in the North Precinct.

In addition, development on lots fronting and flanking these intersections shall be designed so that any buildings reinforce the streetscape edge and have upgraded elevation treatments and gateway features.

c) Redevelopment Sites

Significant redevelopment shall only be permitted on certain strategic sites, as identified on Schedule H3, subject to the relevant policies in Sections H3.5.5, H3.5.6 and H3.6.5. In addition, with respect to such development, careful attention shall be given to:

- i) the relationship between the proposed new development and existing, adjacent *Low Density Residential Areas* to minimize potential height and massing impacts and to protect privacy;
- ii) the relationship of the proposed new development in the North Precinct to the Secondary Plan area as a whole, and surrounding lands generally, taking into account the topography of the area;
- iii) the relationship of the buildings to the street to ensure that the development form reinforces the streetscape with the front entrance oriented to the street, and allows for suitable sidewalk and boulevard widths for pedestrian use and the placement of streetscape amenities; and,
- iv) the relationship of adjacent buildings to the rail corridor to minimize noise and vibration impacts.

In this regard, massing strategies such as the stepping down of buildings to buildings of lower density and the use of two or three storey base podiums, with increased setbacks for storeys above the podium, rather than “slab” building forms, will be employed to minimize impacts, overlook, shadowing, and high wind speeds.

d) Building Elements

Development shall be designed to reflect the character of the surrounding area. In particular:

- i) materials and colours for new buildings and additions or renovations to existing structures should complement and be compatible with adjacent buildings;
  - ii) the facades of buildings should have windows which overlook streets, lanes and parking areas in order to maintain a safe pedestrian environment;
  - iii) the main access to buildings or a group of buildings should face a street;
  - iv) pedestrian entrances should be spatially and architecturally prominent and welcoming;
  - v) buildings on corner lots should have upgraded elevation treatments for both front and flankage;
  - vi) blank walls are discouraged in situations which are exposed to public view. Where such walls are essential upgraded design and material standards are required; and,
  - vii) fine architectural detailing in building facades should be part of any new building or major addition to complement the streetscape.
- e) Landscape, Parking and Service Areas
- i) Surface parking lots should be screened from abutting streets through the use of low walls, decorative fences, planters, low shrubs or other landscaping, without compromising sightlines or maintenance flexibility.
  - ii) Surface parking areas should be visually softened by introducing landscape islands, planters and other landscaping, as well as clearly defined pedestrian routes.
  - iii) All parking areas should be well lit to ensure pedestrian safety, but lighting should be designed to avoid light spillage into adjacent residential areas.
  - iv) Ramps to underground parking areas should not detract from the façade or landscaping of the building.
  - v) Parking shall generally not be permitted between buildings, other than a driveway for low and medium density residential uses, and the street. Landscape planting shall be provided between the street and the building to frame the building.
  - vi) The planting of shade trees shall be encouraged.
  - vii) Service and loading areas should be located away from the primary building face and public view and integrated within buildings.
  - viii) Appropriate screening of service and loading areas should be incorporated.
  - ix) Open storage is prohibited with the exception of open storage related to railway uses. Where required, such uses should not front onto a public street and should be screened using landscaping or built screens or a combination of both.
  - x) Consideration should be given to the creation of a berm and noise attenuation wall along the railway in conformity with the design specifications of CNs Principal Mainline Requirements. The design should soften the visual impact of the wall on both the side

adjacent to development and the rail side to contribute to the ambience of the GO Station.

f) Low Density Residential Areas

No significant changes are planned for the majority of the Secondary Plan area. As stable residential areas, any proposed changes will be reviewed to ensure that the character of the surrounding area and the streetscape is maintained and/or enhanced in accordance with the policies of this Plan.

### H3.3.7 TRANSPORTATION

The Transportation policies of Section F6 of the Official Plan are applicable to the Secondary Plan area. In addition, the following additional policies are applicable:

a) Pedestrian and Cycling Routes and Facilities

The Town shall prepare a pedestrian/cycling plan for the Secondary Plan area which will address enhanced pedestrian/cycling connections to ensure that there is a well-connected system that ensures pedestrian and cyclist safety both for the existing and the ultimate development of the area. The establishment of pedestrian and cycling connections to serve any redevelopment shall be a condition of approval. The system will be designed to maximize:

- i) connectivity within the Secondary Plan area, including between the North and South Precincts. In that regard the Town will work with Metrolinx and the CNR to create improved grade separated and barrier free access across the railway tracks generally in the vicinity of the crossings identified on Schedule H3 and outside the GO Station Paid Fare areas;
- ii) connectivity between the Secondary Plan area and other areas of the Town, particularly the Downtown;
- iii) accessibility to the GO Station both north and south of the railway tracks; and,
- iv) bicycle storage facilities.

b) Road Network

The road network for the Secondary Plan area is established on Schedules B1 and B2 of the Official Plan. However, in addition, as part of any redevelopment application in the North Precinct, a traffic analysis shall be required including a review of possible modifications to the road network. Modifications which shall be considered through such an analysis include:

- i) creation of additional road connections through the area around the GO Station to improve traffic movement and enhance the ability for area residents, GO patrons and GO buses to access the station and/or the proposed new development. Two potential connections are proposed, the extension of St. Michaels Street to Rosetta Street; and the extension of St. Michaels Street to John Street as designated on Schedule H3. The extension of St. Michaels Street to Rosetta Street will require the acquisition of lands or an easement from the railway;

- ii) changes to the intersections of River Drive and Mountainview Road North, and Mountainview Road and Maple Avenue, including signal timing modifications and possibly the extension of the north and/or west bound left turn tapers;
  - iii) improvements where feasible westbound on Maple Avenue west of Mountainview Road; and,
  - iv) potential improvements to the intersections of Mountainview Road North and John Street and Victoria Street and John Street.
- c) Rail Network and Related Facilities

The rail line which runs through the Secondary Plan area is designated on Schedule B1 to the Official Plan and Schedule H3 as Commuter Rail Corridor to reflect its existing and planned role. The GO Station, which is designated as a Major Transit Station on Schedule B1 to the Official Plan and Schedule H3, is a major transit station. The Town recognizes the significance of the GO station and related facilities to the community as a whole, both as a transportation facility and as a focus for intensification. The Town will work with Metrolinx to ensure both that the use of the station is maximized, and that impacts on adjacent residential development are minimized.

In particular, the Town will ensure that:

- i) in conformity with Sections C15 and F6.7 c) of the Official Plan, all proposed development adjacent to, or in proximity to, the railway corridor (including the adjacent layover), is based on the implementation of required noise and vibration studies which take into account CNs Principal Main Line Requirements (PMLR);
  - ii) where new development abuts railway property fencing will be required generally in accordance with the PMLR;
  - iii) the pedestrian/cycling plan is designed to maximize accessibility to the railway station; and,
  - iv) redevelopment of the sites in the North Precinct adjacent to the tracks is designed to be coordinated with the station use including the provision of services and facilities for transit users such as commercial uses, “kiss and drive” facilities, shared parking, and bus stop areas.
- d) Railway Buffer

The Railway Buffer overlay on Schedule H3 identifies the general area where a setback is required from the edge of the railway corridor by the CNR and/or Metrolinx for all buildings and structures. The precise setback will be determined at the time of application in accordance with the PLMR, but is generally a minimum of 30 metres. In some cases, consideration may be given to a crashwall and vertical/horizontal setback combination. The Town will work with CN to maximize the development opportunities in this area, recognizing its intensification potential, without compromising safety considerations.



e) Off- Street Parking

Where redevelopment is proposed in the North Precinct, the Town will work with the proponent and GO Transit to develop a parking strategy which may include the reduction of parking requirements based on a parking analysis which examines factors such as:

- i) the impacts of the proximity to the GO Station;
- ii) the type of uses, and a determination that they require parking at different times of day, including consideration of situations where parking is shared by the GO Station and the adjacent uses;
- iii) the availability of public off-street parking; and,
- iv) the potential for, and scale of, above-ground or underground parking garages.

In addition, based on this analysis, the Town may give consideration to other approaches to the provision of parking in this area including the use of cash-in-lieu of required parking, to allow for the establishment of a reserve fund for the creation, improvement or expansion of public parking facilities.

### H3.3.8 MUNICIPAL WATER AND WASTEWATER SERVICES

The policies of Section F8.2 of the Official Plan are applicable to municipal water and wastewater services.

### H3.3.9 STORMWATER MANAGEMENT

The Secondary Plan area currently has existing, limited stormwater management facilities in place, however the nature of the current development and proposed redevelopment preclude directing runoff to the existing stormwater management facilities.

Therefore, consideration by the Town, in consultation with Credit Valley Conservation, shall be given in the review of all new development, to lot-level controls with a potential for conveyance infrastructure improvements, as well as where appropriate, end-of-pipe retrofits. In addition, the Town may require cash-in-lieu of on-site stormwater management facilities. Further, where any proposed alterations to the existing drainage pattern affect railway lands concurrence must be received from CN based on submission of a drainage report.

### H3.3.10 UTILITIES

The Town will confirm with all utility providers that adequate servicing networks are, or will be established, to serve the anticipated development and that these networks can be phased in a manner that is cost-effective and efficient.

### H3.4 GO SOUTH PRECINCT OBJECTIVES AND POLICIES

#### H3.4.1 OBJECTIVES

- a) To recognize the majority of the South Precinct, with the exception of the lands adjacent to Guelph Street, as a stable residential area where only modest changes in keeping with the existing character of the area will be permitted.
- b) To encourage consolidation of lots on or immediately adjacent to Guelph Street and their redevelopment for medium/high density residential uses.

#### H3.4.2 LOW DENSITY RESIDENTIAL AREA

The lands in the *Low Density Residential Area* designation shall develop in accordance with the provisions of Section H3.3.5 of this Secondary Plan.

#### H3.4.3 MEDIUM DENSITY RESIDENTIAL AREA

The lands in the *Medium Density Residential Area* designation shall develop in accordance with the provisions of Section D1.3.2 of the Official Plan. In addition, in evaluating applications for new development through the site plan process, plans and three dimensional computer models as well as relevant studies and other information shall be submitted which demonstrate that the design of the development:

- a) minimizes height and massing impacts on existing, adjacent low density residential areas;
- b) contributes to the creation of a streetscape on Guelph Street which reflects its role as an Intensification Corridor and a major access route through the community, as well as to the GO Station and the Downtown by:
  - i) requiring a substantial building façade fronting on Guelph Street to define the street edge,
  - ii) encouraging design that promotes a pedestrian oriented streetscape, including variations in the building envelope for entrances, forecourts and other features;
  - iii) prohibiting parking between the building and Guelph Street;
  - iv) prohibiting blank walls on any street; and,
  - v) requiring provision of high quality landscaping, including the protection, where feasible, of existing shade trees on the site, and the planting of new shade trees on the site and in the public right-of-way as directed by the Town; and,
- c) incorporates measures which enhance sustainability in particular construction to an ENERGY STAR standard at minimum or equivalent alternative.

#### H3.4.4 EMPLOYMENT AREA

The *Employment Area* designation on Schedule H3 recognizes three lots abutting the railway which are used for existing industrial uses. The designation recognizes and permits the use of the lots and existing buildings, and additions to the buildings, for industrial, office and other non-residential uses including

retail and service commercial uses. However, should the buildings be demolished, any new development will be subject to the railway buffering policies in Section H3.3.7 d).

### H3.5 GO NORTH PRECINCT OBJECTIVES AND POLICIES

#### H3.5.1 OBJECTIVES

- a) To encourage the intensification and redevelopment of the existing brownfield lands immediately adjacent to the railway corridor and commuter rail station for high density residential with a mixed use component.
- b) To recognize the potential for the development of the lots fronting on the proposed intensification sites for medium density residential and/or office uses to complement the adjacent high density residential development.
- c) To recognize the remaining portions of the North Precinct as a stable residential area where only modest changes in keeping with the existing character of the area will be permitted.

Decision withheld, only on lands legally described as 60 John Street, Georgetown.

#### H3.5.2 LOW DENSITY RESIDENTIAL AREA

The lands in the *Low Density Residential Area* designation shall develop in accordance with the provisions of Section H3.3.5 of this Secondary Plan.

#### H3.5.3 MEDIUM DENSITY RESIDENTIAL AREA

The lands in the *Medium Density Residential Area* designation recognize an area of existing medium density housing and shall develop in accordance with the provisions of Section D1.3.2 of the Official Plan.

#### H3.5.4 MEDIUM DENSITY RESIDENTIAL/OFFICE AREA

##### a) Main Permitted Uses

The main permitted uses in the *Medium Density Residential/Office Area* are limited to:

- existing single detached dwellings;
- duplex dwellings;
- triplex dwellings;
- quattroplex dwellings;
- multiple dwellings;
- street townhouse dwellings;

- commercial/residential or office use in a single detached or street townhouse dwelling; and,
  - office use in a permitted dwelling.
- b) Density and Height
- The maximum permitted density shall not exceed 35 units per residential hectare and the maximum building height shall not exceed three storeys.
- c) Complementary Uses
- Complementary uses that may be permitted in the *Medium Density Residential/Office Area* include the following:
- home occupations subject to Section D1.3.1.4 of the Official Plan; and,
  - local parkland subject to Section F7.3.4 of the Official Plan.
- d) New Development
- Where appropriate, new uses shall be encouraged to locate in existing buildings. However, where new development is proposed, including any significant additions and the creation of new lots, the provisions of Section D1.4.2 of the Official Plan shall be used to evaluate such proposals, in addition to the following additional criteria:
- i) lotting pattern, proposed placement and design of buildings and structures, including additions and facilities such as an air conditioner, on the lot, will promote privacy on adjacent lots, minimize noise and shadow impacts, and respect the existing pattern of setbacks in the immediate area including back yard setbacks;
  - ii) the Town is satisfied with the proposed grading, drainage and stormwater management including lot level controls, and, in particular, there shall be no impact related to these factors on adjacent properties;
  - iii) garages are located so they are not the dominant feature in the streetscape, and new buildings relate to the street; and,
  - iv) incorporates measures which enhance sustainability in particular construction to an ENERGY STAR standard at minimum or equivalent alternative.

### H3.5.5 HIGH DENSITY RESIDENTIAL/MIXED USE AREA I

a) Main Permitted Uses

The main permitted uses in the *High Density Residential/Mixed Use Area I* are limited to:

- apartment dwellings;
- long term care facilities and retirement homes;
- mixed use buildings including high density residential, office and a full range of commercial uses; and,

- parking facilities, including above ground and underground parking garages, which may include a public parking component for GO patrons.

b) Density and Height

A minimum Floor Space Index (FSI) of 2.0 and maximum FSI of 3.0 for the site as a whole, with a maximum height of 12 storeys for certain portions of the site is permitted. Building heights will vary across the site as specified in Section H3.3.6 (c) of this Plan, in order to ensure that there is no significant visual impact on the surrounding area.

In addition, the maximum density and height may be further increased through the development process to a maximum FSI of 3.5 for the site and a maximum height of 16 storeys for some portions of these lands subject to the provisions of Section G4.3 of the Official Plan, and provided that the Town is satisfied that there will be no significant impact on the surrounding area, based on the submission of:

- detailed plans and models; and,
- a traffic analysis addressing the issues identified in Section H3.3.7.

c) Bonusing

In addition to the bonusing triggers in Section G4.3.1 of the Official Plan, the following additional bonusing triggers are applicable:

- development which reflects the principles of sustainable development by including three or more significant attributes which encourage energy efficiency, conservation of resources and minimization of environmental impacts (e.g. green or reflective roofs, LEED certification, water recycling);
- provision of parking, direct pedestrian links to the GO Station, and other facilities which are shared with the GO Station and provide a service to transit users; and,
- development of public parkland and/or private open space which is designed to be accessible to the public, as well as residents of the development; and,
- provision of affordable and special needs housing.

d) Complementary Uses

Complementary uses that may be permitted in the *High Density Residential/Mixed Use Area I* designation include the following:

- home occupations subject to Section D1.3.1.4 of the Official Plan; and,
- local parkland subject to Section F7.3.4 of the Official Plan.

### H3.5.6 HIGH DENSITY RESIDENTIAL/MIXED USE AREA II

#### a) Main Permitted Uses

The main permitted uses in the *High Density Residential/Mixed Use Area II* are limited to:

- apartment dwellings;
- long term care facilities and retirement homes;
- mixed use buildings including high density residential, office and a full range of commercial uses; and,
- parking facilities, including above ground and underground parking garages, which may include a public parking component for GO patrons.

#### b) Timing, Density and Height

The redevelopment of areas within this designation, for any residential development, shall be prohibited either until the relocation of the rail layover facility or until studies are submitted that satisfy the Town, in consultation with CN and Metrolinx, that noise and vibration can be appropriately mitigated in conformity with Ministry of Environment guidelines.

With respect to density and height, a minimum Floor Space Index (FSI) of 1.8 and a maximum FSI of 2.0, with a maximum height of 8 storeys, is permitted. In addition, the maximum density and height may be further increased through the development process to a maximum FSI of 2.5 for sites within this designation, and a height of 12 storeys for some portions of sites within this designation, subject to the provisions of Section G4.3 of the Official Plan, and provided that the Town is satisfied that there will be no significant impact on the surrounding area, based on the submission of detailed plans and models, and a traffic analysis addressing the transportation issues identified in Section H3.3.7. In addition to the bonusing triggers in Section G4.3.1 of the Official Plan, the additional triggers in Section H3.5.5 (c) of this Plan are applicable.

#### c) Complementary Uses

Complementary uses that may be permitted in the *High Density Residential/Mixed Use Area II* designation include the following:

- home occupations subject to Section D1.3.1.4 of the Official Plan; and,
- local parkland subject to Section F7.3.4 of the Official Plan.

### H3.6 MILL STREET CORRIDOR PRECINCT OBJECTIVES AND POLICIES

#### H3.6.1 OBJECTIVES

- To encourage consolidation of lots on or immediately adjacent to Guelph Street and along the Mill Street and rail corridors and their redevelopment for medium density residential uses.
- To permit the redevelopment of certain larger lots in appropriate locations, for medium and/or high density residential uses that are compatible with the surrounding area.

- c) To recognize the potential for the redevelopment of the Memorial Arena site and adjacent lands for high density residential, community facility and related uses.
- d) To recognize the remaining portions of the Mill Street Corridor Precinct as a stable residential area, where only modest changes in keeping with the existing character of the area will be permitted.

### H3.6.2 LOW DENSITY RESIDENTIAL AREA

The lands in the *Low Density Residential Area* designation shall develop in accordance with the provisions of Section H3.3.5 of this Secondary Plan.

### H3.6.3 MEDIUM DENSITY RESIDENTIAL AREA

The lands in the *Medium Density Residential Area* designation shall develop in accordance with the provisions of Section D1.3.2 of the Official Plan. In addition, in evaluating applications for new development through the site plan process, plans and three dimensional computer models shall be submitted which demonstrate that the design of the development:

- a) minimizes height and massing impacts on existing, adjacent low density residential areas; and,
- b) contributes to the creation of a streetscape on Guelph Street which reflects its role as an Intensification Corridor and major access route through the community, as well as to the GO Station and the Downtown by:
  - i) requiring a substantial building façade fronting on Guelph Street to define the street edge,
  - ii) encouraging design that promotes a pedestrian oriented streetscape, including variations in the building envelope for entrances, forecourts and other features;
  - iii) prohibiting parking between the building and Guelph Street;
  - iv) prohibiting blank walls on any street; and,
  - v) requiring provision of high quality landscaping, including the protection, where feasible, of existing shade trees on the site, and the planting of new shade trees on the site and in the public right-of-way as directed by the Town;
- c) contributes to the creation of a streetscape along Mill Street which reflects its importance as a key gateway to the Secondary Plan area, while maintaining the residential character of the street and enhancing the protection of the greenlands area along the Silver Creek by:
  - i) requiring new buildings to define the street edge;
  - ii) encouraging design that promotes a pedestrian oriented streetscape, including variations in the building envelope for entrances, forecourts and other features;
  - iii) prohibiting parking between the building and Mill Street;
  - iv) prohibiting blank walls on any street;

- v) requiring provision of high quality landscaping, including:
  - the protection, where feasible of existing shade trees on the site, and the planting of new shade trees on the site and in the public right-of-way as directed by the Town;
- vi) the introduction of naturalized areas with native plants and trees on and adjacent to, lands designated *Greenlands A*; and,
- vii) requiring buffers adjacent to the boundary of lands designated *Greenlands* on Schedule H3, as determined by the Town after consultation with Credit Valley Conservation; and,
- d) incorporating measures which enhance sustainability in particular construction to an ENERGY STAR standard at minimum, or an equivalent alternative.

#### H3.6.4 MEDIUM/HIGH DENSITY RESIDENTIAL AREA

The *Medium/High Density Residential Area* designation on Schedule H3 recognizes existing or potential sites for the development of townhouses or low-rise apartments at medium to high density, which is compatible with the surrounding area.

a) Main Permitted Uses

The main permitted uses in the *Medium/High Density Residential Area* designation are limited to:

- triplex or quattroplex dwellings;
- multiple dwellings;
- street or block townhouses;
- apartment dwellings; and,
- long term care facilities and retirement homes.

b) Density and Height

A density range of 35 to 75 units per net residential hectare, and a maximum height of 5 storeys, is permitted.

c) Complementary Uses

On properties adjacent to Guelph Street, complementary uses that may be permitted on the first floor of a residential building include the following:

- retail and service commercial uses;
- medical offices;



- day nurseries;
- private and commercial schools;
- places of worship and other institutional uses; and,
- restaurants.

In addition, an existing place of worship shall continue to be permitted as part of the redevelopment of a property adjacent to Guelph Street for a mixed use development which includes one or more of the main permitted uses and which may include one or more of the complementary uses.

d) Compatibility with Surrounding Uses

In evaluating applications for new development through the site plan process, plans and three dimensional computer models shall be submitted which demonstrate that the design of the development:

- i) minimizes height and massing impacts on existing, adjacent low density residential areas; and,
- ii) for designated sites on Guelph Street, meets the requirements contained in Section H3.6.3 (b) of this Plan.

### H3.6.5 HIGH DENSITY RESIDENTIAL/COMMUNITY FACILITY AREA

a) Main Permitted Uses

The main permitted uses in the *High Density Residential/Community Facility Area* are limited to:

- institutional buildings;
- apartment dwellings;
- long term care facilities and retirement homes; and,
- mixed use buildings including high density residential, and community facilities, as well as ancillary retail and service commercial uses, including restaurants and offices.

b) Density and Height

A minimum Floor Space Index (FSI) of 1.8 and maximum FSI of 2.0, with a maximum height of 8 storeys, is permitted. The existing place of worship building on Guelph Street is a heritage building and shall be preserved. However, the density applicable to the building and immediately adjacent lands, as determined by the Town, may be transferred to other portions of the *High Density Residential/Community Facility Area* designation.

c) Complementary Uses

Complementary uses that may be permitted in the *High Density Residential/Community Facility Area* include the following:

- home occupations subject to Section D1.3.1.4 of the Official Plan; and,
- local parkland subject to Section F7.3.4 of the Official Plan.

### H3.7 MAJOR TRANSIT STATION PRECINCT OBJECTIVES AND POLICIES

#### H3.7.1 OBJECTIVE

To recognize the existing GO Station facility and to encourage modifications to the facility which will allow for increased service while improving its integration with the surrounding residential area.

#### H3.7.2 MAJOR TRANSIT STATION

##### a) Permitted Uses

Permitted uses in the Major Transit Station designation include:

- railway related facilities including the existing heritage railway station, the existing layover facility, any new station building(s), parking, including underground parking or parking garages, open storage, bus storage, “kiss and ride” areas, pedestrian walkways and bicycle parking;
- accessory and ancillary uses, including retail and service commercial uses, offices and restaurants; and,
- in the North Precinct, uses permitted in the *High Density Residential/Mixed Use Area II* designation in accordance with the policies of Section H3.5.6.

##### b) Road Network

The extension of St. Michaels Street to allow it to connect to Rosetta Street, either as a public street on a public right-of-way, or easement or as a private road accessible to the public, shall be permitted and encouraged as part of any relocation of the rail layover facility in the North Precinct.

##### c) Improved Rail Service

Metrolinx plans improved service at the GO Station. The Town recognizes the need for balancing improved services in this location with the mitigation of the impacts, particularly those related to traffic, on the existing residential community; and the potential redevelopment for high density residential uses in the North Precinct.

Consequently, the Town will work with Metrolinx to limit the impacts of the station on the community by encouraging and, where possible, facilitating:

- i) the proposed relocation of the layover facility as soon as possible;
- ii) improved design of both the station and related facilities;

- iii) improved design of the transportation system which provides access to the Station, including improved facilities for pedestrians and cyclists and GO buses, in accordance with Section H3.3.6 and H3.3.7 of this Secondary Plan; and,
- iv) by redirecting, where feasible, GO bus service to the portion of the station in the North Precinct.

### H-H3.8 IMPLEMENTATION AND ADMINISTRATION

The Georgetown GO Secondary Plan will be implemented and interpreted in accordance with the provisions of Part G - Implementation and Administration of the Official Plan. In addition, with respect to the provisions of Section G12, the following additional study requirements are applicable to development proposed in all land use designations, with the exception of the *Low Density Residential Area, Greenlands, Parks and Cemetery* designations on Schedule H3:

- a) Sustainable Development Report – A report outlining the sustainable development measures proposed;
- b) Urban Design Report – A report by an urban design professional which addresses the Vision, goals, objectives and urban design policies of the Secondary Plan;
- c) Shadow Study; and,
- d) Computer 3D modeling of the proposed development and adjacent area.

### H3.9 SPECIAL POLICY AREAS

Special Policy Areas apply to those lands that are the subject of a land use policy that is specific to a property or area. All other relevant policies of this Plan are applicable unless otherwise modified or exempted by the provisions applicable to each Special Policy Area.

#### H3.9.1 SPECIAL POLICY AREA 1

The following policies apply to lands designated as *High Density Residential/Community Facility Area* and identified as Special Policy Area 1, as shown on Schedule H3 of this Plan:

- a) Main Permitted Uses

The main permitted uses in the Special Policy Area 1 are limited to:

- institutional buildings;
- apartment dwellings;
- long term care facilities and retirement homes;
- mixed use buildings including high density residential, and community facilities, as well as ancillary retail and service commercial uses, including restaurants and offices; and,
- local parkland subject to Section F7.3.4 of the Official Plan; and
- Multiple Dwellings; and,

- Townhouse Dwellings

b) Density and Height

A maximum Floor Space Index (FSI) of 1.45, with a maximum height of 5 storeys, is permitted.

c) Parkland

The Town shall require the dedication of 1.0 hectare of land per 300 dwelling units for parkland in Special Policy Area 1 through a combination of cash-in-lieu of parkland and the dedication of a minimum of 0.24 hectares for local parkland at the rear of the site adjacent to Dayfoot Drive.

d) New Development and Redevelopment Policies

The following policies are intended to guide proposals for new development or redevelopment in Special Policy Area 1:

- i) new buildings are designed in a manner that is sensitive to the location, massing and height of adjacent buildings;
- ii) any new building will not compromise the ability to redevelop any adjacent property;
- iii) a high standard of urban design is applied and any new building shall generally conform to the policies set out in Section F2 (Urban Design) of the Town of Halton Hills Official Plan;
- iv) any façade at street level shall incorporate broad window treatments and other architectural features and design elements to maintain an open and interesting pedestrian friendly environment. No blank walls shall be located at street level adjacent to a public road;
- v) blank walls or any portion of the foundation or underground garage exposed due to grade changes on the site shall be appropriately landscaped to provide a visual buffer or screen for adjacent residential properties;
- vi) any unenclosed loading or garbage areas shall be appropriately screened through the use of landscaping or fencing;
- vii) any new residential building is encouraged to provide a range of commercial, institutional and community uses that serve the needs of area residents at the street level along the Mill Street frontage;
- viii) new buildings are required to be set back an appropriate distance from the side and rear lot lines to provide sufficient space for the planning of a new local park adjacent to Dayfoot Drive and at the rear of Special Policy Area 1;
- ix) a new public or private road shall be provided through Special Policy Area 1 to allow access to any new buildings; and
- x) adequate underground parking will be provided on-site.

### H3.9.2 SPECIAL POLICY AREA 2

The main permitted uses in the Special Policy Area 2 are limited to:

- semi-detached dwellings;
- multiple dwellings; and,
- street townhouse dwellings.

### H3.9.3 SPECIAL POLICY AREA 3

A maximum of 14 units shall be permitted within the Medium Density Residential Area designation on lands identified as Special Policy Area 3, shown on Schedule H3 of this Plan.

### H3.9.4 SPECIAL POLICY AREA 4

A maximum of nine residential units consisting of one single detached dwelling, four multiple dwellings and four street townhouse dwellings, with a maximum height of 3 storeys for the single detached dwelling and multiple dwellings and 2 storeys for the street townhouse dwellings, shall be permitted within the Medium Density Residential Area designation on lands identified as Residential Special Policy Area 3, as shown on Schedule H3 of this Plan.

### H3.9.5 SPECIAL POLICY AREA 5

That notwithstanding Policy H3.6.4(b), an apartment building with a maximum height of six-storeys and a maximum permitted Floor Space Index of 1.5 shall be permitted within the Medium/High Density Residential Area designation on lands identified as Special Policy Area 5, as shown on Schedule H3 of this Plan. In addition, the minimum setback from the edge of a railway corridor to a building shall be 20 metres so long as a railway crash wall is provided.

## H4 HAMLET OF GLEN WILLIAMS SECONDARY PLAN

### H4.1 OVERALL GOAL OF THE SECONDARY PLAN

The overall goal of the Glen Williams Secondary Plan is to ensure the retention and enhancement of the natural, cultural and heritage resources of the Hamlet and to guide change so that it contributes to and does not detract from the compact character of the Hamlet, in an environmentally protective and cost effective manner.

### H4.2 OBJECTIVES

The objectives of the Hamlet of Glen Williams Secondary Plan are as follows:

- a) To provide for a rational boundary definition that:
  - i) permits limited growth appropriate to the hamlet;
  - ii) preserves hamlet scale and character; and,

- iii) protects the natural features of the area;
- b) To preserve and build upon the unique heritage character of Glen Williams as a distinct hamlet within the Town of Halton Hills;
- c) To preserve the delicate balance between hamlet development and the protection of environmental features and functions such as landforms, vegetation, water and associated features and groundwater recharge;
- d) To reinforce the importance of visual and physical access to open space, including valleylands, trails and parks, as an integral part of Glen Williams' hamlet design pattern;
- e) To provide improved linkages to the commercial and community facilities of the hamlet core area;
- f) To enhance the vitality of the commercial core and maintain the core as a focal point for the community;
- g) To maintain, enhance and restore the health of the natural environment;
- h) To ensure subdivision design that incorporates a wide variety of lot sizes consistent with the hamlet character and the method of water and wastewater servicing;
- i) To encourage architectural styles that are consistent with the hamlet character and meet a broad range of housing needs;
- j) To provide for growth only where it can be shown not to create any negative fiscal impacts to the Town of Halton Hills; and,
- k) To provide an environmental framework which serves both the existing and future community, which is formed by linking existing open spaces, natural features, and parks and the developed areas of the Hamlet.
- l) To maintain and enhance the character of Mature Neighbourhood Areas by ensuring that new housing, replacement housing, additions, alterations, and new or enlarged accessory buildings, are compatible, context sensitive, and respectful of the existing character of the neighbourhood.

#### H4.3 GENERAL POLICIES

The Hamlet of Glen Williams is graphically illustrated on Schedule H4-1. Schedule H4-1 identifies land use designations that apply to Glen Williams and shall be reviewed in conjunction with the following policies.

##### H4.3.1 HAMLET POPULATION

The year 2021 is the planning horizon for this Plan. A planned population of approximately 2,000 persons for the Hamlet has been determined based upon a limited amount of growth that: maintains Hamlet scale and character; and does not exceed the 2,600 population equivalent reserved in the Georgetown Wastewater Treatment Plant for the Hamlets of Glen Williams, Norval and Stewarttown.

#### H4.3.2

#### PROTECTION AND RESTORATION OF VALLEYLANDS AND NATURAL FEATURES

The Hamlet of Glen Williams has historically developed within the valley of the Credit River. Development is normally prohibited within valleylands given the potential for impacts on the watershed. However, given the establishment of a community in this area it is not possible to strictly limit development given the potential economic hardships that may be encountered. Recognizing this, a balance has been sought between the protection of the valleylands and the allowance of limited development consistent with the character of the Hamlet.

The Regulatory Floodplain has been delineated on Schedule H4-1 in order to identify all areas that are susceptible to flooding. Schedule H4-1 further designates areas of environmental importance as *Greenlands*. Schedule H4-2 categorizes areas identified as *Greenlands* on Schedule H4-1 as either *Core Greenlands* or *Supportive Greenlands*, and also identifies other environmental constraints related to policies in this Plan. Section H4.9 of this Plan defines and provides planning policies for each of these categories.

#### H4.3.3

#### TRAIL SYSTEMS

Glen Williams already has a significant start on a public trail system through the area. The ability to circulate through the community on foot or bicycle is viewed as an important element of the Hamlet character. The Plan therefore provides for the possibility of an extensive network of public pathways intended to link new and existing residential areas with the community core and provides a complement to the Halton Hills Trails and Cycling Master Plan.

Conceptual alignments for potential new trails are shown on Schedule H4-1. For those potential trails identified in the Halton Hills Trails and Cycling Master Plan reference should be made to that document for details.

Where trail development or open space linkages are proposed adjacent to, or within, areas designated *Core Greenlands*, the EIS requirements of Section H4.9.2.5 (b) of this Plan shall apply. A minimum 10 metre top of bank setback from proposed development shall be required to accommodate future trail alignments.

The actual need and implementation of any trail system will be assessed through the development approval process and associated public consultations, and the trails shall be developed to municipal standards.

#### H4.3.4

#### PROTECTION OF FISH HABITAT

The reach of the Credit River through Glen Williams is classified by Credit Valley Conservation as a "mixed cool/cold" water fisheries community. This classification requires a 30 metre development setback from the bank of the watercourse. The exact boundaries of the 30 metre buffer zone should be established in consultation with Credit Valley Conservation.

No new developments are permitted within the 30 metre buffer zone without the approval of Credit Valley Conservation. However, there is extensive historical development within the buffer zone and a prohibition on improvements, replacements or minor additions to buildings is not feasible or desirable. Therefore it is a policy of this Plan that Credit Valley Conservation must be consulted to evaluate impacts and propose mitigation strategies as part of any application for replacement and/or expansion within this area.

Schedule H4-2 also identifies tributaries of the Credit River within the Hamlet that provide fish habitat and are included in the *Core Greenlands* designation. A 15 metre development setback shall be required from warm water tributaries designated as *Core Greenlands* on Schedule H4-1 of this Plan. A 30 metre development setback shall be required from cold water tributaries designated as *Core Greenlands* on Schedule H4-1 of this Plan.

#### H4.3.5 WATER AND WASTEWATER SERVICES

A piped Regional water system currently services the majority of the Hamlet of Glen Williams. No expansions of the water service are permitted without the approval of the Region. All new development shall be serviced by piped Regional water.

The primary method of wastewater servicing for new development within the Hamlet shall be piped Regional wastewater services, with connection to the Georgetown Wastewater Treatment Plant. Unless otherwise provided for in this Plan, all new development shall only proceed on the basis of connection to the Regional sanitary sewage system.

Prior to the approval of any new development on piped water and wastewater services, the Region of Halton and the Town of Halton Hills shall require the submission of a Master Servicing Plan and Financial Implementation Strategy that outlines in detail the approach to the provision of these services and the manner by which they will be funded. Specifically, the Master Servicing Plan will include design and cost estimates for:

- a) water supply and distribution; and,
- b) wastewater treatment and disposal.

The Region of Halton and the Town of Halton Hills shall require developers to enter into cost-sharing agreements for wastewater servicing, as conditions of processing and/or approval of development proposals.

The lands described as being Part of Lots 19 and 20, Concession 10, Town of Halton Hills (Esquesing) known as the “Georgetown Investments Subdivision” and the “Desol lands”, as well as the lands described as being Part of the West Half of Lot 20, Concession 9, Town of Halton Hills (Esquesing) known as the “Devins property” are exempt from the Master Servicing Plan, Financial Implementation Strategy and cost-sharing agreement requirements of this Plan.

Extension of the Regional wastewater servicing system to existing development areas within the Hamlet may be approved by the Region of Halton when it is determined to be feasible and prudent due to public health or environmental concerns.

Existing lots of record and individual lots that are created by consent (land severance) as infilling within existing developed areas may be developed on private, individual wastewater systems. The minimum lot size for new development on private, individual wastewater treatment services shall be 1,900 square metres or larger, as may be determined by a hydrogeological study satisfactory to the Town and Region of Halton.

New development in areas designated in this Plan as *Hamlet Estate Residential Area* may proceed on private individual wastewater treatment systems where it has been determined that the provision of piped Regional wastewater services is not required. Additional treatment for nitrogen, phosphorous and



bacteria consistent with the recommendations of the Scoped Subwatershed Plan for Glen Williams and Regional Rural Servicing Guidelines shall be required.

#### H4.3.6 GROUNDWATER PROTECTION

It is an objective of this plan to encourage the protection of groundwater resources.

Schedule H4-2 identifies areas of higher potential groundwater recharge within the Hamlet. Any development proposal within these areas shall be subject to an Environmental Implementation Report, as specified in the Scoped Subwatershed Plan for Glen Williams that will assess the hydrogeological function of the area, and determine how to maintain the quality and quantity of groundwater.

Prior to any development on partial services (municipal water and private septic) an Environmental Implementation Report will be required to the satisfaction of the Regional Municipality of Halton and Credit Valley Conservation to ensure that groundwater resources will not be impacted in terms of either water quantity or quality, or impacts on other environmental resources as defined by Credit Valley Conservation.

Any residential development proposal containing four or more lots including the retained lot and adjacent to residential areas serviced by a private water supply system shall be subject to an Environmental Implementation Report that will contain a hydrogeological study to address the protection of the existing groundwater supply in terms of both quantity and quality.

Applications that have the effect of creating less than four lots may be subject to a hydrogeological study depending on local conditions and the discretion of the Region of Halton, the Town of Halton Hills and/or Credit Valley Conservation.

A generic Terms of Reference for Environmental Implementation Reports, consistent with the Scoped Subwatershed Study for Glen Williams, is contained in Appendix X7 to this Plan.

#### H4.3.7 STORMWATER MANAGEMENT

Any submission of a plan of subdivision must be accompanied by an Environmental Implementation Report including a stormwater management study that will address to the satisfaction of the Town of Halton Hills and Credit Valley Conservation how the quality and quantity of runoff will be managed.

A stormwater management report will also be required for a lot severance, if determined to be necessary by the Town of Halton Hills or Credit Valley Conservation.

#### H4.3.8 HAMLET BOUNDARIES AND BUFFERS

The boundaries of the Hamlet shown on Schedule H4-1 have been determined based upon a limited amount of growth that: maintains Hamlet scale and character; corresponds to available wastewater servicing capacity and identifiable features such as roads, property boundaries, physical features and other logical boundaries.

To further achieve the objective of preserving the hamlet character, a general lot line setback of 20 metres from the hamlet boundary in new development areas should be targeted. However, lesser widths may be approved where it can be shown to the Town of Halton Hills, that this objective has been achieved.

Lands within this setback will be allowed to regenerate as private natural areas or be used for public park purposes such as trail systems.

Mechanisms other than a lot line setback of establishing appropriate hamlet buffers, that meet the intent of the policy, will be considered by the Town, through the development application review process.

#### H4.3.9 NOXIOUS USES

No use, which does, or is likely to cause air pollution, offensive odours, ground or surface water pollution, or noise in excess of Provincial regulations or guidelines, shall be permitted.

#### H4.3.10 ARCHAEOLOGICAL RESOURCES

Development that impacts on areas identified for potential archaeological significance will require a Stage 2 Field Assessment, as outlined in the Provincial Archaeological Assessment Technical Guidelines 1993, to determine the presence of archaeological resources and, if required, provide recommendations for mitigation and conservation.

Development and site alteration may be permitted after the significant archaeological resources have been conserved by removal and documentation, or preservation on site. Where significant archaeological resources must be preserved on site, only development and site alteration, which maintains the heritage integrity of the site, will be permitted.

#### H4.3.11 HAMLET DESIGN AND HERITAGE PROTECTION

The character of the Hamlet of Glen Williams is largely defined by the heritage buildings, which shape the built form of Glen Williams. These buildings help create an environment that is distinctive and lays the foundation for not only a cohesive community but also for tourism development initiatives.

It is the policy of the Town of Halton Hills to implement Hamlet Design and Heritage Protection Guidelines for the Hamlet of Glen Williams in order to protect and enhance community features. These are attached as Appendix X6 to this document and are designed to help the Town and the community guide new development.

The approval process for all planning applications within the Hamlet will include the application of the Hamlet Design and Heritage Protection Guidelines. It is a policy of this Plan that the cost of implementation of on and off-site improvements consistent with the Hamlet Design and Heritage Protection Guidelines will be the responsibility of the proponent to the satisfaction of the Town.

#### H4.3.12 TRAFFIC

A Transportation Assessment completed as part of the development of this Secondary Plan concluded that the level of growth provided for in this Plan is not expected to create demand for new road improvements. However, a traffic study will be required for all new developments and may conclude that there is a need for certain road improvements.

The Background Planning Study noted however, that the level of traffic, generated from outside of Glen Williams, that utilizes roads passing through Glen Williams, is likely to increase over time. Unmitigated, this traffic is likely to have a significant impact on the community character. It is a policy of this Plan that the Town continue to monitor traffic and its impact on the Hamlet of Glen Williams, and determine

appropriate measures to address this impact. Transportation matters with respect to the Glen Williams area will continue to be reviewed through various studies, such as the Region of Halton Transportation Master Plan Study and the Halton Hills Transportation Study Update. In addition, the five-year review of the new Halton Hills Official Plan and Glen Williams Secondary Plan, shall include a review of transportation issues related to Glen Williams and the surrounding area.

This Plan will consider future design standards for roadways within Glen Williams that will be consistent with and complement the existing rural cross section (gravel shoulders and open ditches). The rural cross section is viewed as an important element in the hamlet character and preservation of this standard wherever possible is an objective of the plan.

#### H4.3.13 CHANGE IN MATURE NEIGHBOURHOODS

Mature Neighbourhood Areas are those areas of Glen Williams characterized by older established residential development, either on smaller lots in the historic core, or on larger lots but with a distinct character in other older areas of the hamlet. These areas are delineated in the Zoning By-law.

New housing, replacement housing, additions, and alterations, and new or enlarged accessory buildings within Mature Neighbourhood Areas shall be permitted provided they are compatible, context sensitive, and respectful of the existing character of the neighbourhood.

The implementing Zoning By-law shall further detail appropriate standards for new housing, replacement housing, additions, and alterations to housing within Mature Neighbourhood Areas, including lot coverage, building height, and side yard setbacks amongst other standards.

Minor variances from the implementing Zoning By-law associated with new housing, replacement housing, additions, alterations, and new or enlarged accessory buildings, in Mature Neighbourhood Areas shall consider, where applicable:

- a) compatibility with existing building orientation and building setbacks;
- b) that the scale, massing, building height, and built form features are compatible with the existing character of the neighbourhood;
- c) the preservation of landscaped open space areas and the protection of existing trees; and,
- d) that impacts on adjacent properties are minimized.

#### H4.4 HAMLET COMMUNITY CORE AREA

##### H4.4.1 PURPOSE

The *Hamlet Community Core Area* designation identifies the central portion of the Hamlet where the greatest concentration of commercial activities and heritage features are located. It is an objective of the *Hamlet Community Core Area* designation to define and strengthen the character of the *Hamlet Area* through the protection of its architectural style and natural heritage while at the same time improving the commercial viability and viability as a community centre.

This designation serves to allow for the concentration of the primary commercial and community functions within the historic core area towards the creation of a vibrant centre of activity. Residential

uses are also encouraged that are consistent with the policies of this plan and the Hamlet Design and Heritage Protection Guidelines contained in Appendix X6.

#### H4.4.2 PERMITTED USES

Uses permitted within the *Hamlet Community Core Area* designation include tourism and cultural uses, retail services, residential uses and office space. Non-residential uses can occupy up to 500 square metres of ground floor space. The permitted uses shall be limited to:

- a) bakery;
- b) bank;
- c) bed and breakfast establishment;
- d) business or professional office;
- e) community centre;
- f) home occupations and cottage industries within single detached dwelling units, subject to the policies of the Halton Hills Official Plan. A home occupation use shall not include any adult entertainment uses;
- g) ice cream parlour;
- h) museum;
- i) open space uses;
- j) public parking area;
- k) recreational use;
- l) restaurant, but not a drive through restaurant;
- m) retail and service commercial uses;
- n) single detached dwelling; and
- o) accessory apartments in single detached dwellings subject to Section E1.4.10.

Adult entertainment uses shall be prohibited in the *Hamlet Community Core Area* designation.

#### H4.4.3 LAND USE POLICIES

Development within the *Hamlet Community Core Area* designation shall be consistent with the following land use policies:

- a) Where commercial and residential uses abut, site plan control to achieve adequate buffering in the form of landscaping, or architectural screening shall be required;
- b) Adequate off street parking will be required. Parking facilities will be oriented to the side or rear of all commercial buildings wherever possible;

- c) Building heights will not exceed two storeys unless required to meet the objectives of the Hamlet Design and Heritage Protection Guidelines contained in Appendix X6;
- d) Commercial uses shall generally serve the needs of the community, the surrounding rural area and tourists;
- e) No outdoor storage uses, that were not existing on the day this plan was adopted, are permitted anywhere within the *Hamlet Community Core Area* designation;
- f) The development of office space will be encouraged to develop on second storeys above retail uses;
- g) All development will be consistent with the Hamlet Design and Heritage Protection Guidelines contained in Appendix X6; and,
- h) The majority of the *Hamlet Community Core Area* is situated within the Regulatory Floodplain of the Credit River, and development is subject to the policies contained in Section H4.9 of this Plan.

## H4.5 HAMLET RESIDENTIAL AREA

### H4.5.1 PURPOSE

The *Hamlet Residential Area* designation recognizes existing residential areas and lands that, subject to the fulfillment of the policies of this Plan, may be suitable for new residential development. The objective of the *Hamlet Residential Area* designation is to allow for gradual and limited growth over time in a manner that is consistent with the character of the Hamlet using innovative subdivision design and architectural techniques.

Applications for new development within the Hamlet of Glen Williams will require a variety of lot sizes, setbacks and architectural styles, within the parameters of this Plan, the Hamlet Design and Heritage Protection Guidelines contained in Appendix X6 and the implementing zoning by-law.

### H4.5.2 PERMITTED USES

The uses permitted within the *Hamlet Residential Area* designation shall be limited to:

- a) single detached residential uses;
- b) bed and breakfast establishments (consistent with the policies in the Town of Halton Hills Official Plan);
- c) home occupations and cottage industries within single detached dwelling units, subject to the policies of the Halton Hills Official Plan. A home occupation shall not include any adult entertainment uses; and
- d) accessory apartments in single detached dwellings subject to Section E1.4.10.

Adult entertainment uses shall be prohibited in the *Hamlet Residential Area* designation.

Development within the *Hamlet Residential Area* designation shall be consistent with the following land use policies:

- a) All new development shall be serviced with piped Regional water and wastewater services in accordance with the policies of Section H4.3.5 of this plan;
- b) Lot creation shall occur by way of plan of subdivision for any development proposal if more than four lots, including the retained lands are being created and/or the owner is retaining sufficient lands for the development of additional lots;
- c) Prior to draft plan approval, plans of subdivision must be supported by the following studies, conducted by qualified consultants, which may be scoped as determined to be appropriate by the Town or Region, in consultation with Credit Valley Conservation:
  - i) A Transportation Study that addresses potential impacts on the existing road network, site access, street signage, street and intersection lighting and pedestrian movements;
  - ii) A Design Study that illustrates how the development will achieve a format consistent with the Hamlet Design and Heritage Protection Guidelines contained in Appendix X6 of this Plan, and will preserve or enhance the cultural and natural character of the community.

The Design Study should address the following matters:

- i) Lot design, including conceptual building placements, typical streetscapes, open spaces and roads;
- ii) Pedestrian and cycling connections to the Hamlet core and open space areas, by roads, walkways, trails or other pathways, including implementation of potential trails as shown on Schedule H4-1;
- iii) The sensitive use of topography and vegetation, and the extent of new landscaping;
- iv) Location of public amenities, such as parkettes, terraces and postal kiosks; and,
- v) Typical housing types illustrating architectural style, setbacks and heights;
- iii) A Functional Servicing Plan that will address the approach to supplying water and sanitary services to the proposed development consistent with the Master Servicing Plan as required in Section H4.3.5 of this Plan; and
- iv) An Environmental Implementation Report that implements the Glen Williams Scoped Subwatershed Plan at the tributary level for the subject property, and includes, but is not limited to, a Stormwater Management Plan;
- d) The minimum lot size for residential development on Regional water and wastewater services will be 0.10 hectares (0.25 acres);

- i) Plans of subdivision that are developed on Regional water and wastewater services shall be required to provide for a range of lot sizes, consistent with the existing hamlet character, generally ranging in size from 0.10 hectares (0.25 acres) to 0.4 hectares (1.0 acre). The pattern of lot sizes will generally provide a transition from smaller to larger lots with distance from the core of the Hamlet; and,
  - ii) The maximum permitted density of any plan of subdivision developed on Regional water and wastewater services shall be 5 units per net residential hectare (2 units/net acre), and must not exceed the available wastewater servicing reserve of 2,600 population equivalent at the Georgetown Wastewater Treatment Plan, subject to the policies of Section H4.3.5 of this Plan;
- e) Council shall only adopt zoning by-laws that implement plans of subdivision where the plan can be shown to be consistent with the policies and the Hamlet Design and Heritage Protection Guidelines of this Plan; and,
- f) Notwithstanding the policies of Section H4.3.5 as they pertain to the limitation on private individual wastewater treatment systems and Section H4.5.3 (a) of this Plan, the lands described as being Part of the West Half of Lot 20, Concession 10, Town of Halton Hills (Esquesing) known as the “Devins Subdivision, File No. 24T-03002/H”, may be developed on private, individual wastewater treatment services, subject to all other applicable policies of this Plan.

A hydrogeological study will be required to the satisfaction of the Region of Halton and Credit Valley Conservation. Additional treatment for nitrogen, phosphorous and bacteria shall be required, consistent with the recommendations of the Scoped Subwatershed Plan for Glen Williams and Regional Rural Servicing Guidelines.

## H4.6 HAMLET ESTATE RESIDENTIAL AREA

### H4.6.1 PURPOSE

Areas designated *Hamlet Estate Residential Area* consist of larger estate lots that provide a transition between the rural area and the core area of the Hamlet.

The *Hamlet Estate Residential Area* designation will not be provided with piped Regional wastewater services and no allocation of wastewater servicing capacity has been reserved or will be required as part of an approval by Council of a development application in this designation. Development in these limited areas will therefore proceed on the basis of private wastewater systems and piped water supply subject to the policies of this Plan.

### H4.6.2 PERMITTED USES

The uses permitted within the *Hamlet Estate Residential Area* designation shall be limited to:

- a) single detached residential uses; and,
- b) home occupations and cottage industries within single detached dwelling units, subject to the policies of the Halton Hills Official Plan. A home occupation use shall not include any adult entertainment uses.

- c) accessory apartments in single detached dwellings subject to Section E1.4.10.

Adult entertainment uses shall be prohibited in the *Hamlet Estate Residential Area* designation.

#### H4.6.3 LAND USE POLICIES

Development within the *Hamlet Estate Residential Area* designation shall be consistent with the following land use policies:

- a) Lot creation shall occur by way of plan of subdivision for any development proposal if over four lots, including the retained lands, are being created and/or the owner is retaining sufficient lands for the development of additional lots;
- b) Prior to draft plan approval, plans of subdivision must be supported by the following studies, conducted by qualified consultants, which may be scoped as determined to be appropriate by the Town or Region, in consultation with Credit Valley Conservation:
  - i) A Transportation Study that addresses potential impacts on the existing road network, site access, street signage, street and intersection lighting and pedestrian movements;
  - ii) A study that examines land use compatibility with the sand and gravel aggregate resource on the adjacent property, and provides recommendations regarding subdivision design to address issues of public health, public safety and environmental impact;
  - iii) A Design Study that illustrates how the development will achieve a format consistent with the Hamlet Design and Heritage Protection Guidelines contained in Appendix X6 of this Plan, and will preserve or enhance the cultural and natural character of the community.

The Design Study should address the following matters:

- i) Lot design, including conceptual building placements, typical streetscapes, open spaces and roads;
- ii) Pedestrian and cycling connections to the Hamlet core and open space areas, by roads, walkways, trails or other pathways, including implementation of potential trails as shown on Schedule H4-1;
- iii) The sensitive use of topography and vegetation, and the extent of new landscaping;
- iv) Location of public amenities, such as parkettes, terraces and postal kiosks; and,
- v) Typical housing types illustrating architectural style, setbacks and heights;
- vi) A Functional Servicing Plan that will address the approach to supplying water and sanitary services to the proposed development, consistent with the relevant provisions of Section H4.3.5 of this Plan; and,



- vii) An Environmental Implementation Report that implements the Glen Williams Scoped Subwatershed Plan at the tributary level for the subject property, and includes, but is not limited to, a Stormwater Management Plan;
- c) Lot sizes for residential development on Regional water and private septic services are subject to the suitability of the soil conditions in the development area and Regional criteria for septic systems but under no circumstances shall the minimum lot area be less than 0.4 hectares (1.0 acre); and,
- d) Council shall only adopt zoning by-laws that implement plans of subdivision where the plan can be shown to be consistent with the policies and the Hamlet Design and Heritage Protection Guidelines of this Plan.

#### H4.7 INSTITUTIONAL AREA

##### H4.7.1 PURPOSE

The purpose of the *Institutional Area* designation is to recognize the institutional uses within Glen Williams and their role in the community.

##### H4.7.2 PERMITTED USES

The predominant use of land designated *Institutional Area* shall be for purposes such as:

- a) public and private institutional activities;
- b) public utilities;
- c) schools;
- d) community centres;
- e) day care centres;
- f) nursing homes;
- g) funeral homes; and,
- h) cultural, educational, religious and fraternal buildings.

##### H4.7.3 LAND USE POLICIES

Development within the *Institutional Area* designation shall be consistent with the following land use policies:

- a) Adequate off street parking for institutional uses shall be provided on site; the parking standard to be applied shall be based on the anticipated number of vehicles to be generated by the proposed use and/or the standards contained in the zoning by-law as amended;
- b) Adequate landscaping, tree planting and buffering shall be provided; and,

- c) The majority of the areas designated *Institutional Area* are situated within the Regulatory Floodplain of the Credit River, and development is subject to the policies contained in Section H4.9 of this Plan.

#### H4.8 OPEN SPACE AREA

##### H4.8.1 PURPOSE

The purpose of the *Open Space Area* designation is to recognize and provide for open space uses within the Hamlet of Glen Williams.

##### H4.8.2 PERMITTED USES

The uses permitted within the *Open Space Area* designation will include public and private open space uses such as:

- a) parks;
- b) cemeteries; and,
- c) trails.

##### H4.8.3 LAND USE POLICIES

All parks within the Hamlet exhibit characteristics of both Community and Neighbourhood Parks as defined in the Halton Hills Official Plan. Glen Williams Park serves the local area, as well as acting as a trailhead for the Town-wide network of trails. Development of parkland in the Hamlet shall be consistent with the parkland policies of the Halton Hills Official Plan.

Development within the *Open Space Area* designation shall be consistent with the following land use policies:

- a) Adequate off street parking for park uses shall be provided on site; the parking standard to be applied shall be based on the anticipated number of vehicles to be generated by the proposed use and/or the standards contained in the zoning by-law as amended; and,
- b) Adequate landscaping, tree planting and buffering shall be provided.

#### H4.9 GREENLANDS

##### H4.9.1 PURPOSE

*Greenlands* are designated on Schedule H4-1 of the Secondary Plan. The purpose of the *Greenlands* designation is:

- a) to identify lands which are flood susceptible for the protection of life and property;
- b) to protect the diversity of fauna and flora, ecosystems, plant communities, and significant landforms of Halton Hills;

- c) to maintain the water quality and natural flow regulation of rivers, streams and wetlands within the rural areas of Halton Hills;
- d) to provide opportunities, where appropriate, for passive outdoor recreational activities;
- e) to contribute to a continuous natural open space system, to provide a visual buffer or separation of communities and to provide continuous corridors between ecosystems;
- f) to protect significant scenic and heritage resources; and,
- g) to maintain or enhance fish and wildlife habitats.

Land designated as *Greenlands* on Schedule H4-1 are more specifically categorized as either *Core Greenlands* or *Supportive Greenlands* on Schedule H4-2 of the Plan.

#### H4.9.2 CORE GREENLANDS

The *Core Greenlands* designation contains the most important natural features and areas that perform the most critical ecological functions, as described in the Scoped Subwatershed Plan for Glen Williams. No new development shall be permitted within the *Core Greenlands* designation, with the exception of those uses specified in subsection H4.9.2.2 of this Plan.

##### H4.9.2.1 CRITERIA FOR DESIGNATION AS CORE GREENLANDS

The *Core Greenlands* designation consists of natural areas that meet one or more of the following criteria:

- a) Areas within the Regulatory Flood Plain, as determined and mapped by Credit Valley Conservation, and refined from time to time, as shown on Schedule H4-1 of this Plan;
- b) Areas of fish habitat;
- c) Woodlands within or contiguous to the main valley system of the Credit River; and,
- d) Riparian corridors linked to watercourses with fish habitat.

##### H4.9.2.2 PERMITTED USES

The following uses may be permitted within the *Core Greenlands* designation shown on Schedule H4-2, subject to the policies of this Plan:

- a) Existing agricultural operations;
- b) Non-intensive recreation uses such as nature viewing and pedestrian trail activities on publicly owned lands or privately owned lands if access can be negotiated with the property owner;
- c) Forest, wildlife and fisheries management;
- d) Archaeological activities;
- e) Essential public roads, transmission and utility facilities;

- f) Essential watershed management and flood and erosion control projects carried out or supervised by a public authority; and,
- g) Uses that legally existed on the day of the passing of the bylaw adopting this Plan.

#### H4.9.2.3

#### GENERAL LAND USE POLICIES

The erection of buildings and structures or the placing or removal of fill of any kind or any alteration to a watercourse or valley shall be prohibited within lands designated *Core Greenlands* or lands identified as within the limit of the Regulatory Flood. Buildings or structures associated with the uses permitted in subsection H4.9.2.2 will be developed in accordance with the following:

- a) Any environmental and/or physical hazards which affect the site can be mitigated in a manner consistent with accepted engineering techniques and resource management practices; and
- b) Such buildings, structures and works are designed in a manner which:
  - i) recognizes natural ecological systems and processes and ensures they are maintained and enhanced;
  - ii) minimizes disruption to existing landforms and landscape features including vegetation, wetlands, steep slopes and groundwater discharge areas through such approaches as clustering works on less sensitive parts of the site;
  - iii) will result in no changes to the natural quality and quantity of ground and surface water resources;
  - iv) will result in the maintenance and/or enhancement of existing aquatic ecosystem functions;
  - v) will result in the maintenance and/or enhancement of existing terrestrial ecosystem functions; and,
  - vi) the required setbacks established in accordance with the policies of subsection H4.9.4 of this Plan can be met.

#### H4.9.2.4

#### LAND USE POLICIES – REPLACEMENT OR EXPANSION OF EXISTING USES

It is the policy of this Plan to require that any expansion or replacement of existing uses or permitted buildings within *Core Greenlands* or lands identified as within the limits of the Regulatory Flood, shall only be considered for approval by the Town, in consultation with the Region of Halton and Credit Valley Conservation on the basis of the policies in subsection H4.9.2.3 and the following:

- a) Expansions of existing buildings must be dry flood-proofed to the regulatory flood level and velocity. Floodwater storage and conveyance must also be addressed to the satisfaction of Credit Valley Conservation and the policies of this Plan;
- b) Buildings destroyed by fire or other means will be permitted on the existing footprint providing the reconstruction occurs within two years of the buildings destruction and that the building is

designed so that no structural damage will occur from the regulatory flood and the building is flood proofed to the satisfaction of Credit Valley Conservation;

- c) That safe access is provided to any expanded or replacement development for pedestrian and vehicular access and evacuation routes as determined by the Town of Halton Hills and Credit Valley Conservation;
- d) Notwithstanding the permitted uses of the *Institutional Area* designation, no nursing homes, day care facilities, group homes, seniors homes, schools, fire, police or ambulance stations, or other similar uses shall be allowed to expand or be replaced in the event of destruction; and,
- e) Notwithstanding any policies of this Plan no uses associated with the storage or use of any chemical, hazardous or toxic materials shall be permitted to be reconstructed or enlarged.

#### H4.9.2.5 DEVELOPMENT EVALUATION CRITERIA

The Town in consultation with the Region of Halton and Credit Valley Conservation shall require as a basis for the evaluation of any proposed development within or adjacent to a *Core Greenlands* designation or lands identified as within the limit of the Regulatory Flood the submission of:

- a) Appropriate engineering studies conducted by the applicant to the satisfaction of the Town, the Region of Halton, and Credit Valley Conservation, which address:
  - i) the existing environmental and/or physical hazards which affect the site;
  - ii) the potential impact of such hazards;
  - iii) the potential impact of the proposed works on the lands and features in the *Greenlands* designation, particularly the natural quality and quantity of ground and surface water functions and resources; and,
  - iv) techniques and management practices so that the identified impacts may be mitigated;
- b) An Environmental Impact Study prepared to the satisfaction of the Town and the Region of Halton in consultation with Credit Valley Conservation, which provides an inventory and analysis of all natural features and ecological functions on the site including vegetation, wildlife habitat, fish habitat, wetlands, steep slopes and ground water discharge areas, which are essential for maintaining natural fluvial processes. As part of an Environmental Impact Study, the identification in the field of the exact boundary of all *Greenlands* and other natural areas shall be required; and,
- c) A detailed site plan, and landscaping and grading plans prepared to the satisfaction of the Town which demonstrate:
  - i) how natural ecological, valleyland and erosional systems and processes will be maintained including tree compensation plans where necessary; and,
  - ii) how disruption to existing landforms and landscape features will be minimized.

#### H4.9.3

#### SUPPORTIVE GREENLANDS

The *Supportive Greenlands* designation contains functions and linkages that support the ecological function of the features in the *Core Greenlands* designation. These areas would benefit from rehabilitation or restoration efforts to enhance their ecological value.

Any proposal for development within the *Supportive Greenlands* designation must be accompanied by an Environmental Implementation Report that can demonstrate the ecological function of the area can be maintained, and environmental impacts can be appropriately mitigated. Upon evaluation, these areas may meet the criteria for designation as *Core Greenlands*, and shall be placed in that designation by amendment to this Plan.

A generic Terms of Reference for Environmental Implementation Reports, consistent with the Scoped Subwatershed Study for Glen Williams, is contained in Appendix X7 to this Plan.

#### H4.9.3.1

#### CRITERIA FOR DESIGNATION FOR SUPPORTIVE GREENLANDS

The *Supportive Greenlands* designation contains areas that have natural heritage features and ecological functions but may not have specific provincial policy to regulate development. *Supportive Greenlands* areas include features such as woodlots, unevaluated wetlands, steep slopes and minor tributaries of the Credit River.

#### H4.9.3.2

#### LAND USE POLICIES

In general, the land use policies that apply to the *Core Greenlands* designation shall also apply to the *Supportive Greenlands* designation. However, development may be permitted in *Supportive Greenlands* areas where an Environmental Implementation Report is completed that illustrates how the environmental function of this area can be protected and improved through actions such stream rehabilitation efforts, reforestation and vegetative planting programs.

Development permitted in *Supportive Greenlands* areas through the approval of an Environmental Implementation Report by Credit Valley Conservation and the Town of Halton Hills will be consistent with the policies of the adjacent land use designations. In these cases, an amendment to the Plan will not be required.

#### H4.9.4

#### GREENLANDS SETBACKS

Although not considered to be part of either *Greenlands* designation, a general lot line setback of 5 metres from the stable top or staked top of bank (whichever is greater) or staked bottom of the bank of valley features shall be required for all development unless a subwatershed study, Environmental Impact Study or geotechnical study, undertaken to the satisfaction of Credit Valley Conservation, recommends more appropriate setbacks based on the natural features and functions of the area.

A minimum 5 metre setback is also required from any identified erosion allowance associated with a watercourse.

A minimum building setback of 5 metres from the regulatory flood line will be required for all new development or additions.

A minimum setback based upon the calculation of meander belt width of the watercourse is also required.

In non-valley situations a 30 metre setback from the bank of the watercourse is required, or 5 metres from the floodline, whichever is greater unless the setback is within the *Core Greenlands* designation.

#### H4.9.5 GREENLANDS ZONING

The boundaries of the *Greenlands* are delineated in a conceptual manner on Schedules H4-1 and H4-2. The extent and exact location of the boundaries shall be delineated in the implementing zoning by-law in accordance with detailed mapping, studies or field investigations conducted by the Town, Credit Valley Conservation and Provincial agencies, and will not require an amendment to this Plan.

The zoning by-law shall also incorporate general setbacks for lot lines, buildings, structures, parking areas and other similar facilities from the *Greenlands* designation in relation to the extent and severity of the natural environmental features and ecological functions of the area. This general setback will be evaluated for specific development applications by the Town and the Region of Halton in consultation with Credit Valley Conservation, and as established in the zoning bylaw, to provide the appropriate setback required to protect the area from the impacts of construction. A greater setback may be required to reflect specific circumstances.

#### H4.9.6 OWNERSHIP

Where any land designated as *Greenlands* is held under private ownership, this Plan shall not be construed as implying that such areas are free and open to the general public nor that the lands shall be purchased by the Town or other public agency; although the Region and the Town shall ensure that consideration be given to the acquisition of the critical parts of the *Greenlands* designation through the development approval process as permitted by legislation. Existing agricultural operations within the *Greenlands* designation will be recognized.

#### H4.9.7 PARKLAND DEDICATION

Where new development is proposed on a site, part of which is designated *Greenlands*, such lands shall not be considered acceptable as part of the dedication for park purposes as required by the Planning Act.

#### H4.10 IMPLEMENTATION

The provisions of the Town of Halton Hills Official Plan and the Planning Act, as amended from time to time, regarding Implementation, shall apply in regard to this Plan.

### H5 PREMIER GATEWAY PHASE 1B SECONDARY PLAN

#### H5.1 PURPOSE

The purpose of the Premier Gateway Employment Area Phase 1B Secondary Plan is to implement the policy directions that have been developed by the Town of Halton Hills, through a series of studies and public meetings. The Secondary Plan will establish a more comprehensive planning framework for the area in support of the general policy framework provided by the Official Plan. The more detailed policies

will guide new employment uses as well as address the interface between those new uses and the existing residential uses and the natural heritage system. The Plan incorporates the recommendations of the Scoped Subwatershed Study including the provision of updated natural heritage system mapping. It integrates transportation system updates, servicing policies, and land use compatibility directions, and addresses cultural heritage resources and surrounding agricultural uses. It is also intended to ensure financial sustainability to meet the long-term strategic goals of the Town.

## H5.2 STRUCTURE OF THE SECONDARY PLAN

All of this part of the document, consisting of the following text and Schedule H5 constitutes Amendment No. 31A and 31B to the Official Plan for the Town of Halton Hills. The appendices contain additional information that supports the policies of this Plan but do not form an operative part of the Plan.

## H5.3 LOCATION

The Secondary Plan affects all lands as shown on Schedule H5, which is generally bounded by:

- a) Steeles Ave to the south
- b) Eighth Line to the east
- c) Sixth Line and the boundary of the Greenbelt Plan to the west
- d) The northern limit of Lot 1, Concessions 7 and 8.

The Secondary Plan area contains approximately 279 hectares (689 acres) of land.

## H5.4 VISION STATEMENT

The Secondary Plan Area will be a high quality, comprehensively planned, prestige employment area that forms a key component of the Town's strategic gateway. It will diversify and enhance the Town's economic base and increase employment options. Visually attractive and sustainable development will enhance the stature of the area and improve resiliency. The natural heritage resources, which help to define the character of the area, will be protected, enhanced and where possible, restored. Access and connectivity will be improved, and attractive and comfortable streetscapes will encourage active transportation and support future public transit. Development will proceed based on full municipal services that meet the needs of businesses and existing residents.

## H5.5 GUIDING PRINCIPLES

- H5.5.1 Provide for significant employment growth on full municipal services to create a dynamic, accessible, and economically competitive employment area that can accommodate large and small-scale employment uses, provide a broad range of job opportunities and expand the Town's assessment base.
- H5.5.2 Encourage high quality built form, site design and landscape treatments with a strong visual identity in a sustainable environment that supports the area's gateway location. Ensure a consistent level of quality on both public and private lands with the highest quality of development at key locations.



- H5.5.3 Establish urban design guidelines and other measures, including zoning regulations that provide specific direction on the creation of high quality development and an attractive public realm. Ensure consistency with development in the other phases of the Premier Gateway Employment Area to provide a cohesive and coordinated employment area. Recognize that individual areas may develop their own specific character but will conform to the general vision for the Employment Area.
- H5.5.4 Work with the Region to ensure that the Trafalgar Road and Steeles Avenue intersection reflects its role as major gateway to Halton Hills that functions efficiently, provides a comfortable and engaging environment, and promotes a distinct visual presence and sense of arrival.
- H5.5.5 Provide for a variety of employment uses including light industrial, office and research facilities as primary uses as well as accessory retail and service uses that meet the needs of existing and future businesses to function in a supportive environment. The permitted uses will reinforce a diversified economic base.
- H5.5.6 Recognize that the Natural Heritage System within the Sixteen Mile Creek watershed is an important part of the community. Protect, enhance and where possible restore the natural heritage features and functions and connect the natural heritage systems with the Natural Heritage System within the Town, and the Provincial Greenbelt to the west and south.
- H5.5.7 Respect the existing low density residential and institutional uses within and adjacent the Secondary Plan area and recognize their right to continue to exist but prohibit new residential uses and restrict institutional uses in order to create a stable operating environment for businesses. Allow the creation of office and home based businesses in existing residential uses. Provide for appropriate buffering to minimize impacts on sensitive land uses from adjacent employment uses, while recognizing that the primary and long term use of the area is for employment.
- H5.5.8 Create a safe, efficient and integrated transportation network that provides public road access to all parcels, meets the needs of all modes of travel and promotes active transportation. The transportation system will facilitate connections to major transportation corridors as well as urban development areas within the Town to maximize connectivity to key markets and major areas of population.
- H5.5.9 Establish an appropriate interface between new employment uses and adjacent existing agricultural areas to allow existing farming operations to continue to function effectively and efficiently.
- H5.5.10 Recognize important existing cultural heritage resources and integrate them into future land use development through retention of heritage attributes that express the resource's cultural heritage significance in Lot 1. Recognize important existing cultural heritage resources and retain the heritage attributes that express the resource's cultural heritage significance in Lot 2.
- H5.5.11 Ensure that the full urban infrastructure necessary to support the employment uses and to supply municipal services to existing residential uses is provided in a timely manner in advance of, or in conjunction with, new development.
- H5.5.12 Ensure that development occurs in an orderly and phased manner in order to address the cost-effective and timely supply of major capital projects including municipal services and transportation network upgrades as the area develops.

H5.5.13 Promote sustainable site and building design and construction techniques to reduce energy and water consumption, encourage alternative modes of transportation, protect and enhance the natural environment, protect air and water quality, and improve waste management.

H5.5.14 Encourage a development framework that streamlines the approval process to facilitate development that takes advantage of the area's strategic location.

## H5.6 GENERAL POLICIES

The general policies of the Official Plan will apply to the Secondary Plan area, particularly:

- a) Part F General Development Policies, particularly F1 Subdivision of Land, F2 Urban Design, F5 Cultural Heritage Resources, F6 Transportation, F7 Public Parkland, F8 Community Facilities and Services, and F10 Development Phasing Strategies; and,
- b) Part G Plan Implementation and Administration, particularly G3 Secondary Plans and More Detailed Plans, G8 Site Plan Control, G12 Pre-Consultation and Complete Application Requirements, and G13 Interpretation.
- c) The planning horizon year for the employment land uses in this Plan is 2021 although the policies themselves are intended to address a 20 year time frame.

### H5.6.1 EMPLOYMENT TARGETS

H5.6.1.1 The employment target for the Secondary Plan area at full build out is 2700 jobs in Lot 1 and 1800 jobs in Lot 2.

H5.6.1.2 In order to achieve this target, compact development within the *Prestige Industrial* and *Business Commercial Areas* will be encouraged. It will also be important to ensure that critical linkages such as access to markets, service providers, and available labour pools are maintained.

H5.6.1.3 Any site-specific Official Plan or zoning by-law amendment to convert lands to non-employment uses including *major retail* uses is prohibited unless it is part of a Municipal Comprehensive Review that has been undertaken by the Region of Halton.

H5.6.1.4 It is the policy of this Plan to prohibit new residential uses and other non-employment uses, including *major retail* uses in the *Prestige Industrial Area* and *Business Commercial Area* designations, except:

- a) to recognize existing uses within the *Prestige Industrial Area* and *Business Commercial Area* as shown on Schedule H5 of this Plan;
- b) for institutional uses as a result of a detailed study that sets limits on, and establishes criteria for, such uses based on the following principles:
  - i) the use is small scale, and collectively such uses within the *Prestige Industrial Area* and/or *Business Commercial Area* designations do not change the character of that designation;
  - ii) the location and design of the use addresses land use compatibility in a manner based upon Regional Guidelines and the policies in section H5.13;

- iii) the use is located at the periphery of the Premier Gateway Employment Area; and,
- iv) such uses do not collectively displace employment from the *Prestige Industrial Area* and *Business Commercial Area* designations to an extent that the supply of land within the *Prestige Industrial Area* and *Business Commercial Area* designations are insufficient to meet the employment target contained in Tables A1A and F10 of this Plan.

**H5.6.1.5** It is the policy of this Plan to permit supportive commercial and secondary uses and services that enhance the Premier Gateway as set out in Business Commercial and the Prestige Industrial Area designations. Such uses are particularly encouraged to locate in the Business Commercial Area designation. The policy framework for these secondary uses was informed by a Supportive Commercial Needs Assessment study which provided estimates of how much supportive commercial floor space and acreage might be required. As the Secondary Plan develops, the Supportive Commercial Needs Assessment study, as amended from time to time, will be used as a guide in evaluating individual applications that include permitted secondary uses and/or preparing the implementing zoning by-law for such uses to ensure they do not detract from the Main Permitted Uses in the Prestige Industrial Area.

## **H5.7 LAND USE DESIGNATION**

### **H5.7.1 PRESTIGE INDUSTRIAL AREA**

**H5.7.1.1** The Secondary Plan area is part of a larger Premier Gateway Employment Area that is located in close proximity to Highways 401 and 407 where other lands are also designated *Prestige Industrial Area* and are intended to form an economically competitive, high quality employment area. Industrial and business operations which can benefit from the area's good accessibility will be encouraged.

**H5.7.1.2** Within the Secondary Plan where lands are identified as *Prestige Industrial Area* on Schedule H5 to this Plan, the predominant use of the land will be for employment uses. Development will be designed to reflect and enhance the prominent location of the area through the creation of well-designed and sustainable buildings, structures and landscaping in a visually attractive environment.

**H5.7.1.3** The main permitted uses within the *Prestige Industrial Area* will be limited to:

- a) business and professional offices in free-standing buildings or as part of an industrial mall;
- b) industrial uses, such as manufacturing, assembling, processing, fabricating, warehousing, wholesaling and distribution facilities located primarily within wholly enclosed buildings. Outdoor storage will not be permitted. For the purposes of this Secondary Plan, *trailer waiting spaces* are not considered outdoor storage;
- c) computer, electronics and data processing facilities;
- d) research and development facilities excluding those that produce biomedical wastes;
- e) printing and associated service establishments; and,
- f) industrial malls, containing one or more of the main uses permitted in this designation.

New residential uses are prohibited. No use will be permitted that causes, or is likely to cause air pollution, offensive odours, ground or surface water pollution, or noise in excess of Provincial regulations or guidelines.

#### H5.7.1.4 PERMITTED SECONDARY USES

The following secondary uses that are predominantly ancillary to and supportive of the main permitted uses may be permitted in the *Prestige Industrial Area*:

- a) administrative office uses accessory to, and located on the same lot as a permitted industrial use;
- b) limited commercial uses which are ancillary to the main permitted uses, such as service commercial uses, take-out or full-service restaurants, financial institutions, convenience stores, and commercial fitness centres. *Major retail* uses will not be permitted;
- c) commercial or trade school facilities;
- d) public parks and recreational facilities, in accordance with Section F7 (Parkland) of this Plan;
- e) limited private parks and recreational facilities located on lands adjacent to Major Arterial Roads or Collector Roads;
- f) limited retail sales of goods and materials manufactured, or substantially assembled, on the premises;
- g) limited institutional and post-secondary education facilities in accordance with the requirements of Part H5.6.1.4;
- h) motor vehicle service stations; and,
- i) uses that legally existed at the date of adoption of this Plan and expansions or alterations thereto which conform to the intent of this Plan and the implementing Zoning By-law.

#### H5.7.1.5 DEVELOPMENT POLICY FOR SECONDARY COMMERCIAL USES

The development of new secondary commercial uses will be limited to those uses that serve the Prestige Industrial Area and as noted in H5.7.2.1 for Lot 1 and H5.7.1.5 a) for Lot 2, these uses will be encouraged to locate in the *Business Commercial Area* within Premier Gateway Phase 1B Secondary Plan Area. Secondary commercial uses are subject to the following:

- a) Individual retail and service commercial uses permitted in section H57.1.4b) will not exceed 400 sq m in total floor area within the *Prestige Industrial Area* unless they are located within a freestanding office building where they will be located on the ground floor and will not exceed 750 sq m in total floor area. The total retail and service commercial space on individual sites designated *Prestige Industrial Area* will not exceed 2000 sq m in total floor area.
- b) Full-service restaurants, commercial fitness centres, and financial institutions may be integrated into industrial malls and/or free-standing office buildings, or may be developed as free-standing buildings provided that the building design is consistent and compatible with the overall design of the adjacent development.
- c) Take-out restaurants and convenience stores will be integrated into industrial malls or free-standing office buildings.

- d) Two or more full-service restaurants may be developed in a “campus” development at the intersection of Major Arterial Roads with Collector Roads, subject to a comprehensive site development plan in order to achieve well-designed and integrated development, including
  - i) integrated internal circulation and parking areas;
  - ii) coordinated access points, in order to minimize the number of access points to abutting roads;
  - iii) compatible building design and location;
  - iv) complementary landscaping; and,
  - v) consistent signage and lighting facilities.
- e) A high quality of building and landscaping design will be required for all commercial uses to ensure compatibility with adjacent development.

**H5.7.1.6** The maximum height of all buildings or structures within the *Prestige Industrial Area* will not exceed six storeys although this policy does not apply to signs, utility towers or other non-habitable structures. The greatest heights will be encouraged to locate adjacent to Steeles Avenue and Trafalgar Road in the Premier Gateway Phase 1B Lot 1 Secondary Plan Area. Where heights in excess of 3 storeys are proposed adjacent to residential and institutional uses, appropriate transitions and buffering will be provided in accordance with the Urban Design Guidelines.

**H5.7.1.7** The property at 8173 Trafalgar Road currently contains a residential treatment facility. The subject lands may be zoned to permit the existing use. Any new development or expansion of the existing facility will require rezoning which will be reviewed by the Town in the context of the Secondary Plan policies.

## **H5.7.2 BUSINESS COMMERCIAL AREA**

**H5.7.2.1** The purpose of the *Business Commercial Area* is to provide services that are important to support the primary employment function of the *Prestige Industrial Area* and the gateway function of the area such as hotels, business and professional offices and limited service commercial uses in easily accessible and central locations. Hotels are only permitted in the *Business Commercial Area* and while ancillary service commercial uses are also permitted within the *Prestige Industrial Area* designation, they will be encouraged to locate in the *Business Commercial Area*. Development will be designed to reflect and enhance the visible location of the land within the *Business Commercial Area* through the creation of well-designed and sustainable buildings, structures and landscaping in a visually attractive environment. The designation of new *Business Commercial Area* sites will only be permitted as part of a Municipal Comprehensive Review undertaken by the Region of Halton.

**H5.7.2.2** Permitted Uses within *Business Commercial Area* are limited to:

- a) hotels including full service hotels with conference, exhibition and banquet facilities;
- b) business and professional offices in free-standing buildings or as part of an industrial mall;
- c) research and development facilities excluding those that produce biomedical wastes;

- d) limited retail and service commercial uses that are ancillary to and serve the main permitted uses such as take-out restaurants, banks and financial institutions and services, convenience stores and commercial fitness centres. Limited retail and service commercial uses do not include *major retail* or supermarkets, specialty food stores, department stores, general merchandise stores, and fashion retailers;
- e) motor vehicle service stations, but no new ones will be permitted within 200 metres of the intersection of Steeles Avenue with Trafalgar Road;
- f) printing and associated service establishments;
- g) full service restaurants;
- h) industrial malls, containing one or more of the uses permitted in this designation;
- i) automotive commercial uses excluding motor vehicle body shops and motor vehicle wreckers. The total floor area for an individual automotive commercial use shall not exceed 1500 sq m. A maximum of 20 percent of the land area within the Business Commercial designation may be permitted for automotive commercial uses and they may not be located in a campus style or adjacent to each other;
- j) uses that legally existed at the date of adoption of this Plan and expansions or alterations thereto, which conform to the implementing Zoning By-law.

#### H5.7.2.3

Development within the *Business Commercial Area* is subject to the following:

- a) All individual retail and service commercial uses permitted in section H5.7.2.2b) will not exceed 750 sq m in total floor area within the *Business Commercial Area* designation. The total retail and service commercial space on individual sites designated *Business Commercial Area* will not exceed 2500 sq m in total floor area.
- b) Full-service restaurants, financial institutions, and commercial fitness centres may be integrated into industrial malls and/or free-standing office buildings, or may be developed as free-standing buildings provided that the building design is consistent and compatible with the overall design of the adjacent development.
- c) Take-out restaurants and convenience stores will be integrated into industrial malls or free-standing office buildings.
- d) Two or more full-service restaurants may be developed in a “campus” development, subject to a comprehensive site development plan in order to achieve well-designed and integrated development, including:
  - i) integrated internal circulation and parking areas;
  - ii) coordinated access points, in order to minimize the number of access points to abutting roads;
  - iii) compatible building design and location;
  - iv) complementary landscaping; and,

- v) consistent signage and lighting facilities.

Building and landscaping design will be required to meet the objectives of the Urban Design Guidelines to ensure appropriate design considerations along the Regional Major Arterial, Local Minor Arterial and Local Collector Road frontages.

**H5.7.2.4** The maximum height of any buildings or structures within the *Business Commercial Area* will not exceed six storeys except for hotels which may be permitted up to eight storeys. This policy does not apply to signs, utility towers or other non-habitable structures. Where heights in excess of 3 storeys are proposed adjacent to residential and institutional uses, appropriate transitions and buffering will be provided in accordance with the Urban Design Guidelines.

### **H5.7.3 RESIDENTIAL SPECIAL POLICY AREA**

**H5.7.3.1** The Residential Special Policy Area is an overlay to the *Prestige Industrial Area* designation. The lands in the Residential Special Policy Area on Schedule H5 will be developed in accordance with the underlying land use designation. The Residential Special Policy Area identifies locations where there are existing residential uses which might not redevelop in the short term. To recognize this situation, the following special policies will apply to those lands:

- a) The existing residential uses may be recognized in the Implementing Zoning By-law. Any new development will require rezoning which will be reviewed by the Town in the context of the Secondary Plan policies.
- b) In addition to the uses permitted in the underlying land use designation on Schedule H5, within the Residential Special Policy Area, the Town may permit the use of the existing residential dwelling, including additions to such dwellings, for office uses, home occupations, or other uses which are compatible with the adjacent residential uses and uses permitted by the underlying employment land use designation.
- c) Where development is proposed on lands abutting a building being used for residential purposes in the Residential Special Policy Area, the Town may require the provision of landscaping and other buffering on the boundary of the residential use in accordance with the provisions in section H5.13.

### **H5.7.4 NATURAL HERITAGE SYSTEM**

**H5.7.4.1** The goal of the *Natural Heritage System* is:

- a) to protect areas which have been identified as having environmental significance or contain natural hazards including valleylands, woodlands, wetlands, watercourses and other hazardous lands, based on the functional recommendations of the Premier Gateway Scoped Subwatershed Study, and to ensure that the *Natural Heritage System* will be resilient to the impacts of the adjacent urban development and climate change by achieving enhanced ecological and hydrologic features and functions.

**H5.7.4.2** The Natural Heritage System is a critical component of a healthy community and helps to define the character of the area. The Natural Heritage System reflects the systems approach taken to ensure the protection, preservation and enhancement of *natural features* and to maintain biological diversity and *ecological functions* of the area.

#### H5.7.4.3

Lands within the *Natural Heritage System* will be protected and enhanced over time, and the following uses may be permitted in accordance with the objectives of the Secondary Plan, and subject to other policies of this Plan, applicable Zoning By-laws, and Conservation Halton Regulations:

- a) existing agricultural operations;
- b) single detached dwellings on existing lots;
- c) passive non-motorized recreational uses, such as nature viewing and pedestrian trail activities, only on publicly owned lands;
- d) *forest, wildlife and fisheries management*;
- e) archaeological activities;
- f) *essential* transportation and *utility* facilities; and,
- g) *essential watershed management* and flood and erosion control projects carried out or supervised by a *public authority*.

#### H5.7.4.4

Where any land within the *Natural Heritage System* designation is held under private ownership, this Plan will not be construed as implying that such areas are free and open to the general public. The Town will seek to obtain dedication of these lands through the development review process

#### H5.7.4.5

Where new development is proposed on a site, part of which is designated *Natural Heritage System*, such lands shall not be considered acceptable as part of the dedication for park purposes as required by the Planning Act.

#### H5.7.4.6

The *Natural Heritage System* shown on Schedule H5 is a systems approach to protecting and enhancing *natural features* and functions and consists of the following components:

1) *Key Features*, which include:

- a) habitat of endangered and threatened species,
- b) *significant wetlands*,
- c) *significant woodlands*,
- d) *significant valleylands*,
- e) *significant wildlife habitat*,
- f) *significant* areas of natural and scientific interest; and,
- g) *fish habitat*,

*Key Features* that have been identified are shown on Schedule H5. There may exist other *Key Features* that are not shown on Schedule H5 or that may exist in other land use designations. These unmapped *Key Features*, if found during detailed study, shall be protected through appropriate development approvals in accordance with Section H57.4.10.

2) *Enhancements to the Key Features*,



- 3) *Linkages*,
- 4) *Buffers*,
- 5) *Watercourses* that are within Conservation Halton Regulation Limit or that provide a *linkage* to a *wetland* or a *significant woodland*, and
- 6) *Wetlands* other than those considered *significant*.
- 7) Regulated *Flood Plains* as determined, mapped and refined from time to time by Conservation Halton are also included within the *Natural Heritage System*.

The boundaries of the *Natural Heritage System* may be refined in accordance with Section

- H5.7.4.7** The *Natural Heritage System* may include additional lands beyond identified natural heritage features and their *buffers*, which are identified through an approved Subwatershed Impact Study as being a *Key Feature* and/or which provide additional enhancement to the system. These enhancements provide for increased ecological services such as wildlife habitat and movement opportunities, increased buffering to natural heritage features, and restoration opportunities including additional vegetation plantings.
- H5.7.4.8** The *Natural Heritage System* in the Secondary Plan area connects with the natural heritage system within the Town, and the Provincial Greenbelt to the west and south. This connectivity is important to support natural processes and provide corridors that allow species to move and adapt, thereby ensuring a healthy and resilient ecosystem that protects biodiversity for the long term.
- H5.7.4.9** *Buffers* for natural heritage features have been included within the *Natural Heritage System* illustrated on Schedule H5, so additional *buffers* should not be required except where unmapped *Key Features* have been identified through an approved Subwatershed Impact Study and/or requirements to address any Endangered or Threatened species may be applicable. The following *buffer* widths are provided for general guidance but the final *buffer* widths, which may be smaller or larger, will be determined in the Subwatershed Impact Study based on an analysis of the feature sensitivity and proposed adjacent land uses.
- a) *Watercourse Corridors*: Generally greater of 15 metres from the greatest hazard (Regional Storm flood plain, meander belt or stable top of bank) or 30 metres from the bankfull channel of cold, cool and warm water sportfish watercourses. Where a trail is planned, it may be located within the buffer subject to section H5.7.4.17.
  - b) *Significant woodlands*: Generally 30 metres from the drip line. Where a trail is planned, it may be located within the *buffer* subject to section H5.7.4.17.
  - c) *Wetlands*: Generally 30 metres from the *wetlands* limits. The size and significance of the *wetland* is subject to *wetland* delineation and the Ontario Wetland Evaluation System as part of the Subwatershed Impact Study.
  - d) *Significant wildlife habitat*: Generally 30 metres from the boundary of the habitat unit.
  - e) *Significant habitat of endangered or threatened species*: Generally 30 metres from the boundary of the habitat unit as determined by Ministry of Natural Resources and Forestry in accordance with provincial requirements under the Endangered Species Act, 2007.
- H5.7.4.10** A systems based approach to implementing the Natural Heritage System will be applied by

- a) Prohibiting *development* and *site alteration* within *significant wetlands*, *significant* habitat of endangered and threatened species and *fish habitat* except in accordance with Provincial and Federal legislation or regulations;
- b) Not permitting the alteration of any components of the *Natural Heritage System* unless it has been demonstrated that there will be no *negative impacts* on the *natural features and areas* or their *ecological functions*;
- c) Refining the boundaries of the *Natural Heritage System*, including additions, deletions, and/or boundary adjustments, through a Subwatershed Impact Study in accordance with Section H5.8 and accepted by the *Town* and *Region*, an individual Environment Impact Assessment accepted by the *Town* and *Region*, or similar studies based on terms of reference accepted by the *Town* and *Region*;
- d) Restoring and enhancing, where feasible, *natural features* and *areas* within the *Natural Heritage System*. Where areas are not in a natural state, they should be naturalized and planted with native, non-invasive species. The requisite planting and seeding plans will be established at the time of subdivision or site plan approval.
- e) Requiring the proponent of any *development* or *site alteration*, including public works, that are located wholly or partially within 120m of the *Natural Heritage System* to carry out an Environmental Impact Assessment (EIA), unless:
  - i) the proponent can demonstrate to the satisfaction of the *Town* and *Region* that the proposal is minor in scale and/or nature and does not warrant an EIA;
  - ii) the proponent can demonstrate to the satisfaction of the *Town* and *Region* that a Subwatershed Impact Study prepared in accordance with Section H5.8 that adequately addresses the purpose of an EIA;

The purpose of an EIA is to demonstrate that the proposed *development* or *site alteration* will result in no *negative impacts* to that portion of the Natural Heritage System or unmapped *Key Features* affected by the *development* or *site alteration* by identifying components of the Natural Heritage System as listed in Section H5.7.4.6 and their associated *ecological functions* and assessing the potential environmental impacts, requirements for impact avoidance and mitigation measures, and opportunities for enhancement. The EIA, shall, as a first step, identify *Key Features* on or near the subject site that are not mapped on Schedule H5.

#### H5.7.4.11 WATERCOURSES AND HEADWATER DRAINAGE FEATURES

- H5.7.4.11.1 The watercourses within the Secondary Plan area are part of the *Natural Heritage System* and as noted above, the corridor width will be determined as part of the Subwatershed Impact Study and will contain the watercourse, associated hazards and buffer.
- H5.7.4.11.2 The location of Headwater Drainage Features identified for protection or conservation, and the width of associated *buffers* will be determined as part of the Subwatershed Impact Study.
- H5.7.4.11.3 Schedule H5 illustrates locations where regulated *watercourses* and headwater drainage features may be suitable for relocation. In some cases, the conceptual relocation area has been illustrated on Schedule H5 based on the preliminary work undertaken in the Scoped Subwatershed Study (“Relocated”) and in other cases, the matter has been left to the Subwatershed Impact Study (“Potential Relocation”). Where relocation occurs, the feature should be moved to an area where its form and function can be replicated and it is contiguous with other natural features where possible, in order to

provide overall benefit to the natural heritage feature and the wildlife that use and inhabit them. Final determination as to whether the relocation is appropriate will be determined through the Subwatershed Impact Study (“Potential Relocation”) submitted in accordance with Section H5.8 as part of a development application and should be further supported by a Natural Channel Design Brief at the detailed design stage which will generally include:

- a) Details related to the natural channel design principles applied to the detailed design of the watercourse;
- b) Fluvial geomorphological analysis of the proposed watercourse design;
- c) Rationale for selection of plantings within the riparian zone and floodplain;
- d) Details regarding any enhancements proposed within the adjacent watercourse;
- e) Detailed hydrologic and hydraulic analyses of proposed watercourse and hydraulic structures to demonstrate impacts to floodplains, and freeboard under proposed conditions, maintenance of riparian storage post-development;
- f) Detailed groundwater analysis to demonstrate that the groundwater seepage function can be replicated in proposed condition
- g) Detailed assessment of impacts of proposed watercourse to aquatic habitat and fish species; and
- h) Detailed design of the drainage for proposed watercourse and corridor.

**H5.7.4.11.4** As shown on Schedule H5, a headwater drainage feature which is located northeast of the Steeles Avenue and Trafalgar Road intersection is associated with a *wetland*. The Subwatershed Impact Study prepared as part of the development application will be required to further assess this feature in accordance with the management recommendations of the Scoped Subwatershed Study. As also identified in H5.7.4.13.2, a determination will be made through the Subwatershed Impact Study as to whether to retain the headwater drainage feature and *wetland* in their current location with appropriate *buffers* or to relocate and replicate them in a different location addressing the requirements in H5.7.4.11.3 and the requirements of the management recommendations of the Scoped Subwatershed Study.

#### WOODLANDS

**H5.7.4.12.1** The Woodlands identified within the Secondary Plan area were considered *significant* at the time of the Scoped Subwatershed Study.

**H5.7.4.12.2** The Subwatershed Impact Study will identify or refine the boundaries of *significant woodlands* and implement measures to protect and enhance such *significant woodlands* including refining the width of *buffer*.

#### WETLANDS

**H5.7.4.13.2** *Wetlands* within the Secondary Plan area were not identified as provincially significant at the time of the Scoped Subwatershed Study; however all *wetlands* will be evaluated as part of the Subwatershed Impact Studies as per the Ontario Wetland Evaluation System. Where *wetlands* exist, they have been incorporated into the Natural Heritage System.

- H5.7.4.13.2 As shown on Schedule H5, a *wetland* has been identified at the northeast corner of the Steeles Avenue and Trafalgar Road intersection which is associated with a headwater drainage feature. This *wetland* is regulated by Conservation Halton and will require a development setback based on delineation and evaluation in the Subwatershed Impact Study. Further, Conservation Halton regulations do not permit interference with wetlands including removal and replication. As noted in H5.7.4.11.4, the Subwatershed Impact Study prepared as part of the development application will further assess this feature in conjunction with the headwater drainage feature in this location. A determination will be made through the Subwatershed Impact Study as to whether to retain the wetland in situ and protect it with appropriate buffers or to relocate and replicate it in a different location.

#### SIGNIFICANT WILDLIFE HABITAT

- H5.7.14.1 *Significant Wildlife Habitat* has been identified within the Secondary Plan area and included in the *Natural Heritage System* resulting in an expansion of some areas beyond the boundaries of the Regional Natural Heritage System. Further assessment and mitigation requirements for *Significant Wildlife Habitat* are required in the Subwatershed Impact Study. Any additional *Significant Wildlife Habitat* identified as part of a Subwatershed Impact Study should be incorporated into the Natural Heritage System along with appropriately sized *buffers* to ensure its protection and enhancement.

#### SPECIES AT RISK

- H5.7.4.15.1 Species at Risk were observed within the Secondary Plan area although habitats for those species noted are not considered optimal. Further assessment and mitigation requirements for Species at Risk habitat will be addressed through the Subwatershed Impact Study. Boundaries of any *significant* habitat for endangered and threatened species identified as part of the Subwatershed Impact Study should be delineated in consultation with MNRF in accordance with provincial requirements under the ESA, 2007. The boundaries of this habitat and any buffers deemed necessary through the Subwatershed Impact Study should also be incorporated into the *Natural Heritage System* to ensure its protection and enhancement.

#### ROAD CROSSINGS AND ALIGNMENTS

- H5.7.4.16.1 New roads are intended to be located outside of the *Natural Heritage System* and its associated *buffers* to the extent possible and to minimize road crossings in order to reduce impacts on the natural heritage features. The approach to be taken in addressing road locations will be to first avoid impacts if possible, and then minimize, mitigate and finally compensate for any resulting impacts. Only *essential* road infrastructure may be permitted in the *Natural Heritage System* and only after all other alternatives have been considered through an appropriate study. Crossings and alignments within or in close proximity to the *Natural Heritage System* will need to be assessed through an Environmental Assessment, Subwatershed Impact Study, or an equivalent comprehensive study to address *negative impacts*. Best management practices to facilitate fish and wildlife passage across road crossings will be addressed as part of the detailed design. This is to include appropriate *buffers* to the road alignment.
- H5.7.4.16.2 A new north-south road has been illustrated west of Hornby Road which may be located slightly outside of the Secondary Plan area within the Greenbelt Plan in order to align with Sixth Line south of Steeles Avenue. The alignment for this road will be determined through an Environmental Assessment, Subwatershed Impact Study or an equivalent comprehensive study taking into consideration the Greenbelt Plan policies, Conservation Halton Regulations and other applicable legislation.

- H5.7.4.16.3 As noted in section H5.9.1.8, two options have been identified for proposed Collector Road 1 as shown on Schedule H5. Only one of these options was considered necessary at the time of the Scoped Subwatershed Study. The preferred option, its alignment and measures to minimize impact will be determined as part of the Subwatershed Impact Study.

## TRAILS

- H5.7.4.17.1 A trail system should be established along the edge of the *Natural Heritage System* where lands are being put into public ownership to allow public use and access. The trails and associated activities will not negatively impact ecologically sensitive areas. Where possible, the trail system should be constructed at the start of development, to provide immediate access and connection with the natural environment, while discouraging the establishment of footpaths and ensuring that proper regard is given to avoiding trespassing on private properties. For all trails proposed within *Natural Heritage System buffers* and enhancement areas, trail siting should be located close to the development side of the *buffer* to provide as much area as possible for naturalization plantings between the development and key natural heritage features. Adjacent landowners potentially affected by the trails will be consulted.
- H5.7.4.17.2 Fencing should be considered along the edge of the *Natural Heritage System* irrespective of ownership in order to restrict access from people and pets. Public access to the key natural heritage features should be discouraged and trails should avoid the restored amphibian breeding habitat near Hornby Road, south of the Regional Forest, in order to protect this habitat from further disturbance impacts.

## H5.8 SUBWATERSHED STUDY

- H5.8.1 The Premier Gateway Scoped Subwatershed Study was undertaken in conjunction with preparation of this Secondary Plan in order to characterize the terrestrial and aquatic ecology, stream systems and surface water and groundwater resources, refine the *Natural Heritage System*, assist with development of the land use plan, determine the impacts from development and address the associated requirements and opportunities for mitigation and management. All new development in the Secondary Plan area will generally comply with the recommendations of the Premier Gateway Scoped Subwatershed Study. No amendments to the Secondary Plan will be required to implement the recommendations of the Premier Gateway Scoped Subwatershed Study.
- H5.8.2 A Subwatershed Impact Study will be required in support of a complete development application. The goal of the Subwatershed Impact Study will be to achieve a greater level of detail in the integration of land use, servicing, stormwater management and the *Natural Heritage System*. The study area for the Subwatershed Impact Study should be based on subcatchment areas but could be modified or consolidated subject to the approval of the Town, in conjunction with Conservation Halton and the Region of Halton.
- H5.8.3 Greater detail regarding the matters to be addressed by the Subwatershed Impact Studies is outlined in Appendix A and should be incorporated into the Terms of Reference for the Subwatershed Impact Study. The Terms of Reference will be refined on an individual basis and subject to the approval of the Town, in conjunction with Conservation Halton and the Region of Halton.
- H5.8.4 Notwithstanding Section H5.8.2, in some circumstances, a Subwatershed Impact Study may not be required where the proponent can demonstrate to the satisfaction of the Town, the *Region* and *Conservation Halton*:
- a) a Subwatershed Impact Study that applies to the subject lands has already been completed to the satisfaction of the *Town*, the *Region* and *Conservation Halton*, and on this basis it is more

appropriate to complete a Subwatershed Impact Study Update or an Environmental Impact Assessment in accordance with section H5.7.4.10; or

- b) the proposal is minor in scale and/or nature and does not warrant a Subwatershed Impact Study or Subwatershed Impact Study Update and considerations related to the *Natural Heritage System* are addressed in accordance with H5.7.4.10.

## H5.9 TRANSPORTATION

The Secondary Plan area will provide an integrated, holistic, multi-modal transportation network that is well connected to other lands within the Premier Gateway Employment Area, and provides easy access to major population areas within the Town as well as key transportation corridors. The transportation system is based on the work undertaken through the Secondary Plan Study process including the Premier Gateway West Scoped Area Transportation Study (ATS). The Premier Gateway West Scoped ATS was initiated to identify the local, Regional and Provincial transportation network improvements required to serve both the Premier Gateway Phase 1B Employment Area Secondary Plan and other proposed development outside of the Secondary Plan area. As a result of this work, a road network as well as a number of transportation infrastructure improvements were identified as required to support the development of the Secondary Plan area. All future development will be tied to the provision of these required infrastructure improvements. The Transportation policies of Section F6 of this Plan are applicable to the Secondary Plan area and in addition, the following policies which identify the road network and provide direction regarding its implementation, apply:

### H5.9.1 ROAD NETWORK

#### H5.9.1.1 CONTEXT

The road network will provide for the safe and efficient movement of people and goods. Road right of way widths shown on Schedule B2 to this Plan, will be provided in accordance with Table F6 (Function of Transportation Facilities) of this Plan. The overall road network for the Phase 1B Employment Area as a whole is shown in Schedule H5. As development occurs within Lot 1, the Town will ensure such development considers and supports the implementation of the overall road network as shown Schedule H5. The Premier Gateway West Scoped ATS also identified the need for transportation infrastructure improvements outside of the Secondary Plan area which are shown for reference on Schedule H5. The Town will identify a process for implementing these improvements to ensure they are in place to support the development of the Secondary Plan area in a timely manner. In the interim, development may commence in accordance with the Secondary Plan, provided each Traffic Impact Study prepared in accordance with Section H59.1.6 demonstrates sufficient capacity within the road network. Should the results of a Traffic Impact Study show that capacity is no longer available, the Town and/or Region may not support the development until the improvements required to address the capacity constraints are implemented.

#### H5.9.1.2 ROAD ALIGNMENTS AND IMPLEMENTATION

The location and general alignment of new collector roads as shown on Schedule H5 are approximate. These roads, and their connections to other roads to form a network, are necessary to ensure appropriate overall function of a transportation system that can support the planned development of the area and therefore cannot be changed without an amendment to this Secondary Plan. However, adjustments to the precise alignment of these roads, provided they maintain their function and

connections to other roads in accordance with the intent of this Secondary Plan may be permitted and shall be determined through municipal studies or during the consideration of studies prepared in support of development applications. Such studies shall confirm that the alignment:

- a) supports the overall function of the road network for the Phase 1B Secondary Plan area as identified on Schedule H5;
- b) addresses the requirements related to the Natural Heritage System as set out in Section H57.4.16;
- c) addresses any applicable further study identified as required in Section H59.1.8 and shown on Schedule H5;
- d) supports the delivery of water and wastewater infrastructure; and
- e) addresses any additional requirements as determined by the Town and/or Region.

To facilitate the development of an effective road network, the Town and/or Region may require landowners in the Secondary Plan area to enter into an agreement or agreements amongst themselves to address matters related to the road network including the distribution of costs and the provision of infrastructure facilities.

#### H5.9.1.3 OTHER EMPLOYMENT ROADS

Other Employment Roads are not identified on Schedule H5. The provision of local such roads, where deemed necessary, may be determined as part of the development approval process and will be in accordance with the requirements of Section F6 of the Official Plan. In addition, the identification of other employment roads will be based on the following principles:

- a) designed to augment the existing system,
- b) improve connectivity within the Secondary Plan area and to adjacent areas,
- c) provide opportunities to relieve pressure on key intersections,
- d) facilitate access to individual properties in a coordinated manner that supports and does not preclude access to other properties within the Secondary Plan Area; and
- e) reduce driveway accesses onto arterial roads.

To facilitate the development of an effective local road network, the Town and/or Region may require landowners in the Secondary Plan area to enter into an agreement or agreements amongst themselves to address matters including but not limited to the road network including the distribution of costs and the provision of infrastructure facilities.

#### H5.9.1.4 SPACING

All roadway and driveway spacings will conform to standard roadway engineering practices and will be approved by the respective roadway jurisdiction.

#### H5.9.1.5 ACCESS

Access to the Regional Road network should be limited to the connections with the road network identified for the Secondary Plan area as shown on Schedule H5. Access to the Regional Road network from a private property shall only be permitted where access to a local road is not feasible as demonstrated through a Traffic Impact Study that considers all reasonable alternative access locations, types and designs and identified an access that conforms to the Regional Access Management Guidelines to the satisfaction of the Region in consultation with the Town. To facilitate the development of an effective local road network, the Town and/or Region may require landowners in the Secondary Plan area to enter into an agreement or agreements amongst themselves to address matters including but not limited to accessing the local road network.

#### H5.9.1.6 TRAFFIC IMPACT STUDIES

As part of any development application, a Traffic Impact Study that is approved by the respective roadway jurisdiction may be required to demonstrate the following:

- a) that adequate network capacity exists to accommodate the proposed development,
- b) the need for road and/or intersection improvements (such as traffic signals, turn lanes, access construction, illumination etc.) and the timing for such improvements;
- c) the development supports the overall function of the road network for the Phase 1B Secondary Plan area as identified on Schedule H5;
- d) any additional requirements as determined by the Town and/or Region.

The Traffic Impact Study shall be completed in accordance with the Region's Transportation Impact Study Guidelines.

#### H5.9.1.7

In order to ensure that the Trafalgar Road and Steeles Avenue intersection functions efficiently within the Secondary Plan area, the capacity available at this intersection will be monitored through each individual Traffic Impact Study completed in accordance with Section H59.1.6. Should the results of a Traffic Impact Study show that capacity is no longer available, the Town and/or Region may not support the development until the required improvements to address the capacity constraints at the intersection of Trafalgar Road and Steeles Avenue are implemented.

#### H5.9.1.8 AREAS SUBJECT TO FURTHER STUDY

Certain components of the road network were identified as requiring further study through the Premier Gateway West Scoped ATS. These areas are identified below and shown on Schedule H5. The implementation of these components of the road network may be advanced through municipal studies or during the consideration of studies prepared in support of development applications as set out in Section H59.1.2.

- a) Steeles Avenue & Eighth Line

Given the future projected traffic volumes, it is anticipated that improvements will be required around the intersection of Steeles Avenue and Eighth Line. There are currently cemeteries located on both the northeast and northwest corners of this intersection and



as a result, further analysis will be required to determine the most appropriate means to address future traffic volumes in this area.

b) Collector Road 1 & Sixth Line

Two alternative east-west collector road options have been provided east of Sixth Line as illustrated in Schedule H5. Only one of these alternatives will be built and the appropriate option will be determined when development applications are brought forward, taking into consideration the impacts on the Natural Heritage System as identified in section H5.7.4.16.1.

c) Collector Road 2 & Steeles Avenue

A new collector road is proposed to extend north from Sixth Line south of Steeles Avenue in order to provide access to the employment area while avoiding truck traffic on Hornby Road where there are existing residential uses. The intersection of Sixth Line South and Steeles Avenue is proposed to be located slightly outside of the Secondary Plan area and on the north side of Steeles Avenue; it is located within the Greenbelt Plan. As noted in section H5.7.4.16.2, the alignment of this road will be subject to study.

d) Ring Road Connection

The Premier Gateway West Scoped ATS identified the need for and benefit of a Ring Road connection from the Trafalgar Road and Highway 401 north ramp terminal to Proposed Collector Road 3 at Steeles Avenue, on lands outside the Secondary Plan area as shown on Schedule H5. The implementation of this new connection would improve forecasted traffic operations within the Secondary Plan area. Given the importance of this Ring Road connection and its location outside the Secondary Plan area, the Town and Region will work with the relevant stakeholders to ensure that an appropriate process for implementing this critical improvement occurs in order to support the development of the Secondary Plan area and provide the required support to the transportation network as shown on Schedule H5.

#### H5.9.1.9 DESIGN CONSIDERATIONS

- a) The Trafalgar Road and Steeles Avenue intersection should be designed to reflect its role as major gateway to Halton Hills. As such, the intersection will include enhanced streetscape design, landscaping, signage and lighting.
- b) Truck traffic on Hornby Road north of Steeles Avenue should be restricted and traffic calming measures near existing residential uses may be considered.

#### H5.9.2 ACTIVE TRANSPORTATION

- H5.9.2.1 Development will be based on facilitating the use of walking, cycling and lightly motorized modes of travel within the Secondary Plan area in order to assist the Town in achieving its environmental stewardship goals and to encourage increased activity levels regardless of age, fitness, or ability as a component of healthy living by those who live and work within the Town.

- H5.9.2.2 The transportation system will be designed to maximize connectivity, safety and comfort for pedestrians, cyclists, those with accessibility issues and those using lightly motorized modes of travel both within the Secondary Plan area and between the Secondary Plan area and other activity areas within the Town.
- H5.9.2.3 The Town's Cycling Master Plan, which supports and promotes an increased level of cycling within the Town, has identified a two tiered cycling network that provides for both on-road opportunities such as bike lanes and off-road opportunities such as trails and multi-use paths.
- H5.9.2.4 The Region of Halton Active Transportation Master Plan also identifies regional cycling and walking networks. Within the Secondary Plan area, on-road Bike Lanes and multi-use trail have been identified on Steeles Avenue, to be confirmed through a future Municipal Class Environmental Assessment and a paved shoulder has been identified on both sides of Trafalgar Road along with a multiuse path on the east side of Trafalgar Road.
- H5.9.2.5 In addition to the infrastructure identified, Council will also:
- a) require the provision of sidewalks as new roads are created or existing roads are redeveloped in the Secondary Plan area;
  - b) ensure the provision of safe and convenient on-site pedestrian connections and adequate bicycle storage facilities in the review of development applications;
  - c) require barrier-free design of all new pedestrian facilities; and
  - d) ensure that lands for bicycle, pedestrian and lightly motorized facilities are included with the land requirements for roads.

### H5.9.3 TRANSIT

- H5.9.3.1 Although the Town does not have a full transit system, all development within the Secondary Plan area will be designed to promote transit supportive land use in order to facilitate its use when services are provided. Future transit stops should be designed to capture the majority of development within a 10-minute walk.
- H5.9.3.2 The Region of Halton has identified Trafalgar Road and Steeles Avenue as Transit Priority Corridors and the intersection of Trafalgar Road and Steeles Avenue as a Proposed Transit Node in their Mobility Management Strategy for Halton. As the Region undertakes further work, the form and function of the node and corridors may be updated.

### H5.9.4 TRANSPORTATION DEMAND MANAGEMENT

- H5.9.4.1 In support of the creation of a multi-modal transportation system, the Town will actively work with developers and businesses to develop and implement Transportation Demand Management measures that aim to reduce motorized vehicular trips and promote the use of active transportation modes, car-sharing and/or carpooling, public transit when it becomes available, and travel during off-peak hours.

### H5.9.5 PARKING

H5.9.5.1 Surface parking will be minimized in order to reduce large expanses of hard surfaces and to encourage the use of alternative means of transportation. Where surface parking is provided, it should be appropriately landscaped and screened from the street in accordance with F2.2.2.5, with clearly defined pedestrian connections between the parking area and the building entrances.

## H5.10 SERVICING

### H5.10.1 FUNCTIONAL SERVICING STUDY

H5.10.1.1 A Functional Servicing Study will be prepared as part of the subdivision or site plan approval process, which will generally include:

- a) Location and preliminary sizing of wastewater sewers;
- b) Location and preliminary sizing of storm sewers;
- c) Location and preliminary sizing of watermains;
- d) Preliminary site grading plan;
- e) Location and preliminary sizing of stormwater management facilities;
- f) Location where Low Impact Development Best Management Practices may be considered;
- g) Location and preliminary sizing of hydraulic structures (i.e. bridges and culverts);
- h) Preliminary channel grading plans and supporting analyses; and
- i) Assessment of riparian storage for existing channel and preliminary channel designs.

The report will be prepared to the satisfaction of the Town and the Region in consultation with the appropriate agencies in accordance with the policies in Section C8 and G12 of this Plan.

### H5.10.2 MUNICIPAL WATER AND WASTEWATER SERVICES

H5.10.2.1 All new development will occur on the basis of full municipal services. Background analysis has confirmed that the Region's water distribution system and wastewater collection system have sufficient capacity to support development within the Secondary Plan area. The Region of Halton is preparing a water and wastewater Area Servicing Plan to develop a detailed infrastructure plan for the Secondary Plan area. The Area Servicing Plan will set out the framework for infrastructure requirements for servicing the Secondary Plan area. Functional Servicing Studies in support of development applications will be prepared and reviewed in accordance with the Area Servicing Plan.

H5.10.2.2 Existing houses in the area are currently serviced by individual water wells. Houses within the Secondary Plan area will be eligible for connection to full municipal services as part of the provision of municipal services to the employment area. The use of Best Management Practices to protect local aquifer and water supply wells will be required for all development within the Secondary Plan area.

### H5.10.3 STORMWATER MANAGEMENT

H5.10.3.1 The Stormwater Management Facilities shown on Schedule H5 represent the general locations for stormwater management facilities within the Secondary Plan Area. These locations are intended to

maintain the supply of runoff to receiving systems and to minimize requirements for operation and maintenance by the Town. Stormwater Management Facilities will accommodate the ultimate Trafalgar Road and Steeles Avenue drainage. Notwithstanding the location of these designations, such facilities will be permitted in all designations except the *Natural Heritage System*, where only those components of such facilities that are *essential* for conveying stormwater from the facility to the receiving *watercourse* may be permitted. Stormwater Management Facilities will be located and designed to the satisfaction of the Town and Conservation Halton, and should be integrated with the open space system. Final details including location and design criteria for stormwater management facilities, including Low Impact Development are to be determined through the Subwatershed Impact Study and Functional Servicing Study taking into account any proposed watercourse and headwater drainage feature alterations.

**H5.10.3.2** All new development will be supported by a Stormwater Management (SWM) report unless waived by the Town through a pre-consultation process. The purpose of the report is to demonstrate capability and compliance with the principles and criteria in the Scoped Subwatershed Study as well as the Subwatershed Impact Study. It will identify the specific stormwater management infrastructure to be implemented, and the details related to its design, maintenance and subsequent monitoring requirements.

**H5.10.3.3** In preparing stormwater management plans, a treatment train approach is recommended in order to address requirements for water quality, erosion control, flood control, thermal mitigation and water budget. Low Impact Development Best Management Practices such as bioswales/biofilters with underdrains, infiltration trenches, rain gardens and perforated pipes will be encouraged in addition to wet end of pipe facilities. Consideration will be given to minimizing the size of storm water management facilities through the use of such measures as roof top storage, parking lot storage, infiltration galleries, modified pond design standards, and other appropriate measure to the satisfaction of the Town and Conservation Halton in order to manage water budget and maintain groundwater recharge.

**H5.10.3.4** In accordance with the Greenbelt Plan, stormwater management systems are prohibited in key natural heritage features, key hydrologic features and their associated vegetation protection zones except for those portions of the Protected Countryside within the Greenbelt Plan that define the major river valleys that connect the Niagara Escarpment and Oak Ridges Moraine to Lake Ontario. In those areas, notwithstanding the restriction on the components of stormwater management facilities within the Natural Heritage System as set out in Section H5.10.3.1, components of naturalized stormwater management facilities beyond those that are essential for conveying stormwater to a receiving watercourse may be permitted within the vegetation protection zone of a significant valleyland, in accordance with the following:

- i) they are located a minimum of 30 metres from wetlands, seepage areas and springs, fish habitat, permanent and intermittent streams, lakes and significant woodlands and they are located outside of the vegetation protection zone of any other key natural heritage feature or key hydrologic feature;
- ii) they conform to the policies in Section 4.2.1 and 4.2.3 of the Greenbelt Plan;
- iii) they are determined to be essential in accordance with the Regional Official Plan; and,
- iv) through the Subwatershed Impact Study and Functional Servicing Study it is demonstrated that the Natural Heritage System in the surrounding area will be enhanced and integrated with the naturalized stormwater management system to the satisfaction of the Town, Region and Conservation Halton.

#### H5.10.4 UTILITIES

- H5.10.4.1 The Town will confirm with all utility providers that adequate servicing networks are, or will be established to serve the anticipated and existing development, and that these networks can be phased in a manner that is cost-effective and efficient.

#### H5.11 AGRICULTURE

- H5.11.1 The full range of existing and new agricultural, agriculture-related, and secondary uses are permitted within the Secondary Plan Area until lands are developed in accordance with the policies in the Secondary Plan.
- H5.11.2 Nothing in this Plan will limit the ability of farmers to carry out normal and reasonable farm practices in accordance with the Farming and Food Production Protection Act. All new development, including development on existing lots of record, will be sensitive to existing agricultural uses and will be required to address Minimum Distance Separation One formula, as amended with respect to setbacks from agricultural operations. The identified Minimum Distance Separation is illustrated on Schedule H5 and will be reflected in the Zoning By-law.

#### H5.12 CULTURAL HERITAGE

- H5.12.1 The Secondary Plan area was historically an agricultural area and it also provided transportation and automotive services around the Hornby historic settlement area and the intersection of Steeles Avenue and Trafalgar Road. The cultural heritage resources reflect that history.
- H5.12.2 It is an objective of the Town to conserve significant cultural heritage resources and to ensure that all new development and any site alteration conserves significant cultural heritage resources. They shall be maintained and integrated into new development, where appropriate and feasible. The Town will exercise the powers and apply the tools provided by legislation, particularly the Ontario Heritage Act, the Planning Act, the Environmental Assessment Act, the Building Code Act, and the Municipal Act in implementing and enforcing the cultural heritage policies of the Town.
- H5.12.3 The following properties within the Secondary Plan area are currently listed on the Town's Municipal Heritage Register and shown on Schedule H5 as locations of Cultural Heritage Value:
- Former Loyal Orange Lodge, 12927 Steeles Avenue;
  - Wesleyan Cemetery, 8002 Eighth Line
  - Coulson Cairn, 8315 Hornby Road
  - Former James Snow farmstead, 8585 Trafalgar Road.
- They may be considered appropriate for municipal designation under the Ontario Heritage Act.
- H5.12.4 Other cultural heritage resources have been identified as candidates for conservation and are shown on Schedule H5 as locations of Cultural Heritage Value. These comprise:
- 8140 Hornby Road;
  - 8156 Hornby Road;

- Former Hornby General Store, 12993 Steeles Avenue;
- Former Hornby School, 13029 Steeles Avenue; and
- 13571 Steeles Avenue.

The abovementioned cultural heritage resources retain historical and associative, design and architectural and/or contextual value. Council, with advice from its Municipal Heritage Committee, will consider whether they should be included on the Town's Municipal Heritage Register. They may also be considered appropriate for municipal designation under the Ontario Heritage Act.

**H5.12.5** In evaluating development applications, the Town:

- Will encourage the use or adaptive reuse of cultural heritage resources, or key components of such resources, whenever possible as part of the new development in situ, or on an alternate site; or
- May, where resources which are not designated and are not to be conserved, request the documentation of such resources in a cultural heritage report with a detailed property history, architectural description and photographic recording.

The Town may impose, as a condition of any development approvals, the implementation of appropriate measures to ensure the conservation of any affected cultural heritage resources, and where appropriate, their integration into new development.

**H5.12.6** The Town will require a Cultural Heritage Impact Statement (CHIS) be prepared in accordance with section F5.1.2 of this Plan to determine the resource's specific heritage significance and to establish appropriate conservation plans and/or mitigation measures be prepared where development or redevelopment is proposed:

- on, within, adjacent to, or in the immediate vicinity of any designated cultural heritage resource; or
- on a property listed on the Town's Heritage Register.

The Town may require a Cultural Heritage Impact Statement (CHIS) be prepared in accordance with section F5.1.2 of this Plan to determine the resource's specific heritage significance and to establish appropriate conservation plans and/or mitigation measures for any development or redevelopment proposal that affects any of the cultural heritage resources mentioned above in policies H5.12.3 and H5.12.4.

**H5.12.7** The conservation of cultural heritage resources should be integrated with the conservation strategies for natural heritage features and environmentally sensitive areas where appropriate.

**H5.12.8** New development should achieve a compatible relationship with cultural heritage resources in their context (both those within and those adjacent to the Secondary Plan area) through consideration of such matters as, but not limited to, building height, massing, scale, setbacks, stepbacks, roof line and profile and architectural character and expression.

**H5.12.9** The Town may take additional steps to recognize the heritage of Hornby, including:

- a) the use of interpretative plaques and displays; and
- b) the commemoration of historic persons, families and events in the naming of public buildings, streets, parks and other public places.

**H5.12.10** Development that impacts on areas identified for potential archaeological significance will require assessment in accordance with Provincial requirements and the Regional Archaeological Management Plan to determine the presence of archaeological resources and, if required, provide recommendations for mitigation and conservation. Development and site alteration may be permitted after the significant archaeological resources have been conserved by removal and documentation, or preservation on site. Where significant archaeological resources must be preserved on site, only development and site alteration, which maintains the heritage integrity of the site, will be permitted.

## **H5.13 LAND USE COMPATIBILITY**

The Official Plan recognizes that residential and institutional uses are sensitive to noise, vibration, odour, dust or other emissions, particulates or contaminants from industrial, transportation and utility sources. Schedule H5 has identified that the need for buffering and/or mitigation must be addressed adjacent to existing residential uses. Where new facilities are proposed in proximity to existing sensitive uses, a proponent will be required to undertake the necessary impact analysis and implement, as a condition of approval, the appropriate abatement measures, in accordance with Halton Region and Ministry of Environment and Climate Change guidelines. The potential for impacts (Air and Noise) associated with new facilities will be addressed by provincial permitting and review tools such as Environmental Compliance Approvals, Environmental Activity and Sector Registry, or Environmental Assessments.

### **H5.13.1 NOISE AND VIBRATION**

**H5.13.1.1** Noise can be expected to vary significantly within the Secondary Plan area and will occur from both stationary sources (the employment and commercial land uses) and transportation sources within and in the vicinity of the Secondary Plan area.

**H5.13.1.2** A Noise and/or Vibration Impact Study will be required in accordance with Ministry of Environment and Climate Change guidelines and Halton Region “Noise Abatement Guidelines” to the satisfaction of the Town and the Region and will address Environmental Protection Act requirements with respect to existing sensitive land uses. The Noise Impact and/or Vibration Study will consider potential impacts and address mitigation measures through building design, setbacks and/or other measures such as berms and fencing. The Study will be provided early in the site plan approval process to assist in determining the most efficient way to mitigate noise emissions. Changes such as building orientation can significantly affect noise impacts at nearby sensitive land uses and address applicable Ministry of Environment and Climate Change noise criteria without the need for other costly or operationally restrictive mitigation measures.

**H5.13.1.3** Design measures to avoid queuing or traffic congestion such as strategic siting of entrances and exits and a reasonable setback from sensitive land uses may be required as part of site plan approval in order to limit nuisance effects associated with the truck traffic.

## H5.13.2 AIR QUALITY

H5.13.2.1 Air quality studies will be required in support of Class II industrial development in accordance with Region of Halton and Ministry of Environment and Climate Change guidelines. Odour may be caused by discharges from a variety of sources including stationary sources such as buildings, outdoor sources, or fugitive sources such as equipment leaks. As a result, odour assessment, and odour management plans and control measures that are specific to the facility that is being proposed should be required at the Site Plan application stage to avoid odour release and off-site effects.

H5.13.2.2 Effective fugitive dust management will be required in support of Class II industrial development in accordance with Ministry of Environment and Climate Change guidelines. A Best Management Plan outlining procedures and practices to prevent nuisance effects and deposition of fugitive dust should be prepared in support of development applications.

## H5.13.3 OTHER COMPATIBILITY ISSUES

H5.13.3.1 All development applications will be required to identify means to minimize and mitigate light pollution and should consider International Dark Sky guidelines. The Town may consider developing a strategic lighting master plan that addresses both private lighting of facilities and municipal lighting of roadways and supporting facilities such as future transit stops in order to address:

- Light intrusion or light trespass of unwanted light onto adjacent properties and the Natural Heritage System;
- Timing of lighting; and
- Light intensity, spectrum, clutter and glare.

H5.13.3.2 Construction activities can be a source of emissions and should be managed to control effects from these emissions, with consideration of scheduling, monitoring and mitigation.

H5.13.3.3 Effective communication with residents during planning and construction phases will be encouraged and the Town may consider establishing a public liaison committee to encourage resident participation.

H5.13.3.4 *Sensitive land uses*, if proposed within the Secondary Plan area, shall require the completion of a land use compatibility assessment to ensure that the use is appropriate and the location and design of the use can minimize and mitigate any adverse impacts on industrial, manufacturing or other uses that are particularly vulnerable to encroachment.

## H5.14 URBAN DESIGN

### H5.14.1 URBAN DESIGN POLICIES

H5.14.1.1 The Secondary Plan area is intended to reflect its role as a high profile gateway into the Town from Highway 401 and to ensure that future development reflects high quality architectural and urban design standards. All development within the Secondary Plan area will address the Urban Design objectives and policies in section F2 of this Plan, which are intended to ensure an attractive public realm, a comfortable and engaging environment and a consistent level of quality on both public and private lands. The highest quality of development will occur at key locations, particularly at the Trafalgar Road and Steeles Avenue intersection, in order to promote a distinct visual presence and sense of arrival.



- H5.14.1.2 The zoning by-law and site plan approval process will assist in controlling development along Trafalgar Road and Steeles Avenue to ensure a high quality of site design. In particular:
- a) buildings will be designed to front on these roads;
  - b) parking including the parking of transport trucks and trailers and delivery trucks regularly used in the business operation will be at least partially screened from these streets by berms and landscaping;
  - c) the majority of service and loading facilities will be screened from these roads; and,
  - d) safe and functional vehicular and pedestrian access will be provided.
- H5.14.1.3 Development standards, including zoning provisions and urban design guidelines, will be coordinated with the other phases of the Premier Gateway Employment Area in order to provide a cohesive built environment and an overall unified employment area.
- H5.14.1.4 Development will support sustainable site and building design and construction practices.
- H5.14.2 URBAN DESIGN GUIDELINES
- H5.14.2.1 Urban Design Guidelines will be prepared to establish a planning framework that will help the Town achieve its goal to create a dynamic, accessible, and economically competitive employment area that includes sustainable development practices. The Urban Design Guidelines will focus on the creation of unique, well connected gateway to the Town that includes high quality streetscape design and urban design which is supportive of transit and active transportation.
- H5.14.2.2 The Urban Design Guidelines will be based on the following principles:
- a) Enhancing Natural Features and mitigating impacts;
  - b) Achieving a Strong Visual Identity for Trafalgar Road and Steeles Avenue;
  - c) Developing Nodes at the Gateway Locations;
  - d) Ensuring a High Quality of Built Form and Landscape Development;
  - e) Providing Flexible Development to Accommodate Evolving Needs;
  - f) Encouraging the Protection and Adaptive Reuse of Heritage Elements; and
  - g) Encouraging the Construction of Sustainable Buildings.
- H5.14.2.3 The Urban Design Guidelines will address matters related to site plan design, built form design, and environment and open space.
- H5.15 SUSTAINABLE DEVELOPMENT
- H5.15.1 This Plan is intended to facilitate development by addressing such matters as: energy conservation; water conservation and quality; natural environment; air quality; waste management; communication; and, transportation and/or community design. References are made throughout this Secondary Plan to

where sustainable development practices may be considered or encouraged. Further guidance and recommendations will be provided in the Urban Design Guidelines.

H5.15.2 Section C19 addresses how development will have been deemed to have met the goals for sustainable or green development.

## H5.16 PHASING

H5.16.1 Development should occur in a logical and orderly manner in order to ensure that major capital projects including municipal services and transportation network upgrades are provided in a cost-effective and timely manner as the area develops.

H5.16.2 Development within the Secondary Plan area shall occur in accordance with the Premier Gateway West Scoped Area Transportation Study and the Area Servicing Plan which generally articulate required infrastructure improvements and their timing. Some flexibility may be permitted at the development stage if a proposal is supported by updated studies that are to the satisfaction of the Town and the Region.

H5.16.3 Development within the Secondary Plan area shall be in accordance with the policies of this Plan and the following criteria:

- a) the proposal can be adequately and economically serviced by municipal water, wastewater, stormwater, and transportation infrastructure;
- b) if required, the proposal participates in Local or Regional servicing and financial agreements pursuant to the Development Charges Act, or, where applicable, in a private cost sharing agreement or agreements amongst landowners to address the distribution of costs of development for the provision of matters such as infrastructure facilities; and
- c) appropriate development charge and/or community benefit charge by-laws are in place to the satisfaction of Council.

## H5.17 IMPLEMENTATION

H5.17.1 The Secondary Plan will be implemented and interpreted in accordance with the provisions of Part G - Implementation and Administration of this Plan.

H5.17.2 The Town, at its discretion, may request a peer review of any of the reports, plans and/or studies required in support of complete applications.

### H5.17.3 Definitions

***Buffer*** means an area of land located adjacent to ***Key Features*** or ***watercourses*** and usually bordering lands that are subject to ***development*** or ***site alteration***. The purpose of the ***buffer*** is to protect the features and ***ecological functions*** of the Natural Heritage System by mitigating impacts of the proposed ***development*** or ***site alteration***. The extent of the ***buffer*** and activities that may be permitted within it shall be based on the sensitivity and significance of the ***Key Features*** and ***watercourses*** and their contribution to the long term ***ecological functions*** of the Natural Heritage System as determined through a Subwatershed Study, an Environmental Impact Assessment or similar studies that examine a sufficiently large area.

**Enhancements to the Key Features** means ecologically supporting areas adjacent to *Key Features* and/or measures internal to the *Key Features* that increase the ecological resilience and function of individual *Key Features* or groups of *Key Features*.

**Essential** means that which is deemed necessary to the public interest after all alternatives have been considered and, where applicable, as determined through the Environmental Assessment process.

**Forest management or forestry** means the wise use and management of forests for the production of wood and wood products, to provide outdoor recreation, to maintain, restore, or enhance environmental conditions for wildlife, and for the protection and production of water supplies.

**Key features** mean key natural heritage and hydrological features described in section H57.4.6.

**Linkage** means an area intended to provide connectivity supporting a range of community and ecosystem processes enabling plants and animals to move between *Key Features* over multiple generations. *Linkages* are preferably associated with the presence of existing natural areas and functions and they are to be established where they will provide an important contribution to the long term sustainability of the *Natural Heritage System*. They are not meant to interfere with *normal farm practice*. The extent and location of the *linkages* can be assessed in the context of both the scale of the proposed *development or site alteration*, and the *ecological functions* they contribute to the *Natural Heritage System*.

**Major Creek or Certain Headwater Creek** means, as it applies to subsection (4) of the definition for *significant woodland*, all *watercourses* within a *Conservation Authority* Regulation Limit as of the date of the adoption of this Plan and those portions of a *watercourse* that extend beyond the limit of the *Conservation Authority* Regulation Limit to connect a *woodland* considered *significant* based on criteria under subsections (1), (2), and (3) of the definition for *significant woodland*, and/or *wetland* feature within the *Natural Heritage System*. The extent and location of *major creeks* or *certain headwater creeks* will be updated from time to time by the appropriate *Conservation Authority* and as a result may lead to refinements to the boundaries of *significant woodlands*.

**Major Retail** is defined as large scale or large format stand alone retail stores or retail centres that have the primary purpose of commercial activities.

**Natural Features or Natural Heritage Features and Areas** means features and/or areas which are important for their environmental and social values as a legacy of the natural landscapes of an area.

**Negative Impacts** means:

- 1) in regard to water, degradation to the quality and quantity of water, sensitive surface water features and sensitive ground water features, and their related hydrologic functions, due to single, multiple or successive *development or site alteration* activities;
- 2) in regard to *fish habitat*, any permanent alteration to, or destruction of fish habitat, except where, in conjunction with the appropriate authorities, it has been authorized under the Fisheries Act; and
- 3) in regard to other components of the *Natural Heritage System*, degradation that threatens the health and integrity of the *natural features* or *ecological functions* for which an area is identified due to single, multiple or successive *development or site alteration* activities.

**Public Authority** means any federal, provincial, regional, county or municipal agency including any commission, board, authority or department established by such agency exercising any power or authority under a Statute of Canada or Ontario.

**Significant** means:

- 1) in regard to wetlands, an area as defined in the definition of Significant Wetlands in this Secondary Plan;
- 2) in regard to the habitat of endangered species and threatened species, the habitat, as approved by the Ontario Ministry of Natural Resources, that is necessary for the maintenance, survival, and/or the recovery of naturally occurring or reintroduced populations of endangered species or threatened species, and where those areas of occurrence are occupied or habitually occupied by the species during all or any part(s) of its life cycle;
- 3) in regard to woodlands, an area as defined in the definition of Significant Woodlands in this Secondary Plan; and,
- 4) in regard to other components of the Natural Heritage System, ecologically important in terms of features, functions, representation or amount, and contributing to the quality and diversity of an identifiable geographic area or natural heritage system.

**Significant wetlands** means:

- 1) for lands within the *Natural Heritage System* but outside the Greenbelt Plan Area, *Provincially Significant Wetlands* and wetlands that make an important ecological contribution to the *Natural Heritage System*; and,
- 2) outside the *Natural Heritage System*, *Provincially Significant Wetlands*.

**Significant woodland** means a Woodland 0.5ha or larger determined through a *Watershed Plan*, a *Sub-watershed Study* or a site-specific Environmental Impact Assessment to meet one or more of the four following criteria:

- 1) the *Woodland* contains forest patches over 99 years old,
- 2) the patch size of the *Woodland* is 2 ha or larger if it is located in the Urban Area, or 4 ha or larger if it is located outside the Urban Area but below the *Escarpment Brow*, or 10 ha or larger if it is located outside the Urban Area but above the *Escarpment Brow*,
- 3) the *Woodland* has an interior core area of 4 ha or larger, measured 100m from the edge, or
- 4) the *Woodland* is wholly or partially within 50 m of a *major creek* or *certain headwater creek* or within 150m of the *Escarpment Brow*.

**Trailer Waiting Spaces** is defined as on-site designated commercial vehicle waiting spaces that are restricted in scope and/or extent in the Zoning By-law. *Trailer Waiting Spaces* do not include loading spaces. No other commercial vehicle parking is permitted, as it would be considered outdoor storage.

**Utility** means a water supply, storm water or wastewater system, gas or oil pipeline, the generation, transmission and distribution of electric power including renewable energy systems, the generation, transmission and distribution of steam or hot water, towers, communication or telecommunication facilities and other cabled services, a public transit or transportation system, licensed broadcasting receiving and transmitting facilities, or any other similar works or systems necessary to the public interest, but does not include a new sanitary landfill site, incineration facilities or large-scale packer and/or recycling plants or similar uses.

**Watercourse** means an identifiable depression in the ground in which a flow of water regularly or continuously occurs.

***Watershed management*** means the analysis, protection, development, operation and maintenance of water, water-related features, terrestrial resources and fisheries of a drainage basin.

***Wetlands*** means lands that are seasonally or permanently covered by shallow water, as well as lands where the water table is close to or at the surface. In either case, the presence of abundant water has caused the formation of hydric soils and has favoured the dominance of either hydrophytic or water tolerant plants. The four major types of wetlands are swamps, marshes, bogs and fens. Periodically soaked or wetlands being used for agricultural purposes which no longer exhibit wetland characteristics are not considered to be wetlands for the purposes of this definition.

## **H5.18 SPECIAL POLICY AREA**

Special Policy Areas apply to those lands within the Prestige Industrial Area that are subject to a land use policy that is specific to a property or area. All other relevant policies of this Plan are applicable unless otherwise modified or exempted by the provisions applicable to each Special Policy Area.

### **H5.18.1 SPECIAL POLICY AREA 1**

**H.5.18.1** Special Policy Area 1 shall only apply to the portion of lands designated as Prestige Industrial Area.

**H.5.18.2** The following additional uses are permitted and prioritized for the lands designated as Prestige Industrial Area and identified as Special Policy Area 1:

- a) hotels including full-service hotels with conference and exhibition facilities including accessory food services, restaurants and retail uses;
- b) conference and exhibition facilities; and,
- c) indoor waterpark consisting of waterpark slides, pools and wave pools and other water related entertainment structures that may also include outdoor waterpark facilities, accessory office space, accessory retail and food services, accessory outdoor storage, and maintenance buildings.

**H.5.18.3** That the following uses are also permitted only on the same property and ancillary to one or more of the uses permitted under Policy H5.18.1.2:

- a) place of entertainment, including:
  - i) amusement rides and structures;
  - ii) games of skill or chance excluding those regulated by the Province of Ontario as a gambling establishment;
  - iii) retail directly associated with a place of
  - iv) entertainment;
  - v) e-sport and e-gaming activities;
  - vi) go-carting;
  - vii) office uses accessory to the place of entertainment;

- viii) miniature golf;
  - ix) theatres and cinemas but excluding stand alone multi-plex cinemas that predominantly feature new release movies;
  - x) food services associated with another place of entertainment use.
- b) spa and wellness centre with associated outdoor pools, saunas, steam rooms and/or buildings, accessory food services, etc.;
- c) Full-service restaurants.

**H.5.18.4** That notwithstanding Policy H5.7.1.6 a hotel, or the portion of a building containing the hotel if the hotel is contained within the same building as another use permitted in Policies H5.18.1.2 and H5.18.1.3 is permitted to have a maximum height of eight storeys. Policy H5.7.1.8.3 in its entirety shall continue to apply to all non-hotel land uses. Appropriate transitions and buffering will be provided in accordance with the Urban Design Guidelines where heights more than 3-storeys are proposed adjacent to residential and institutional uses.

**H.5.18.5** Any use permitted under Section H5.7.1.3 and H5.7.1.4 may be subject to a Holding Provision in the implementing Zoning By-law, consistent with the objectives set out under Section G4.2 of the Town of Halton Hills Official Plan.

**H.5.18.6** Industrial uses noted under Section H5.7.1.3(b) shall only be permitted at such time that intersection improvements at Eighth Line and Steeles Avenue associated with the Eighth Line Environmental Assessment have been completed or that the East/West Collector Road intended between Eighth Line and Trafalgar Road (as shown on Schedule H5) has been completed and is open for public travel, or that a Transportation Impact Study (TIS) has been prepared demonstrating that the existing road network and intersections can satisfactorily accommodate truck traffic and turning movements to the satisfaction of the Town of Halton Hills and the Region of Halton. A Holding Provision in the implementing Zoning By-law may be utilized to implement this policy.

**H.5.18.7** The following policy shall apply to uses permitted under Section H5.7.1.3:

- a) Any proposed development must demonstrate how it could support achieving the long-term intent of the jobs per hectare Employment Area targets set out under Policy 83.2(5) and Table 2 of the Region of Halton Official Plan.
- b) To fulfill the vision and guiding principles of the Secondary Plan, which encourage a variety of higher density employment uses, prior to considering any application, Council shall be satisfied that:
  - i) the development provides land uses which could contribute to achieving the required density targets as prescribed by Provincial and Regional Policy;
  - ii) the development is planned to encourage shared use of land, secondary uses such as office spaces, efficient use of multi-storey buildings (full or partial), and to maximize the space to encourage higher employment densities;
  - iii) elements of the public realm which will serve both employees and the community alike will be improved as a condition of development/redevelopment; and,

- iv) low density development such as warehousing / logistics does not preclude intensification in the future. Therefore, a plan must be submitted that demonstrates how the site and buildings could transition to higher density employment uses in keeping with the Town's objective to pursue investment that generates the greatest community benefits, including a higher number of jobs per sq. ft. of gross floor area.

## H6 VISION GEORGETOWN SECONDARY PLAN

### H6.1 VISION STATEMENT

The Vision Georgetown community is an inspiring new urban community; distinctive in the way it looks and functions, fostering healthy lifestyles, neighbourliness, economic prosperity, and local pride. It is a resilient, sustainable, complete, and compact community, with a thriving natural heritage system. It feels like a small Town and is physically connected to the broader community of Georgetown and the Town of Halton Hills. It honours the rich heritage of the Town, emphasizes people, and provides choices for day-to-day living. Overall, the Vision Georgetown community is an exceptional, forward thinking, and innovative model for new community development.

### H6.2 GUIDING PRINCIPLES

1. To design a community that is connected internally and integrated with the rest of Georgetown, and other surrounding communities, through a network of roads, paths and trails.
2. To provide a wide range of residential, commercial, and institutional uses, in a manner that reduces the need for an automobile and supports all modes of transportation to meet the daily needs of life.
3. To protect existing natural heritage features and areas, and their associated ecological functions, and identify a linked natural heritage and open space system.
4. To create distinct neighbourhoods that feature community focal points and bring people and activities together.
5. To provide a range and mix of housing that is available to all ages, abilities, incomes and household sizes.
6. To provide adequate retail and service commercial development in a timely manner through various commercial areas, which are designed for people and pedestrians.
7. To encourage a high standard of design that reflects existing small town character, creates a sense of place, and contributes to civic pride.
8. To ensure convenient access to a range of types and sizes of parks and public spaces, which provide opportunities for recreation, neighbourliness, community events, and cultural activities.
9. To provide a range of accessible community facilities in a timely manner and to co-locate these facilities where possible.
10. To establish a transportation system that safely and efficiently accommodates different forms of travel (including automobiles, walking, and cycling) and plans for future public transit.
11. To provide opportunities for local economic development in a manner that fosters competitiveness and a prosperous business environment.
12. To ensure infrastructure is developed in a manner that minimizes social and environmental impacts, and considers long-term maintenance, operational, and financial requirements.



13. To apply sustainable development practices and encourage innovation, in order to maximize resource and energy conservation.
14. To conserve key cultural and built heritage resources as a vital link to our rich history.

### H6.3 EXCELLENCE IN COMMUNITY LIVING

It is the intent of this Plan to support excellence in community living based on the application of the following principles that result in:

- a) A well balanced community in terms of an appropriate mix and distribution of residential densities and complementary uses;
- b) The promotion of excellence in civic design in both the public and private realm;
- c) An interconnected system of open spaces, including recreational areas and natural features and areas;
- d) A range of recreational and community facilities that facilitate shared use where practical;
- e) The integration of new roads with existing roads adjacent to the Vision Georgetown Secondary Plan area;
- f) An attractive built form of appropriate building heights, massing, setbacks, streetscapes, gateways and architectural treatments;
- g) Efficient transportation links that provide for all modes of travel through and in and out of the Vision Georgetown Secondary Plan area and which are planned with a strong pedestrian orientation;
- h) Sustainable Community and Neighbourhood design in accordance with the Halton Hills Green Development Standards as updated from time to time; and,
- i) Practical and cost effective innovations to support the development of a sustainable community that encourages where possible, the application of low impact development, alternative energy sources and energy conservation, water conservation, approximate targets for an urban forest canopy and, the restoration, linkage and enhancement of natural features where appropriate.

### H6.4 COMMUNITY STRUCTURE

On the basis of the natural and fixed elements that exist on the landscape, the main elements of the community structure are shown on Schedule H6-1 and are described below:

- a) Natural Heritage System - this area is comprised of a number of natural heritage features, watercourse corridors, enhancement areas and buffer areas that will be protected and enhanced over the long term. Much of the Natural Heritage System is expected to come into public ownership as development occurs and it will be comprised of a number of passive recreational uses and most notably, a trail system that will link all elements of the Vision Georgetown together;

- b) Collector Road System - The road system is made up of one continuous north-south Major Collector Road that would provide for north-south travel through the Vision Georgetown lands and which would be primary route for transit when it is introduced. Three east-west Major and Minor Collector Roads extending between the Eighth Line and Trafalgar Road (Regional Road 3) are also proposed to provide for east-west connectivity and to Georgetown South via extensions to Danby Road and Miller Drive. These Collector Roads are intended to provide for the movement of motor vehicles, pedestrians and alternative forms of transportation in both a north-south and east-west direction. These Collector Roads are to be planned as complete streets;
- c) Community Core area - This area is to be planned as the main concentration of urban activities where a fully integrated array of institutional, retail and service, recreational, cultural and supportive uses are provided. A core commercial mixed use area fronting on Trafalgar Road (Regional Road 3) is also included within the Community Core to meet the needs of the new residents and those travelling on Trafalgar Road (Regional Road 3). A local commercial mixed use area is also located in the southern portion of the Community Core. Included within the community core is a secondary school, Community Park and library/community centre that will be integrated with each other;
- d) Major commercial area - The major commercial area will be where higher order commercial uses are established to support both the existing Georgetown South community and new residents on the Vision Georgetown lands. Located to the west of the major commercial area is a high density residential mixed use area. This area will also complement the existing Gellert Centre located on the east side of the Eighth Line;
- e) Local commercial mixed use areas on the Eighth Line and Trafalgar Road - Three local commercial mixed uses areas (one on the 8th Line at Street B and two on Trafalgar Road at Street D) will be where locally serving retail and service uses are located. Adjacent to these local commercial mixed use areas are planned elementary schools and neighbourhood parks that combine to form focal points in the new community; and,
- f) Schools - in addition to the one secondary school proposed in the Community Core, an additional secondary school will be combined with an elementary school on 10 Side Road. Four other elementary schools are also located in central locations throughout the Vision Georgetown lands.

On the basis of the above arrangement of land uses, a number of distinct neighbourhoods are created, with each being the site of parks and some with schools and connected with other neighbourhoods by collector and local roads, the proposed trail system, dedicated bike lanes and multi-use pathways. The system of proposed trails dedicated bike lanes and multi-use pathways are shown on Schedule H6-3.

In order to support population growth on the Vision Georgetown lands, the conveyance of lands for community facilities shall keep pace with growth in the Secondary Plan area to the maximum extent possible and practical, to avoid or minimize a reduction in service standards for such facilities.

In addition to the above, and to the maximum extent possible and practical, the conveyance of lands for, and the construction of, other public infrastructure shall keep pace with the growth in the Secondary Plan area so that the impacts of such growth can be appropriately managed, both fiscally and physically.

To support the objectives above, overall development within the Secondary Plan area shall be phased in accordance with Section H6.23 of this Plan.

## H6.5

### AMOUNT OF PLANNED GROWTH

- a. It is the intent of this Secondary Plan to accommodate approximately 23,800 residents and 2,400 jobs on the Vision Georgetown lands by 2031, and to establish a framework for the continued development of additional residential uses and jobs over the longer term as the area continues to evolve and mature into a dynamic urban area;
- b. The planned density for the Vision Georgetown lands is approximately 75 residents and jobs per hectare, with the calculation being net of the lands within the Natural Heritage System;
- c. The approximate number of residents and housing units by type are below, with these numbers subject to updating through the required Block Planning process:

	Units	Population
Low Density	2,287	8,257
Medium Density	4,012	10,351
High Density	3,043	5,234
Total	9,342	23,842

- d. In addition to the above, the potential exists for additional dwelling units to be developed in the form of accessory apartments. The longer term potential also exists for residential development on the Major Commercial Area, Core Commercial Mixed Use Area and Local Commercial Mixed Use Area designations; and,
- e. The approximate housing targets by dwelling unit type for the Vision Georgetown lands are below:

HOUSING MIX		
	Secondary Plan	Halton Region Best Planning Estimates (BPE)
Low Density	24%	62%
Medium Density	43%	21%
High Density	33%	17%
Total	100%	100%

## H6.6

### IMPLICATIONS OF PLANNED DENSITY ON BUILT FORM

The target number of people and jobs to be planned for will have a significant impact on built form, resulting in a mix of singles, semi-detached, townhouses and apartments.

In order to achieve this planned density, this Secondary Plan provides for and anticipates that:

- a) The proportion of dwelling units made up of single detached dwellings will be less than in other recent developments in Halton Hills - and this has the effect of providing more housing units on less land;
- b) Lot sizes, particularly for single and semidetached dwellings will generally be smaller than in other areas of the Town;
- c) Most new buildings will generally be located closer to the street to maximize the use of land and provide for a more pedestrian oriented environment;
- d) Rear public or private laneways will be permitted in strategic locations on the arterial and major collector roads to provide access that minimizes conflicts and provides for a more pedestrian oriented environment;
- e) The proportion of land devoted to surface parking may potentially be reduced in areas where a mix of uses is proposed and shared parking is possible, such as in the Community Core area which has the effect of providing additional land for new dwelling units and other uses;
- f) The amount of land covered by commercial and institutional buildings in relation to lands used for parking and open space uses will increase to reduce the amount of land area required for these uses;
- g) There also will be more of a reliance placed on on-street parking and lay-by lanes in key strategic locations internal to the community, such as the Community Core area; and
- h) There will be a greater emphasis on the integration of all land uses to make more efficient use of land.

## H6.7

### SUSTAINABLE DEVELOPMENT

#### H6.7.1

#### INTRODUCTION

- a) It is the intent of the Town that development and redevelopment is carried out in a manner that furthers the goals and objectives of this Plan, and particularly those that deal with sustainable development and healthy communities;
- b) In addition to the above, the Town will also consider developing and implementing a range of appropriate mechanisms and tools to promote and facilitate new development and redevelopment that addresses the sustainability objectives and policies of this Plan; and,
- c) One of these mechanisms and tools are the Town's Green Development Standards, and it is the intent of this Plan that new development within the Secondary Plan area will comply with the

standards established by the Town's Green Development Standards as updated from time to time.

#### H6.7.2 OBJECTIVES

It is the objective of the Town to:

- a) Encourage land use and development patterns that support the health and well-being of the people of Halton Hills and contribute to a higher quality of life;
- b) Promote the development of complete, sustainable and healthy communities that create and improve physical and social environments and expand community resources which enable people to mutually support each other in performing all the functions of life and in developing to their maximum potential, including:
  - i) Providing choices and opportunities for all residents of all ages, by providing a diverse range of housing types, transportation modes, employment options, and recreation or leisure activities, including opportunities for local food production; and
  - ii) Efficiently managing the natural and social resources of the community to achieve the optimal benefits for all residents of all ages;
- c) Recognize that the built environment plays a critical role in shaping the physical, psychological and social health of individuals and the communities they live within;
- d) Recognize that a number of factors, such as land use patterns, transportation networks, public spaces and natural systems can all promote increased physical activity, psychological well-being and healthier lifestyles for residents;
- e) Ensure the development of healthy and sustainable communities with an emphasis on the importance of design and green infrastructure;
- f) Recognize that healthy communities attract investment and labour, particularly for those working at home, in small spaces and in a collaborative setting;
- g) Adapt to and mitigate the impacts of climate change through the creation of resilient communities;
- h) Ensure that development and land use patterns consider the impacts of climate change;
- i) Promote improved accessibility for persons with disabilities and the elderly;
- j) Coordinate with other service providers, municipalities, government agencies, non-profit, and private partners to deliver, and where appropriate, to lead, healthy communities initiatives;
- k) Coordinate and appropriately deliver where possible social and community services to meet the needs of the population, including co-location or clustering of facilities in strategic locations to facilitate maximum access by residents and visitors; and,
- l) Promote public art to help create distinctive areas and people-places.

## H6.7.3 VISION GEORGETOWN SUSTAINABLE DESIGN GUIDELINES

### H6.7.3.1 INTRODUCTION

The Vision Georgetown Sustainable Design Guidelines prepared in support of this Secondary Plan provide a suite of proactive and forward thinking design considerations for the planning and development of the Vision Georgetown lands. These Guidelines are intended to be read in conjunction with this Plan and assist in the review of development applications.

The sections below from the Vision Georgetown Sustainable Design Guidelines include a number of objectives that are to be considered as this Secondary Plan is implemented.

### H6.7.3.2 NATURAL HERITAGE PROTECTION

The primary goal of natural heritage system protection in the Vision Georgetown Secondary Plan is to increase the certainty that the biological diversity and ecological functions of the area and the broader Natural Heritage System will be preserved and enhanced for future generations. Appreciation for natural areas also contributes to the quality of life that Georgetown residents enjoy. Future growth and development should be planned and constructed in such a way as to preserve and enhance the Natural Heritage System, while also providing access to educational and recreation opportunities through a network of parks, trails, and public spaces, where appropriate. On the basis of the above, it is the objective of this Plan that:

- a) Community members of all ages are provided access and opportunities to connect with and enjoy the natural environment;
- b) Streets and roads shown crossing the Natural Heritage System on Schedules H6-1 to H6-3 are planned to minimize impacts on the natural heritage system features and functions, and be designed to accommodate wildlife passage, transit, cyclists and pedestrians as well as motor vehicles;
- c) Streets and roads that have not been identified on Schedules H6-1 to H6-3 are planned to ensure there are no negative impacts on the Natural Heritage System features and functions, and be designed to accommodate transit, cyclists and pedestrians as well as motor vehicles;
- d) Streets and parking areas be designed to encourage infiltration into the ground with permeable paving where possible;
- e) The location and orientation of buildings frame and address parks and open space, and where possible, provide new opportunities for access and visual connections to the landscape as part of everyday life in Georgetown;
- f) Pedestrian and multi-use trails provide access to and through parks and where appropriate, the Natural Heritage System, to help encourage active transportation as a viable means of both recreation and transportation;
- g) Community initiatives, which educate and celebrate the importance of the natural environment are supported;

- h) Community awareness about climate change is promoted, and local action to help preserve the environment is supported; and,
- i) Natural Heritage System features and functions are monitored with established targets, measurable objectives and adaptive management responses through the development process as per the Vision Georgetown Subwatershed Study as defined in Policy H6.13.1 below, and as updated/revised through Environmental Implementation Reports (EIRs) and Environmental Impact Studies (EISs).

#### H6.7.3.3 ENERGY EFFICIENCY AND PRODUCTION

- a) The feasibility of establishing a cogeneration plant (also known as CHP - Combined Heat & Power) in the Community Core area be explored through the required Block Plan required by Section H6.10.2;
- b) Throughout the Secondary Plan area, new developments are encouraged to incorporate both active and passive strategies to reduce demand and increase energy efficiency to minimize the impact on the conventional energy distribution network, while also promoting the use of alternative clean and renewable energy sources. On the basis of the above, it is the objective of this Plan that, where feasible:
  - i) Programs and partnerships to leverage municipal investment and demonstrate excellence in energy efficient design be promoted;
  - ii) Passive strategies in building design and construction be employed to reduce total energy consumption and peak energy use;
  - iii) Renewable energy technologies be integrated into the building façade, roof and site design, while not detracting from the public realm;
  - iv) Renewable energy production be showcased as prominent design elements to promote their use;
  - v) The integration of active renewable energy production facilities onsite to help offset conventional demand be considered when larger institutional, commercial and residential buildings are developed;
  - vi) New buildings attain a level of sustainability with particular attention to achieving energy use reduction credits through the Halton Hills Green Development Standards;
  - vii) Reflective or light-coloured roofs, or other alternatives be considered for medium and high density residential, commercial, industrial and institutional buildings where green roofs are not feasible, to reduce the urban heat island effect and energy expenditure for climate control;
  - viii) Alternative or renewable energy sources such as solar panels are encouraged to be incorporated within building designs;
  - ix) Lighting for pedestrian and multi-use trails should, where feasible, implement solar panels to reduce energy demand from non- renewable sources;

- x) Other methods for improving energy efficiency and air quality such as earth source energy, passive solar design, building orientation, ventilation, increased insulation, photovoltaic panels, green roofs, cool roofs, and high quality windows be considered;
- xi) Net zero or net zero ready buildings be encouraged; and
- xii) Energy efficient lighting fixtures and appliances are encouraged.

#### H6.7.3.4 WILDLIFE HABITAT PROTECTION

New neighbourhoods will not be the exclusive domain of human beings, with the habitat being shared with many wildlife species. Bees, butterflies and birds are especially important as pollinators but are particularly vulnerable to changes in their habitat and migratory routes. On the basis of the above, it is the objective of this Plan that:

- a) Community gardens and public parks prioritize low-maintenance, drought resistant species;
- b) Bio-diversity be encouraged through the selection of native, non-invasive species of plant life;
- c) In order to ensure adequate nectar and pollen supply throughout the year, consideration be given to a range of flowering species which blossom successively throughout the spring, summer and fall seasons;
- d) Large expanses of glazed areas on buildings employ bird strike deterrent strategies;
- e) Building systems, as appropriate, be set up to automatically turn off major lighting after hours or direct light away from the Natural Heritage System once the sun has set to reduce energy use and minimize interference with the flight patterns of migratory birds; and
- f) Linkages are established between wildlife habitat features, including consideration for opportunities at proposed road crossings, to maintain habitat connectivity and wildlife passage.

#### H6.7.3.5 WASTEWATER, WATER AND STORMWATER MANAGEMENT

1. The locations of stormwater management facilities as shown on the Secondary Plan schedules represent their general location. The final location, number and configuration of such facilities will be more specifically delineated through an EIR/EIS or Functional Servicing Study (FSS). Further refinement of the locations, number and sizes may be done through an applicable Stormwater Management Plan prepared in support of individual development applications, without amendment to this Plan (i.e. Vision Georgetown Secondary Plan).
2. Throughout the community, development should be designed to conserve water use and to manage stormwater on-site through Low Impact Development techniques such as bioswales, rainwater harvesting systems, infiltration trenches, and stormwater management facilities. On the basis of the above, it is the objective of this Plan that:
  - a) New buildings be designed where possible to collect rainwater for irrigation on site, and reduce excess stormwater runoff, which carries pollutants into natural waterways and groundwater recharge areas, with these features allowing for the consideration of reduced sizes for stormwater management facilities;



- b) Stormwater management features be strategically located to take advantage of the existing topography and drainage patterns and to minimize their footprint;
- c) Stormwater management features be developed as naturalized facilities, and incorporate native planting to help support pollinator species, and enhance biodiversity;
- d) Stormwater management facilities be designed to support key features and ecological functions of the Natural Heritage System;
- e) Rainwater harvesting systems, such as rain barrels and other simple cisterns, be installed where feasible to capture rainwater, which can be used for landscape irrigation, thereby reducing unnecessary use of potable water;
- f) All buildings be designed for efficient water use using conventional methods, such as ultra-low flow fixtures and dual flush toilets and other innovative water saving measures like waterless urinals, and grey-water recycling systems;
- g) The re-use of relatively clean domestic waste water, or “grey water”, often from laundry machines, sinks, showers, baths and other appliances be encouraged to help minimize the use of the potable water supply;
- h) Landscaped areas be located to optimize water infiltration potential;
- i) Landscaping of public and private facilities utilize drought tolerant native and non-invasive species that require minimal irrigation;
- j) Surface parking areas minimize the use of impervious surface materials, such as through the incorporation of permeable pavers and trenches, where feasible;
- k) Impermeable hard surfaced areas (i.e. driveways and parking areas) be reduced and opportunities for ground water infiltration be encouraged;
- l) Rain gardens, complete with native plant species and soil media, be encouraged to detain, infiltrate and filter runoff discharge from roof leaders, or integrated into surface parking areas where feasible;
- m) Stormwater management facilities shall be located and designed such that they will accommodate stormwater runoff from new pavement (quality and quantity) from Trafalgar Road (Regional Road 3) identified in the Trafalgar Road (Regional Road 3) Transportation Corridor Improvements Class Environmental Assessment Study – Steeles Avenue to North of 10 Side Road – Environmental Study Report, and from 10 Side Road (Regional Road 10); and
- n) The number of stormwater management facilities shall be refined in a manner that considers both the need to provide for efficient development patterns and the need to protect and support the natural heritage system.

#### H6.7.3.6 LOCAL FOOD PRODUCTION

Throughout the community, opportunities should be sought to highlight local food production, urban agriculture and community gardens. On the basis of the above, it is the objective of this Plan that:

- a) Open spaces and roof tops on buildings that receive good sunlight be designed to incorporate urban agriculture and community gardens where appropriate;
- b) Space be allocated in the public realm for the retail sale of locally grown food;
- c) The selection of native, low maintenance and drought resistant plants be prioritized to minimize the spread of invasive species; and
- d) Local agricultural products are promoted to help ensure that they remain productive components of the local economy.

#### H6.7.3.7 MATERIAL SELECTION AND SOLID WASTE MANAGEMENT

New development and construction should incorporate sustainable materials and promote waste diversion strategies in order to minimize environmental impacts and reduce the amount of waste heading to conventional landfill sites. On the basis of the above, it is the objective of this Plan that:

- a) Light coloured materials be considered for large hardscape areas such as surface parking lots, driveways, pedestrian walkways and urban plazas;
- b) The use of salvaged or re-purposed construction materials for new buildings and public spaces, including the use of such materials for the construction of roads, multi- use pathways and trails be encouraged wherever feasible;
- c) Construction materials containing post-consumer waste or recovered materials be used in new construction, where permitted and feasible;
- d) Building materials be selected based on their durability, energy efficiency, lifecycle cost, and environmental impact; and
- e) Waste Reduction Plans be prepared for use during the construction process.

#### H6.7.3.8 GREEN ROOFS

Green roofs or vegetated roofs serve to absorb rainwater and reduce stormwater runoff, provide additional insulation to the building envelope, create habitat for wildlife and pollinators, and help mitigate the urban heat island effect.

On the basis of the above, it is the objective of this Plan that:

- a) Green roofs be encouraged throughout the community, as appropriate;
- b) Where green roofs are accessible, use of these spaces for local food production be encouraged; and
- c) Where green roofs are not easily accessible, the use of native, low maintenance plant species is encouraged.

#### H6.7.3.9

#### INNOVATION AND FUTURE TECHNOLOGIES

Part of planning for sustainability today means preparing for the seamless integration of the technologies and systems of tomorrow. Everyday renewable energy technologies become smaller, more affordable and more efficient. On the basis of the above, it is the objective of this Plan that:

- a) Where possible, the consideration for the integration of future technologies and infrastructure be part of community planning and design;
- b) Charging stations, which supply electricity for electric vehicles, be encouraged in new developments and parking lots and be incorporated into the design of high density development and mixed use buildings, as well as small and large-format commercial buildings and institutional buildings; and,
- c) All ground oriented developments be designed to accommodate electrical connections in all garages to facilitate the installation of car chargers at a later date and that all Part 3 and Part 9 buildings be designed to accommodate shared electric vehicle charging within associated visitor parking areas and the option for resident charging if selected by the purchasers of residential units.

#### H6.8

#### DESIGNING STREETS FOR ACTIVE TRANSPORTATION

Schedule H6-3 shows the Vision Georgetown Transportation Network. One of the keys to the success of the Vision Georgetown Secondary Plan will be the ease by which residents and others travel through the community and to adjoining areas. On the basis of the above, it is the objective of this Plan that:

- a) A comprehensive and integrated continuous trail network be established, in order to contribute to the establishment of walkable, bicycle friendly and active neighbourhoods;
- b) Trails, where feasible and appropriate, be utilized to create connections and linkages between parks, the Natural Heritage System, the community core, community facilities, and other activity nodes throughout Vision Georgetown;
- c) New trails provide seamless connections to Georgetown's existing active transportation network;
- d) Street and block configurations provide street exposure for natural features, and strengthen their presence as focal features;
- e) Streets be designed, where appropriate, to reflect complete street design principles, in order to balance the competing needs of pedestrians, cyclists, transit users and motorists;
- f) Streets be designed and laid out based on a modified grid pattern, which responds to topographical features, natural open spaces, built heritage, and existing development patterns;
- g) Street patterns provide continuous, safe and comfortable avenues of public movement and promote connections to neighbourhood focal points;
- h) Street patterns establish significant views and vistas, where feasible;

- i) Block lengths should generally range between 200 and 250 metres and in special circumstances, where blocks lengths exceed 250 metres, a through-block pedestrian walkway should be provided;
- j) Collector Roads have an urban character, and be designed with equal consideration given to the needs, safety and comfort of pedestrians, cyclists, transit and motorists;
- k) Dedicated bicycle lanes, with a minimum width of 1.5 metres, be provided on either side of Major Collector Roads and on one side of Minor Collector Roads;
- l) Multi-use paths, with a minimum width of 3 metres, be provided on one side of Major Collector Roads outside of the Community Core;
- m) Local Roads be designed with equal consideration given to the needs, safety and comfort of pedestrians and motorists, and reflect an intimate, pedestrian-scaled neighbourhood setting;
- n) Window Roads be considered adjacent to Arterial Roads, in order to promote neighbourhood visibility and provide a street-oriented built form presence, while eliminating the need for rear lotting;
- o) Public or private laneways be considered in strategic locations adjacent to Arterial and Collector Roads, in order to provide a street- oriented built form presence with a continuous rhythm of building frontages and front yard landscaping, while eliminating the need for front yard driveways; and,
- p) The most current Regional Active Transportation Plan be considered when implementing the active transportation policies of this Plan. Land uses should be aligned to support all modes of transportation while maintaining the mobility function of Major Arterial Roads.

## H6.9 BUILT FORM AND THE PRIVATE REALM

### H6.9.1 NEED FOR URBAN DESIGN GUIDELINES

All development applications shall be supported by urban design guidelines. Proponents shall have regard to the final version of the 'Vision Georgetown Sustainable Design Guidelines'. In addition to the above, it is the objective of this Plan that:

- a) Sites be planned and designed in keeping with Accessibility for Ontarians with Disabilities Standards (2005) and Crime Prevention Through Environmental Design Principles;
- b) New development be planned to attain a level of sustainability by complying with the Town's Green Development Standards;
- c) Landowners consider seeking current LEED Neighbourhood Development Certification, achieving efficiencies in the following categories: Smart Location and Linkage, Neighbourhood Pattern and Design, Green Infrastructure and Buildings, and Innovation and Design Process;
- d) Landowners consider seeking current LEED Building Design and Construction Certification, achieving efficiencies in the following categories: Sustainable Sites, Water Efficiency, Energy and Atmosphere, Materials and Resources, Indoor Environmental Quality, and Innovation and Design Process;

- e) Landowners consider seeking current LEED Homes Certification, achieving efficiencies in the following categories: Location and Transportation, Sustainable Sites, Water Efficiency, Energy and Atmosphere, Materials and Resources, Indoor Environmental Quality, and Innovation;
- f) The primary facade of all buildings in Vision Georgetown relate directly to the street and be sited generally parallel to it, creating a well- balanced, human-scale street and building relationship, which encouraged pedestrian activity;
- g) Where building elevations are visible from adjacent streets and open spaces, a variety of massing be achieved through alternative facade treatments, roof line, emphasis, building projections, materials, colours and certain architectural styles;
- h) A variety of roof types and forms should be provided, and be selected on a case-by-case basis, in order to ensure consistency with the architectural style of the buildings; and
- i) All buildings are designed to individually and collectively contribute to the character of the surrounding neighbourhood or district.

#### H6.9.2

#### INCREASED HEIGHT AND DENSITY

- a) Where a building height of greater than eight storeys and/or a density of greater than 250 units per net residential hectare is proposed in the Core Commercial Mixed Use Area, Local Commercial Mixed Use Area or High Density Residential Mixed Use Area designations, the policies of this section apply to the requested increases.
- b) The maximum height that may be considered without an Official Plan Amendment is ten storeys and the maximum density that may be considered is 400 units per net residential hectare subject to meeting the policies of this section.
- c) Additional height and density subject to the limitations set out in sub-section b) may be considered through the required Block Plan process set out in Section H6.23 subject to the preparation of urban design guidelines that establish the appropriate maximum height and density in the context of the site and surrounding existing and planned development, including with respect to topography, to ensure such buildings will be designed to:
  - i) be integrated with appropriate transitions to surrounding development particularly public uses such as schools and parks;
  - ii) establish an attractive streetscape with a defined street edge, which contributes to the pedestrian experience;
  - iii) reduce the perceived building mass through design measures such as the vertical articulation of the facades, building step-backs at the upper floors and the use of a podium;
  - iv) respond to the existing topography to minimize the use of retaining walls and blank street walls;
  - v) provide architectural expression which emphasizes the entry area and other special building areas, while screening mechanical penthouses and elevator cores and integrating them with the building design;

- vi) maximize sunlight as demonstrated, if required by the Town, through a sun/shadow analysis; and,
- vii) establish variations in building materials and design treatments.

## H6.10 COMMUNITY CORE

### H6.10.1 LONG TERM VISION

- a) The Community Core shown on Schedule H6-1 is envisioned as an important character area that functions as the primary gathering place in Vision Georgetown, and allows for various amenity and programming opportunities aimed at providing purpose and interest throughout the day and evening. Land use policies applying to the land use designations in the Community Core shown on Schedule H6-2 are contained in Section H6.11 of this Plan;
- b) Place making is critical to the long- term success of the Community Core, which shall strive to achieve a setting that reflects high quality design, where people can live, work, shop, learn and play;
- c) Streets within the Community Core will be planned as complete streets to balance the needs of all road users, including pedestrians, cyclists, transit-users, and motorists. In particular, the streets will be pedestrian oriented, and accessible for people of all ages and abilities. They will be framed by animated building edges including wide sidewalks, weather protection, lighting and wayfinding. A network of public and semi-private open spaces and pathways will be created to complement the Natural Heritage System and increase accessibility to outdoor open space, local public parks, and the Community Park;
- d) A mix of uses should be planned for that attracts a diversity of people throughout the day and evening, including seniors, students, shoppers, recreation and library facility users, cyclists and other residents; and,
- e) It is the intent of this Plan that the right conditions are created in the Community Core to encourage the short trip over the long trip. The variety of functions and amenities within the Community Core is intended to attract pedestrians from the surrounding neighbourhoods as an alternative to residents using their cars to go elsewhere for some of their day-to- day recreation, leisure and shopping requirements.

### H6.10.2 REQUIREMENT FOR A COMMUNITY CORE PLAN

- a) Prior to, or concurrent with, the consideration of individual applications within the Community Core, a Block Plan in accordance with Sections G3.2 and G3.3 of the Official Plan and Section H6.23.3 of this Secondary Plan shall be prepared first to guide development applications. The Community Core Plan may be part of a Block Plan for the Core only, or for all or part of the Secondary Plan Area.
- b) The Block Plan shall be prepared to the satisfaction of Council and contain the following:
  - i) A detailed overall land use plan, identifying the location of all of the proposed uses and in particular the location of medium and high density residential uses and the Core Commercial and Local Commercial Mixed Use Areas (the land use plan may show

modified boundaries of the land use designations and/or the Collector Roads based on more detailed analysis without requiring an amendment to this Secondary Plan provided that the long term vision for the area is achieved in particular the location of public facilities as a focal point for the Core);

- ii) A phasing strategy completed to the satisfaction of the Town that describes the proposed sequencing of development and the timing of any key public infrastructure improvements;
  - iii) A plan showing the location of any future public lands that may be dedicated to the Town as part of the approval process;
  - iv) The proposed built-form of the development including type, height, and architectural treatments;
  - v) The location of appropriate access points onto the abutting road and trail network;
  - vi) The location of pedestrian, bicycle, vehicular and service circulation and access and parking areas in the context of the overall parking management strategy;
  - vii) Measures that implement the feasibility study required by Section H6.7.3.3a) of this plan as appropriate, and where it has determined to be feasible to establish a cogeneration plant (also known as CHP - Combined Heat & Power) in the Community Core area; and
  - viii) Measures that implement the environmental mitigation and enhancement recommendations contained in the final approved EIR/EIS required by Section H6.13.4 of this Plan, or the final approved Subwatershed Study if an EIR/EIS has not yet been approved by the Town.
- c) A key element of the Block Plan required above will involve demonstrating how various land uses can share amenities such as open space and other facilities such as parking and loading areas. In this regard, the co-location of uses within buildings and on individual properties is strongly encouraged. In addition, barriers between public uses, particularly between parks and schools should be eliminated wherever possible.

#### H6.10.3 COMMUNITY HUBS

- a) The Town supports and encourages buildings and structures to be utilized to their fullest potential for the provision of programs and services, provided or subsidized, by a government or other body, such as social assistance, recreation, police and fire protection, health and educational programs or cultural services. When and where available these uses are encouraged to co-locate within the Community Core as a Community Hub; and
- b) Community Hubs may offer school-community partnerships, respond to local service or recreational needs, and provide more efficient and sustainable services, improved access to services and a positive social return on the investment to the community.

#### H6.10.4 COMMUNITY USES IN THE COMMUNITY CORE

A secondary school and a community centre/library are planned in the Community Core. It is the objective of this Plan that:

- a) The secondary school, the community centre/library and the neighbouring Town Square Park and Community Park be the subject of an integrated planning process that is undertaken in conjunction with the Block Plan required by Section H6.10.2 or through a separate public sector led process that ensures that all uses relate to each other, share space and land wherever possible and be designed in a manner where all uses complement each other;
- b) The secondary school and community centre/library incorporate the highest standard in architectural and sustainable design, with equal priority given to all visible building facades;
- c) The secondary school and community centre/library embody a distinct visual identity, while respecting the character of the Community Core and surrounding neighbourhoods through the complementary use of architectural styles;
- d) The library and community centre animate the two Collector Roads they front on, as well as the adjacent Town Square Park and Community Park, with active interior uses such as pools, gymnasiums, atriums, and cafeterias, where appropriate; and
- e) The secondary school and community centre/library promote safety and ease of access through well-defined entrances and windows facing the public streets and primary walkways.

#### H6.11 LAND USE DESIGNATIONS IN THE COMMUNITY CORE

##### H6.11.1 OVERVIEW

- a) Schedule H6-2 identifies the land use designations that apply in the Community Core. The land use designations are listed below:
  - i. Core Commercial Mixed Use Area;
  - ii. High Density Residential Mixed Use Area;
  - iii. Local Commercial Mixed Use Area;
  - iv. Medium Density Residential Area;
  - v. Major Institutional Area; and
  - vi. Major Parks and Open Space Area.
- b) In instances where the policies and designations contained in Section H6.11 vary with the policies within Part D of this Plan, the more specific and detailed policies of this Section shall prevail, provided the overall intent of the Official Plan is maintained;
- c) A Community Park within the Major Parks and Open Space Area designation is also planned in the Community Core and policies for the Community Park are contained in Section H6.16.4 of this Plan; and,



- d) A Town Square Park is also planned in the Community Core and policies on the Town Square Park are contained in Sections H6.11.7 and H6.16.7 of this Plan.

## H6.11.2 CORE COMMERCIAL MIXED USE AREA

### H6.11.2.1 GOAL

It is the intent of this Plan that the Core Commercial Mixed Use Area designation be planned to accommodate a diverse range of retail and service uses and complementary medium and high density residential uses.

### H6.11.2.2 PERMITTED USES

- a) Main permitted uses are limited to:
  - viii) Child care centres;
  - ix) Commercial fitness centres;
  - x) Department stores;
  - xi) Financial institutions and services;
  - xii) Offices including medical offices;
  - xiii) Places of entertainment;
  - xiv) Private and commercial schools;
  - xv) Public service uses;
  - xvi) Restaurants;
  - xvii) Retail and service commercial uses; and
  - xviii) Supermarkets, pharmacies and specialty food stores.
- b) Permitted complementary uses in single purpose or mixed use buildings uses are limited to:
  - i) Apartment dwellings;
  - ii) Long term care homes;
  - iii) Multiple dwellings;
  - iv) Places of worship; and,
  - v) Retirement homes.

#### H6.11.2.3 DENSITY AND HEIGHT

- a) The minimum height of any new residential building shall be four storeys and the maximum height of any building shall be eight storeys. The maximum permitted density shall be 250 units per net residential hectare;
- b) Notwithstanding the above, the Town may approve without amendment to this Plan, development with a building height higher than eight storeys and/or a density of greater than 250 units per net residential hectare subject to additional study through the Block Plan process in accordance with the provisions of Section H6.9.2; and
- c) The minimum height for non-residential buildings shall be the equivalent of two storeys and the minimum height of mixed use buildings comprising both residential and non-residential uses shall be three storeys.

#### H6.11.2.4 SPECIAL DEVELOPMENT POLICIES

- a) The lands in the Core Commercial Mixed Use Area designation should have an approximate area of 4.0 hectares;
- b) The establishment of a supermarket is a key component of the land use plan for the area;
- c) Prior to any development occurring on the lands within the Core Commercial Mixed Use Area designation, a Comprehensive Development Plan shall be prepared to the satisfaction of Council. The Comprehensive Development Plan shall consider all matters listed in Section D2.5.2.3.3 of the Town of Halton Hills Official Plan and the other matters listed in this Section;
- d) A key element of the Comprehensive Development Plan will be the preparation of a long term parking management strategy that considers the needs of both permitted and complementary uses and which takes into account the ultimate provision of underground and/or structured parking;
- e) Permitted complementary uses as per Section H6.11.2.2 b) shall only be permitted once 12,000 square metres of gross leasable floor area has been approved. Within the 12,000 square metres shall be a single use food store premises having a minimum gross leasable floor area of 1,800 square metres. The remaining 10,200 square metres may be located in stand-alone buildings or in the base of mixed-use buildings. For the purposes of this policy, 'approved' shall mean a site plan approval in accordance with Section 41 of the Planning Act. Consideration may be given by the Town to the phased site plan approval of the 12,000 square metres, provided a minimum of 7,500 square metres (which includes the 1,800 square metre food store premises) is included in the first phase;
- f) Development shall be planned to be pedestrian, bicycle and transit friendly from the outset with a pattern of streets and blocks that encourages pedestrian circulation even where the "streets" in large developments may initially be privately owned and maintained. In particular, development shall be oriented to the street and designed to promote a vital and safe street life and to support the early provision of transit;
- g) The development of a pedestrian oriented focal point or points that are walkable from nearby areas internal to the Community Core shall be a component of development in the Core

Commercial Mixed Use Area designation. Such features are intended to provide an internal focal point for the area and function as a small scale public gathering area serving the immediate neighbourhood and visitors to the area. These spaces should be easily accessible and visible to the public, contain facilities such as seating amenities, hard landscaping, and natural elements, and potentially also provide passive recreation uses, possible public or private programmed activities, and public art;

- h) Buildings shall be located on or close to the street line and massed at intersections to establish a strong street edge;
- i) Gateway features shall be required at the intersection of the Collector Road and Trafalgar Road that highlight the site as the entrance to the Community Core;
- j) Front yard parking shall not be encouraged and larger parking areas shall be set back an appropriate distance from the street edge, or landscaped to ensure that the majority of the street edge is the site of buildings or landscaping; and
- k) Well-delineated pedestrian walkways should be provided between the street and main entrances.

### H6.11.3 HIGH DENSITY RESIDENTIAL MIXED USE AREA

#### H6.11.3.1 GOAL

It is the intent of this Plan that the High Density Residential Mixed Use Area designation be planned to accommodate a range of housing types with permissions for non-residential uses.

#### H6.11.3.2 PERMITTED USES

- a) Main permitted uses are apartment dwellings and long term care homes and retirement homes;
- b) Stacked, back-to-back, block townhouse and multiple dwelling units may be considered provided they are not located closer than 50 metres from Collector Roads and Arterial Roads since lands adjacent to these roads are to be reserved for apartment dwellings, long term care homes and retirement homes; and
- c) Permitted complementary uses are set out below:
  - i) Home occupations subject to Section D1.3.1.4 of this Plan; and,
  - ii) Non-residential uses including child care centres, places of worship, retail uses, personal service uses, office uses and restaurants in the first storey of any building fronting on a Collector Road.

#### H6.11.3.3 DENSITY AND HEIGHT

- a) The density range shall be 75 to 250 units per net residential hectare and the maximum building height shall not exceed eight storeys. However, the Town may approve, without amendment to this Plan, development with a building height higher than eight storeys and/or a

density of greater than 250 units per net residential hectare, with the exception of lands designated High Density Residential Mixed Use Area on the Eighth Line, subject to additional study through the Block Plan process in accordance with the provisions of Section H6.9.2. The minimum building height shall be four storeys.

#### H6.11.3.4 SPECIAL DEVELOPMENT POLICIES

- a) The first storey of buildings within 50 metres of the intersection of two Collector Roads in the Community Core shall be designed to accommodate permitted non-residential uses over the long term. In this regard and where non-residential uses are planned, glazing should occupy about 50% of the first storey façade and first storey heights should be designed to accommodate a range of non-residential uses;
- b) If non-residential uses are proposed in the Community Core, some reliance will be placed on on-street parking to meet parking demand for permitted non-residential uses, as set out in the required Block Plan for the Community Core;
- c) Buildings should incorporate a high standard in architectural and sustainable design, with the highest priority given to street and open space facing facades, and secondary priority given to all other visible building facades; and,
- d) Buildings should be designed to have articulated facades primarily on the upper floors to provide for a more visually pleasing streetscape.

#### H6.11.4 LOCAL COMMERCIAL MIXED USE AREA

##### H6.11.4.1 GOAL

It is the intent of this Plan that the Local Commercial Mixed Use Area designation be primarily the site of retail and personal service uses that are designed to be transit supportive and pedestrian oriented. Residential uses shall also be permitted provided local commercial uses are also developed.

##### H6.11.4.2 PERMITTED USES

Permitted uses are set out below and are permitted in single purpose or mixed use buildings:

- a) Block townhouse dwellings;
- b) Child care centres;
- c) Commercial fitness centres;
- d) Financial institutions and services;
- e) Long term care facilities and retirement homes;
- f) Apartment dwellings;
- g) Multiple dwellings;

- h) Office uses including medical offices;
- i) Places of entertainment;
- j) Places of Worship;
- k) Private and commercial schools;
- l) Public service uses and other institutional uses;
- m) Restaurants;
- n) Retail and service commercial uses;
- o) Specialty food stores;
- p) Stacked townhouse dwellings; and,
- q) Supermarkets, pharmacies and grocery stores.

#### H6.11.4.3 SPECIAL DEVELOPMENT POLICIES

- a) Local Commercial Mixed Use Area sites should have an approximate area of 0.9 to 2.5 hectares;
- b) The planned built form characteristics for this designation encourage the development of a wide variety of building forms. In this regard, the maximum height shall be eight storeys. However, the Town may approve, without amendment to this Plan, development with a building height higher than eight storeys and/or a density of greater than 250 units per net residential hectare subject to additional study through the Block Plan process in accordance with the provisions of Section H6.9.2;
- c) Notwithstanding the permission for increased height beyond 8 storeys as set out above in subsection b), no more than eight storeys are permitted on lands designated Local Commercial Mixed Use at the 8th Line and Street B. In addition, any portion of a building on this site greater than 6 storeys is required to be generally set back a minimum of 40 metres from the centre line of the Eighth Line;
- d) A smaller scale of commercial use is anticipated with emphasis on good building/street relationships. On this basis, below is the design and built form criteria that shall be applied in the Local Commercial Mixed Use Areas:
  - i) Those buildings fronting on Collector Roads should be located on or close to the street line to reinforce a strong street edge;
  - ii) Front yard parking shall not be encouraged and larger parking areas shall be set back an appropriate distance from the street edge, or landscaped to ensure that the majority of the street edge is the site of buildings or landscaping;
  - iii) A strong street edge landscape treatment should be provided to contribute to the streetscape; and,

- iv) Well-delineated pedestrian walkways should be provided between the street and main entrances.
- e) The development of pedestrian oriented focal points that are walkable from nearby areas is required to be a key component of development in the Local Commercial Mixed Use Area designation. These spaces are intended to provide an internal focal point for the area and function as a public gathering area serving the immediate neighbourhood and visitors to the area. These spaces should be easily accessible and visible to the public, contain seating amenities, hard landscaping, and natural elements, and provide passive recreation uses, possible public or private programmed activities and public art. Adjacent commercial uses are encouraged to be integrated with and front upon these spaces;
- f) Permitted residential uses may be considered provided:
  - i) No less than 50% of the ground floor of all buildings on the lands within the Local Commercial Mixed Use Area designation within the Community Core at the intersection of Streets A and C is the site of, or planned to be site of, non-residential uses;
  - ii) No less than 50% of the ground floor of all buildings within 80 metres of the centre-line of the Trafalgar Road right-of-way on the lands within each of the two sites that are designated Local Commercial Mixed Use Area at Trafalgar Road and Street D is the site of, or planned to be site of, non-residential uses;
  - iii) A minimum of 3,700 square metres of floor area within the Local Commercial Mixed Use Area designation at the 8th Line and Street B is the site of, or planned to be site of, non-residential uses;
  - iv) The function of the lands as a focal point and local destination for goods and services is maintained or enhanced;
  - v) The non-residential uses on the site are visible from surrounding roads to encourage their viability; and,
  - vi) The density range shall be 40 to 250 units per net hectare.

#### H6.11.5 MEDIUM DENSITY RESIDENTIAL (IN COMMUNITY CORE)

##### H6.11.5.1 GOAL

It is the intent of this Plan that the Medium Density Residential Area designation be planned to accommodate a range of medium housing types in the Community Core and elsewhere on the Vision Georgetown lands where permitted.

##### H6.11.5.2 PERMITTED USES

- a) Main permitted uses are multiple, street townhouse, block townhouse, stacked townhouse, back-to-back townhouse and low-rise apartment dwellings and long term care homes and retirement homes; and

- b) Permitted complementary uses are set out below:
  - i) Home occupations subject to Section D1.3.1.4 of this Plan;
  - ii) Accessory apartments subject to Section D1.3.1.6 of this Plan;
  - iii) Institutional uses which generally serve the needs of the surrounding residential areas such as private elementary schools, child care centres and places of worship, subject to Section D1.3.1.9 of this Plan;
  - iv) Public elementary schools subject to Sections D1.3.1.9 and H6.17 of this Plan; and,
  - v) Live/work units which may include residential or non-residential uses in the first storey.

#### H6.11.5.3 DENSITY AND HEIGHT

The density range shall be 30 to 120 units per net residential hectare and the maximum building height shall not exceed four storeys. However, the Town may approve, without amendment to this Plan, development with a building height of a maximum of five storeys and/or a density of up to 150 per net residential hectare at the intersections of Collector and Arterial Roads or Collector and Collector Roads subject to additional study through the Block Plan process.

#### H6.11.5.4 SPECIAL DEVELOPMENT POLICIES

- a) All medium density development in the Community Core shall be accessed by Local Roads or private or public lanes to minimize access onto Collector roads and support the development of complete streets;
- b) Buildings fronting on the Collector Roads in the Community Core shall have its main facade facing the Collector Road and be located close to the street and designed to frame the street; and,
- c) In areas outside of the Community Core, direct access to Arterial Roads and Collector Roads for individual dwelling units is not permitted.

#### H6.11.6 MAJOR INSTITUTIONAL AREA (IN COMMUNITY CORE)

##### H6.11.6.1 GOAL

It is the intent of this Plan that the Major Institutional Area designation in the Community Core be the site of a Secondary School that may be combined with an elementary school and other community facilities such as a library/community centre that supports the Community Core and the broader Georgetown community.

##### H6.11.6.2 PERMITTED USES

- a) Permitted uses in the Major Institutional Area designation are limited to a stand-alone Secondary School or a Secondary School combined with an elementary school. Child care centres are permitted on a school site as an accessory use. Other permitted uses include other

community facilities such as a library and community centre (which may also include a child care centre) and public service uses to support the development of a community hub. The location of both the Secondary School and the library and community centre is shown on Schedule H6-2;

- b) Minor changes to the location of the planned library and community centre are permitted, provided they continue to be functionally connected with the planned Community Park and Secondary School;
- c) The maximum permitted height for a Secondary School is five storeys;
- d) The Town will explore either 'alternative' or 'reduced' setbacks and parking standards to support the efficient use of any school site and in support of reduced parking standards, a parking study shall be prepared to determine the appropriate parking spaces for the school site to the satisfaction of the Town, in consultation with the relevant School Board;
- e) Notwithstanding Section F8.1.1 of this Plan, both medium and high-density uses are permitted if the Secondary School site within the Community Core is not required;
- f) If the Secondary School is not required, an amendment to the Community Core Plan as specified in Section H6.10.2 would be required; and
- g) Minor changes to the location of the planned Secondary School are permitted, provided it continues to be functionally connected with the planned Community Park and the library and community centre.

#### H6.11.7 MAJOR PARKS AND OPEN SPACE AREA (IN COMMUNITY CORE)

##### H6.11.7.1 GOAL

It is the intent of this Plan that the Major Parks and Open Space Area designation in the Community Core be the site of a Community Park and Town Square Park.

##### H6.11.7.2 PERMITTED USES

- a) Permitted uses in the Major Parks and Open Space Area designation are set out in Section B2.3 of this Plan;
- b) Policies on the Community Park are located in Section H6.16.4 of this Plan; and
- c) Policies on the Town Square Park are located in Section H6.16.7 of this Plan.

#### H6.12 LAND USE DESIGNATIONS OUTSIDE THE COMMUNITY CORE

##### H6.12.1 OVERVIEW

- a) Schedule H6-2 provides the detailed land use designations for lands outside of the Community Core. The land use designations are listed below:
  - i) Low Density Residential Area;



- ii) Medium Density Residential Area;
- iii) Mixed Use Area (Gateway);
- iv) High Density Residential Mixed Use Area;
- v) Major Commercial Area;
- vi) Local Commercial Mixed Use Area;
- vii) Major Institutional Area;
- viii) Natural Heritage System; and
- ix) Eighth Line Special Study Area.

- b) In instances where the policies and designations contained in Section H6.12 vary with the policies within Part D of this Plan, the more specific and detailed policies of this Section shall prevail, provided the overall intent of the Official Plan is maintained.

## H6.12.2 LOW DENSITY RESIDENTIAL AREA

### H6.12.2.1 GOAL

It is the intent of this Plan that the Low Density Residential Area designation be planned to accommodate a range of housing types on a network of local roads and condominium roads that are designed for the motor vehicle, cyclists and pedestrians.

### H6.12.2.2 PERMITTED USES

- a) Main permitted uses are limited to single detached, semi-detached and duplex dwellings;
- b) In addition, street townhouse dwellings, stacked townhouse dwellings, back-to-back and block townhouse dwellings are also permitted provided the total number of such units does not exceed 10% of the total number of units in a Plan of Subdivision; and,
- c) Permitted complementary uses are set out below:
  - x) Home occupations subject to Section D1.3.1.4 of this Plan;
  - xi) Bed and breakfast establishments in single detached dwellings subject to Section D1.3.1.5 of this Plan;
  - xii) Accessory apartments subject to Section D1.3.1.6 of this Plan;
  - xiii) Garden suites subject to Section D1.3.1.7 of this Plan;
  - xiv) Institutional uses which generally serve the needs of the surrounding residential areas such as private elementary schools, child care centres and places of worship, subject to Section D1.3.1.9 of this Plan;
  - xv) Public elementary schools subject to Sections D1.3.1.9 and H6.17 of this Plan; and,

- xvi) Special needs housing subject to Section D1.3.1.8 of this Plan.

#### H6.12.2.3 DENSITY AND HEIGHT

- a) The minimum permitted density shall be 24 units per net residential hectare and the maximum permitted density shall be 35 units per net residential hectare;
- b) Notwithstanding the above, the minimum and maximum density permitted for street townhouse dwellings, stacked townhouse dwellings, back-to-back and block townhouse dwellings are 30 to 100 units per net residential hectare; and,
- c) The maximum building height shall not exceed four storeys.

#### H6.12.2.4 SPECIAL DEVELOPMENT POLICIES

- a) Dwellings should incorporate appropriate architectural and sustainable design, with highest priority given to street and open space facing facades, and secondary priority given to all other visible building facades;
- b) Each dwelling should have a unique identity, while respecting and responding to the surrounding context;
- c) Each dwelling should have appropriate facade detailing, materials and colours consistent with its architectural style;
- d) Identical building elevations should not be located side by side or directly opposite from one another. Such elevations should be separated by a minimum of 2 single detached dwellings;
- e) Identical building elevations should not appear more than 3 times within a cluster of 10 dwelling units; and,
- f) Variety of architectural expression is encouraged through the use of alternative façade treatments, rooflines, building projections, materials, colours and architectural styles.

#### H6.12.3 MEDIUM DENSITY RESIDENTIAL (OUTSIDE COMMUNITY CORE)

The lands in the Medium Density Residential Area designation shall be developed in accordance with Section H6.11.5 of this Plan. However, a limited number of single detached, semi-detached and duplex dwellings may also be permitted provided such units are generally located adjacent to lands in the Low Density Residential Area designation.

#### H6.12.4 MIXED USE GATEWAY AREA

##### H6.12.4.1 GOAL

It is the intent of this Plan that the Mixed Use Gateway Area designation be planned to accommodate a range of medium density housing types and a limited amount of non-residential uses at the intersections of Trafalgar Road (Regional Road 3) and the 10 Side Road (Regional Road 10) and Trafalgar Road and the 15 Side Road.

#### H6.12.4.2 PERMITTED USES

- a) Main permitted uses are multiple, street townhouse, block townhouse, stacked townhouse, back-to-back townhouse and low-rise apartment dwellings and long term care homes and retirement homes;
- b) Places of worship may also be permitted;
- c) The existing Stewarttown Public School is recognized as a permitted use;
- d) Elementary schools are also permitted as a stand-alone use on lands designated Mixed Use Gateway Area at the intersection of Trafalgar Road and 15 Side Road;
- e) Child care centres are permitted on a school site as an accessory use;
- f) Permitted complementary uses are set out below:
  - i) Home occupations subject to Section D1.3.1.4 of this Plan;
  - ii) Accessory apartments subject to Section D1.3.1.6 of this Plan; and,
  - iii) Limited non-residential uses including child care centres, retail uses, personal service uses, public service uses, office uses and restaurants provided the total floor area planned for these uses does not exceed 20% of the total amount of residential floor area within each area that is designated Mixed Use Gateway.

#### H6.12.4.4 DENSITY AND HEIGHT

The density range shall be 40 to 150 units per net residential hectare and the maximum building height shall not exceed five storeys.

#### H6.12.4.5 SPECIAL DEVELOPMENT POLICIES

- a) Prior to any development occurring on the lands within the Mixed Use Gateway Area designation, a Comprehensive Development Plan shall be prepared to the satisfaction of Council. The Comprehensive Development Plan shall include:
  - i) A detailed overall land use plan, identifying the location of all of the proposed uses;
  - ii) A detailed phasing plan that describes the sequencing of development and the timing of any infrastructure improvements;
  - iii) A plan showing the location of any future public lands that may be dedicated to the Town as part of the approval process;
  - iv) The means by which the non-residential uses are to be accessed by abutting Arterial Roads;
  - v) The proposed built-form of the development including type, height, and architectural treatments;
  - vi) The location of appropriate access points onto the abutting road network; and,

- vii) The location of pedestrian, bicycle, vehicular and service circulation and access and parking areas in the context of the overall parking management strategy.
- b) Notwithstanding the above, a Comprehensive Development Plan would not be required if the existing Stewarttown School was proposed to be expanded or if the lands designated Mixed Use Gateway Area at the intersection of Trafalgar Road and 15 Side Road are being redeveloped with a new elementary school;
- c) The development of a range of medium density housing types (street townhouse, stacked townhouse, block townhouse and back to back townhouses) is encouraged; and,
- d) Given the prominent location of this land use designation, special consideration will be given to establishing gateway features at the intersection of Arterial Roads.

#### H6.12.5 HIGH DENSITY RESIDENTIAL MIXED USE AREA

The lands in the High Density Residential Mixed Use Area designation shall be developed in accordance with Section H6.11.3 of this Plan.

#### H6.12.6 MAJOR COMMERCIAL AREA

##### H6.12.6.1 GOAL

It is the intent of this Plan to:

- a) establish the Major Commercial Area designation as a major activity area in the Secondary Plan Area;
- b) provide a focus for the development of major retail uses in the Secondary Plan Area;
- c) provide for the establishment of a focal point that is easily accessed by pedestrians, bicycles and transit; and,
- d) provide for a diverse range of retail and service uses to serve the Town and the Region.

##### H6.12.6.2 PERMITTED USES

- a) Main permitted uses are limited to:
  - i) Adult specialty stores;
  - ii) Child care centres;
  - iii) Commercial fitness centres;
  - iv) Department stores;
  - v) Financial institutions and services;
  - vi) Hotels and convention centres;
  - vii) Offices including medical offices;

- viii) Motor vehicle service stations;
  - ix) Places of entertainment;
  - x) Places of worship;
  - xi) Private and commercial schools;
  - xii) Private recreational uses, such as banquet halls and private clubs;
  - xiii) Public service uses;
  - xiv) Restaurants;
  - xv) Retail and service commercial uses; and
  - xvi) Supermarkets, pharmacy and specialty food stores.
- b) Permitted complementary uses in single purpose and mixed use buildings are limited to:
- i) Apartment dwellings;
  - ii) Long term care homes;
  - iii) Multiple dwellings; and
  - iv) Retirement homes.

#### H6.12.6.3 DENSITY AND HEIGHT

- a) The minimum height of any new residential building shall be four storeys and the maximum height of any building shall be six storeys; and
- b) The permitted density range for permitted complementary uses shall be 75 to 250 units per net residential hectare.

#### H6.12.6.4 SPECIAL DEVELOPMENT POLICIES

- a) The lands in the Major Commercial Area designation should have an approximate area of 5.0 hectares;
- b) The establishment of a higher-order supermarket serving a large trade area is a key component of the land use plan for the area;
- d) Prior to any development occurring on the lands within the Major Commercial Area designation, a Comprehensive Development Plan shall be prepared to the satisfaction of Council. The Comprehensive Development Plan shall consider all matters listed in Section D2.5.2.3.3 of the Town of Halton Hills Official Plan and the other matters listed in this Section;
- e) The Comprehensive Development Plan shall include a co-ordinated concept plan that also includes adjacent lands that are within the High Density Residential Mixed Use Area designation;

- f) A key element of the Comprehensive Development Plan will be the preparation of a long term parking management strategy that considers the needs of both permitted and complementary uses and which takes into account the ultimate provision of underground and/or structured parking;
- g) Complementary uses permitted as per Section H6.12.6.2 b) shall only be permitted once a minimum of 13,500 square metres of gross leasable floor area of non-residential uses has been approved. Notwithstanding the above, a retirement home or a long term care home, with permitted non-residential uses on the ground floor, shall be permitted once a supermarket is approved, provided the use does not occupy more than approximately 10% of the lands in the Major Commercial Area designation subject to consideration of shared parking and drive aisle with other uses. For the purposes of this policy, 'approved' shall mean a site plan approval in accordance with Section 41 of the Planning Act. The development of the 13,500 square metres may occur in phases;
- h) Development shall be planned to be pedestrian, bicycle and transit friendly from the outset with a pattern of streets and blocks that encourages pedestrian circulation even where the “streets” in large developments may initially be privately owned and maintained. In particular, development shall be oriented to the street and designed to promote a vital and safe street life and to support the early provision of transit;
- i) As this area develops, it is the intent of this Plan that a pedestrian oriented environment that is integrated with surrounding lands be established;
- j) The development of a pedestrian oriented focal point or points that are walkable from nearby areas is to be a component of development in the Major Commercial Area designation. However, these spaces shall be carefully designed to ensure they enhance, and do not limit, the commercial function, or restrict the achievement of the minimum gross leasable floor area and may be internal or external to buildings. Such features are intended to provide an internal focal point for the area and function as a small scale public gathering area serving the immediate neighbourhood and visitors to the area. These spaces should be easily accessible and visible to the public, contain facilities such as seating amenities, hard landscaping, and natural elements, and potentially also provide passive recreation uses, possible public or private programmed activities, and public art. Adjacent commercial uses are encouraged to be integrated with and front upon these spaces;
- k) Buildings shall be located on or close to the street line and massed at intersections to establish a strong street edge;
- l) Gateway features shall be required at the intersection of the 8th Line and 10 Side Road that highlight the site as the entrance to the Vision Georgetown Secondary Plan area; and
- m) Front yard parking shall be discouraged and larger parking areas shall be set back an appropriate distance from the street edge, or landscaped to ensure that the majority of the street edge is the site of buildings or landscaping.

#### H6.12.7 LOCAL COMMERCIAL MIXED USE AREA

The lands in the Local Commercial Mixed Use Area designation shall be developed in accordance with Section H6.11.4 of this Plan.

## H6.12.8 MAJOR INSTITUTIONAL AREA (OUTSIDE OF COMMUNITY CORE)

### H6.12.8.1 GOAL

It is the intent of this Plan that the Major Institutional Area designation be the site of a Secondary School that may be combined with an elementary school.

### H6.12.8.2 PERMITTED USES

- a) Permitted uses in the Major Institutional Area designation are limited to Secondary Schools that may be combined with an elementary school and/or an elementary school or Secondary School. The elementary school may be developed prior to the Secondary School. Child care centres are permitted on a school site as an accessory use. The joint Secondary School/elementary school site size shall be approximately 6.1 hectares. Public service uses are also permitted to support the development of a community hub;
- b) Notwithstanding Section F8.1.1 of this Plan, both medium and high-density uses are permitted if the Elementary/Secondary School site is not required;
- c) If the Secondary School is not required, a Comprehensive Development Plan prepared in accordance with Section G3.3 of the Town of Halton Hills Official Plan would be required before development applications are considered;
- d) The maximum permitted height for a Secondary School or a joint Secondary School/elementary school is five storeys. The maximum permitted height for a stand-alone elementary school is four storeys; and
- e) The Town will explore either 'alternative' or 'reduced' setbacks and parking standards to support the efficient use of any school site and in support of reduced parking standards, a parking study shall be prepared to determine the appropriate parking spaces for the school site to the satisfaction of the Town, in consultation with the relevant School Board.

## H6.12.9 NATURAL HERITAGE SYSTEM

### H6.12.9.1 GOAL

The primary goal of natural heritage system protection in the Vision Georgetown Secondary Plan is to increase the certainty that the biological diversity and ecological functions of the area and the broader Natural Heritage System will be preserved and enhanced for future generations.

It is the intent of this Plan that the features and functions of the Natural Heritage System be protected and enhanced over time, while providing opportunities for passive recreation and nature appreciation.

### H6.12.9.2 PERMITTED USES

- a) Permitted uses in the Natural Heritage System are limited to conservation uses and compatible passive recreation, which includes trails, as outlined in b) below. Lands that are within the Natural Heritage System are encouraged to be dedicated to the Town or another public authority as appropriate;

- b) Trails shall be permitted within linkage and enhancement areas of the Natural Heritage System provided that they:
  - i) Are not located in hazard lands;
  - ii) Use native species to naturalize trail edges;
  - iii) Are the minimum width required;
  - iv) Are designed with suitable surfacing material compatible with their surroundings; and
  - v) Are designed and located to manage access to the Natural Heritage System by minimizing impacts to Key Features.
- c) Trails shall be permitted within buffers of the Natural Heritage System, as approved by the Town, in consultation with the Region, and applicable Conservation Authority, where it can be demonstrated that there is no negative impact on key features and functions;
- d) Essential utility facilities may also be permitted, if it is deemed necessary in the public interest after all alternatives have been considered and, where applicable, as determined through an Environmental Assessment Process; and
- e) Stormwater Management facilities are not permitted. Notwithstanding the foregoing:
  - i) stormwater management components such as ancillary pipes, outlets, headwalls, and other associated infrastructure required to convey flow from facilities outside the Natural Heritage System to receiving water bodies may be permitted where deemed essential and it is determined there are no negative impacts on ecological features and functions through an EIR/EIS or other appropriate study; and
  - ii) appropriately designed Low Impact Development measures may be permitted within the buffer, linkage and enhancement areas of the Natural Heritage System if it is determined that there are no negative impacts on ecological features and functions through an EIR/EIS or other appropriate study.

#### H6.12.9.3 ENHANCEMENT AND RESTORATION

It is the intent of this Plan that the Natural Heritage Systems will, where possible, be enhanced both in the short and long terms through the development approvals process in accordance with the Vision Georgetown Subwatershed Study. Such enhancements may include but not necessarily be limited to:

- a) Increase in biological and habitat diversity;
- b) Enhancement of ecological system function;
- c) Enhancement of wildlife habitat;
- d) Enhancement of natural succession;
- e) Creation of new wetlands or woodlands;
- f) Enhancement of riparian corridors;



- g) Enhancement of groundwater recharge or discharge areas; and,
- h) Establishment or enhancement of linkages between significant natural heritage features or and areas.

#### H6.12.9.4 INTERFACE WITH THE NATURAL HERITAGE SYSTEM

The establishment of visual connections to the Natural Heritage System is a key objective of the Town. In this regard, every effort will be made to locate parks, community facilities and stormwater management facilities adjacent to or near the Natural Heritage System to allow for those linkages and connections to occur.

In addition, through the development approval process, efforts will be made to establish more than just connections at the ends of roadways into the Natural Heritage System. In this regard, opportunities to locate single loaded roads to maximize access will be explored, where possible, through the Block Planning Process.

#### H6.12.10 EIGHTH LINE SPECIAL STUDY AREA

##### H6.12.10.1 LOCATION

The Eighth Line Special Study Area identified on Schedule H6-2 applies to lands at the northwest corner of the Eighth Line and the 15 Side Road.

##### H6.12.10.2 OBJECTIVES

It is the objective of this designation to:

- a) Ensure that all land use and servicing options are carefully considered prior to development occurring;
- b) Ensure that development does not occur until a comprehensive review of land use and servicing options, urban design and environmental constraints is undertaken; and,
- c) Ensure that all new development is integrated with and enhances existing development in the Georgetown Community.

##### H6.12.10.3 NEED FOR COMPREHENSIVE PLANNING

The lands are the site of a treed slope that slopes to the south. As a consequence, the location of the slope will have impacts on the siting of new roads/accesses and development areas. Consideration will also need to be given to how the lands will be accessed by the 15 Side Road or the Eighth Line or both.

On the basis of the above, it is the intent of this Plan to require the preparation of a Comprehensive Development Plan for all lands within this Special Policy Area in accordance with Section G3.3 of the Town of Halton Hills Official Plan, before a determination of which uses are appropriate and how they are to be sited and serviced is required.

On this basis, development shall not be permitted on the subject lands until a Comprehensive Development Plan (CDP) applying to all the lands is prepared to the satisfaction of Council. The CDP shall deal with such issues as:

- a) The proposed form of servicing;
- e) The protection and enhancement of any natural heritage features and related ecological functions;
- f) The nature, location and density of all uses and the manner in which they are integrated on the subject lands and with existing development;
- g) The nature, extent and timing of any required road improvements and the overall road pattern for the subject lands; and,
- h) The urban design standards that are to apply.

The CDP shall form the basis of an Official Plan Amendment that will place the lands in appropriate land use designations in accordance with this Plan.

#### H6.12.11 REGULATED WATERCOURSE AND ASSOCIATED FLOODING HAZARD SOUTHWEST QUADRANT

A flooding hazard exists in the southwest quadrant of the secondary plan area east of Trafalgar Road (Regional Road 3). Conservation Halton has indicated that, pursuant to the Conservation Authorities Act and Ontario Regulation 162/06 (as amended), a regulated watercourse and associated flooding hazard is located in this area as generally shown on Schedule H6-4 of this Plan.

While a considerable amount of technical work was completed in advance of the preparation of this Plan, EIR-FSSs and/or EISs and FSRs will be required to support future development in this area to refine the Conservation Halton regulated area.

The flooding hazard shown on Schedule H6-4 as Conservation Halton's Regulated Watercourse and Associated Flooding Hazard Southwest Quadrant can be relocated within a future Tributary A watercourse corridor subject to the replication of riparian storage volume and the management of the flooding hazard within Tributary A. An area adjacent to the future Tributary A may be used as an enhanced floodplain area. The location and preliminary design of Tributary A and the enhanced floodplain area shall be finalized to the satisfaction of Conservation Halton and the Town of Halton Hills in advance of Draft Plan of Subdivision approval.

At the time this policy was approved, Schedules H6-1 and H6-2 designated portions of Tributary A and lands adjacent to Tributary A as Natural Heritage System. By way of this policy, the location and extent of the Natural Heritage System can be modified without an Amendment to this Plan to address Conservation Halton's regulated area within the southwest quadrant.

Other lands not included within Conservation Halton's regulated area within the southwest quadrant can be developed in accordance with the adjacent land use designation without requiring an Amendment to this Plan.

#### H6.13 SUBWATERSHED STUDY

##### H6.13.1 PURPOSE

Town Council endorsed the Vision Georgetown Subwatershed Study, prepared by AECOM and dated May 2017 in June 2017 and the Subwatershed Study Addendum dated September 2020 (collectively referred to in this Official Plan as the Vision Georgetown Subwatershed Study). The purpose of the

Vision Georgetown Subwatershed Study was "to develop a subwatershed plan that allows sustainable development while ensuring maximum benefits to the natural and human environments on a watershed basis. The subwatershed areas in this study include the headwaters of Sixteen Mile Creek and a headwater tributary of Silver Creek (part of the Silver Creek Watershed)."

The Vision Georgetown Subwatershed Study is regarded as a "Living Document" whose findings and recommendations may be updated/revised as appropriate based upon new information and analysis as noted in this Plan. Studies completed by the landowners which build upon the Vision Georgetown Subwatershed Study may also be considered as appropriate. Further analysis, updates and refinements will be set out in the EIR/EIS submitted in support of development applications.

The Vision Georgetown Subwatershed Study Addenda provide additional information regarding Special Study Areas that must be addressed to the satisfaction of the Town, Region and Conservation Authorities at the EIR/EIS or later stage of development.

#### H6.13.2 EXTENT OF NATURAL HERITAGE SYSTEM (NHS)

- a) A review and assessment of the Vision Georgetown lands was undertaken as part of the Vision Georgetown Subwatershed Study. The steps followed in developing the Natural Heritage System (NHS) on the Vision Georgetown lands included the identification of natural heritage features within and adjacent to the Vision Georgetown lands, screening for core areas and opportunities for enhancing the NHS, and the identification of ecological linkages, enhancement areas and buffers. This process includes the refinement of the Regional NHS to produce an area specific NHS, based on a detailed study. Further refinement may occur at the EIR/EIS or later stage of development in consideration of the Vision Georgetown Subwatershed Study and its associated Addenda. As such, the NHS is illustrated on Schedules H6-1 and H6-2 should be considered preliminary and subject to further refinement.
- b) A number of modifications and improvements to the existing Regional NHS were recommended through the Subwatershed Study process, along with additional requirements in the Addenda, including, but not limited to:

- i) Black Locust Woodland Special Study Area:

The Vision Georgetown Subwatershed Study and associated Addenda contain initial recommendations related to the protection and enhancement of the broader Block D significant woodland associated with the Silver Creek Tributary B valley. The broader significant woodland includes a black locust woodland community which is considered to be an invasive species, along the Eighth Line in a former wayside pit.

The Vision Georgetown Subwatershed Study and associated Addenda concludes that portions of the black locust woodland community adjacent to the Eighth Line could be removed and mitigated through the establishment of reforestation areas and infill restoration opportunities.

The extent of the black locust woodland community that can be removed, along with the location of the reforestation areas and infill restoration opportunities shall be determined through the EIR/EIS which will provide a detailed assessment of the black locust woodland ecological functions in accordance with relevant Provincial, Regional and Town policies. The EIR/EIS shall also identify the locations of reforestation areas

that best support the ecological function of the significant woodland and shall have regard to the recommendations made in the Vision Georgetown Subwatershed Study and associated Addenda.

Based on this detailed assessment the EIR/EIS shall delineate the portion of the Study Area that is to be included in the Natural Heritage System. Lands that are not integrated into the Natural Heritage System may develop in accordance with the adjacent Low Density and Medium Density Residential Area designations.

The final determination of land use within the Special Study Area shall be completed in accordance with the above policies and through a Planning Act application without requiring a subsequent Regional Official Plan Amendment or Local Official Plan Amendment.

- ii) Enhancement and Restoration Areas: The Subwatershed Study identifies the need for a number of enhancement areas, replication features and restoration opportunities required to mitigate for potential negative impacts and to increase the certainty that the biological diversity and ecological functions of the NHS will be preserved and enhanced for future generations. There are several enhancement areas, infill restoration opportunity sites and replication features required to be created as outlined in the Subwatershed Study – including targets for feature type and ecological functions to guide detailed design;
- iii) Local Linkage/Enhancement Area: The Subwatershed Study identifies a key local linkage to be restored and enhanced between the Silver Creek Tributary B system (Block D) and the adjacent significant woodland to the south (Block C). Consistent with the recommendations of the Subwatershed Study and input from the Region of Halton, reforestation areas, infill restoration opportunities, replication features and other enhancements will be included within or adjacent to the linkage based on established targets and as refined through subsequent stages of development; and,
- iv) Natural Channel Design and Riparian Enhancements: The Vision Georgetown Subwatershed Study Addenda and supporting technical documents provide detailed direction on channel realignments, natural channel design riparian storage and low flow channel enhancement areas for the Sixteen Mile Creek Tributary A reaches.
- v) Woodland A2

The relocation of woodland A2 as shown on Schedules H6-1 and H6-2 within the Vision Georgetown Secondary Plan area shall be permitted where the following principles are satisfied:

- a) The new woodland shall result in a net ecological benefit through the provision of improved vegetation community types and wildlife diversity;
- b) The new woodland shall be located within or immediately adjacent to the Natural Heritage System to ensure functional connections will be achieved and ecological interactions are enhanced over time and to increase habitat opportunities in the Natural Heritage System;

- c) The configuration of the new woodland shall be designed to minimize edge effects (i.e. reduce the edge to interior ratio) recognizing that it will be at least 0.5 ha in size;
- d) The new woodland shall form part of the Natural Heritage System and be designated as such; and
- e) The new woodland shall be 0.5 ha in area and the new woodland shall be provided with a buffer except where adjacent to other components of the Natural Heritage System which components may be counted towards the required buffer width. In the event that the new woodland is approved to be located within an enhanced floodplain area, a larger woodland of up to 0.7 hectares in size will be required.

The location and area of the new woodland shall be determined through the EIR/EIS process. Upon Woodland A2 being relocated, an Amendment to Schedules H6-1 and H6-2 of this Plan shall not be required to remove the lands on which Woodland A2 was previously situated from the Natural Heritage System and such lands may be used in accordance with the adjacent land use designation.

vi) AM-7

AM-7 as shown in the Town's Subwatershed Study and Addenda and on Schedule H6-4 provides for a water conveyance function however is not regulated and is not a component of the Natural Heritage System. The design of AM-7 and its function shall be addressed through EIR/EIS/FSR reports.

### H6.13.3

#### BUFFERS

The NHS as shown on Schedule H6-2 includes buffers that have been established in an effort to protect key natural features and ecological functions by mitigating the impacts of proposed development or site alteration.

Buffers are important components of the overall NHS and are required to maintain and protect natural features and the ecological functions of the NHS.

The Regional Official Plan allows for refinement of the NHS and buffers through a subwatershed study or an individual EIR/EIS, provided that these studies are accepted by the Region.

The Vision Georgetown Subwatershed Study and related addenda, as interpreted in accordance with the policies of Section H6.13.1, contemplate a variable buffer framework that generally ranges between 10 and 30 metres. This framework represents an initial assessment and recommendation of buffer widths based on general information on land uses contemplated adjacent to the NHS.

The final buffer width is to be determined through an EIR/EIS at the development stage when additional information is available to determine the nature of adjacent uses and related impacts on the system and may include additions or deletions to the buffer widths.

When determining the final buffer width, consideration shall also be given to matters such as:

- enhancement and mitigation opportunities such as fencing and vegetative planting within the buffers or on abutting lands;
- the location of passive trails, recognizing that trails will not be permitted in buffers of less than 10 metres, and the location of stormwater management components in accordance with Section H6.12.9.2 d); and,
- minor fill and grading encroachments established as part of an overall site alteration plan developed through the EIR/EIS, where no negative impact has been demonstrated in accordance with Section H6.13.4.1 f). This may include where features are being created or reconstructed as determined through the EIR/EIS.

The EIR/EIS shall be prepared in accordance with the policy directions in Section H6.13.4, Environmental Implementation Reports (EIR) of this Plan and any required EIR/EIS shall be prepared in accordance with terms of reference established by the Town/Region in consultation with the applicable Conservation Authority.

With respect to the establishment of buffers, through the EIR/EIS there will be consideration of the nature of abutting land uses in relation to the adjacent key features and the significance of those key features. In particular, the intensity of the abutting land uses as illustrated through specific plans for such uses (i.e. grading, setbacks maintenance, servicing, built form including height, location of buildings and structures and other activities) will be evaluated. In all cases, the establishment of buffer widths must have regard to the following:

- Maintain or improve the level of certainty regarding buffer function effectiveness post refinement including factors such as the timing of planting and certainty regarding the nature of the abutting development
- Achieve no negative impacts to the NHS key features and their ecological functions;
- The ecological functions, characteristics, significance and sensitivity of the NHS key features which will ultimately determine the final buffer width through the EIR/EIS; and
- Adhere to the relevant goals, objectives and policies of this Plan, Regional Official Plan and relevant Provincial policies to the satisfaction of the Town, and Region and in consultation with the applicable Conservation Authority;
- With regard to Fish Habitat, additional buffer requirements as per Provincial and Federal requirements may be necessary; and,
- Where a feature/area is regulated by a Conservation Authority, and the regulatory requirements exceed the required buffer, those regulatory requirements shall apply, subject to the provisions of the Conservation Authorities Act.
- Notwithstanding the buffer range, a lesser buffer width may only be considered abutting a public facility such as a stormwater management facility, school, or park:
- If it can be demonstrated through a facility fit plan or detailed engineering design that examines the intensity of use throughout the whole site and, in particular the location of buildings and structures and outside activity areas in relationship to the buffer, and that recommends appropriate mitigation

measures such as fencing and planting, such that there will be no negative impacts on the natural features and ecological functions of the NHS;

- Certainty can be established with respect to the proposed development form through zoning or some other legal mechanisms; and,
- After evaluation with respect to all other criteria set out with respect to buffers in Section H6.13.3.
- Where a public use such as a school does not proceed, the new use shall be developed based on evaluation in accordance with the requirements of this buffer policy (Section H6.13.3).

#### H6.13.4 ENVIRONMENTAL IMPLEMENTATION REPORTS (EIR)

##### H6.13.4.1 PURPOSE

- a) The purpose of an EIR/EIS is to clearly demonstrate how specific development applications (such as a Draft Plan) will implement the management strategy recommendations contained within Vision Georgetown Subwatershed Study as interpreted in accordance with the policies of Section H6.13.1, However, it is recognized that management strategy recommendations may be refined by additional analysis completed through the EIR/EIS subject to the satisfaction of the Town of Halton Hills and, where there is a regulated feature/area, to the satisfaction of the Conservation Authority.
- b) The proponent will be required to demonstrate, through the preparation of an EIR/EIS, that the issues of stormwater management, infiltration, Natural Heritage System delineation and protection and stream corridor design have been addressed through the Draft Plan of Subdivision process, for the entire sub-catchment area;
- c) During the preparation of the EIR/EIS, the boundaries of the Natural Heritage System are subject to appropriate refinements, based on more detailed information, additional surveying of features and final buffer, corridor, linkage, enhancement and restoration area design established through an EIR/EIS;
- d) Additional analysis related to the Black Locust Woodland Special Study Area, and the Regulated Watercourse and Associated Flooding Hazard Southwest Quadrant will be subject to review by the Town, the Region and applicable Conservation Authority.
- e) The EIR/EIS reporting is to respect the management requirements for the Natural Heritage System as outlined in Section H6.13.4.1, and in Section 7.4.2 and illustrated in Figure 7.3.1 of the Vision Georgetown Subwatershed Study as interpreted in accordance with the policies of Section H6.13.1. However, it is recognized that management strategy recommendations may be refined by additional analysis completed through the EIR/EIS subject to the satisfaction of the Town of Halton Hills and, where there is a regulated feature/area, to the satisfaction of the Conservation Authority.
- f) The proponent will be required to demonstrate to the satisfaction of the Town, in consultation with the Region and the applicable Conservation Authority that the refinements to the NHS through the EIR/EIS will occur in accordance with a systems approach by:

- i) Prohibiting development and site alteration within significant wetlands, significant habitat of endangered and threatened species and fish habitat except in accordance with Provincial and Federal legislation or regulations;
- ii) Not permitting the alteration of any components of the NHS unless it has been demonstrated that there will be no negative impacts on the natural features and areas or their ecological functions.

#### H6.13.4.2 EIR/EIS STUDY BOUNDARIES

- a) The Vision Georgetown Subwatershed Study as interpreted in accordance with the policies of Section H6.13.1, shows how the Secondary Plan has been broken into separate sub- catchment areas for the purposes of EIR/EIS preparation, which should be undertaken in conjunction with the Block Plans required by Section H6.23.4 of this Plan;
- b) The study area for an EIR/EIS will include not only the detailed assessment of the lands subject to application, but also an evaluation of how the lands subject to the application function within the subwatershed context; and,
- c) Where a portion of the Natural Heritage System is located within the sub-catchment area, it will be important to demonstrate that any required EIR's/EIS's were completed on the basis of logical ecological boundaries or tributary areas.

#### H6.13.4.3 EIR/EIS REQUIREMENTS

The EIR/EIS will examine and further assess issues not detailed in the Vision Georgetown Subwatershed Study including:

- a) Watercourse relocations and modifications, floodplain (riparian) storage, conveyance, sediment transport, as well as associated riparian enhancements and aquatic habitat assessment;
- b) Stormwater quantity (flood and erosion), quality and erosion control targets and requirements;
- c) Specific location and detailed design for Enhancement Area and Replication Wetlands;
- d) Natural heritage system feature- based water balance assessments;
- e) Specific buffer width requirements;
- f) Wildlife surveys at a greater level of detail than the Vision Georgetown Subwatershed Study, where appropriate;
- g) Multi-landowner facility design and locations;
- h) Operations and Maintenance Plans;
- i) Discrete monitoring requirements;



- j) Adherence to the Approved Halton – Hamilton and Credit Valley – Toronto and Region – Central Lake Ontario Source Protection Plan Policies, including identification of which land use activities may require development of Risk Management Plans;
- k) Facility cost sharing;
- l) Conceptual fisheries compensation plans where necessary; and,
- m) Location and design of stormwater management facilities in accordance with Section H6.7.3.5 of this Plan.

#### H6.13.4.4 NEED FOR TECHNICAL STUDIES

EIRs/EISs may also require a number of technical studies, the need for which will have been identified in the Vision Georgetown Subwatershed Study. Although individual studies are listed below, it is possible that they will be combined given the interrelationship of these issues. Studies may include:

- a) Aquatic habitat assessment including fish and aquatic invertebrate studies and riparian vegetation assessments where watercourse relocations and modifications are proposed;
- b) Studies to demonstrate or confirm that enhancement areas, restoration opportunities, replacement features, linkages and buffer treatments will implement subwatershed objectives and recommendations;
- c) Determining impacts associated with transportation, servicing and utility corridors (including detailed mitigation measures as required);
- d) Natural Heritage System feature-based water balance assessments and water balance assessment for baseflow contributing areas;
- e) Water balance assessment of recharge within the Wellhead Protection Area (WHPA)-Q1/Q2, including consideration of recharge water quality within the Issue Contributing Area (ICA) (chloride), to comply with Source Protection Plan policies;
- f) Additional monitoring of groundwater levels along the upper reaches of Tributary A to further refine hydrogeological linkages with the watercourse;
- g) Additional flow monitoring of all three tributaries for one year (four seasons) to further verify and/or calibrate the hydrologic model parameters;
- h) Functional SWM plan and outline approach and location of facilities to meet management strategy requirements;
- i) Natural Channel Design, informed by geomorphic parameters of the existing watercourse, where watercourse relocations and modifications are proposed and fisheries compensation plans are required;
- j) Additional geotechnical investigations to confirm valley slope stability and setback options for Tributary B and Tributary A, (Reaches AM-2 and AM-3);
- k) Additional water quality analysis to support LID best practice applications to ensure that Total Phosphorous (TP) reduction targets are met or exceeded; and,

- l) Additional servicing details for the proposed future development, either in the EIR/EIS or SWM Plan needs to consider and coordinate with Regional Road projects as well as Regional water and wastewater infrastructure projects.

#### H6.13.4.5 FLOODPLAIN AT THE EIGHTH LINE

The floodplain delineation for this study area starts at the Eighth Line crossing and moves upstream. Conservation Halton and Credit Valley Conservation have requested a flood hazard risk analysis be undertaken downstream of Eighth Line to ensure that proposed future development with the recommended SWM approach does not increase flood risk downstream of Eighth Line. This can be carried out as part of the SWM plan or EIR/EIS process (as long as the EIR/EIS is carried out to include the entire tributary).

#### H6.13.4.6 STORMWATER MANAGEMENT

- a) Planning for stormwater management shall:
  - i) Minimize, or, where possible, prevent increases in contaminant loads;
  - ii) Minimize changes in water balance and erosion;
  - iii) Refine the number of stormwater management facilities, in a manner that considers both the need to provide for efficient development patterns and the need to protect and support the natural heritage system and in a manner that reflects the objectives of Section H6.7.3.5 m) and n);
  - iv) Maximize the extent and function of vegetative and pervious surfaces;
  - v) Promote stormwater management best practices, including stormwater attenuation and re-use, and Low Impact Development practices; and
  - vi) Consider the impacts of climate change in the design of stormwater management systems.
- b) All proposals for development shall provide for a Low Impact Development approach to stormwater management that may include techniques such as rainwater harvesting, bio-retention swales, green roofs, permeable surfaces, clean water collection systems, and the preservation and enhancement of native vegetation cover; and,
- c) In considering proposals for stormwater management, the Town will assess alternatives for stormwater quantity and quality control and sustainable best management practices with regard to the following:
  - i) Location of stormwater management facilities with a preference for at source controls, and Low Impact Development practices where feasible and compatible with planning and engineering objectives;
  - ii) Impact of maintenance costs for wet and/or dry ponds and other stormwater management facilities to the Town; and

- iii) Minimize the number of stormwater management facilities without compromising the benefits of stormwater management.

#### H6.13.5 MONITORING

- a) According to the Subwatershed Planning Report prepared by the Province in 1993: "A subwatershed plan cannot be considered complete until its monitoring program is established. Monitoring programs should be designed to assess environmental changes in the subwatershed, to evaluate compliance with the plans, goals and objectives, and to provide information which will assist custodians of the plan to implement it and update it. The monitoring program should be presented as part of the subwatershed implementation plan."; and,
- b) On the basis of the above, monitoring in accordance with the Vision Georgetown Subwatershed Study as interpreted in accordance with the policies of Section H6.13.1 (as updated/revised through EIRs/EISs) will be required.

#### H6.14 ROAD NETWORK

##### H6.14.1 COLLECTOR ROAD NETWORK

- a) Streets A, B and C are considered to be Major Collector Roads and will have a minimum right-of-way width of 22.75 metres, which may be increased to 25 metres in the Community Core. A reduction from 25 metres to 24 metres may be considered provided all elements of the pedestrian realm can be provided in a manner satisfactory to the Town;
- b) Streets D, E and F are considered Minor Collector Roads and will have a minimum right-of-way width of 21 metres; and,
- c) Major and Minor Collector Roads are shown on Schedule H6-3.

##### H6.14.2 DESIGN OF COLLECTOR ROADS

- a) While the Major and Minor Collector Roads are designed to allow for the ease of motor vehicle traffic throughout Secondary Plan area, regard must be had in their design to the other public interest objectives established by this Plan, which require that higher density development be established along Collector Roads in a pedestrian oriented and transit supportive environment to enable the development of complete street;
- b) In this regard, Collector Roads must be designed in a manner that provides for other modes of non-motorized travel and the pedestrian. In addition, priority shall be given to ensuring that public transit facilities, such as stops, shelters and dedicated lanes are all part of the overall design of the Collector Road over the long term; and,
- c) Within the Community Core, it is the intent of this Plan that a pedestrian oriented public realm be established in this area to promote safe and walkable and a vibrant urban environment. Traffic calming measures may be utilized in this area and alternatives for motor vehicle traffic in terms of connecting roads through parallel roads shall be considered.

- d) The location and general alignment of new Collector Roads as shown on Schedule H6-3 are approximate. Based on the Transportation Studies undertaken in support of the Secondary Plan, the collector road network is integral to the overall transportation system and the planned development of the area. Adjustments to the precise alignment of the collectors may be permitted without an amendment to this Plan provided they maintain connections to the arterial roads and other collectors and shall be determined through municipal studies or studies prepared in support of development applications.

#### H6.14.3

##### STREET A

- a) Street A is planned as the central character avenue for the Vision Georgetown Secondary Plan Area. It is planned to serve a vital function within the community by providing a critical link between neighbourhoods, open space amenities and community facilities. As the main internal transit corridor, it is essential in facilitating public transit, cycling, pedestrian and vehicular connections throughout the community;
- b) As a character avenue, Street A shall be distinguished by streetscape treatments corresponding to the land uses and built form types found along its edges. As such, street character will vary according to neighbourhood context, with opportunities to define areas through upgraded streetscape treatments;
- c) The southern portion of Street A, between the Community Core and 10 Side Road, will have higher vehicular traffic volumes. As a result, direct access for individual driveways is discouraged and laneway access and the development of window streets is preferred;
- d) Within the Community Core area, direct access for individual uses will not be permitted to support an urban streetscape treatment that responds to a greater level of pedestrian traffic associated with adjacent higher density residential, street related retail and service functions, public transit facilities and open space amenities; and,
- e) The northern portion of Street A is intended to have lower traffic volumes, which will enable a mix of dwellings with direct access to Street A and dwellings that front on intersecting Local Roads.

#### H6.14.4

##### LOCAL ROADS

- a) Local roads are not identified on Schedule H6-3. The provision of local roads will be determined through the development process and will be in accordance with the requirements of Section F6 of the Halton Hills Official Plan and the additional policies of this Plan.
- b) Local Roads will have a minimum right-of-way width of 16 metres;
- c) A minimum right of way width of 14 metres for window streets may be considered subject to a report that demonstrates to the satisfaction of the Town how vehicular and pedestrian traffic, on-street parking and utilities can be appropriately accommodated; and,
- d) Components of the active transportation network as per Section H6.15 of this Plan will be planned on Local Roads.

#### H6.14.5 PUBLIC LANEWAYS

- a) Public laneways will have a minimum right-of-way width of 7.5 metres.

#### H6.14.6 TREE CANOPY

- a) Collector Roads and Local Roads shall be planned to be the site of a tree canopy that will provide shade and enhance and establish a vibrant urban environment. In this regard, a Tree Canopy Plan shall be prepared for each of these roads and the trees shall be planted as soon as feasible to ensure that a canopy is established in the shorter term; and,
- b) Each of the local roads shall also be the site of street trees that are planted in a manner that provides for the establishment a vibrant and healthy tree canopy. In order to maximize the amount of tree planting, the co-location of utilities is encouraged.

#### H6.14.7 SIDEWALKS

- a) All Collectors Roads shall have sidewalks on both sides; and,
- b) Given anticipated densities and the built form, all Local Roads shall generally have a sidewalk on one side in all cases. Exceptions may be considered in circumstances where the density is lower.

#### H6.14.8 ROUNDABOUTS

- a) Roundabouts shall be designed to incorporate pedestrian crossovers on each approach. Additionally, roundabouts shall include bicycle bypasses on approaches with bike lanes; and,
- b) Where the Town has identified the need for single or multi-use roundabouts at the intersection of collector roads, the Town may require the conveyance of additional lands for right-of-way purposes. Such additional right-of- way requirements shall be determined at the time of the design of the road facilities and will become part of the total required right-of-way.

#### H6.14.9 DEVELOPMENT ADJACENT TO ARTERIAL ROADS

- a) The development of reverse frontage lots on Arterial Roads is strongly discouraged to minimize the use of noise attenuation walls on the edges of the Vision Georgetown Secondary Plan Area.
- b) Noise attenuation walls as a mitigation measure shall only be considered where it has been demonstrated that there are no other reasonable alternatives.

#### H6.14.10 REGIONAL ARTERIAL ROAD NETWORK

10 Side Road (Regional Road 10) and Trafalgar Road (Regional Road 3) are Major Arterial Roads under the jurisdiction of the Region of Halton and are subject to the policies of the Regional Official Plan. Access to Regional Roads shall be in accordance with the most current Halton Region Access Management Guidelines and by-laws.

Trafalgar Road (Regional Road 3) has been identified as a Transit Priority Corridor by the Region's Mobility Management Strategy as shown on Schedule H6-3 to this Plan.

The Region is undertaking the design for the widening of Trafalgar Road from Steeles Avenue to Highway 7. The provisions of the Trafalgar Road Corridor Study - Steeles Avenue to Highway 7 - Municipal Engineer's Association Class Environmental Assessment will be implemented by the Region.

#### H6.14.11 TRANSPORTATION IMPACT STUDIES

Transportation Impact Studies for any parcel of land located within the Secondary Plan Area are required to build on the results of the Transportation Study completed in support of the Secondary Plan. Transportation Impact Studies shall be completed in accordance with the Region's Transportation Impact Study Guidelines.

#### H6.15 ACTIVE TRANSPORTATION

Schedule H6-3 establishes the proposed active transportation network in Vision Georgetown. In this regard, it includes the following components:

- a) Multi-use pathways;
- b) Bike-lanes within road right-of- ways; and
- c) Trails.

#### H6.16 PARKLAND

##### H6.16.1 AMOUNT AND LOCATION OF PARKLAND

- a) The dedication of parkland shall be in accordance with the Planning Act unless this is modified by a Master Parks Agreement;
- b) The purpose of the Master Parks Agreement is to facilitate Town acquisition of an optimal type and distribution of parkland throughout the entire Secondary Plan regardless of the size and location of the individual subdivision plans located therein; and,
- c) Cash-in-lieu of parkland may be considered by the Town as the smaller landholdings are developed.
- d) To the extent possible, stormwater facilities will be incorporated into the adjacent parkland and integrated into the trail and active transportation network where possible and designed in a manner to enhance the aesthetic appeal of the overall development.

##### H6.16.2 TYPES OF PARKLAND

The following types of parkland are identified on Schedule H6-2:

- a) A Community Park, which has an approximate area of 8.0 hectares, or as set out in the Master Parks Agreement;

- b) Five Neighbourhood Parks, which have been co-located with elementary schools and have approximate areas of 1.5 to 2.5 hectares, or as set out in the Master Parks Agreement;
- c) A number of Parkettes, which have approximate areas of 0.2 to 0.6 hectares each or as set out in the Master Parks Agreement; and,
- d) A Town Square Park located in the Community Core that has an approximate area of 0.5 hectares or as set out in the Master Parks Agreement.

#### H6.16.3 GENERAL PARKLAND SITING CRITERIA

All public parkland shall:

- a) Have as much street frontage as possible and be open to view on as many sides as possible to provide visibility from adjacent streets and promote safety;
- b) Maximize public safety through park block size, visibility, configuration and location of park fixtures and facilities;
- c) Have direct and safe pedestrian access from adjacent residential areas or adjacent environmental areas where appropriate;
- d) Be designed to minimize any potential negative impacts on adjacent residential areas through the use of such measures as planting, fencing and the provision of appropriate access, parking and buffers to active recreational facilities;
- e) Incorporate natural heritage features wherever possible into the design of the parkland;
- f) Be integrated into the fabric of the adjacent neighbourhood by promoting open space or walkway linkages to adjacent facilities, neighbourhoods and natural features;
- g) Incorporate natural and built shade features;
- h) Incorporate appropriate lighting, seating, level pathways, walkways and entrances where appropriate to assist in creating a more accessible and inclusive environment; and,
- i) Be connected, wherever possible, to trail systems, cycling routes, walkways, natural heritage corridors, utility corridors and drainage systems.

#### H6.16.4 COMMUNITY PARK

- a) The Community Park should incorporate recreational programming elements that target visitors from throughout Georgetown and the Town of Halton Hills, in addition to the neighbourhood residents;
- b) The design of the Community Park and the adjacent planned Secondary School should be coordinated in order to capitalize on opportunities for shared facilities and amenities, such as parking and playfields.
- c) The Community Park should incorporate seating areas, refuse / recycling receptacles, bicycle locks, pedestrian-scaled lighting, trees, accent / decorative planting, hard and soft landscaping, shade structures, public art and sports fields, where appropriate; and,

- d) The Community Park should incorporate on-site parking facilities. Such facilities should be accessed via Streets B and/or C, and should incorporate wayfinding signage and decorative / ornamental plantings adjacent to driveway entrances. Surface parking areas should incorporate permeable surface paving materials, landscaped medians with tree plantings, and designated pedestrian walkways where appropriate.

#### H6.16.5 NEIGHBOURHOOD PARK

- a) Neighbourhood Parks should be situated in the centre of Neighbourhoods, should front onto Local or Collector Roads, and should be accessible within a 500 metre walking distance of most residents;
- b) Neighbourhood Parks should be framed by public streets (or other public uses such as schools and/or the Natural Heritage System) on at least three sides;
- c) Neighbourhood Parks should incorporate recreational programming elements that target neighbourhood residents;
- d) Where Neighbourhood Parks are located adjacent to school sites, the design of both entities should be coordinated in order to capitalize on opportunities for shared facilities and amenities, such as parking and playfields; and,
- e) Neighbourhood Parks should incorporate seating areas, refuse / recycling receptacles, bicycle locks, pedestrian-scaled lighting, trees, accent / decorative planting, hard and soft landscaping, shade structures, public art and sports fields, where appropriate.

#### H6.16.6 PARKETTES

- a) Parkettes should be situated centrally within individual neighbourhoods, and should be accessible within walking distance of most residents;
- b) Parkettes should be framed by public streets or private streets (or other public uses such as the Natural Heritage System) on at least two sides;
- c) Parkettes should incorporate recreational programming elements that target neighbourhood residents; and,
- d) Parkettes should incorporate seating areas, refuse / recycling receptacles, bicycle locks, pedestrian-scaled lighting, trees, accent / decorative planting, hard and soft landscaping, shade structures and public art, where appropriate.

#### H6.16.7 TOWN SQUARE PARK

- a) The Town Square Park will be of the highest landscape and urban design. It should make a significant contribution to the character and identity of the community;
- b) The Town Square Park should be integrated with other public sector uses with active frontages to promote direct views and access;
- c) Storefronts should be located close to the edges of Town Square Park where feasible to create an active and vibrant pedestrian shopping environment;



- d) The Town Square Park should establish and frame prominent views and vistas, and should establish direct pedestrian connections, functioning as a prominent gateway feature within the Community Core;
- e) The Town Square Park should incorporate Low Impact Development techniques such as pervious paving treatments, bioswales, rainwater harvesting systems, and infiltration trenches, seating areas, refuse / recycling receptacles, bicycle locks, pedestrian-scaled lighting, banners, trees, accent / decorative planting, hard landscaping, shade structures and public art, where appropriate and,
- f) The Town Square Park should incorporate an appropriate range and variety of active and passive recreational uses. Such features may include patios, cafes, pergolas, event and gathering spaces, performing areas, fountains, and water features and skating rinks.

#### H6.17 LOCATION OF ELEMENTARY SCHOOLS

- a) The policies contained within Section F8.1.1 of this Plan apply, unless modified by this Section of the Plan;
- b) Notwithstanding Section F8.1.1 of this Plan, both low and medium density uses are permitted if a school site within the Vision Georgetown Secondary Plan is not required;
- c) Child care centres are permitted on a school site as an accessory use;
- d) Changes to the number of school sites or minor changes to the location of school sites identified in this Plan are permitted to satisfy locational, school accommodation needs and/or other general requirements of the relevant School Board without the need to amend this Plan;
- e) The maximum permitted height for elementary schools is four storeys;
- f) Draft Plans of Subdivision shall include designated school sites as appropriate with a shape, size and frontage satisfactory to the relevant School Board. The Town will explore either 'alternative' or 'reduced' setbacks and parking standards to support the efficient use of any school site and in support of reduced parking standards, a parking study shall be prepared to determine the appropriate parking spaces for the school site to the satisfaction of the Town, in consultation with the relevant School Board;
- g) Landowners will be required to submit at the Draft Plan of Subdivision stage an alternative lotting plan to facilitate development should the site not be used for school purposes; and
- h) For the elementary school located adjacent to the lands designated Major Institutional Area on the 10 Side Road, the elementary school site shall have a site size of approximately 2.4 hectares if located adjacent to a neighbourhood park.

#### H6.18 ENERGY CONSERVATION AND UTILITIES

- a) The Town will promote development on the Vision Georgetown lands that utilizes its best efforts to achieve carbon neutrality for buildings and infrastructure to reduce its greenhouse gas emissions and increase its climate resiliency. This will be accomplished through a range of strategies as set out below:

- i) All new buildings will be required to implement to the extent possible current energy efficiency strategies through approaches related to factors such as building design, efficient technologies and behavioural change initiatives;
- ii) The Town in consultation with stakeholders will explore the potential for the introduction of a cogeneration plant in the Community Core;
- iii) Where a cogeneration plant under development or has been developed, the Town shall require new buildings in the area served by the system to utilize the system. Where projects proceed prior to construction of the system but after such a system has been deemed to be viable and construction plans are underway, development plans shall be required to demonstrate that the project can link into the system;
- iv) The potential to use waste heat from sources such as retail and institutional uses, sewers, and wastewater will be explored through the development process as appropriate;
- v) Renewable energy generation and use will be maximized as much as possible. Renewable heat sources include solar, thermal and geo-exchange. Renewable energy generation can include biomass or biogas, combined heat and power, wind, active solar, and geothermal; and,
- vi) The Town will confirm with all utility providers that adequate servicing networks are, or will be established to serve the anticipated and existing development, and that these networks can be phased in a manner that is cost-effective and efficient.

## H6.19 CULTURAL HERITAGE

### H6.19.1 BUILT AND CULTURAL HERITAGE RESOURCES

- a) It is an objective of the Town to conserve significant cultural heritage resources and to ensure that all new development and any site alteration conserves significant cultural heritage resources. They shall be maintained and integrated into new development, where appropriate and feasible;
- b) The Town will exercise the powers and apply the tools provided by legislation, particularly the Ontario Heritage Act, the Planning Act, the Environmental Assessment Act, the Building Code Act, and the Municipal Act in implementing and enforcing the cultural heritage policies of the Town;
- c) The following properties within the Secondary Plan area are currently listed on the Town's Municipal Heritage Register and shown on Schedule H6-2 as locations of Cultural Heritage Value:
  - i) 10686 Eighth Line;
  - ii) 10677 Trafalgar Road; and
  - iii) 10579 Trafalgar Road (Mount Pleasant Wesleyan Methodist Cemetery) - which is identified as a cemetery on Schedule H6-1;

They may be considered appropriate for municipal designation under the Ontario Heritage Act;

- d) Other cultural heritage resources have been identified as candidates for conservation and are shown on Schedule H6-2 as locations of Cultural Heritage Value. These comprise:
  - i) 10229 Trafalgar Road.
- e) The abovementioned cultural heritage resources retain historical and associative, design and architectural and/or contextual value. Council, with advice from its Municipal Heritage Committee, will consider whether they should be included on the Town's Municipal Heritage Register. They may also be considered appropriate for municipal designation under the Ontario Heritage Act.

#### H6.19.2 IMPLEMENTATION

- a) In evaluating development applications, the Town:
  - i) Will encourage the use or adaptive reuse of cultural heritage resources, or key components of such resources, whenever possible as part of the new development in situ, or on an alternate site; or
  - ii) May, where resources which are not designated and are not to be conserved, request the documentation of such resources in a cultural heritage report with a detailed property history, architectural description and photographic recording.
- b) The Town may impose, as a condition of any development approvals, the implementation of appropriate measures to ensure the conservation of any affected cultural heritage resources, and where appropriate, their integration into new development;
- c) The Town will require a Cultural Heritage Impact Statement (CHIS) be prepared in accordance with Section F5.1.2 of this Plan to determine the resource's specific heritage significance and to establish appropriate conservation plans and/or mitigation measures be prepared where development or redevelopment is proposed:
  - i) On, within, adjacent to, or in the immediate vicinity of any designated cultural heritage resource; or
  - ii) On a property listed on the Town's Heritage Register.
- d) The Town may require a Cultural Heritage Impact Statement (CHIS) be prepared in accordance with Section F5.1.2 of this Plan to determine the resource's specific heritage significance and to establish appropriate conservation plans and/or mitigation measures for any development or redevelopment proposal that affects any of the cultural heritage resources mentioned above in Sections H6.19.1 c) and d);
- e) The conservation of cultural heritage resources should be integrated with the conservation strategies for natural heritage features and environmentally sensitive areas where appropriate;
- f) New development should achieve a compatible relationship with cultural heritage resources in their context (both those within and those adjacent to the Secondary Plan area) through consideration of such matters as, but not limited to, building height, massing, scale, setbacks, stepbacks, roof line and profile and architectural character and expression; and,

- g) The Town may take additional steps to recognize the heritage of Esquesing Township, which was first surveyed in 1818, and the Hamlet of Ashgrove by:
  - i) Creating interpretative plaques and displays; and
  - ii) Commemorating historic persons, families and events in the naming of public buildings, streets, parks and other public places.

#### H6.20

#### ACCESABILITY

- a) Accessibility shall be improved for persons with disabilities and seniors by removing or preventing land use barriers that restrict full participation in society; and,
- b) In reviewing applications under the Planning and Condominium Acts, the Town will have regard for accessibility to all facilities, services and matters to which these Acts apply and will identify, prevent, and/or remove land use barriers which may restrict full participation in society for persons with disabilities and seniors.

#### H6.21

#### AFFORDABLE HOUSING

The Town supports the provision of housing which is affordable to low and moderate- income households.

Affordable housing, including both rental and ownership, is important to providing housing opportunities for current and future residents. Supporting opportunities and incentives for affordable housing will improve market accessibility for current and future residents. On the basis of the above, it is the objective of this Plan that:

- a) A minimum of 30% of new housing units be affordable;
- b) Affordable housing units will include a mix and range of types, lot sizes, unit sizes functions and tenures to provide opportunity for all household types, including larger families, older adults, students and residents with special needs;
- c) The Town will encourage the provision of affordable housing through:
  - i) Working with the Region of Halton and the development community to consider progressive financial incentives to encourage and support the development of affordable housing;
  - ii) Supporting assisted housing, which is housing that is available to low and moderate income for households for rent or purchase where part of the housing cost is subsidized through a government program;
  - iii) Supporting accessory apartments;
  - iv) Considering innovative and alternative residential and community design standards that facilitate affordable housing; and,
  - v) Encouraging the development of purpose built rental housing with a full mix and range of unit sizes.

- d) An affordable housing strategy is required for the Secondary Plan Area through the Block Plan process that provides for the achievement of the affordable housing requirements of the Town and the Region. The affordable housing strategy will be initiated by the landowners and will involve the Region and the Town.

## H6.22 EXISTING LAND USES AND SMALL LAND HOLDINGS

- a) Existing dwellings or buildings located on lands designated for development in accordance with this Secondary Plan shall continue to have direct access to Trafalgar Road, Eighth Line and 10 Side Road and 15 Side Road until such time as access from an alternative road becomes available or the property is redeveloped;
- b) Existing land uses are allowed to continue and expansion to those existing uses, such as additions, decks and accessory buildings are also permitted;
- c) Development proposals for very small holdings will be evaluated with reference to their land use designations on Schedule H6-2 but in most cases, not until Subdivision Plans for larger, adjacent landholdings are submitted for approval; and,
- d) Provision shall be made in abutting plans of subdivision to ensure compatibility of new development with existing residential holdings and, where feasible, to provide for their ultimate redevelopment in accordance with this Plan.

## H6.23 IMPLEMENTATION

### H6.23.1 PHASING PLAN AND BLOCK PLAN REQUIREMENT

Prior to draft plan approval of individual applications for development, a Phasing Plan shall be prepared to the satisfaction of the Town. The Phasing Plan shall be supported by an Infrastructure Staging Plan and a Block Plan, both of which shall identify how the development in Vision Georgetown is to be phased in accordance with this Section of the Plan.

Draft plans of subdivision and zoning by-law amendment applications can be processed concurrently with the preparation of these Plans.

### H6.23.2 PHASING PLAN

- a) The Phasing Plan shall divide the phasing of development into a minimum of two phases and shall ensure that the sequencing of development within the Vision Georgetown Secondary Plan area occurs in a manner that:
  - i) Provides for the early development of a range of housing types (including low, medium and high density);
  - ii) Supports the early servicing of retail and other non-retail and service uses needed to support the new residents;
  - iii) Ensures that the entirety of the Community Core is in the first phase;

- iv) Supports the early servicing of school sites, their acquisition by the school boards and their development;
  - v) Supports the early servicing of the community centre and library in the Community Core;
  - vi) Supports the conveyance of lands within the Natural Heritage System into public ownership;
  - vii) Supports the early servicing of the Community Park;
  - viii) Supports the early development of centralized stormwater management facilities and associated infrastructure;
  - ix) Supports the early construction of Street A to provide for continuous north-south travel through Vision Georgetown;
  - x) Supports the early construction of Streets B, C and F to provide for east-west travel through Vision Georgetown;
  - xi) Supports the necessary transportation infrastructure improvements to Trafalgar Road (Regional Road 3), Eighth Line, 10 Side Road (Regional Road 10) and 15 Side Road, all of which may be subject to studies as required by the Town and/or Region; and,
  - xii) Allows for the completion of distinct components of the Vision Georgetown Secondary Plan area so that the length of construction in any given area is kept to a minimum where possible.
- b) The Phasing Plan shall be supported by an Infrastructure Staging Plan that sets out the sequencing of the installation and delivery of sewer, water and stormwater infrastructure on the Vision Georgetown lands.
  - c) In each phase, sub-phases generally corresponding to the Neighbourhoods, as delineated on Schedule H6-1, may be identified through the Block Planning process.
  - d) Prior to the commencement of the development in each phase, all requirements of the Town and the Region shall be satisfied and confirmation shall be received from utility providers and school boards that appropriate services and facilities can be accommodated.
  - e) The progression of development shall be contingent on the availability and efficient utilization of public infrastructure and services, including the construction of critical elements of the road network and that adequate schools and community facilities are provided in a timely fashion, in keeping with the complete communities principles of the Secondary Plan.
  - f) Prior to development commencing in a subsequent phase, a minimum of 75 percent of the gross developable area in the Low and Medium Density Residential Area designations in the preceding phase must be within draft plans of subdivision or site plans.
  - g) Public infrastructure such as roads, parks, fire halls, schools and servicing facilities may proceed at any time, subject to the availability of servicing infrastructure and other requirements of the Town and the Region.

- h) Council may, at its sole discretion, determine to permit development in a subsequent phase prior to a minimum of 75 percent of the gross developable area as set out in Subsection f) if it is determined by Council that the development for which the application is made is in accordance with the general purpose and intent of this Secondary Plan and if it is demonstrated, to the satisfaction of Council, in consultation with Halton Region, that there are no negative impacts on the Town or Region, including from a land use planning (development of complete communities) perspective, and infrastructure and financial impact perspective.
- i) Notwithstanding the phasing provisions in this section, in no case will one owner or group of owners be permitted to unreasonably delay the normal progression of development contemplated by this Plan. Where unreasonable delay is occurring as determined at the Town's sole discretion, the phasing may be re-evaluated to the satisfaction of the Town in consultation with Halton Region. In such circumstances, Council may revise the phasing, if it is determined by Council that such a proposal is in accordance with the general intent and purpose of this Secondary Plan, and if there are no unacceptable impacts on the Town as determined by Council in consultation with Halton Region.

### H6.23.3

### BLOCK PLAN

- a) The required Block Plan shall, in addition to identifying development phases in accordance with Section H6.23.2, identify the following:
  - i) various land uses and densities, with a breakdown of the anticipated range and mix of dwelling types and associated population yields;
  - ii) the limits of the Natural Heritage System, as recommended in the EIR/EIS prepared in accordance with the policies of this Plan;
  - iii) public transit facilities;
  - iv) the means by which affordable housing is to be delivered in accordance with Section H6.21 of this Plan;
  - v) all existing and proposed roads and public laneways including the location of sidewalks, multi-use paths, the trail system, and cycling infrastructure;
  - vi) sizes of schools, parks, and other community facilities, which are informed by facility fit plans that are supported by the Town and/or the school boards;
  - vii) conceptual location of centralized mailboxes;
  - viii) cultural heritage resources;
  - ix) proposed stormwater management ponds and related infrastructure; and
  - x) any essential servicing corridors that are to traverse the Natural Heritage System.
- b) The approval of the Block Plan by Council in consultation with the Region of Halton, the Conservation Authorities and the School Boards shall be required.

- c) The Block Plan shall be supported by a Traffic Impact Study that is supported by a Terms of Reference that is accepted by the Town and Region that will analyze existing and future traffic conditions based on any upgrades to the roadway network that are planned to occur and make recommendations on:
  - i) appropriate improvements needed to the external road network to accommodate the proposed development; and
  - ii) the timing of the development of external road improvements and internal road infrastructure in relation to overall development timing.
- d) The Block Plan shall provide the anticipated schedule of the residential and non-residential development in the Block Plan area.

#### H6.23.4 OTHER TOWN REQUIREMENTS

In addition to the requirement for a Phasing Plan and a Block Plan as set out in this section, the draft plan approval of individual applications for development may only be considered when:

- a) The Town has in full force and effect, and not subject to appeal, a Development Charges By-law enacted under the Development Charges Act, 1997 or any successor legislation, identifying and imposing charges applicable to the lands in the Secondary Plan Area;
- b) Landowners within the Secondary Plan area have entered, or will enter, into an agreement or agreements with the Town in accordance with the Infrastructure Staging Plan and the approved Block Plan;
- c) Landowners within the Secondary Plan have entered, or will enter, into a private cost-sharing agreement(s) amongst themselves to address the equitable distribution of costs of development, for the provision of matters such as community uses and infrastructure facilities, including, where appropriate, the lands required for such uses and facilities;
- d) Landowners within the Secondary Plan area have entered, or will enter, into an Allocation Agreement with the Region of Halton addressing the provision of water and wastewater servicing and roads;
- e) Water treatment and distribution and wastewater collection and treatment are available in accordance with Town and Regional policies;
- f) The preparation of a Master Parks Agreement which further expresses the requirements of the Planning Act has been initiated; and
- g) The requirements of Section G.12 (Pre-consultation and Complete Applications) of the Town of Halton Hills Official Plan have been addressed to the satisfaction of the Town in consultation with the Region and other applicable agencies.

#### H6.23.5 DEVELOPMENT MONITORING

The Town shall establish a development monitoring program for the Vision Georgetown lands that may include:

- a) Level of population and employment growth;



- b) Supply of existing lots and number of building permits granted;
- c) General achievement of housing mix targets;
- d) Occupancy permits granted; and
- e) Development application status.

#### H6.23.6 NET RESIDENTIAL HECTARE

- a) Means for detached, semi-detached, duplex, street townhouses and other dwelling types with individual frontages on a public street, the area of land measured in hectares for residential dwelling units and consists of only the residential lots and blocks.
- b) Means for cluster or group housing and apartment blocks, the area of land measured in hectares for residential units, private internal roads and parking, landscaped areas, and other associated amenities.

## H7 DESTINATION DOWNTOWN SECONDARY PLAN

### H7.1 INTRODUCTION

- a) This Secondary Plan is a refinement of the existing policy framework for development within Downtown Georgetown. This Secondary Plan does not propose any development, but rather, it establishes a framework within which the Town will review future redevelopment applications.
- b) In considering the policies of this Secondary Plan, Provincial and Regional policies shall be read in their entirety and all relevant policies are to be applied to each application for development.
- c) This Secondary Plan shall be read in its entirety, and all relevant policies applied to every application for development. Further, this Secondary Plan shall be read in conjunction with all of the applicable policies of the Town of Halton Hills Official Plan. Where there is a conflict between the policies of this Secondary Plan and any policies in the Official Plan, the policies of this Secondary Plan shall apply.
- d) This Secondary Plan includes the following text, as well as:
  - i. Schedule H7-1 - Downtown Georgetown Land Use Plan;
  - ii. Schedule H7-2 - Downtown Georgetown Built Form; and,
  - iii. Schedule H7-3 - Downtown Georgetown Active Transportation Plan.
- e) Attached to this Secondary Plan are the following Appendices:
  - i. Appendix I – Downtown Georgetown Demonstration Plan;
  - ii. Appendix II - Downtown Georgetown Design Guidelines; and,
  - iii. Appendix III - Downtown Georgetown Cultural Heritage Resources.
- f) The Appendices are intended to provide additional detail and clarity to the policies and Schedules of this Secondary Plan. They are Council endorsed, but are not a statutory component of this Secondary Plan.

### H7.2 VISION + PRINCIPLES

Today the Downtown remains the administrative, cultural and historic heart of Georgetown. It is also a residential neighbourhood, containing a significant cluster of service commercial and retail activity. The retail component generally includes smaller-scale stores that serve a 'boutique' function, serving both the local residential community, and beyond.

Downtown Georgetown includes an eclectic mixture of buildings - different styles, different scales and from different eras. The mixture of land uses varies from single detached homes to apartments and small-scale office uses. Main Street supports an inventory of historic buildings that accommodate a range of retail and service commercial land uses and restaurants.

Downtown Georgetown includes distinct districts that reflect the historic development pattern that has evolved over the past century and a half. These districts form the basis for the land use and policy framework articulated in this Secondary Plan.

New development within Downtown Georgetown will happen incrementally over the long-term. New development will take many forms and will respond to market forces, financial feasibility and political directions over many years. To achieve long-term success, it is imperative that a commitment to revitalization and redevelopment is shown by the public sector, including the Town, the Region and, where appropriate, the Province of Ontario.

#### H7.2.1 VISION STATEMENT

Downtown Georgetown is a vibrant destination that serves the residents of Georgetown and Halton Hills and draws visitors from all corners of the Greater Golden Horseshoe Area.

New development will build on the rich natural and cultural heritage that makes Downtown Georgetown unique and so cherished by all who live there and visit.

Through sustainable development and enhanced public realm initiatives, Downtown Georgetown will continue to grow and offer an increasingly diverse range of places to live, work, shop, be entertained, and enjoy community life in a setting that artfully integrates old and new development into a picturesque landscape.

#### H7.2.2 PRINCIPLES

- Ensure new development celebrates and protects the existing built heritage character of the Downtown.
- Establish a variety of beautiful public gathering spaces to support cultural events, festivals and community life throughout the year.
- Create vibrant, safe and comfortable pedestrian- oriented streets that enhance mobility for pedestrians, cyclists and drivers and support existing and future transit.
- Promote a mix of uses in a variety of building forms, including a range of housing types and opportunities for retail, commercial and community uses.
- Protect and enhance natural features while broadening opportunities for public access, enjoyment, education and stewardship.
- Demonstrate high-quality design in new development and incorporate best practices that respect and complement the character of Downtown Georgetown and its adjacent neighbourhoods.
- Incorporate sustainable development and construction practices to maximize resource conservation.

#### H7.3 OBJECTIVES FOR A SUCCESSFUL DOWNTOWN

Building upon the Vision + Principles, fundamental to realizing a successful Downtown Georgetown are a number of interrelated objectives. These objectives are as follows:

#### H7.3.1

#### A COMPLETE COMMUNITY

- a) A complete community meets people's needs for daily living throughout an entire lifetime by providing convenient access to an appropriate mix of jobs, local services, a full range of housing including affordable housing, public services and community infrastructure including educational and recreational facilities, and a robust open space system. Convenient access to transit and options for active transportation are crucial elements of a complete community. Objectives are:
  - i) To support the creation of a complete community with easy access for all residents of all ages and abilities to a wide range of uses including shopping and restaurants, parks and open spaces, employment opportunities, educational and cultural opportunities, live-work options, mobility options, a mix and diversity of housing types, and a range of community facilities, services and amenities;
  - ii) To encourage the development of a complete community that includes higher intensity built forms and land uses that optimize the use of land, while maximizing the efficiency and cost effectiveness of municipal service infrastructure. Higher intensity forms of housing may also enhance life-cycle and affordable housing options; and,
  - iii) To support access to shared community infrastructure, between the Georgetown Major Transit Station Area (MTSA) and Downtown Georgetown as a way to complement the objectives of this Plan.

#### H7.3.2

#### A HIGH-QUALITY DOWNTOWN

- a) A high-quality and ultimately a beautiful Downtown will include well designed buildings, streetscapes, parks and open spaces. A beautiful Downtown protects natural and cultural heritage features and views and includes an accessible and well- designed system of public parks and open spaces that celebrate the area, and provide opportunities for enjoyment by the entire population.
- b) A high-quality Downtown recognizes the importance of the inventory of cultural heritage resources and builds upon that context, identifying destinations, landmarks and gateways that distinguish the Downtown within its broader context and strengthens its sense of place. Crucial to a high- quality Downtown is the attention to the interplay among built form, the public realm and the natural environment.
- c) A high-quality Downtown should engender a sense of pride as a place to live and a sense of stewardship in its long-term care and maintenance. Objectives are:
  - i. To develop a welcoming Downtown that encourages and supports active living, social engagement, civic pride and the creation of a sense of place and well-being for all people of all ages and abilities;
  - ii. To include landmarks and gateways that clearly identify where you are, and when you have arrived. Landmarks must be recognizable and visible from a distance. Gateways help recognize entry points into the Downtown. Landmarks and gateways can include buildings, structural elements and/or landscape features;

- iii. To build beautiful public and private streets and streetscapes. Streets need to accommodate all modes of transportation and be designed to be pedestrian friendly and safe. Building facades play a crucial role in defining and animating the street edge and creating the image and character of the Downtown. Together the roads and the adjacent building facades create a streetscape;
- iv. To ensure that parks and open spaces, including the existing natural features, are beautiful, accessible and linked together. Parks and open spaces need to be visible and accessible from, and integrated with, the street network, and include a full array of opportunities for outdoor festivals, recreation and play, as well as quiet contemplation. High quality landscape architecture will ensure that these outdoor spaces include public art and appropriate grading, paving and planting materials that celebrate the landscape context; and,
- v. To require high quality architecture that transcends a theme or a specific period in time is fundamental to a beautiful community. Buildings should be compatible with one another, but there must be a diversity of height and scale, and a diversity of style that may be defined through building materials, colour and architectural details. The historic context of Downtown Georgetown presents an opportunity for outstanding urban design that showcases a high-quality destination, desirable to both residents and visitors alike.

### H7.3.3

#### A HEALTHY DOWNTOWN

- a) A healthy Downtown consciously seeks to improve the health of its citizens by putting public health high on the social and political agenda. Physical, social and mental well-being are the necessary components of public health.
- b) The built environment should be designed to create opportunities that encourage residents to be physically active and socially engaged. A fundamental element of a healthy community is the inclusion of active transportation. Active transportation refers to any form of human- powered transportation. Objectives are:
  - i. To plan for an active transportation system which is highly integrated and connected within the Downtown, the adjacent communities, the Town and to transportation systems that serve the broader Region;
  - ii. To design the Downtown around pedestrian activity with a substantial number of destinations, including parks, cultural and community facilities, shopping and restaurant opportunities, all within walking distance to promote walking and cycling and which, in turn, encourages daily physical activity and lessens the dependence on automobiles;
  - iii. To ensure that the appropriate level of infrastructure and amenities are provided along active transportation routes to ensure pedestrian comfort and to create enjoyable and safe environments through which to travel; and,
  - iv. To provide for “age-in-place” facilities within Downtown Georgetown that anticipate changing housing needs for an ageing population.

#### H7.3.4

#### A SUSTAINABLE DOWNTOWN

- a) A sustainable Downtown is economically, environmentally, culturally and socially healthy and resilient. It meets the challenges of climate change and other environmental issues through integrated solutions rather than through fragmented, incremental approaches that meet one objective at the expense of another.
- b) A sustainable Downtown manages its human, natural and financial resources equitably and takes a long-term view – one that is focused on both present and future generations. Sustainability success relies upon having specific and measurable targets for indicators related to energy, water and waste. Objectives are:
  - i. To protect and enhance local and regional ecosystems and biological diversity;
  - ii. To promote the responsible use of resources to ensure long-term sustainability, reduce greenhouse gas emissions, and reduce demands for energy, water and waste systems;
  - iii. To demonstrate leadership in sustainable forms of green building design and technology, including the incorporation of renewable and alternative energy sources, as well as opportunities to improve air quality;
  - iv. To incorporate low impact design and other site design strategies to mitigate environmental impacts and to create a more comfortable urban environment;
  - v. To require a transportation system that reduces reliance on the automobile as the primary mode of transportation, promotes the use of no/low carbon vehicles, promotes active transportation and is transit ready; and,
  - vi. To identify, understand and address high priority risks and vulnerabilities through the integration of practical and effective resiliency measures in the design of buildings, streetscapes, parks and natural areas.

#### H7.3.5

#### A VIABLE DOWNTOWN

- a) A viable Downtown provides needed goods and services to a growing local residential community, and is a desirable destination for other people in Halton Hills and beyond. A successful retail and service commercial community is crucial to a viable Downtown. Objectives are:
  - i. To ensure that development optimizes the use of land and is appropriate for the market place, and that this Secondary Plan has the flexibility to respond to, and encourage, positive changes in the market place over time;
  - ii. To maximize the efficient use of existing infrastructure investments and to promote the coordinated, efficient and cost effective delivery of service infrastructure and community infrastructure that is appropriate for the planned urban development forms;
  - iii. To ensure that new development supports a full range of mobility options, including transit;

- iv. To ensure that all development is sustainable and financially viable over its life-cycle; and,
- v. To continue to encourage and support partnerships and collaboration between the Town and service delivery groups such as the Downtown BIA, the Chamber of Commerce and Heritage Halton Hills.

#### H7.4

#### GROWTH MANAGEMENT

- a) Downtown Georgetown is identified as one of two Downtown Areas within the Town of Halton Hills Official Plan. It is within the Built Boundary and it is to be a focus for contextually appropriate intensified development.
- b) It is estimated that the Downtown is approximately 21 gross hectares in size and currently accommodates approximately:
  - i. A total of 650 population-related job opportunities, accommodated in approximately 32,500 square metres of non-residential Gross Floor Area; and,
  - ii. 445 dwelling units accommodating about 1,350 residents in an array of house forms.

The existing gross density of Downtown Georgetown is estimated to be approximately 105 persons and jobs combined per hectare

- c) To the year 2031, the identified time horizon of this Secondary Plan, Downtown Georgetown is expected to accommodate approximately:
  - i. A total of 730 population-related and/or knowledge-based jobs (80 new or replacement jobs); and,
  - ii. 1,000 total dwelling units, accommodating approximately 2,875 residents (555 new dwelling units, accommodating an additional 1445 residents).

The anticipated gross density of Downtown Georgetown by 2031 is estimated to be approximately 170 persons and jobs combined per hectare.

- d) Like all historic downtowns, Downtown Georgetown is expected to continue to grow and to evolve well beyond 2031. To that end, the land use designations and associated policy framework in this Secondary Plan are intended to implement the long term vision for this area and anticipate ongoing growth beyond the 2031 planning horizon. A further amendment to this Secondary Plan to address post 2031 growth expectations, gross density and to extend the planning horizon shall be required upon completion of the current Regional Official Plan review.
- e) All new development within Downtown Georgetown is to be counted toward the Town's residential intensification target to the year 2031.
- f) Phasing development in an existing, built context like Downtown Georgetown is both complex and highly subjective. The actual achievement of the intensification opportunities identified in this Secondary Plan is reliant upon:

- i. Financial feasibility and market acceptance;
  - ii. The subsequent planning approvals processes;
  - iii. The availability of municipal water and wastewater infrastructure. All policies under H7.4 of this Secondary Plan are subject to the capabilities of Halton Region's water and wastewater infrastructure to support development, in accordance with the current Water and Wastewater Master Plan, the Regional Official Plan, and the Development Charges Background Study;
  - iv. The implementation of enhanced transit facilities;
  - v. Landowner motivation and urgency;
  - vi. Alternative opportunities for development in greenfields;
  - vii. Council priorities; and,
  - viii. The provision of development incentives.
- g) The Town of Halton Hills will work with the Region of Halton to achieve a Region wide affordable housing target of 30 percent of all new dwelling units.
- h) The Town will encourage the provision of affordable housing through:
- i. Working with the Region of Halton and the development community to consider progressive financial incentives to encourage and support the development of affordable housing;
  - ii. Supporting assisted housing, which is housing that is available to low and moderate income households for rent or purchase where part of the housing cost is subsidized through a government program;
  - iii. Supporting accessory apartments;
  - iv. Considering innovative and alternative residential and community design standards that facilitate affordable housing; and
  - v. Encouraging the development of purpose built rental housing with a full mix and range of unit sizes.

## H7.5 LAND USE DESIGNATIONS

### H7.5.1 INTRODUCTION

- a) The policy directives of this Secondary Plan make the Downtown Area a focus for investment in the years to come. The designations and policies of this Secondary Plan are intended to:
- i. Identify the current structure of the Downtown and determine where new development activity should be focused;



- ii. Reduce barriers to redevelopment and support the re-use of existing buildings through a flexible policy regime;
  - iii. Provide clear direction that:
    - › Protects and enhances the character and viability of the historic Main Street;
    - › Promotes the Downtown as a focal area for investment in commercial, institutional, cultural, entertainment and residential uses; and,
    - › Ensures that new development is compatible with existing development patterns both within the Downtown and within adjacent Mature Neighbourhoods;
  - iv. Promote new programs and/or financial mechanisms to ensure more urban lifestyle choices and a high standard of urban design, while reducing the costs of new forms of development in the Downtown;
  - v. Enhance the appearance and encourage a high level of property maintenance for buildings and their surroundings from both the public and private sectors;
  - vi. Ensure that public sector improvement projects are undertaken within a comprehensive design and implementation program that is consistent with the policy objectives of this Secondary Plan; and,
  - vii. Encourage the Town to work with other government agencies and the private sector to promote innovative housing forms and development techniques in the Downtown that will facilitate the production of housing that is affordable to the residents of Halton Hills.
- b) Appendix I - Downtown Georgetown Demonstration Plan is attached to this Plan to provide general guidance and context to the Town in consideration of development applications as they are received over time.
- The Demonstration Plan provides a conceptual example of how the Downtown may develop comprehensively over time, including ideas about built form, height distribution and the potential locations for park spaces. The Demonstration Plan does not represent any implicit development scenario nor does it preclude any subsequent development approvals process.
- c) All development, with the exception of additions to commercial buildings less than 75 square metres, within the Downtown shall be:
- i. Subject to the provisions of a community benefits, under Section 37 of the Planning Act;
  - ii. Subject to Site Plan Control and may be subject to a Zoning By-law Amendment. The Town may, through the provisions for Site Plan Control, consider requirements for landscape design, building design, sustainable design, colour, building material and architectural detail as identified in Section 41 of the Planning Act; and,
  - iii. In conformity with the Urban Design Policies of this Secondary Plan and consistent with the Design Guidelines attached to this Secondary Plan as Appendix II.

- d) The land use designations that apply to Downtown Georgetown are identified on Schedule H7-1 - Downtown Georgetown Land Use Plan, and include:
- i. Historic Main Street Area;
  - ii. Downtown Regeneration Area;
  - iii. Downtown Neighbourhood Area;
  - iv. Natural Heritage System; and,
  - v. Major Parks and Open Space Area.

## H7.5.2 HISTORIC MAIN STREET AREA

### H7.5.2.1 GOAL

It is a goal of this Secondary Plan to promote the economic vitality of the Historic Main Street Area, as identified on Schedule H7-1 - Downtown Georgetown Land Use Plan, within a context of historic preservation, while at the same time encouraging adaptive reuse, redevelopment and intensification where appropriate.

It is also a goal of this Secondary Plan to ensure new development is appropriately designed to reflect the heritage character of Main Street, and is compatible with existing development within this Area, and with development within abutting Areas.

### H7.5.2.2 PERMITTED LAND USES

- a) Land uses within this Area will continue to function as a focal point for commerce and hospitality in the Town, accommodating a diverse mix of uses and opportunities. Permitted uses on lands identified as within the Historic Main Street Area on Schedule H7-1 - Downtown Georgetown Land Use Plan may include:
- i. Residential apartments and townhouse dwelling. Townhouse dwellings may include accessory apartments in accordance with Section D1.3.1.6 of the Official Plan;
  - ii. Group home and special needs housing;
  - iii. Bed and breakfast establishments;
  - iv. Home occupations;
  - v. Live-work uses;
  - vi. Day Nursery;
  - vii. Retail and service commercial uses;
  - viii. Restaurants;
  - ix. Hotels;

- x. Places of worship and other institutional uses;
  - xi. Cultural, administrative, recreational and entertainment uses;
  - xii. Private and commercial schools;
  - xiii. Libraries, museums and art galleries;
  - xiv. Commercial and professional offices;
  - xv. Parking facilities at-grade and/or in structure;
  - xvi. Parks and urban squares; and,
  - xvii. Public uses and pquasi-public uses.
- b) The Town will, through the Implementing Zoning By-law, refine the list of permitted land uses to ensure that new development is consistent with the Vision and Guiding Principles of this Secondary Plan, as well as appropriate and compatible in the context of adjacent land uses and the surrounding community.
  - c) The existing Halton Hills Library and Cultural Centre is identified on Schedule H7-1 - Downtown Georgetown Land Use Plan

#### H7.5.2.3 DEVELOPMENT + DESIGN POLICIES

- a) Land assembly is encouraged to create larger, more viable development parcels that may, or may not, incorporate existing buildings, within the Historic Main Street Area.
- b) Buildings and sites should accommodate an array of compatible development. A mixture of uses in proximity to each other is encouraged not just within this Area in general, but also on individual development sites, and within individual buildings.
- c) All permitted uses shall be accommodated within Low-Rise Buildings, or Mid-Rise I Buildings, in accordance with the policies of this Secondary Plan. The implementing Zoning By-law may provide more specific height limits on a site by site basis, based on the context of adjacent buildings.
- d) High activity uses that animate the streetscape and encourage foot traffic are required at-grade abutting Main Street, and are encouraged at- grade along the side streets within the Historic Main Street Area, with uses such as offices and residential apartments on second floors and above. At- grade façades shall incorporate primary building entrances and broad window treatments at street level.
- e) New stand-alone residential buildings, including Group home and special needs housing as well as tourist accommodations are permitted at-grade only on side streets within the Historic Main Street Area, provided Council is satisfied that:
  - i. The proposed at-grade facade is pedestrian in scale and incorporates a primary entrance; and,

- ii. The building is designed to be compatible with and complement adjacent buildings.
- f) Permitted retail and service commercial uses, and restaurants shall have a maximum Gross Floor Area of 1,500 square metres per use.
- g) Parking areas for any permitted land use shall be located in structure, where possible. Where surface parking is necessary, it is preferred that it be located within the rear yard. Where any parking facility is located adjacent to a public street, it shall be designed to complement and integrate with adjacent buildings and the streetscape.
- h) All development proposals within the Historic Main Street Area shall incorporate the following requirements for the design of front and exterior side façades:
  - i. The minimum building height shall be a functional 2 storeys;
  - ii. Buildings adjacent shall be required to articulate the historic 2 storey building height, both through architectural detailing and where taller than adjacent building, with a building step-back above the 2nd or 3rd storey; and,
  - iii. The floor-to-ceiling height of ground floors for all new buildings shall be a minimum of 4.25 metres.
- i) Low-Rise Buildings within Downtown Georgetown, as identified on Schedule H7-2 - Downtown Georgetown Built Form, including Live-Work Units, shall be a maximum of 3 storeys. The regulation of building height for all Low-Rise Buildings shall be exclusive of rooftop mechanical penthouses and roof ornamentation. The implementing Zoning By- law may provide more specific height limits on a site by site basis, based on the context of adjacent buildings.
- j) To facilitate the approval of any Low-Rise Building development within the Historic Main Street Area, the Town shall be satisfied that:
  - i. Exterior building colours and materials are high quality and are selected from a traditional palette;
  - ii. Building mass, lot coverage, scale and building set-backs shall be compatible with buildings in the immediate vicinity;
  - iii. Additions or renovations to an existing building shall be complimentary to the architectural style, exterior colour and materials of the primary building;
  - iv. Façades of any building facing a street shall feature a prominent entrance or other architectural gestures that provides a 'public face'. The main front door must be clearly visible from the street; and,
  - v. The elevation of the front door shall preferably be flush with grade, and shall be no more than 1 step above grade;
- k) Mid-Rise I Buildings within Downtown Georgetown, as identified on Schedule H7-2 - Downtown Georgetown Built Form, are to be a minimum of 3 storeys and a maximum of 6 storeys. The regulation of building height for all Mid-Rise I Buildings shall be exclusive of rooftop mechanical penthouses and roof ornamentation. The implementing Zoning By- law may provide more specific height limits on a site by site basis, based on the context of adjacent buildings.

- l) Prior to approving an Implementing Zoning By-law Amendment that permits a Mid-Rise I Building, the Town shall be satisfied that:
- i. The development has appropriately applied the policies for compatible development and development transition included in this Plan, and is representative of good planning;
  - ii. The site is suitable for the proposed development and that it has adequate land area to incorporate required parking, landscaping and buffering on-site;
  - iii. The built form respects the character of, and can be suitably integrated with adjacent existing and planned developments;
  - iv. Exterior building colours and materials are high quality and are selected from a traditional palette;
  - v. Facades of any building facing a street shall feature a prominent entrance or other architectural gestures that provides a 'public face'. The main front door must be clearly visible from the street;
  - vi. The elevation of the front door shall preferably be flush with grade, and shall be no more than 1 step above grade;
  - vii. Buildings shall be sited to align to streets and open spaces to frame these areas;
  - viii. Buildings include a podium element that approximates the scale of adjacent buildings (2 or 3 storeys). The portion of the building above the podium structure shall step back a minimum of 2 metres from the podium facade that forms the front and exterior street wall;
  - ix. Permanent parking, loading and service areas should be located in rear yards. It is also preferable for parking, loading and service areas to be located in structure; and,
  - x. Rooftop mechanical equipment shall be screened from view with materials that are complementary to the building or through architectural features.

### H7.5.3 DOWNTOWN REGENERATION AREA

#### H7.5.3.1 GOAL

It is a goal of this Secondary Plan to promote significant development and intensification within the Downtown Regeneration Area and that this Area becomes the focus for higher density residential uses in Downtown Georgetown, together with complementary commercial and/or institutional uses. This emphasis recognizes the redevelopment potential of the Area, and its location on the Guelph Street Intensification Corridor, in proximity to the GO Station/Mill Street Corridor.

It is also a goal of this Secondary Plan to ensure that new development is appropriately designed, and is compatible with existing development within this Area, and with development within abutting Areas. The introduction of new residents is expected to support the ongoing success of the businesses within the Historic Main Street Area.

- a) Permitted uses on lands identified as Downtown Regeneration Area on Schedule H7-1 - Downtown Georgetown Land Use Plan may include:
  - i. Existing single-detached and semi-detached dwellings, including associated accessory apartments in accordance with Section D1.3.1.6 of the Official Plan;
  - ii. Home occupations;
  - iii. Converted dwellings;
  - iv. Live-work uses;
  - v. Bed and breakfast establishments;
  - vi. Residential apartments;
  - vii. Group home and special needs housing;
  - viii. Hotels;
  - ix. Conference centres;
  - x. Commercial and professional offices;
  - xi. Institutional uses;
  - xii. Administrative, cultural, recreational and entertainment uses;
  - xiii. Parking facilities at-grade and/or in structure;
  - xiv. Parks, community gardens and urban squares; and,
  - xv. Public uses and quasi-public uses.
- b) Complementary uses that may be permitted on the first floor of any building within the Downtown Regeneration Area may include:
  - i. Retail and service commercial uses;
  - ii. Private and commercial schools;
  - iii. Day nursery;
  - iv. Places of worship and other institutional uses; and,
  - v. Restaurants.
- c) The Town will, through the Implementing Zoning By-law, refine the list of permitted land uses to ensure that new development is consistent with the Vision and Guiding Principles of this

Secondary Plan, as well as appropriate and compatible in the context of adjacent land uses and the surrounding community.

#### H7.5.3.3

#### DEVELOPMENT + DESIGN POLICIES

- a) Land assembly within the Downtown Regeneration Area is encouraged to create larger, more viable development parcels.
- b) Buildings and sites should accommodate an array of compatible development. A mixture of uses is encouraged not just within the Area in general, but also on individual development sites, and within individual buildings.
- c) All permitted uses, with the exception of permitted uses within existing buildings, shall be accommodated within Mid-Rise I Buildings or Mid- Rise II Buildings, in accordance with the policies of this Secondary Plan. The implementing Zoning By- law may provide more specific height limits on a site by site basis, based on the context of adjacent buildings.
- d) High activity uses that animate the streetscape and encourage foot traffic are required at-grade abutting Main Street and Mill Street within the Downtown Regeneration Area with uses such as offices and residential uses on second floors and above. At-grade façades shall incorporate primary building entrances and broad window treatments at street level.
- e) Abutting other streets within the Downtown Regeneration Area high activity uses that animate the streetscape and encourage foot traffic, are encouraged at-grade. At-grade façades shall incorporate primary building entrances and broad window treatments at street level, where appropriate.
- f) Permitted retail and service commercial uses, and restaurants shall have a maximum Gross Floor Area of 500 square metres per use.
- g) Parking areas for any permitted land use shall be located in structure wherever possible. Where surface parking is proposed, it is preferred that it be located within the rear yard or the interior side yard. Where any parking facility is located adjacent to a public street, it shall be designed to complement and integrate with adjacent buildings and the streetscape.
- h) Where a new development site abuts a property line within the Historic Main Street Area, or the Downtown Neighbourhood Area, building setbacks and angular planes shall be established within the Implementing Zoning By-law to ensure an appropriate interface with abutting lower scale built forms.

The angular plane (as defined in Appendix II - Downtown Georgetown Design Guidelines) will require the stepping back of the upper floors of the building, moving away from abutting lower scale built forms. Property lines that abut a public street allowance are not subject to the angular plane provisions.

- i) Development within the Downtown Regeneration Area shall:
  - i. Ensure that enhanced connections to the Town's parks and trails are provided;
  - ii. Provide enhanced on-site pedestrian elements;

- iii. Consider the topography of the area in determining appropriate building design and massing; and,
  - iv. Incorporate appropriate conservation and integration of any on-site Designated or Listed Cultural Heritage Resource.
- j) All development proposals shall incorporate the following requirements for the design of front and exterior side façades:
  - i. The minimum building height shall be 3 storeys;
  - ii. The floor-to-ceiling height of ground floors for all new buildings shall be a minimum of 4.25 metres;
  - iii. The maximum floor plate for any building component above the sixth floor shall be 750 square metres; and,
  - iv. The minimum separation distance between adjacent buildings above the sixth floor shall be 25 metres, notwithstanding any other setbacks established by the Implementing Zoning By- law.
- k) Mid-Rise I Buildings within Downtown Georgetown, as identified on Schedule H7-2 - Downtown Georgetown Built Form, are to be a minimum of 3 storeys and a maximum of 6 storeys. The regulation of building height for all Mid-Rise I Buildings shall be exclusive of rooftop mechanical penthouses and roof ornamentation. The implementing Zoning By- law may provide more specific height limits on a site by site basis, based on the context of adjacent buildings.
- l) Prior to approving an Implementing Zoning By-law Amendment that permits a Mid-Rise I Building, the Town shall be satisfied that:
  - i. The development has appropriately applied the policies for compatible development and development transition, and is representative of good planning;
  - ii. The site is suitable for the proposed development and that it has adequate land area to incorporate required parking, landscaping and buffering on-site;
  - iii. The built form respects the character of, and can be suitably integrated with, adjacent existing and planned developments;
  - iv. The façades of any building facing a street feature a prominent entrance or other architectural gestures that provides a 'public face'. The main front door must be clearly visible from the street;
  - v. The elevation of the front door is preferably flush with grade, and is no more than 1 step above grade;
  - vi. Buildings are sited to align to streets and open spaces;
  - vii. Buildings include a podium element that approximates the scale of adjacent buildings (2 or 3 storeys). The portion of the building above the podium structure steps back a minimum of 2 metres from any podium façade that faces a public street;



- viii. Permanent parking, loading and service areas should be located in rear yards. It is also preferable for parking, loading and service areas to be located in structure;
  - ix. Rooftop mechanical equipment is screened from view with materials that are complementary to the building or through architectural features.
- m) Mid-Rise II Buildings within Downtown Georgetown, as identified on Schedule H7-2 - Downtown Georgetown Built Form, are a minimum of 6 storeys and a maximum of 12 storeys.
- n) The regulation of building height for all Mid- Rise II Buildings shall be exclusive of mechanical penthouses and roof ornamentation. The implementing Zoning By-law may provide more specific height limits on a site by site basis, based on the context of adjacent buildings.
- o) Prior to approving an Implementing Zoning By-law that permits a Mid-Rise II Building, the Town shall be satisfied that:
  - i. The development has appropriately applied the policies for compatible development and development transition, and is representative of good planning;
  - ii. The site is suitable for the proposed development and that it has adequate land area to incorporate required parking, landscaping and buffering on-site;
  - iii. The built form respects the character of and can be suitably integrated with adjacent existing and planned developments, in terms of height and massing;
  - iv. Buildings include a podium element that approximates the scale of adjacent buildings (2 or 3 storeys). The portion of the building above the podium element steps back a minimum of 2 metres from any podium façade that faces a public street. Additional step backs may be required as the building gets taller;
  - v. Where a building abuts another building, either existing or permitted by this Secondary Plan, the podiums may have a minimal separation;
  - vi. Permanent parking, loading and service areas are located in side or rear yards and set back from the front facade of the building. It is preferable for parking, loading and service areas to be located in structure; and,
  - vii. Rooftop mechanical equipment is screened from view with materials that are complementary to the building or through architectural features.

#### H7.5.4 DOWNTOWN NEIGHBOURHOOD AREA

##### H7.5.4.1 GOALS

It is a goal of this Secondary Plan to protect the areas within the Downtown Neighbourhood Area, as identified on Schedule H7-2 - Downtown Georgetown Land Use Plan, from incompatible forms of development and, at the same time, permit them to evolve and be enhanced over time.

It is also a goal of this Secondary Plan that the Downtown Neighbourhood Area function as a transitional area between the lands within Downtown Georgetown and the established and the mature residential neighbourhoods that are adjacent to the Downtown.

#### H7.5.4.2

##### PERMITTED LAND USES

- a) It is anticipated that the Downtown Neighbourhood Area will accommodate existing house form buildings that may accommodate some other complementary small-scale uses that will maintain the residential character of the area. Permitted uses on lands within the Downtown Neighbourhood Area on Schedule H7-2 - Downtown Georgetown Land Use Plan may include:
  - i. Residential uses, including single-detached, semi-detached and townhouse dwellings;
  - ii. Accessory apartments in accordance with Section D1.3.1.6 of the Official Plan;
  - iii. Group home and special needs housing;
  - iv. Day Nursery;
  - v. Home occupations;
  - vi. Converted dwellings;
  - vii. Live-work uses;
  - viii. Bed and breakfast establishments;
  - ix. Professional office uses;
  - x. Parking facilities at-grade and/or in structure;
  - xi. Parks and urban squares; and,
  - xii. Public uses and pquasi-public uses.
- b) The Town will, through the Implementing Zoning By-law, refine the list of permitted land uses to ensure that new development is consistent with the Vision and Guiding Principles of this Secondary Plan, as well as appropriate and compatible in the context of adjacent land uses and the surrounding community.

#### H7.5.4.3

##### DEVELOPMENT + DESIGN POLICIES

- a) Permitted service commercial or professional office use shall be limited to a maximum of 100 square metres per building.
- b) All permitted uses within the Downtown Neighbourhood Area, including Live-Work Units, shall be accommodated within Low-Rise Buildings that are residential in character. Low-Rise Buildings are a maximum of 2.5 storeys. The implementing Zoning By-law may provide more specific height limits on a site by site basis, based on the context of adjacent buildings.

- c) To facilitate the approval of any Low-Rise Building development within the Downtown Neighbourhood Area, as identified on Schedule H7-2 - Downtown Georgetown Built Form, the Town shall be satisfied that:
- i. The residential character of the neighbourhood is maintained;
  - ii. The use complements, and is similar to other existing residential buildings in proximity in terms of:
    - › Landscape treatments;
    - › Building height, scale, lot coverage and massing;
    - › Roof line;
    - › The location and size of parking facilities/ garages;
    - › Architectural detail;
    - › Building materials and colour; and,
    - › Front and side-yard setbacks.
  - iii. The façade of any building facing a street shall feature a porch, prominent entrance or other architectural gestures that provides a ‘public face’. The main front door must be clearly visible from the street;
  - iv. The elevation of the front door shall be no more than 1.2 metres above grade;
  - v. Appropriate buffering such as landscaping and fencing will be provided on-site to ensure the compatibility of the use with adjacent uses; and,
  - vi. Appropriate on-site parking is available. Driveways and/or garage doors shall not dominate the front façade of the primary building or the view from the street. All on-site parking shall be located within the interior side and/or rear yard.

#### H7.5.5 NATURAL HERITAGE SYSTEM

The policies applicable to this land use designation shall be those of Section B1A of the Halton Hills Official Plan.

#### H7. 5.6 MAJOR PARKS AND OPEN SPACE AREA

The policies applicable to this land use designation shall be those of Section B2 of the Halton Hills Official Plan.

### H7.6 URBAN DESIGN POLICIES

#### H7.6.1 DESIGN OBJECTIVES

In Downtown Georgetown there is a strong sense of value attached to the historic structures and associated heritage character that permeates throughout the area. As a result, heritage conservation is

an important aspect of this Secondary Plan, and this Secondary Plan provides substantial policy guidance for future development that follows historic development patterns, without making impositions of a particular aesthetic.

The Town's objectives for urban design in Downtown Georgetown are based on the belief that the livability and physical appeal of a community can be enhanced by the quality, layout and attractiveness of its public and private realm.

The design policies in this Section of this Plan provide development proponents with an understanding of the design intent of the Town.

- a) The basis of these design policies is to create a built environment, which provides:
  - i. An attractive and distinctive Downtown that achieves outstanding architecture that respects the integrity of the past, yet sits well in the context of visual diversity, interest and beauty;
  - ii. A well-defined public realm, including interconnected open space, trails and sidewalk networks;
  - iii. Sensitive integration of new development with existing development through the promotion of compatible development;
  - iv. A full range of retail and service commercial uses, restaurants and community services and facilities in proximity to housing to promote walking and to help reduce automobile trips; and,
  - v. A pedestrian-oriented, transit supportive and cyclist-oriented development pattern and street network where priority is given to the pedestrian, cyclist and transit user, as well as motorists.

#### H7.6.2 HERITAGE DESIGN POLICIES

- a) Appendix III includes an inventory of both Designated and Listed Cultural Heritage Resources. Significant Cultural Heritage Resources, including buildings and associated landscapes shall be developed in conformity with the policies of this Secondary Plan and the policies of the Town of Halton Hills Official Plan.
- b) All development proposals within Downtown Georgetown shall design, restore or enhance their building façades in order to maintain the historic architectural character and identity of the Downtown as follows:
  - i. Where development is proposed that includes a Designated or Listed Heritage Building (as identified in Appendix III), the original architectural details and features shall be restored and appropriately incorporated into the development; and,
  - ii. Where an existing building lacks significant architectural detail, or a new building is proposed, the façade shall be representative of, or consistent with the historic architectural character and identity of the Designated or Listed Heritage Buildings in proximity.

### H7.6.3

#### DESIGN POLICIES FOR BUILDINGS

- a) An array of building types are encouraged throughout the Downtown. It is the intent of this Secondary Plan that built form be the key determining factor for the types of development permitted in each land use area.
- b) Built form will be specifically regulated in this Secondary Plan by building height. Lot coverage, setbacks and density may appropriately be considered within the Implementing Zoning By-law.
- c) All development within Downtown Georgetown shall be compatible with development on adjacent properties. In Downtown Georgetown new building design shall incorporate universal design principles and shall be compliant with the requirements of the Accessibility for Ontarians with Disabilities Act. Further, all development shall be carried out in conformity with the applicable policies of this Secondary Plan, and consistent with the Design Guidelines included as Appendix II to this Secondary Plan.
- d) Architectural variety is crucial in creating a visually stimulating urban environment. Streetscapes composed of buildings of similar style and form can succeed through subtle variations in the façade treatment and building mass in order to improve the overall architectural richness, variety, and building articulation in the community.

#### H7.6.3.1

##### ARCHITECTURAL QUALITY

- a) Downtown Georgetown has a proud history of well- designed buildings. All development shall ensure excellence in design and demonstrate high quality architectural detailing and the use of high quality materials.

The intent of the policies of this Secondary Plan is to achieve a balance between a consistency of design as well as individual expression in new developments. This Secondary Plan promotes innovation. Rather than requiring a strict level of conformity, the design and architectural quality of development shall be measured according to its level of consistency with the Design Guidelines attached to this Secondary Plan as Appendix II.

#### H7.6.3.2

##### LANDMARK SITES/BUILDINGS

- a) Schedule H7-1 - Downtown Georgetown Land Use Plan identifies several Landmark Sites that warrant special design treatment because of their location and visibility. These Landmark Sites have tremendous potential to strengthen Downtown Georgetown's identity and enhance the quality of the Public Realm Network

On identified Landmark Sites, exceptions to the policies governing building height may be considered without an Amendment to this Secondary Plan if the proposed building is deemed by the Town to be consistent with the Design Guidelines attached to this Secondary Plan as Appendix II.

#### H7.6.3.3

##### GATEWAYS

- a) Schedule H7-1 - Downtown Georgetown Land Use Plan identifies several Gateways that mark key locations that announce the key entrances to the Downtown and/or important locations within the Downtown. Gateways provide opportunities for signage that may also include landscape treatments, built form elements and public art. These Gateways have tremendous

potential to promote wayfinding, to strengthen Downtown Georgetown's identity and enhance the quality of the Public Realm Network.

#### H7.6.3.4 COMPATIBLE DEVELOPMENT

- a) All development applications within Downtown Georgetown will be required to demonstrate compatibility with their surrounding context. Compatible development is central to the vision of preserving and enhancing the unique character of the entire area. Development is defined in the Town of Halton Hills Official Plan.

#### H7.6.3.5 DEVELOPMENT TRANSITION

- a) This Secondary Plan promotes a transition in development height. More specifically, the transition between building types between abutting Areas and properties shall be established through Implementing Zoning By-laws that apply a combination of transition tools. Transition requirements shall specifically consider and mitigate impacts related to overlook and loss of privacy, to the satisfaction of the Town.
- b) Where any Area of this Secondary Plan abuts the Downtown Neighbourhood Area:
  - i. A minimum landscape buffer strip of 3 metres should be encouraged within the setback, abutting any rear or side lot line. A 3 metre landscape buffer provides the opportunity for substantial tree planting and appropriate privacy fencing; and,
  - ii. An angular plane shall be used to establish the maximum height of any new development.

Unless otherwise identified in this Secondary Plan, front lot lines and exterior side lot lines are specifically exempt from the requirements of this Policy.

- c) Where the transition techniques identified in this Secondary Plan are to be applied, they shall be applied only to those developments that are expected to accommodate Mid-Rise I Buildings or Mid-Rise II Buildings. It is the responsibility of the taller buildings to establish appropriate transition to the abutting smaller scale development.

#### H7.6.4 DESIGN POLICIES FOR THE PUBLIC REALM

##### H7.6.4.1 INTENT

As the population of Downtown Georgetown continues to grow over time, it will require a diverse Public Realm Network where residents and visitors can walk, sit, socialize and engage in the street life of the area. The Public Realm Network for Downtown Georgetown is comprised of various and diverse components, including Park Spaces and Streetscapes. The Public Realm Network will be diverse in design, and will serve a variety of functions.

A comprehensive understanding of how the components of the Public Realm Network work together and complement each other, and their adjacent uses, will lead to a more connected, accessible and logical network of pedestrian spaces throughout Downtown Georgetown. Moving people into, out of and through the community easily and safely, and providing a variety of spaces for socializing, special events and recreation, is a priority of this Secondary Plan.

- a) Collectively, the Streetscapes in Downtown Georgetown comprise a substantial percentage of community open space - which solidifies their importance as a defining feature of the Public Realm Network. Streets and lanes can be engaging and safe outdoor places with beautiful trees and plants, seating, shade and Public Art for everyone to enjoy.
- b) This Secondary Plan ensures that the design and maintenance of the range of Streetscapes in the Downtown reflects its high quality character and that they create an enhanced Public Realm Network that supports pedestrian, cyclist and vehicular movement.
- c) The Streetscape Hierarchy in Downtown Georgetown identified on Schedule H7-3 - Active Transportation Plan, is comprised of the following:
  - i. Main Street;
  - ii. Guelph Street;
  - iii. Mill Street;
  - iv. Church Street; and,
  - v. Local Streets.
- d) How built form interfaces with the streetscape is important to animating the street and creating a safe, welcoming environment for pedestrians and cyclists. This crucial interface of streetscape and built form is dependent upon the scale of the buildings, the width and function of the street and the size of the building setbacks. Active frontages generally correspond to existing at-grade uses,
- e) and the level of animation also define the type of streetscape that is appropriate.
- f) Main Street is both a traditional commercial Main Street, as well as a promenade that fronts a number of stately residential properties. As such, it will be given special consideration in terms of both its three identified contexts in terms of the design treatment of the street itself, and its adjacent pedestrian realm, as identified on Schedule H7-3 - Active Transportation Plan and as articulated in the Design Guidelines attached to this Secondary Plan as Appendix II.
- g) Guelph Street, as identified on Schedule H7-3 - Active Transportation Plan, has a distinct identity and character that should be enhanced consistent with the Design Guidelines attached to this Secondary Plan as Appendix II.
- h) Mill Street, as identified on Schedule H7-3 - Active Transportation Plan, has a distinct identity and character that should be enhanced consistent with the Design Guidelines attached to this Secondary Plan as Appendix II.
- i) Church Street, as identified on Schedule H7-3 - Active Transportation Plan, adjacent to the Library and Art Gallery, is a unique public space that requires a flexible design, consistent with the Design Guidelines attached to this Secondary Plan as Appendix II.
- j) Within Downtown Georgetown, Local Streets are intimate neighbourhood public spaces where children play and neighbours meet. They are lined with residential gardens, lawns and mature

trees and shall be designed to be consistent with the Design Guidelines attached to this Secondary Plan as Appendix II.

#### H7.6.4.3

#### PARK SPACES

- a) Existing major Parks and Open Space Areas, as well as Potential Parks are identified on Schedule H7-1- Downtown Georgetown Land Use Plan. People must be able to walk their dogs, eat lunch, play with children and access nearby amenities. These animated, interesting and unique Park Spaces are the jewels of the community and foster a strong sense of place within Downtown Georgetown.
- b) The Potential Parks, which are identified symbolically on Schedule H7-1 - Downtown Georgetown Land Use Plan are not defined specifically in terms of their size, configuration or character. In addition, not all of the identified Potential Parks may be achieved, and, additional locations for park spaces may be achieved through the development approval process, without the need for an Amendment to this Secondary Plan. Parks and Urban Squares are a permitted use within all of the land use designations in Downtown Georgetown, with the exception of the Natural Heritage System.
- c) The Park Space Hierarchy appropriate for Downtown Georgetown is comprised of the following components:
  - i. The Existing Park;
  - ii. Urban Squares;
  - iii. Pocket Parks;
  - iv. Sliver Parks; and,
  - v. Enhanced Connecting Links.
- d) All of these Park Spaces within the hierarchy may be publicly owned or privately owned. Privately owned Park Spaces should be considered accessible to the public, has been designed to Town standards, and maintained to Town standards. Legal agreements to ensure the long-term adherence to these requirements may be executed.
- e) New Park Spaces in Downtown Georgetown shall:
  - i. Include a signed Park Manifesto that outlines appropriate conduct for Park Space users;
  - ii. Be safe, secure and accessible to the public;
  - iii. Be AODA compliant and well maintained;
  - iv. Prioritize pedestrian comfort;



- v. Be designed to the highest standards;
  - vi. Include opportunities for Public Art; and,
  - vii. Create and enhance opportunities for greening, and shall be appropriately linked with other elements of the Public Realm Network.
- f) Urban Squares are small pedestrian spaces that accommodate socializing in a dense urban area. An Urban Square is defined as a pedestrian space larger than 1,000 square metres and smaller than 8,000 square metres in size with street frontage. They include both hard and softscaped elements and are equipped with seating opportunities and ample pedestrian amenities.
  - g) Pocket Parks are small scaled components of the Public Realm Network. They are expected to be less than 1,000 square metres in size, but generally greater than 75 square metres.
  - h) Sliver parks are small scale components of the Public Realm Network that add to the width of the public sidewalk system, and create plazas or forecourts between the face of the adjacent building and the street right-of-way.
  - i) Enhanced Connecting Links are outdoor walkways through a development site, connecting two streets together. They need to be provided in high pedestrian volume areas, for ease of movement as well as the creation of unique urban spaces. Enhanced Connecting Links are in addition to typical sidewalk connections and should contribute to the logical wayfinding system and help to establish a network of publicly accessible spaces within Downtown Georgetown.

## H7.6.5 SUSTAINABLE DESIGN POLICIES

### H7.6.5.1 BUILDINGS

- a) All new buildings are required to conform with applicable policies for:
  - i. Air Quality Impacts;
  - ii. Source Water Protection; and,
  - iii. Water Balance Assessments.
- b) For all buildings with a GFA of 600 square metres or greater and/or identified under Part 3 of the Ontario Building Code, the achievement of the following sustainability targets is encouraged:
  - i. Generally:
    - › Be certified LEED v4 Silver, or equivalent, or be compliant with the Town's Green Development Standards Program, whichever is greater;
  - ii. For Energy:
    - › Meet the requirements for LEED v4 EA Credit Optimize Energy Performance, or equivalent, and demonstrate a 25% reduction in energy consumption per ASHRAE 90.1-2010;

- › Include on-site renewable or alternative energy systems which produce at least 15% of building energy use;
- › Be “solar ready”, and maximize solar gains through:
  - + Orienting building to maximize potential for passive and active solar energy;
  - + Designing roofs to support solar panels and rough-ins for wiring needs;
  - + South facing windows;
- › Incorporate green/white or another high albedo roofing;
- › Use regionally and locally sourced building materials to the greatest extent possible;
- iii. For Water:
  - › Meet the requirements for two points under LEED v4 Credit Indoor Water Use Reduction (30% reduction) OR provide water consuming fixtures that are each at least 20% better than 6 LPF toilets, 2.2 GPM faucets and 2.5 GPM showers;
- iv. For Waste:
  - › Divert 75% of all construction waste away from landfill sites; and,
  - › Provide trisorting facilities for waste.
- c) For all buildings with a GFA of 600 square metres or less and/or identified under Part 9 of the Ontario Building Code, achievement of the following sustainability targets is encouraged:
  - i. For Energy:
    - › Be Energy Star certified. Grade-related residential units (3.5 storeys and less) to be designed to EnerGuide 83, or equivalent, per Energy Star for Homes;
    - › Meet the requirements for LEED v4 EA Credit Optimize Energy Performance, or equivalent, and demonstrate a 25% reduction in energy consumption per ASHRAE 90.1-2010, which is 5 to 10% better than the Ontario Building Code;
    - › Include on-site renewable or alternative energy systems which produce at least 15% of building energy use. Alternatively, identify opportunities for the provision of centralized, integrated energy systems, such as district energy for heating and cooling;
    - › Be “solar ready”, and maximize solar gains through:
      - +Orienting building to maximize potential for passive and active solar energy;
      - + Designing roofs to support solar panels and rough-ins for wiring needs;
      - +South facing windows;

- + Incorporate green/white or another high albedo roofing;

- + Use regionally and locally sourced building materials to the greatest extent possible;

- ii. For Water:

- › Meet the requirements for 2 points under LEED v4 Credit Indoor Water Use Reduction (30% reduction), OR provide water consuming fixtures that are each at least 20% better than 6 LPF toilets, 2.2 GPM faucets and 2.5 GPM showers;

- iii. For Waste:

- › Divert 75% of all construction waste away from landfill sites.

#### H7.6.5.2 PARK SPACES + STREETSCAPES

- a) For all new landscapes and streetscapes, the achievement of the following sustainability targets is encouraged:

- i. For Energy:

- › Require that new construction use regionally and locally sourced building materials to the greatest extent possible;

- ii. For Water:

- › For projects where soft landscapes exceed 5% of the building Ground Floor Area, meet the requirements of LEED v4 Credit Outdoor Water Use Reduction (50% reduction in irrigation water), or equivalent

- › Promote sustainable landscape practices by requiring the use of water efficient, drought resistant Secondary Plant materials in parks, along streetscapes, and in public and private landscaping.

- iii. For Waste:

- › Divert 75% of all construction waste away from landfill sites; and,

- iv. To mitigate heat island effects by:

- › Locating trees or other Secondary Plantings to provide shading for a least 50% of sidewalks, patios, and driveways within 15 metres of new buildings; and,

- › Installing light-colored paving materials including white concrete, grey concrete, open pavers and any material with a solar reflectance index of at least 29.

#### H7.6.5.3 STORM WATER MANAGEMENT

- a) All new development in Downtown Georgetown shall incorporate the following into its design for storm water management:

- i. Ensure that the maximum allowable annual runoff volume for the development site does not exceed the runoff under pre-development conditions;
  - ii. Promote the implementation of Low Impact Design Standards that emphasize the use of bioswales, innovative storm water and run-off techniques and practices, at-source infiltration, and greywater re-use systems, with reference to Conservation Authority guidelines ; and,
  - iii. As required by CTC Source Protection Plan policies, in locations where the Issue Contributing Area for chloride overlaps the Secondary Plan Area, design considerations will be required to ensure that measures to promote groundwater recharge limit the infiltration of runoff from areas where road salt is applied. Recharge enhancements in these areas should focus on infiltrating runoff from areas where road salt is not applied (i.e., greenspace, rooftops, etc
- b) Introduce green infrastructure, such as bioswales, within the street rights-of-way to enhance ground water infiltration and improve water quality as part of a comprehensive water management plan.
  - c) Require water conservation by including the installation of rainwater harvesting and re-circulation/reuse systems for outdoor irrigation and outdoor water use, including absorbent landscaping, landscaped walls, cisterns, rain barrels, underground storage tanks and/or infiltration trenches.
  - d) Require landscaping using native, drought-tolerant Secondary Plants, a cost-effective landscape method to conserve water and other resources on a community-wide level.
  - e) Implement curb cuts along sidewalks and driveways to allow water to flow onto planted zones or infiltration basins.
  - f) Require the installation of subsurface basins below parking lots to enable storm water to be stored and absorbed slowly into surrounding soils.

#### H7.6.5.4 NATURAL HERITAGE SYSTEM

- a) The Natural Heritage System is identified on Schedule H7-1 - Downtown Georgetown Land Use Plan. The Town recognizes the important contribution that Silver Creek makes to the creation of a vibrant and livable Downtown.
- b) The biodiversity, ecological function, and connectivity of the natural features within Downtown Georgetown shall be protected, maintained, restored or, where possible, improved for the long-term, recognizing linkages between and among natural heritage features and areas, surface water features, and ground water features.

This Secondary Plan is intended to:

- i. Protect the health and water quality of Silver Creek;
- ii. Conserve biodiversity;
- iii. Protect all significant natural features and their ecological functions; and,

- iv. Protect surface and underground water resources.
- c) Lands within the Silver Creek valley system will be integrated with other open spaces and will accommodate a public trail where appropriate, in accordance with section B1A of the Halton Hills Official Plan, providing opportunities to link various neighbourhoods to the Downtown.
- d) Activities considered to be Significant Drinking Water Threats will be subject to the Credit Valley, Toronto and Region, Central Lake Ontario (CTC) Source Protection Plan policies.

#### H7.6.5.5 THE URBAN FOREST

- a) The urban forest includes trees and shrubs on public and private lands, provides ecological benefits, and assists in mitigating the urban heat island effect. Trees or other plantings shall be located throughout the Downtown to provide shading for sidewalks, parks and open spaces and other publicly accessible areas. To this end, the Town shall:
  - i. Implement tree protection policies, which will include a tree replacement ratio where tree removal is unavoidable;
  - ii. Implement street tree and naturalization programs to increase urban canopy cover; and,
  - iii. Require the planting of trees in all public works projects.
- b) The urban forest shall include a variety of trees that are hardy, resilient, non-invasive, salt tolerant, drought resistant, and low maintenance. All trees shall provide a large canopy and shade over sidewalks, parks, and open spaces. Native tree species are preferred in the urban forest, except in areas that are contained within/connected to the Natural Heritage System designation, where any new trees planted should be native varieties that are common to the surrounding system.

#### H7.6.5.6 LOCAL FOOD PRODUCTION

- a) The creation of opportunities for local food production is supported by the Town. Development plans and building designs shall consider opportunities for local food growing and production through:
  - i. Community gardens;
  - ii. Edible landscapes;
  - iii. Food-related home occupations/industries;
  - iv. Small and medium scaled food retailers; and,
  - v. Local market space (i.e. a farmer's market).

#### H7.6.5.7 SUSTAINABILITY CERTIFICATION

- a) Design, construction, and monitoring within the Downtown should be evaluated in accordance with the EcoDistricts rating system, WELL Building Standard, and Active Design Guidelines

(Center for Active Design). Equivalent rating systems, or other similar rating/evaluation approaches may be considered by the Town.

- b) The design and deployment of infrastructure shall be evaluated in accordance with the Envision Systems rating system, a rating system and Secondary Planning guide for sustainable infrastructure projects.

## H7.7 MOBILITY POLICIES

### H7.7.1 THE STREET NETWORK

- a) The street network serving Downtown Georgetown will be redeveloped over time utilizing the principles of “complete streets”, with appropriate facilities provided for pedestrians, cyclists, transit and cars/trucks. The street network and mobility opportunities within the Downtown are identified on Schedule H7-3 - Downtown Georgetown Active Transportation Plan.
- b) Alternative design standards and opportunities for public or private ownership for the street network are encouraged by this Secondary Plan.
- c) Lanes widths for vehicular travel shall be minimized, and traffic control measures will be implemented, while still allowing for transit and emergency services circulation. Pedestrian spaces and landscaped boulevards should be maximized to support active transportation initiatives.

### H7.7.2 PUBLIC TRANSIT

- a) The Town will develop an enhanced transit system for Georgetown that includes the Downtown, when population levels justify such an enhanced system. Transit services will be implemented on a phased basis and based on acceptable operational and financial criteria.
- b) Development within Downtown Georgetown must contribute to the creation of a sufficient density to make transit feasible and efficient in the long-term. As such, this Secondary Plan promotes compact and intensified development to support future transit services.
- c) This Secondary Plan is designed in anticipation of a transit service that has convenient links to, between, and through major destinations within the community, and to the broader region. Transit supportive design requires the following:
  - i. Locate stops within a 5-minute walk of a destination. The maximum walking distance for residents is generally 200 to 400 metres (3 to 5-minute walk);
  - ii. Ensure the coordination of the transit network with the multi-use trails and the sidewalk system to further the accessibility of transit; and,
  - iii. The Town will provide a range of transit facility amenities including but not limited to: bicycle parking, weather protection, seating, waste baskets, lighting, route information, and automated fare machines at transit stops.

### H7.7.3 ACTIVE TRANSPORTATION

- a) This Secondary Plan provides for a balanced transportation system that promotes active transportation facilities to encourage walking and cycling and other non-motorized forms of transportation. This Secondary Plan requires that all development contribute to the creation of a walkable and connected Downtown with multiple destinations within walking distance of all residents and visitors to the Downtown.
- b) Sidewalks, bike lanes, and multi-use trails will connect to public sidewalks and to community amenities and will ensure that corridors between key destinations are fully accessible and support active transportation.
- c) Active transportation routes, such as sidewalks, bike lanes, trails, and multi-use trails will include streetscaping elements that promote pedestrian and cyclist comfort and safety and will be designed to enhance accessibility for all residents and will comply with the Accessibility for Ontarians with Disabilities Act.

#### H7.7.4 MULTI-USE TRAILS

- a) Multi-use trails are identified conceptually on Schedule H7-3 - Downtown Georgetown Active Transportation Plan and a component of the active transportation network for use by pedestrians, cyclists, and other non-motorized modes of transportation, and are a crucial component of an integrated parks and open space system.
- b) In developing a comprehensive multi-use trail system throughout the Downtown, the following shall apply:
  - i. The multi-use trails network will include and link to trails adjacent to, or within the Silver Creek Valley, other parks and open spaces, and the sidewalks and bike lanes within the street network; and,
  - ii. Where a multi-use trail is adjacent to, or within the Silver Creek Valley, it will be designed to avoid and mitigate impact on the features and its associated ecological and hydrogeological functions, including the requirement to utilize native, non-invasive plant materials in accordance with Section B1A of the Halton Hills Official Plan.

#### H7.7.5 TRANSPORTATION DEMAND MANAGEMENT

- a) Appropriate Transportation Demand Management measures to reduce single occupancy automobile trips will be identified in transportation studies that accompany development applications. This could include strategies to enhance the use of transit, provide preferential carpool parking, and/or requirements for bicycle facilities.

#### H7.7.6 PARKING

- a) Alternative (reduced) parking requirements are expected to be necessary to address new development forms and broader community needs within Downtown Georgetown in the future. The following policies will be considered in establishing an appropriate parking supply within the Downtown:
  - i. All new residential development shall provide adequate parking, in consideration of Downtown Georgetown's urban context and proximity to the Georgetown GO Station.

- ii. The Town currently provides, and should continue to provide public parking areas (surface lots and/or structured parking facilities) within the Downtown to augment the supply of parking for bicycles and automobiles;
- iii. Where new development is proposed within 250 metres of an existing public parking facility, the Town may, subject to a Parking Needs Study, reduce the minimum non-resident parking requirement, identified in the Zoning By-law in recognition of the enhanced public parking supply; and,
- iv. Where a development proposal is unable, or does not wish to provide all of the required parking spaces, the Town may accept cash-in-lieu of parking spaces. The minimum parking requirement shall be used to calculate any parking space deficiency.

The cost of each parking space shall be established by the Town, and may be waived for any specific development, at the discretion of the Town. The funds raised through this provision shall be utilized by the Town solely for the purchase of property for public parking and/or the building of public parking within the boundaries of the Downtown.

- b) All new development will include EV charging stations.
- c) All new development shall include parking for bicycles, in accordance with the following:
  - i. Provide bicycle parking at retail, commercial, and employment areas, as well as at destinations to promote purposeful cycling;
  - ii. Provide bike storage sheltered from weather for 15% of total building occupants for all multiple and apartment form residential developments; and,
  - iii. For non-residential development, place accessible and secure bike racks at the front of buildings.

## H7.8 IMPLEMENTATION POLICIES

- a) All development within Downtown Georgetown shall be subject to the Implementation Policies of the Town of Halton Hills Official Plan, in addition to the following more specific policies:

### H7.8.1 COMMUNITY IMPROVEMENT

- a) All lands within Downtown Georgetown are identified as within the Halton Hills Comprehensive Community Improvement Plan (CIP). The Town will use the CIP to further the objectives of this Secondary Plan.

### H7.8.2 DOWNTOWN BUSINESS IMPROVEMENT AREA

- a) All lands within Downtown Georgetown are included within the boundaries of the Downtown Business Improvement Area (BIA). The Town will work with the BIA to further implement the objectives of this Secondary Plan.

### H7.8.3 HERITAGE CONSERVATION DISTRICT



- a) The Town may consider the establishment of a Heritage Conservation District under Part V of the Heritage Act for the area identified as Potential Heritage Conservation District on the map included as Appendix III - Downtown Georgetown Cultural Heritage Resources.

#### H7.8.4 SITE PLAN CONTROL

- a) Where Site Plan Control is required, no building permit may be issued by the Town without first reviewing the development application in the context of the Vision, Principles, Objectives and Policies identified within this Secondary Plan, as well as in consideration of the Design Guidelines attached to this Secondary Plan as Appendix II.

#### H7.8.5 SERVICING

- a) Ensuring sufficient municipal servicing capacity to accommodate the anticipated growth is critical to the success of this Secondary Plan. The Town shall complete an Area Servicing Plan for the Secondary Plan Area developed in accordance with the Region's Water and Wastewater Master Plan, the Regional Official Plan, Development Charges Background Study, and the Region's financing policies for growth infrastructure to the satisfaction of Halton Region.
- b) Until the requirements of H7.8.5 a) are met, development applications shall be required to complete supplemental servicing analysis in addition to the requirements for a Functional Servicing Report detailed in section H7.8.6 a), to the satisfaction of the Town and Halton Region. Requirements shall include:
  - › Water and wastewater hydraulic modeling to include analysis of the local Functional Servicing Report area as well as the impacted area external to the Secondary Plan area in order to confirm there are no system capacity constraints accumulatively.
- c) Notwithstanding H7.8.5 b), the requirements for supplemental servicing analysis may be scoped or waived at the sole discretion of Halton Region.
- d) Upon completion of the requirements of H7.8.5 a), Functional Servicing Reports shall be completed in accordance with section H7.8.6 a) and the Downtown Georgetown Area Servicing Plan to the satisfaction of the Town and Halton Region.

#### H7.8.6 COMPLETE APPLICATION REQUIREMENTS

- a) A development application under the Planning Act will be required in accordance with section G12 of the Halton Hills Official Plan. In addition to the requirements in section G12, the following policies apply:
  - i. As part of the land use planning report an analysis of current rental housing supply, demand and risk of loss through development.
  - ii. Where required by the policies of the Town of Halton Hills Official Plan and/or Regional Official Plan, an Environmental Impact Assessment will be prepared in support of applications for proposed development or site alteration. The EIA will be prepared in accordance with the Downtown Georgetown Natural Heritage Assessment, Regional Official Plan, and the Halton Region Environmental Impact Assessment Guideline to the satisfaction of the Town, the Region, and the Conservation Authority. In particular, the EIA must identify the appropriate buffer width in conjunction with other mitigation

techniques to ensure the features and functions of the NHS are protected, and include an environmental management strategy.

- iii. Functional Servicing Reports shall be prepared in accordance with section H7.8.5, specifying how water and wastewater servicing will be provided for proposed developments. The Functional Servicing Report will be prepared to the satisfaction of the Town and Halton Region and must include, at a minimum:
  - › Servicing design requirements;
  - › A preferred servicing plan based on analysis of servicing requirements, in accordance with Downtown Georgetown Area Servicing Plan as well as the current Water and Wastewater Master Plan;
  - › Preliminary sizing and location of water and wastewater infrastructure.

## H7.8.7 COMMUNITY BENEFITS

### H7.8.7.1 HEIGHT BONUSING

- a) In considering the use of a height bonus under Section 37 of the Planning Act, the Town shall ensure:
  - i. The development has appropriately applied the tests for compatible development and built form transition, and is representative of good planning;
  - ii. The proposed community benefits are to be located within the boundaries of Downtown Georgetown;
  - iii. Where cash-in-lieu of a community benefit is accepted by the Town, the funds shall be spent on appropriate community benefits within Downtown Georgetown; and,
  - iv. The community benefit, or cash for a community benefit shall be used to help ameliorate the impacts on the community generally attributable to the permitted height bonus.
- v. In determining community benefits, the following shall be considered priorities for Downtown Georgetown:
  - › Improvements to public parks, including parkland acquisition;
  - › Improvements to public streetscapes;
  - › Establishment of new, or enhancements to existing public art installations;
  - › Establishment of new, or expansions to existing non-profit community services and facilities, including child care;
  - › Improvements to library and/or museum facilities;

- › Additional affordable housing units;
  - › The conservation of identified cultural heritage resources; and/or,
  - › Achievement of the Sustainable Design Policies of this Plan.
- b) Notwithstanding the maximum height for Mid- Rise I Buildings identified in this Secondary Plan, the Town may only consider the achievement of any height above 4 storeys in exchange for the provision of community benefits, in accordance with the provisions of Section 37 of the Planning Act. the Planning Act, as they are expressed in the Town of Halton Hills Official Plan. The Town may establish a lower parkland dedication requirement that is applicable specifically within Downtown Georgetown.

#### H7.8.7.1 HEIGHT BONUSING

- a) All development within the Downtown shall be subject to the parkland dedication requirements of the Planning Act, as they are expressed in the Town of Halton Hills Official Plan. The Town may establish a lower parkland dedication requirement that is applicable specifically within Downtown Georgetown.
- b) In addition, Section 37 of the Planning Act may be used to acquire land, or provide funds to build components of the Pedestrian Realm Network, and the Development Charges Act can be used to fund Pedestrian Realm development/improvements that are required to properly accommodate a growing population. Public Art contributions shall be secured through Section 37 of the Planning Act.
- c) All developments on sites less than 1,000 square metres in size shall be required to dedicate land and/or make a cash-in-lieu of parkland payment.
- d) All primarily residential development on sites greater than 1,000 square metres shall include:
  - i. An Urban Square or Pocket Park with a minimum frontage on a public street of 7.5 metres, and a minimum size of 75 square metres. Larger sites shall include larger Pocket Parks/Urban Squares;
  - ii. Enhanced Connecting Links with a minimum width of 6 metres (indoor or outdoor); and/or,
  - iii. A cash or additional land contribution that makes up any land dedication shortfall, once the minimum of 7.5 percent of the land area has been dedicated.

#### 7.8.8 DEVELOPMENT MONITORING

- a) The Town shall establish a development monitoring program, in consultation with Halton Region, for the Downtown Georgetown lands to track and report on the status of development (both allocated and built units) on an annual basis. This may include:
  - i. Level of population and employment growth;

- ii. Supply of existing lots and number of building permits granted;
- iii. General achievement of housing mix targets;
- iv. Building permits granted; and
- v. Development application status.

## H7.9 SPECIAL POLICY AREAS

- a) Special Policy Areas apply to those lands shown on Schedule H7-1 and/or H7-2 that are the subject of a land use policy that is specific to a property or area. All other relevant policies of this Secondary Plan are applicable unless otherwise modified or exempted by the provision specific to each Special Policy Area.

### H7.9.1 SPECIAL POLICY AREA 1

- a) In addition to the policies of the Historic Main Street Area and any other applicable policies of this Secondary Plan, development on the site identified as Special Policy Area 1 on Schedule H7-1 - Downtown Georgetown Land Use Plan and Schedule H7-2 - Downtown Georgetown Built Form shall be subject to the provisions of By-Law No. 2017-0064, as enacted by the Ontario Municipal Board Order / Decision of November 1, 2017.

### H7.9.2 SPECIAL POLICY AREA 2

- a) The lands designated Major Parks and Open Space Area, and identified as Special Policy Area 2 on Schedules H7-1 and H7-2, function as a gateway to the Downtown Georgetown Area. Notwithstanding this designation, the lands are classified as a Neighbourhood Park and are subject to the policies contained in Section F7.3.4.2 of the Official Plan.

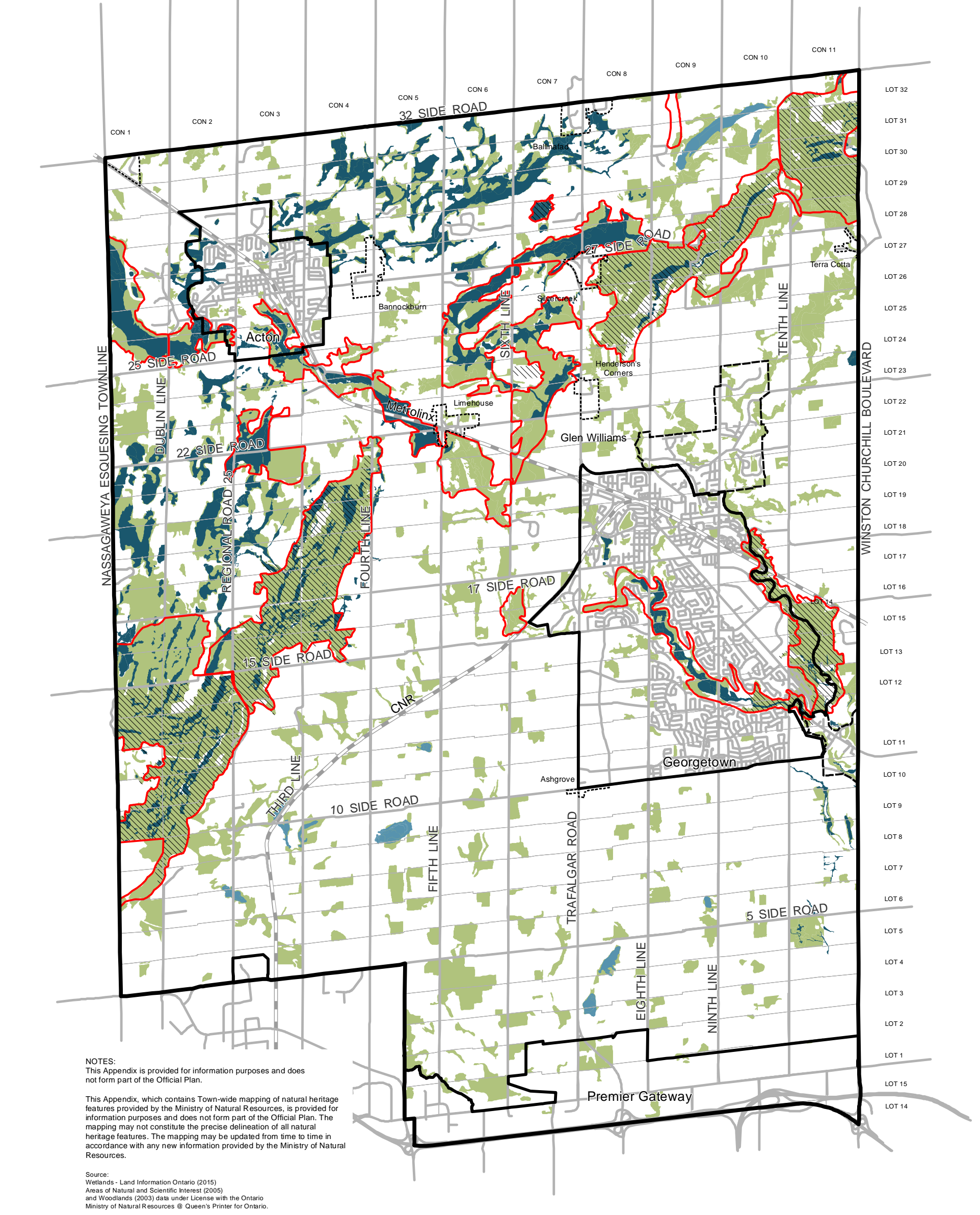













The appendices do not form part of the operative part of the Official Plan. They contain additional information to assist in implementing the Official Plan. Modifications to the appendices will not require an amendment to this Plan.

The appendices are comprised of the following:

- Appendix X1A Environment – Natural Areas;
- Appendix X1B Environment – Water Resource Areas;
- Appendix X2 High Potential Mineral Aggregate Resource Areas;
- Appendix X3 Waste Disposal Sites;
- Appendix X4 Town of Halton Hills Urban Design Guidelines;
- Appendix X5 Hamlet of Norval Design and Heritage Protection Guidelines;
- Appendix X6 Hamlet of Glen Williams Design and Heritage Protection; Guidelines;
- Appendix X7 Hamlet of Glen Williams Terms of Reference for Environmental Implementation Reports;
- Appendix X8 Premier Gateway Phase 1B Secondary Plan Subwatershed Impact Study Matters to be Addressed;
- Appendix X9 Premier Gateway Employment Area Urban Design Guidelines;
- Appendix X10 Downtown Georgetown Secondary Plan Urban Design Guidelines;
- Appendix X11 Downtown Georgetown Secondary Plan Demonstration Plan; and,
- Appendix X12 Downtown Georgetown Secondary Plan Cultural Heritage Resources.



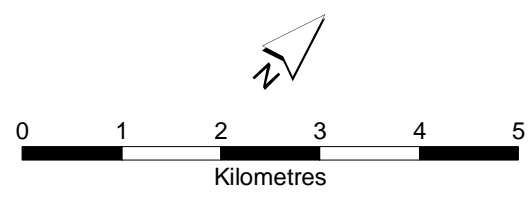


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|---|---|--|
|  Provincially Significant Wetland (evaluated provincial) |  Woodland (0.5 ha or greater)            |  Urban Boundary         |
|  Regionally Significant Wetland (evaluated other)        |  Environmentally Sensitive               |  Hamlet Boundary        |
|  Identified Wetland (not evaluated)                      |  Area of Natural and Scientific Interest |  Rural Cluster Boundary |
|   |  Town of Halton Hills Boundary           |  Railway Line           |

APRIL 30, 2024  
CONSOLIDATION



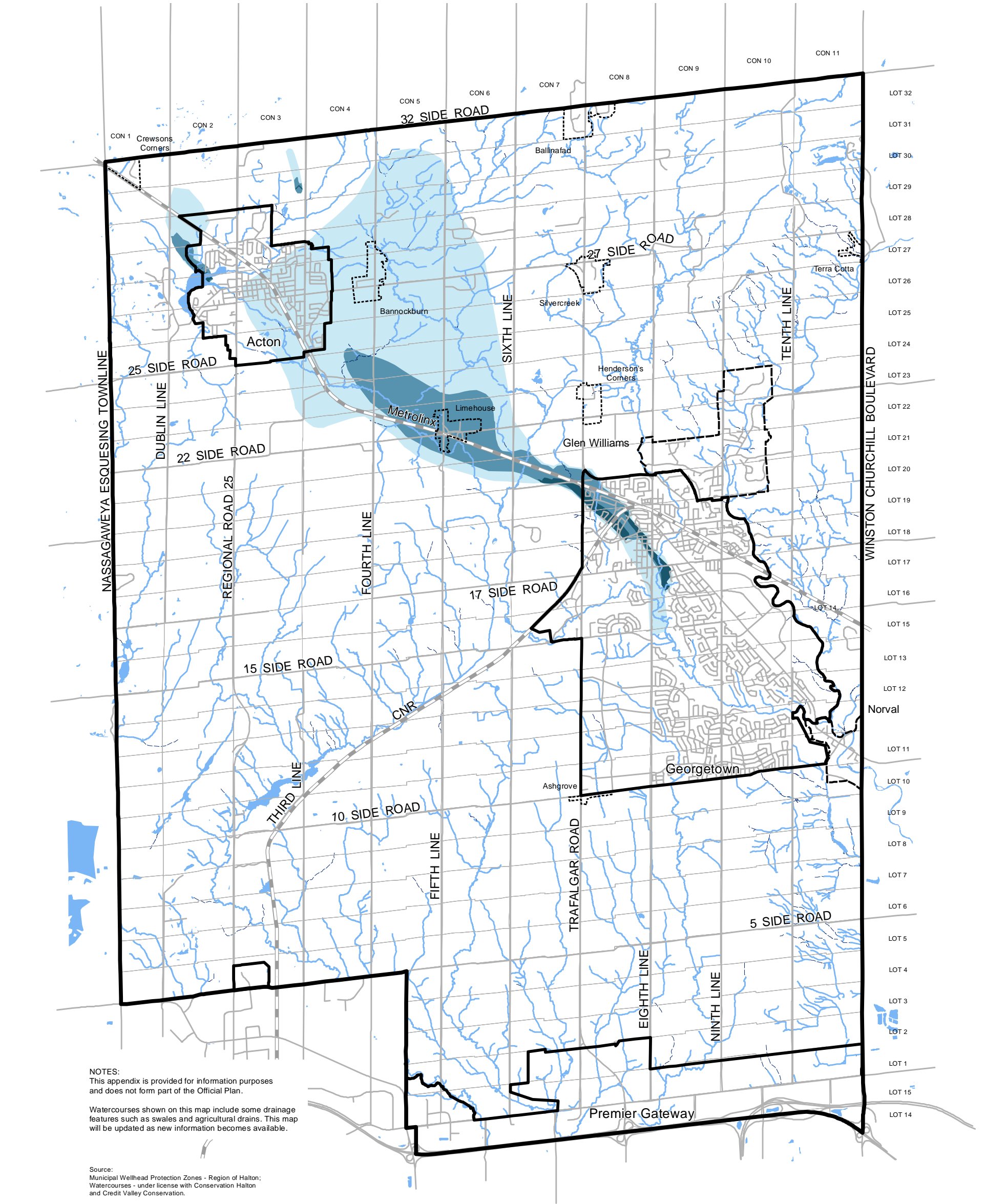
Town of Halton Hills Official Plan



APPENDIX X1A  
ENVIRONMENT  
NATURAL AREAS





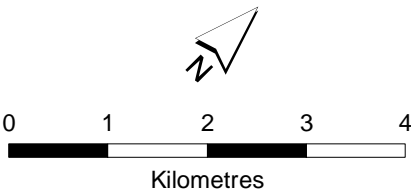


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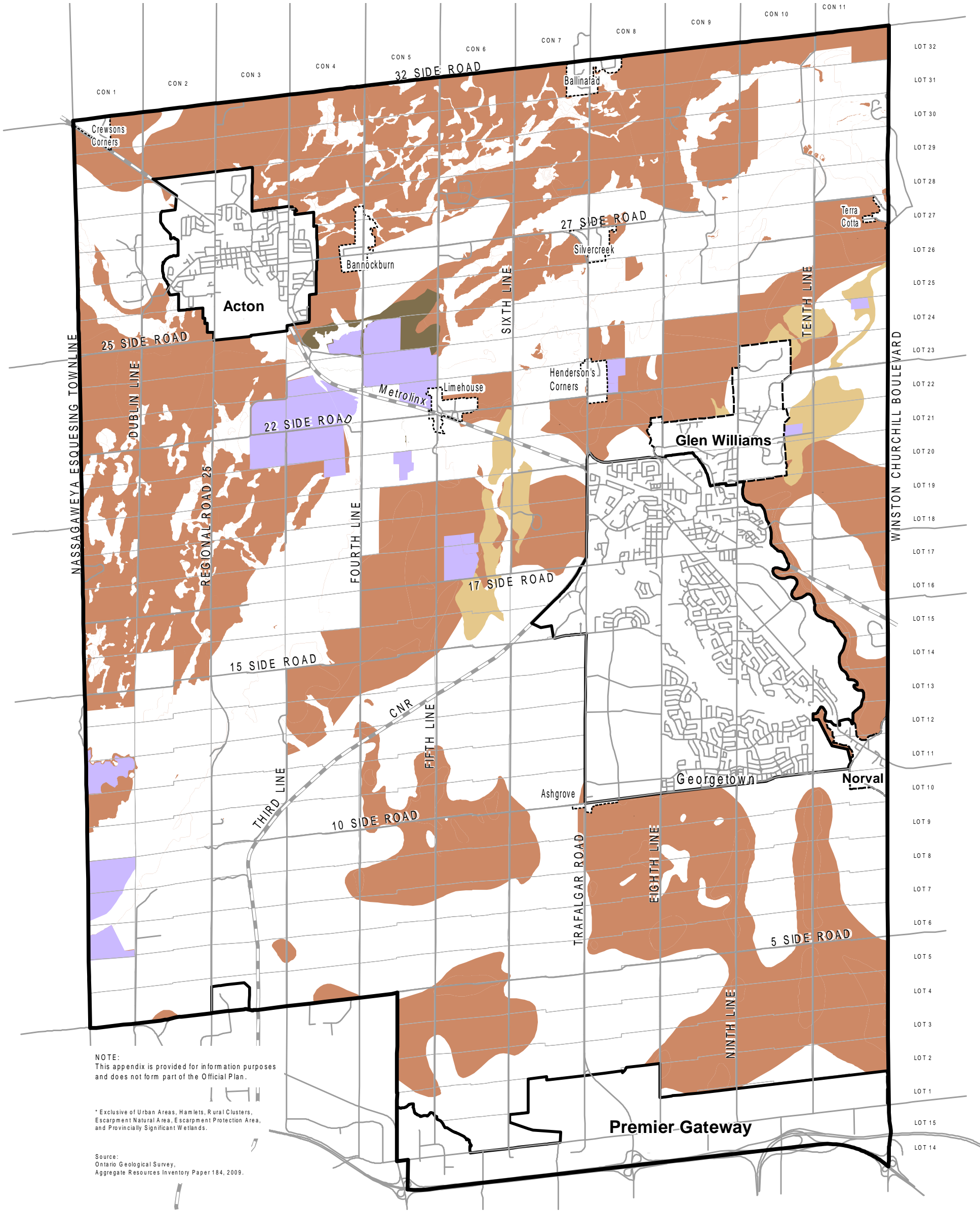









Town of Halton Hills Official Plan



APPENDIX X1B  
 ENVIRONMENT  
 WATER RESOURCE AREAS





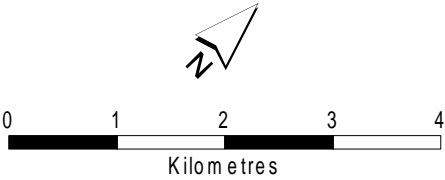
- |  |  |  |
|--|--|--|
|  Selected Bedrock/Shale Resource Area |  Town of Halton Hills Boundary |  Railway Line |
|  Primary Sand Gravel Resource Area    |  Urban Boundary                |  |
|  Secondary Sand Gravel Resource Area  |  Hamlet Boundary               |  |
|  Licensed Pits and Quarries           |  Rural Cluster Boundary        |  |

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CONSOLIDATION

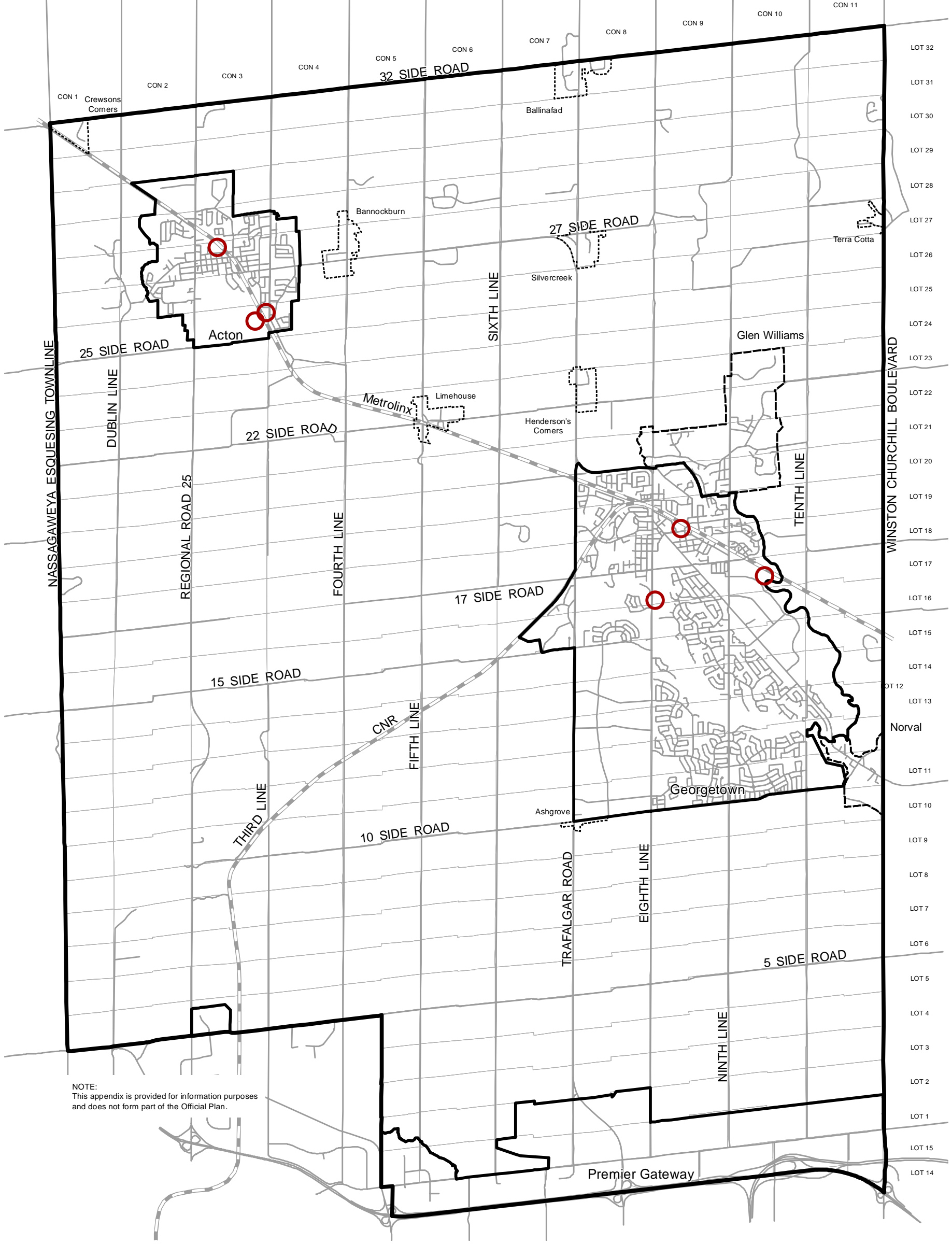


Town of Halton Hills Official Plan



APPENDIX X2  
HIGH POTENTIAL MINERAL  
AGGREGATE RESOURCE AREAS





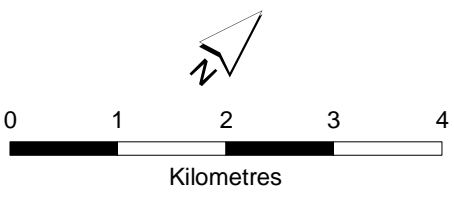
- Waste Disposal Site
- Town of Halton Hills Boundary
- Urban Boundary
- Hamlet Boundary
- Rural Cluster Boundary
- Railway Line

APRIL 30, 2024

CONSOLIDATION



Town of Halton Hills Official Plan



APPENDIX X3  
WASTE DISPOSAL SITES



## X4 TOWN OF HALTON HILLS URBAN DESIGN GUIDELINES

### X4.1 GEORGETOWN DOWNTOWN DISTRICT

The following urban design guidelines are applicable to the Georgetown DOWNTOWN District.

#### X4.1.1 BUILT FORM

##### X4.1.1.1 HISTORICAL PRESERVATION

###### Inner and Outer Core

- Where a new or infill development is to be constructed adjacent to a building of architectural or historical significance, consideration with regard to built form, scale, detailing, colour, and materials should be given to ensure harmony with the features of the existing building (Figure X4.1).
- For exterior improvements or renovation of buildings of architectural or historical significance within North Precinct of the Inner Core as defined herein, the requirements established in “Georgetown – Façade Improvement Project” and “Façade Improvement Plan” should be applied.



Figure X4.1: Undesirable Infill Elevations

#### X4.1.1.2

#### BUILDING SETBACKS

##### Inner Core

- Front building walls will be encouraged to align with the front walls of adjacent buildings in order to maintain a consistent building mass along the street line (Figure X4.2).
- In the North Precinct, front building wall of new infill buildings should occupy a majority of the lot frontage to minimize the unwarranted gap in between buildings. It ensures the creation of a consistent street façade. Traditional urban form is established by small scale attached buildings with no open space between them. The only exception is well-lit public passageways to the rear parking area (Figure X4.3).
- For corner lots in the North Precinct, the design of the side wall of the building should be extended along the exterior side lot line to at least 60% of the lot depth and this side wall should be located within 3m of the side lot line,
- Where breaks in the street building wall occur, the continuation of the linear street form will be encouraged with the use of site elements such as street trees, planters, gates or low walls.
- The provision of landscaped open space and amenity areas such as entrance plazas, forecourts and outdoor cafes are encouraged in places where buildings are not built to the street line. These open space areas can provide added interest to the street environment.



Figure X4.2: Main Street Streetscape





Figure X4.3: Public passages between buildings on Main Street

#### X4.1.1.3 BUILDING HEIGHT AND MASSING

##### Inner Core

- Maintain a consistent “base building” height of two to three storeys within the North Precinct. This base height should be equivalent to the heights of the adjacent buildings (Figure X4.4).
- Generally, a minimum two to three storey “base building” height should be maintained for proposed buildings within the South Precinct.
- Any portion of the building greater than the specified base height should be set back or tapered above the base.
- Maximum building heights should not exceed the height limits established in a new comprehensive zoning by-law.
- Continue the finer grain of architectural massing established in the North Precinct by discouraging new buildings with monolithic façades.
- To contribute to the District’s identity, taller decorative building structures should be encouraged at the Main Street/Mill Street intersection (Photo 6) at corner lots.
- Taller new buildings should have regard to micro-climate control to minimize shadow impacts to public open spaces, private amenity areas and important public sidewalks, and to protect pedestrians from negative effects of wind induced by buildings.

#### Outer Core

- Maintain heights of up to three storeys for proposed low density residential building.
- Maintain maximum residential building heights in medium density areas of three to four storeys.
- Maximum building height for high density residential building is four to six storeys, except as otherwise specified in the Zoning By-law.
- In low density residential areas, proposed residential building massing and height should be related to the adjacent buildings to enhance the existing neighbourhood character.
- Multiple unit residential buildings should be designed to retain the general massing character of large detached homes. The exterior walls should not have the appearance of an overly massive building block.
- Row or townhouse blocks should not exceed widths of eight attached units.
- Individual residential units should have clear identities through varying architectural treatment, (i.e. varying façades, window sizes/styles, columns, etc.) but should remain consistent in overall theme.
- Residential units on corner lots which are exposed to a public street and in public view, should have upgraded elevation treatments for both the fronting and flanking sides of the buildings.

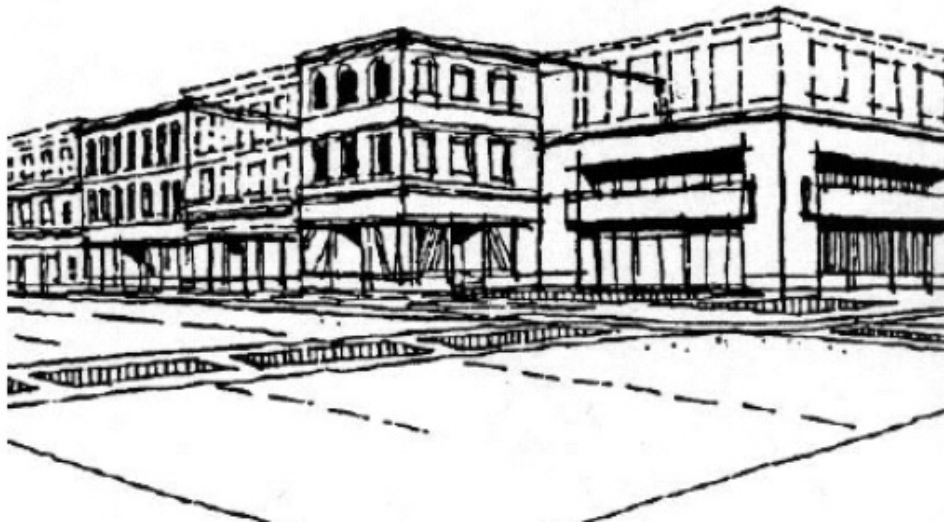


Figure X4.4: Maintain a Consistent Building Height

## Inner Core

- Buildings should accentuate the corners of significant street intersections through the use of both prominent building massing, addressing the intersection as well as the street fronts, and building features to provide landmarks within the Town's street system.
- New buildings should be designed and sited to preserve and enhance special street views and significant buildings.
- The use of blank walls that are visually prominent is discouraged. Where such façades exist, the use of murals or landscaping to screen walls and provide visual interest will be encouraged.
- Spacing, proportions, dimensions, quantity and general patterning of windows in new buildings should be sympathetic and similar to buildings of historical and architectural significance, and to adjacent existing buildings. Rows of windows should be used to define each floor level.
- Continuation of the rhythm of storefront windows in new developments along Main Street South of the North Precinct will be encouraged. Storefronts at the street level traditionally have large window openings to expose goods within shops to passers-by and to promote retail activity.
- The use of large sheets of reflective glass at street level should be discouraged.
- The provision of retail or personal service uses along pedestrian routes is encouraged in order to provide for pedestrian interest and security.
- The roof forms of proposed buildings should complement those of existing adjacent buildings. In particular, flat roof forms should be encouraged along the main commercial streets where most existing buildings have flat roofs.
- Pitched roof forms are appropriate along side streets with a more residential nature, assuming existing buildings along these streets have pitched roofs.
- Parapet walls should be used to screen rooftop equipment where flat roofs are proposed.
- Materials and colours for proposed building, and for additions or renovations to existing structures, should complement and be compatible with adjacent buildings.
- The use of fine architectural detailing in the building façades is encouraged, especially around windows, and at the building's cornice lines. A variety of architectural details adds richness and interest to façades, and can build on the tradition of craftsmanship established in heritage

buildings of Downtown Georgetown. Detailing should be compatible and in harmony with adjacent buildings.

- The use of canopies should be encouraged as canopies and awnings add colour and interest to streetscapes, and they provide weather protection for pedestrians and for merchant's goods. Canopies also reinforce the identity of individual retail and service outlets and may be used to accent entrances to residential buildings located on main streets.
- Canopies should have a minimum 1m depth from the building face and a minimum clearance above sidewalks of 2.4m. The encroachment of any canopy is subject to the Zoning By-law.
- Grade changes at the street level should be minimized to allow pedestrians to move directly from the street into buildings. Where a grade change is necessary, the difference in elevation between street level and building entrance should not exceed 1.0m at the exterior face of the building. In these cases, steps should be expansive to allow for maximum pedestrian movement, and ramps for barrier free access must be provided.

#### Outer Core

- The roof forms of proposed buildings should be compatible with those of existing adjacent buildings. Pitched roofs are characteristic in the Outer Core, and are recommended for proposed residential buildings.
- Building materials, colours and details should be compatible and complement those of adjacent buildings.
- Pedestrian entrances to both single and multiple residential buildings should be spatially and architecturally prominent and welcoming.
- The façades of buildings should have windows that overlook streets and/or lanes, in order to maintain the character of a safe and pedestrian friendly community.
- Garages for low and medium density residential buildings should not form the prominent feature of the dwelling façade. Garages should be well integrated into the overall residential building design.
- For block residential developments, garages and parking structures should not be located in front of medium and high residential buildings.

### X4.1.2 STREETScape

#### X4.1.2.1 MAIN STREET: NORTH AND SOUTH PRECINCT

##### Inner Core

- Sidewalk widths should be maintained or increased, where possible, along retail commercial streets of the North Precinct, as ease of pedestrian movement is an important factor in retail areas.

- A continuous and consistent streetscape should be developed along Main Street South and selected side streets through the selection of suitable streetscape elements, such as street trees, sidewalk materials, and lighting, and repeating these elements along the Main Street.
- Design detailing and scale of streetscape amenities should be coordinated with the overall streetscape design and architectural character.
- Paving materials and patterns for the Main/Mill Street intersection should complement the historic architectural character, and these sidewalk patterns and materials should be continued along Main Street South.
- The Main Street streetscape theme should be extended along selected side streets, pedestrian laneways, walkways and forecourts in order to link streets to parking areas and other mid-block pedestrian connections.
- Pedestrian cross-walk linkages should be created by extending paving materials across key intersections within the Inner Core, such as Main Street South and Mill Street.
- Street tree species that are low maintenance, pollution tolerant, narrow form, and have good year round appearance should be selected.
- Trees should be planted at grade wherever possible for ease of pedestrian movement and to reduce visual clutter.
- At the Main Street South and Mill Street intersection, urban landscaping should be encouraged to emphasize this intersection.
- Where site conditions permit, trees should be spaced an average of 9 metres apart along Main Street South within the North Precinct and the South Precinct. The placement of trees may be impacted by underground and above ground utilities, and the availability of a suitable planting space in the boulevard and between curbs and/or buildings. A tree replacement program should be implemented in areas where the street opens because of tree dieback.
- Where important intersections require that no trees impede sight lines, shrub planting (possibly in planters) should be considered. Where Main Street meets the access to the trails, a row of trees should guide the visitor to and from these trails and to the Inner Core.
- A program to replace wood planters with precast planters should be undertaken.
- Planters should be used for seasonal effects.
- A tree replacement program for mature trees should be established, which replaces those trees with similar tree species.

#### X4.1.2.2 RESIDENTIAL LOCAL ROADS

##### Outer Core

- A sidewalk system should be established throughout the Outer Core and Inner Core to ensure the continuation of pedestrian movement.
- Sidewalks should be added to at least on side of the streets where sidewalks presently do not exist and where possible, such sidewalks should be located within the planned street right-of-way.
- Existing sidewalk widths and materials should be maintained.
- A tree replacement program for mature trees should be established, which replaces those trees with similar tree species.

#### X4.1.2.3

#### GATEWAY FEATURES

- The entrance points to the Downtown Georgetown District should be visually defined by the use of special streetscape features, such as flags, banners, accent planting or feature structures, to give a sense of arrival. The entrance points occur at the following four intersections:
  - a) Main Street South and Guelph Street (North Gateway);
  - b) Main Street South and Maple Avenue (South Gateway);
  - c) Mill Street and Guelph Street (East Gateway); and,
  - d) Charles Street and James Street (West Gateway).
- At the North and South Gateways, redevelopment of sites in this area should contribute to a visual “framing” of downtown with respect to their contribution to building and awareness of the District.
- At the North Gateway, the triangular landscaped space on the north west corner of the Guelph Street and Main Street intersection should be redeveloped to include signage or other features that draw attention to the downtown, without impeding the vista to the downtown (Figure X4.5, Figure X4.6).
- The East Gateway should be defined at Mill Street and Guelph Street. Elements drawn from the features at the North and South Gateways should be incorporated.
- The West Gateway is James Street at Remembrance Park and should incorporate gateway features from the other downtown gateways, without disturbing the overall park design.
- Where a gateway structure is to be located within the jurisdiction of the Ministry of Transportation (MTO) or the Regional/local municipality, design and construction of the structure will require the approval of the respective authority.

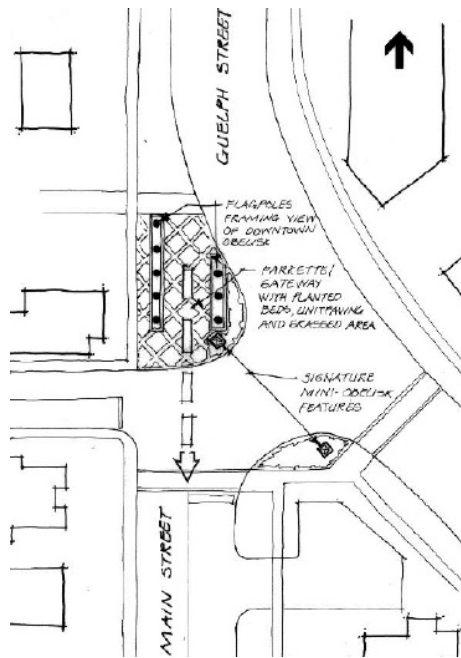


Figure X4.5: North Gateway



Figure X4.6: Median with Obelisk as an extension of the North Gateway

#### X4.1.2.4 STREET FURNISHINGS

- As existing sidewalks in the North Precinct of the Downtown Georgetown District are fairly narrow, planters within sidewalk areas should be discouraged. Planters may be used along the street line and within the development parcel to define the street edge.
- Hanging flower baskets, as a viable alternative, can be incorporated with pedestrian lampposts as an attractive streetscape element.



- Wherever sidewalk width permits, streetscape elements such as benches, planters, waste receptacles, and newspaper boxes should be grouped together in nodal areas, and located in conjunction with street trees and lights (Figure X4.7).
- In major new development, bicycle racks should be provided (Figure X4.8).
- Pedestrian scale streetlights should be located along intensively used areas such as Main Street South north of Park Avenue, the Main Street and Mill Street intersection, and along pedestrian laneways within the Inner Core.
- Light fixtures should be chosen to integrate into the overall streetscape design and architectural character. Fixtures and poles should provide visual interest and pedestrian scale during daylight hours.
- A style for municipal street signage, which reflects the traditional image of the Downtown Core, should be established.



Figure X4.7: Grouping of streetscape elements



Figure X4.8: Street Furnishings



#### X4.1.3

#### OPEN SPACE

##### X4.1.3.1

##### REMEMBRANCE PARK

- Development of a gateway at this location should be explored to incorporate Remembrance Park as part of enhanced streetscape at James and Charles Streets.
- Connection of Remembrance Park to the Inner Core should be enhanced, where feasible, by the construction of a continuous special walkway on the south side of James Street, subject to pedestrian safety, paved in concrete and pavers. This special walkway will require the partnership of landowners on the south side of James Street, the Georgetown Downtown BIA and the Town. Parking should be maintained, where possible. Further, consideration should be given to expanding the walkway on the commercial building front. New developments between Main Street and Remembrance Park should be required to construct a sidewalk to provide and continue this connecting link.

#### X4.1.4

#### ACCESS AND CIRCULATION

##### X4.1.4.1

##### VEHICLE ACCESS AND CIRCULATION

- Access into, and circulation within, an individual site should provide safe and well-defined routes.
- Proper lighting, landscaping, and pedestrian amenities along the circulation routes will enhance overall site appearance, promote public safety, and encourage the use of public transit in the future. Reference should be made to the relevant Regional or municipal documents in regards to safety.
- Landscaped traffic islands should be used, where possible, to delineate the main drive aisles, subdivide large parking areas into smaller “courts”, and improve edge conditions between the public street and adjoining properties.

##### X4.1.4.2

##### PEDESTRIAN AND CYCLING ROUTES

- Identifying markers should be installed at the corner of Park Avenue and Main Street to direct cyclists and pedestrians to the multi-use pathway, as referenced in the Trails and Cycling Master Plan.
- Markers should be installed on the multi-use trail to identify the direction to the Downtown Core.
- Markers should be installed to connect Cedarvale Park and the Downtown Core.

- The markers should be designed with similar architectural characteristics to that of the Downtown Core.
- Lighting levels along pedestrian routes should provide appropriate, continuous illumination.
- The design of pedestrian and cycling routes should have regards to the relevant Regional or municipal documents in regards to safety.

#### X4.1.5 PARKING AND SERVICE AREAS

##### X4.1.5.1 ON-STREET PARKING

- Parking on all side streets should be allowed, where feasible.

##### X4.1.5.2 OFF-STREET PARKING

- Large expanses of grade-level parking areas, especially those located in front of buildings, should be discouraged.
- Where surface parking for new development is necessary, parking lots should be located at the rear of buildings.
- Vehicular entrances to parking structures and areas should be located on side streets and entrances along Main Street should be avoided, wherever possible.
- The number of vehicular site entrances from the street should be kept to a minimum.
- A clearly defined pedestrian access route should continue through all driveways into parking areas and facilities.
- Surface parking area should be well lit to ensure pedestrian safety.
- Where possible, the integration of small, interconnected parking areas distributed throughout the site should be encouraged.
- Large surface parking areas should be visually softened by introducing street trees, planters and clearly defined parking routes.
- Delineate parking areas that abut streets with the use of attractive low walls, fences or planters, or with rows of small street trees (Figure X4.9).
- Incentives for the provision of public parking in the new infill development should be promoted.
- Designated barrier free parking spaces should be located close to the barrier-free building entrances and clearly identified by signs or markings.
- Adequate bike racks should be provided in the parking lot and/or near the building entrances.



Figure X4.9: Fence and landscaping to screen parking area

#### X4.1.5.3 STRUCTURED PARKING

- To increase parking capacity, the option of converting the existing parking lot east of Main Street, between Cross and Mill Streets, to a structures parking should be considered.
- Where feasible, parking should be incorporated into proposed buildings in above grade or underground parking structures.
- Above grade parking structures should be screened at grade and designed as part of the overall building. The design guidelines for building details also apply to parking structures.
- Retail or personal service uses should be encouraged at the street level of parking structures, where feasible.
- When exiting from the parking structure, vehicles should have a clear view of the pedestrian zone. Every entry into and exit from the parking structure should have a minimum stacking area equal to one car length or 6.0 metres behind the property line. The gradient of this stacking area should be less than 5%.
- Parking structures should be well lit to ensure pedestrian safety and provide for ease of identification.
- The exteriors of pedestrian entrances to parking structures should be illuminated with overhead lighting.

#### X4.1.5.4 PARKING STRATEGY PLAN

- Update of the Parking Study (1990) for the Georgetown Downtown District is recommended. It should be initiated by the local BIA, in conjunction with the Town and the Region (where appropriate), to have an updated analysis of the parking supply and demand and to further develop solutions and implementation strategy for the potential parking shortage, if any.

#### X4.1.5.5 SERVICE AND LOADING AREAS

- Loading, garbage facilities and other service functions should be screened from the street and from public view. Location of these facilities within or at the rear of buildings is encouraged.
- Coordinate the entrances to service areas with those of parking areas to reduce the amount of vehicular interruptions along pedestrian streets.
- Service area entrance locations along Main Street South should be discouraged and such entrances should be encouraged to locate off side streets, or at the rear of the properties.

#### X4.1.6 OPEN STORAGE AREAS

- No open storage area should be allowed.

#### X4.1.7 SIGNAGE

- Signage should form a part of the overall design of buildings and design, materials and colours of signage should reflect building scale and complement the building façade.
- The existing signage by-law should be updated, amended and applied as a regulating legislation.

### X4.2 GO STATION DISTRICT

#### X4.2.1 BUILT FORM

##### X4.2.1.1 BUILDING SETBACKS

- In the North Precinct, building setbacks from the street line should reinforce the streetscape edge and allow for a suitable sidewalk, boulevard and landscape strip width for general pedestrian use and the placement of streetscape amenities.
- In the North Precinct, the main wall of a residential building should be located closer to the street line with front entrance(s) oriented toward the street.
- In the South Precinct, new development associated with the GO Station with a King Street frontage should be built to align with the front walls of adjacent buildings, in order to maintain a consistent building mass along the street line.

#### X4.2.1.2

#### BUILDING HEIGHT AND MASSING

- In the North and South Precincts, building heights and massing should have regard to the scale, shadow impact, and privacy impact of adjacent properties.
- In the North Precinct, the scale of the new building massing should complement neighbouring properties, particularly where dissimilar land uses abut.
- In the North Precinct, the maximum building height for high density residential uses should be restricted to 6 storeys. However, additional height may be permitted subject to the criteria outlined in the Official Plan. Articulation of the building height and mass to include a 2- to 3-storey base podium, with increased setbacks for the storeys above the base podium, is encouraged in order to establish a unified building edge that related to the adjacent low and medium density residential uses.
- In the North Precinct, taller new buildings should have regard to micro-climate control to minimize shadow impacts to public open spaces, private amenity areas and important public sidewalks, and to protect pedestrian from negative effects of wind induced by buildings.
- In the North Precinct, the maximum building height for medium density residential uses should be restricted to 4 storeys. The massing of townhouse dwellings should comprise a maximum of eight units to prevent a single monotonous elevation to the street.
- In the South Precinct, the height and massing of any new development within the GO station should be integrated with the existing station building and should complement and relate to the existing residential neighbourhood in order to enhance the existing neighbourhood character (Figure X4.10).
- In the South Precinct, taller non-habitable structures (such as clock towers, decorative poles with banners, and/or specialized lighting) should be encouraged in order to provide a focal point at the entrance to the GO station.



Figure X4.10: Example of enhancement of a train station

#### X4.2.1.3

#### BUILDING ELEMENTS

- Materials and colours for new buildings, and for additions or renovations to existing structures, should complement and be compatible with adjacent buildings.
- The façades of buildings should have windows, which overlook streets, lanes and parking areas, in order to maintain the character of a safe pedestrian environment.
- The main entrance of the residential buildings should have direct access facing the street.
- Pedestrian entrances to both single and multiple residential buildings should be spatially and architecturally prominent and welcoming.
- Residential units on corner lots which are exposed to the public street and in public view, should have upgraded elevation treatments for both the fronting and flanking sides of the buildings.
- For new residential buildings with the North Precinct, building walls exposed to a public street or public view should be upgraded to a design and material standard equal to the main wall treatment.
- Blank walls demonstrating no specific architectural design are discouraged. Where such façades exist, the use of murals or landscaping to screen walls and provide visual interest will be encouraged.
- The use of reflective (mirror) glass should be discouraged.
- Garages for low and medium density residential buildings should not form the prominent feature of the dwelling façade and garages should be well integrated into the overall residential building design.
- In the South Precinct, new development with pitched roof forms should be encouraged to reflect the residential character of the area.
- Fine architectural detailing in the building façades should be promoted in the South Precinct to complement the ‘character streets’ and to provide a focal point at the terminus of Queen Street.

#### X4.2.2

#### STREETSCAPE

##### X4.2.2.1

##### ‘CHARACTER STREETS’

- The quality of ‘character streets’, including the tree-lined boulevard with highly detailed styles of residential dwellings, should be preserved.
- At the gateways to Queen Street and King Street from Guelph Street and Mountainview Road North respectively, Linden trees should be planted in a grouping, together with a gateway sign, to make a visual link with the GO Station entrance.
- A specific gateway design should be created to distinguish this access to the GO station.

- Where a gateway structure is to be located within the jurisdiction of the Ministry of Transportation (MTO) or the Regional/local municipality, design and construction of the structure will require the approval of the respective authority.
- Markers in the form of bollards to match those in the downtown and to reflect the gateways to King and Queen Streets, should be installed along the south sides of King Street and Queen Street at approximate 12-metre intervals (Figure X4.11).

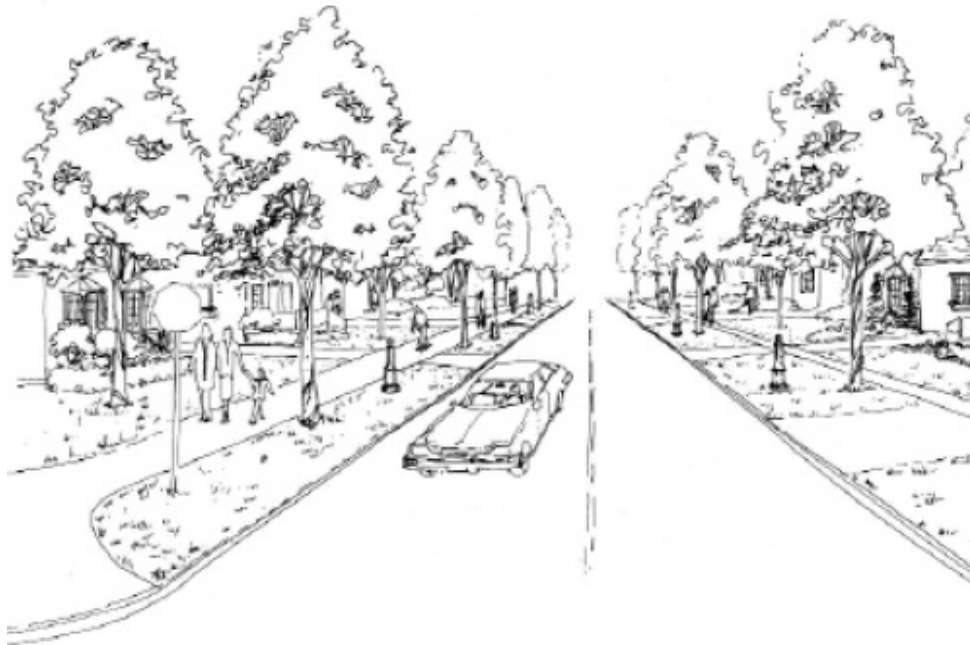


Figure X4.11: Markers along Queen Street as a wayfinding device to the GO station

#### X4.2.3 LANDSCAPE

- Landowners should be encouraged to provide landscape buffers or islands within parking lots on private lands (Figure X4.12).
- Planting strips with minimum widths of 4.5 metres should be provided between the street line and parking lots.
- Low fencing, combined with low shrubs, may be used along property lines to screen/protect parked vehicles, and also to provide visual interest.
- Decorative fences higher than 1.2 metres, or continuous planting of tall shrubs and coniferous trees, which obscure pedestrian views, should be discouraged.
- On sites where buildings are to be located close to the front lot line and no parking in front of the building is proposed, landscape planting will be required in order to frame the building. Alternatively, the construction of low metal and masonry fences to define the site will be considered and it should be coordinated with the overall streetscape design.

- Landscape treatments should be provided within medians at major access driveways in the form of high branching street trees and low shrub planting, which do not obstruct vehicular views.
- Shrubs should cover a minimum of 50% of the planting strip.
- Screen planting, where provided, should cover a minimum of 50% of the planting strip area and should form a continuous visual screen between properties.
- All landscape plans should be reviewed by Halton Hills staff to ensure that the proposed landscape design is compatible with adjoining land holdings.



Figure X4.12: Existing GO station parking lot

#### X4.2.4 OPEN SPACE

##### X4.2.4.1 PARKETTE

- The design of the parkette north of the track should respect its use as a pedestrian gateway to the GO station. As such, a landscape structure should announce such an entrance.
- A minimum 3-metre wide sidewalk should be provided for pedestrians, cyclists and roller-bladers.
- The parkette's utility should be enhanced through the incorporation of a totlot play equipment to serve the surrounding community.

#### X4.2.5 ACCESS AND CIRCULATION

##### X4.2.5.1 VEHICULAR ACCESS AND CIRCULATION



- Access into, and circulation within, an individual site should provide safe and well-defined routes.
- Proper lighting, landscaping, and pedestrian amenities along the circulation routes will enhance overall site appearance, promote public safety, and encourage the use of GO transit. Reference should be made to the relevant Regional or municipal documents in regard to safety.
- In the North Precinct, road connections to existing residential streets are encouraged to complete a connected street network. However, any new street connections should have regard to the topographic conditions (i.e. particularly between Rosetta and John Street, and between River Drive and Mountainview Drive/Stuart McLaren Road intersection).

#### X4.2.5.2 PEDESTRIAN AND CYCLING ROUTES

- A multi-use pathway from north of John Street along an abandoned rail corridor should be formalized and linked to the GO station.
- Signage at the GO station should direct cyclists and pedestrians to the trails as illustrated on the Trails and Cycling Master Plan.
- Formal pedestrian link to Dominion Gardens Park through an internal sidewalk system within the neighbourhood should be established.
- The Town's Trails and Cycling Master Plan shall identify a comprehensive pedestrian route system within the vicinity of the GO Station District.
- The design of pedestrian and cycling routes should have regard to the relevant Regional or municipal documents in regards to safety.

#### X4.2.6 PARKING AND SERVICE AREAS

- Parking areas that abut streets should be screened through the use of attractive low walls, decorative fences, planters, low shrubs, or rows of smaller street trees.
- Large surface parking areas should be visually softened by introducing street trees, planters and clearly defined pedestrian routes.
- Deciduous tree planting should provide definition to the street, shade opportunities, wind control or become part of a visually improve planting strip.
- Coniferous trees should provide wind control and become part of a visually improve planting strip.
- Shrub plants shall be used for wind control, visual screening and become part of a visually improved planting stop. Caution is made that any planting should not encourage places for hiding near pedestrian routes.
- In the North Precinct, large expanses of unbroken proposed surface parking for the GO station or high density residential uses should be avoided and elements including landscaping islands

incorporated with plantings and light standards, should be used to define smaller, interconnected parking areas.

- Designated barrier-free parking spaces should be located as close as possible to barrier-free access routes and building entrances.
- Adequate bike racks should be provided at the existing and future GO parking lots.
- A clearly defined pedestrian access route should continue through all driveways into parking areas and facilities.
- Surface parking areas should be well lit to ensure pedestrian safety.
- Surface parking areas for the GO station or high density residential uses should avoid light spillage to the adjacent residential properties.
- In the North Precinct, ramps to underground parking structures should not detract from the façade or landscaping of the building.
- For medium density residential uses within the North Precinct, paired driveways should be encouraged to enhance the visual effect of an integrated front yard landscaped area shared between two neighbouring units and to provide opportunities for on-street visitor parking.
- Service (including exterior garbage storage and recycling containers) and loading areas should be located away from the primary building face and the public view, preferable in the rear or side yard of the building and integrated within the building, where possible.
- Appropriate screening of service and loading areas should be incorporated using landscaping or built screens, or a combination of both, Any building materials used to create screens should relate to those of the primary building exterior.

#### X4.2.7 OPEN STORAGE AREAS

- In the South Precinct, open storage areas for the proposed GO station expansion should not front onto the public streets and should be clearly designated and defined with landscaping and/or architectural edge treatments.
- In the South Precinct, lighting for any open storage areas should not spill over onto adjacent residential properties.
- In the North Precinct, open storage area should be permitted in order not to detract from the residential character of the area.

#### X4.2.8 RAIL CORRIDOR

##### X4.2.8.1 NOISE, VIBRATION AND SAFETY MITIGATION MEASURES

- A berm, at approximately 2.5 metres in height, should be constructed as a safety element along the railway (Figure X4.13, Figure X4.14).

- A noise attenuation wall should be provided above the safety berm to protect residential properties.
- Design specifications of the safety berm, noise fence and vibration measure and development setback should conform to the CN's Principal Mainline requirements.
- The noise wall should have a unique design character. Plantings should be provided along the whole north side of the noise attenuation wall to soften the visual impact of the wall, while a low maintenance planting material (such as fescues and wildflowers) should be planted to the rail side to contribute to the ambience of the GO station.
- Tree planting in these areas should be generous, providing high level screening. Shrubs should be planted according to their size to create masses and continuous planting beds on 50% of the land surface of the buffers. Grass maintenance should be minimized or eliminated from areas within the jurisdiction of GO transit.



Figure X4.13: Existing site condition north of CPR



Figure X4.14: Landscaped safety berm for future development along CPR line

### X4.3 GUELPH STREET CORRIDOR DISTRICT

#### X4.3.1 BUILT FORM

##### X4.3.1.1 BUILDING SETBACKS

- Where feasible, encourage building placement at the minimum set back required by the Zoning By-law, thus avoiding large setbacks and substantial areas of surface parking in the front yard.
- In the East Precinct, building placement should respect the minimum front yard setback as prescribed in a new comprehensive zoning by-law, but also should be restricted in locating no more than 25 metres back from the front lot line. The maximum setback is established on the basis to include maximum two rows of parking, adequate landscape screen and building forecourt (Figure X4.15).
- At the Marketplace Shopping Centre, placement of our buildings closer to the street should be strongly encouraged to foster a visually pleasant and pedestrian friendly streetscape environment, while the overall parking supply should not be compromised (Figure X4.16, Figure X4.17).
- In the Regional Focus Zone, to foster a more urban street related image, the building orientation and placement should address the street frontage with main wall and entrances located close to the minimum street yard setback, where possible (Figure X4.18). Where feasible, building placement should respect the minimum front yard setback as prescribed in a new comprehensive zoning by-law, but also should be restricted in locating no more than 25 metres back from the front lot line. The maximum setback is established on the basis to include maximum two rows of parking, adequate landscape screen and building forecourt.
- Within approximately 200 metres of the intersection of Guelph Street and Mountainview Road (the Crossing), a significant built form presence at street frontage will be required in order to transform its urban form into a vibrant and intensely developed urban center and provides a visual icon in contrast to the rest of Guelph Street corridor.
- Street yard setbacks within the Crossing should be restricted to a minimum of 0 metres and a maximum of 3 metres.
- Within the West Precinct along the south side of Guelph Street, mixed use development should be encouraged to intensify the land use along the corridor and to create its own identity for this section of the Guelph Street Corridor. Given the configuration and size of the street blocks, it is preferred that future redevelopment should incorporate buildings within 3 metres of the street line in order to define the street edge, to promote a pedestrian-oriented streetscape and to minimize the impact to the adjoining stable residential neighbourhood.
- Building treatments, such as unenclosed entrance structures, colonnades, overhangs, canopies, awnings, and landscape elements should be allowed to encroach into the front yard.

- If alternate site planning demonstrates that the intent of this subsection is achieved, flexibility in the application of these guidelines should be considered.

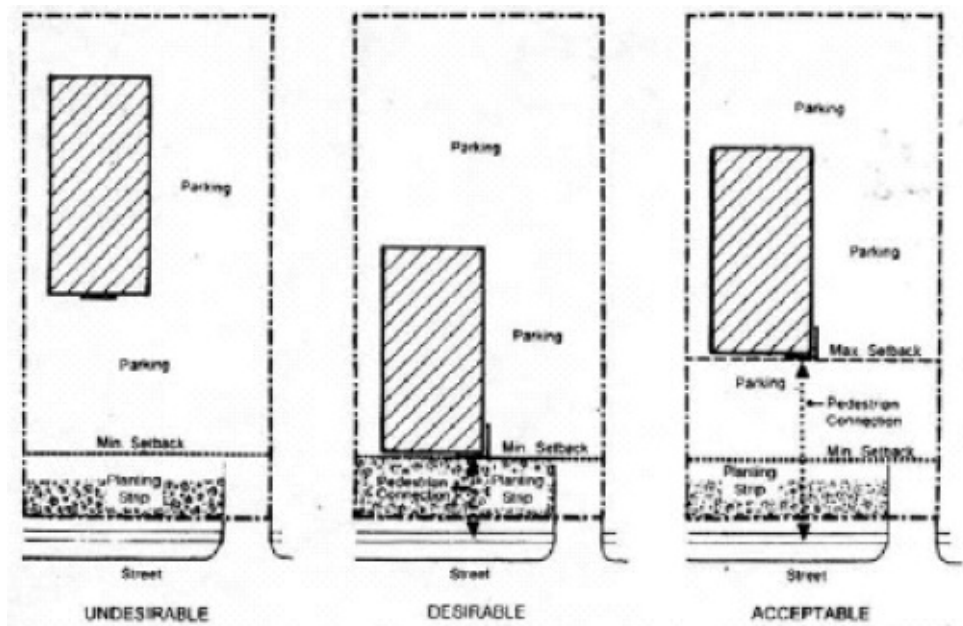


Figure X4.15: Building placement

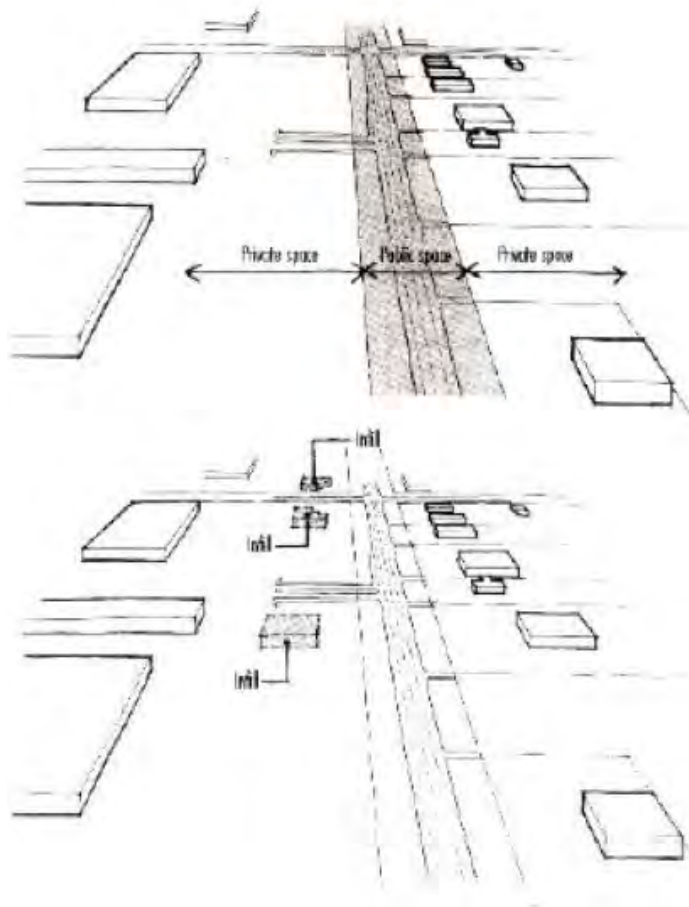


Figure X4.16: Out buildings for the existing development

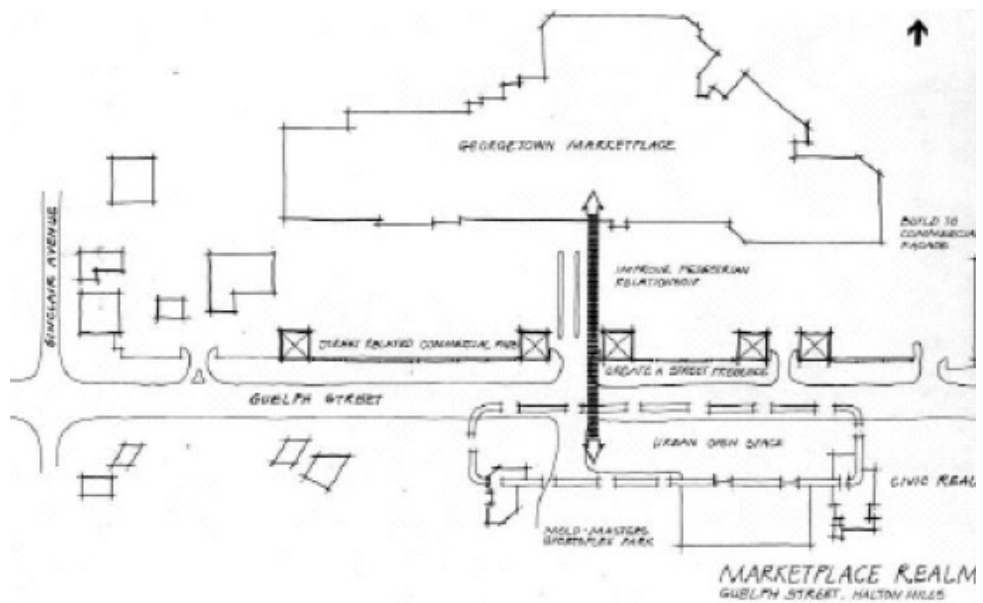


Figure X4.17: Promote pedestrian linkage



Figure X4.18: Commercial building oriented to public street

#### X4.3.1.2 BUILDING HEIGHT AND MASSING

- Maximum building heights should conform to the relevant zoning by-law.
- At Gateway Nodes, the Crossing, and within the Regional Focus Zone, taller structures should be encouraged in order to contribute to a stronger area identity through inclusion of vertical emphasis at the intersections.
- A minimum two storey and maximum four storey building height should be maintained for the proposed buildings within the West Precinct, and taller structures with architectural articulation anchoring street corners should be promoted (Figure X4.19).
- Within the Crossing, built form should be heightened to signify the intensity and importance of this activity centre of the Guelph Street Corridor (Figure X4.20).
- The bulk or mass of buildings, in conjunction with setback lines, should, where possible, establish a consistent urban streetscape to assist in defining a recognizable street edge.
- The scale of building mass should be integrated with neighbouring properties, particularly where dissimilar land uses abut.
- Building massing should be designed to minimize impacts on adjacent properties with respect to privacy overview, sunlight access, and wind tunneling effect.
- Building massing should include variations in the building envelope to allow for elements such as entrances, forecourts, or other specific building articulations. These elements should provide interest and detail when viewed from the public street, particularly at corner locations.





Figure X4.19: Taller structure anchoring street corner



Figure X4.20: Built form to address the intersection



#### X4.3.1.3

#### BUILDING FRONTAGE

- A more substantial building façade fronting the public street at the minimum setback line is encouraged, in order to define a more urban street edge.
- In the West Precinct, it is encouraged that a minimum of 2/3 of the main wall of the building should be built within 3m from the street line. Where parking has to be located in front of the building, a continuous and well landscaped green strip should be provided to enhance the streetscape (Figure X4.21), and a well-defined pedestrian connection between the sidewalk and the front entrance of the building should be established to promote pedestrian activity.

The following guidelines relate to built form within the East Precinct:

- To encourage presence of building façade at the street edge, the minimum building frontage should be in proportion to the front yard setback.
- At the minimum front yard setback, a minimum building frontage should be 50% of lot frontage. As the front yard setback increases, the minimum building frontage should increase proportionately up to 60% at the 25-metre front yard setback.
- For the purpose of building frontage calculation, the length of all major building components (primary building façades or extensions of the façade as decorative screens, and colonnades) could be applied cumulatively as a sum of the total building frontage.
- If alternate site planning demonstrates that the intent of this subsection is achieved, flexibility in the application of these guidelines should be considered.



Figure X4.21: Landscaped green strip to enhance the streetscape

#### X4.3.1.4

#### CORNER BUILDINGS

- At Gateway Nodes and the Crossing, corner buildings should be located at the minimum building setbacks in order to enhance the role of these areas as focal points.

- Where possible, major building entrances should also occur at, or near, the corner.
- The height of corner buildings can be stepped back, or other building recessions or projections can be employed to incorporate the entrance or other structures, which mark these as significant locations.
- At Gateway Nodes and at the Crossing, corner buildings could incorporate taller non-habitable structures or freestanding elements (such as clock towers, decorative masts with banners, specialized lighting and/or information displays) to frame and signal the importance of corner locations (Figure X4.22).
- Corner building façades should address both street frontages with equal importance (Figure X4.23).



Figure X4.22: Corner building addresses the intersection



Figure X4.23: Building façades address both streets

#### X4.3.1.5 BUILDING ELEMENTS

- A strong articulation of building façades is encouraged.

- Doorways/entrances to buildings, especially commercial buildings, should be prominent and visible, oriented toward the public street where possible, and emphasized through entrance canopies, awnings, and other architectural elements.
- In multi-tenant development, the use of multiple pedestrian entrances into the building at street level is encouraged.
- Walls parallel to the public street should be required to have windows, display windows, or a high level of architectural detail involving the use of two or more materials.
- Retail area display openings should be provided along pedestrian routes to maintain interest and improve security.
- Windows facing the street frontage, whether display windows for retail use or windows for office space, should be large, occupying a minimum of 30% of the street elevation between the ceiling and floor at grade.
- Clear glass is preferred for glazing, but some tinting based on functional considerations (building orientation, for example) is acceptable. Reflective (mirror) glass at grade should not be used and its use at upper level windows/curtain wall should be examined for architectural merit.
- Although there is no strong preference for any specific materials, exterior finishes should exhibit quality of workmanship and be relatively easy to maintain or, preferable, maintenance free. Materials such as wood or natural stucco should be avoided unless maintenance programmes can be secured.
- Use of architecturally detailed cornices is encouraged to define the top of the building façade.
- Canopies and awnings are encouraged as means of weather protection, shading, as well as adding articulation, colour and interest to the building elevation.
- Blank or single material walls that extend for the entire length of the building, parallel to the public street, should not be permitted.
- Where blank walls occur, the use of additional architectural details and building materials to enhance the visual appearance should be encouraged. Alternatively, the use of murals, painted wall signs, and other sculptural or graphic artwork should be encouraged to promote visual interest. The context of graphics and artwork should be subject to the conformity of the relevant Municipal regulations, where applicable.
- Where steps and ramps provide access, these should be architecturally integrated with the building.
- As a minimum, circulation and building access for pedestrians should conform to barrier-free access requirements as set out by the Ontario Building Code.
- Although the predominant use of flat roofs in commercial development will likely be the norm, pitched or sloped roofs should be permitted providing the design respects the context and the elevations of adjacent buildings. Dormers for fenestration or mechanical venting are encouraged.

- Rooftop mechanical equipment should be integrated with the building design, and rooftop units and vents should be screened using materials complementary to the building, where applicable.
- Where possible, parapets should be used to provide approximate building height continuity between adjacent buildings. Roof uses are acceptable and encouraged where appropriate (i.e., roof 'patios', terraces, etc.).

#### X4.3.2 STREETScape

##### X4.3.2.1 GUELPH STREET (EAST AND WEST PRECINCTS)

- An enhanced streetscape should be developed to define the street and give travellers a sense of place within the two precincts of this district.
- Continuous sidewalks should be maintained along both sides of Guelph Street.
- Sidewalks should continue across driveways to indicate pedestrian priority.
- Consistent sidewalk widths and materials should be maintained within the East and West Precincts.
- In the East Precinct, street trees should be planted at minimum 15-metre intervals between the Gateway and the Mold-Masters SportsPlex.
- Within approximately 200 metres of the Crossing, street tree planting should be provided at 12-metre intervals to indicate an increasingly urban built-form.
- In the West Precinct, street trees should be incorporated, where possible, at 15-metre intervals.
- Landscape strips that are less than 1.0 metres in width and are adjacent to a hard edge, such as a wall or curb, should be paved with feature paving material.
- Landscape strips between 1.0 and 2.5 metres in width, which are adjacent to grassed private property, should consist of grass or other ground cover.
- Landscape strips that are greater than 2.5 metres in width should consist of grass, ground cover and deciduous/coniferous trees, wherever possible.
- Standard shade trees planted within landscape strips should be spaced as recommended above. Small trees (such as Chanticleer Pear, Shubert Cherry, or Japanese Tree Lilac) should be used beneath hydro wires or in narrow (less than 2m) landscape strips, and planted on 10-metre intervals.
- High branching tree species (such as Honey Locust and maiden hair), or columnar tree species, may be used to allow views through o private commercial development (Figure X4.24).
- Two or three tree species should be used consistently within landscape strips along Guelph Street, and should be comprised of low-maintenance and pollution- and salt-tolerant species. Groupings of the same species of tree can be used provided that there are occasional plantings of other species to prevent monocultures and their associated hazards.

- In existing conditions, where the landscape strip in combination with available private property adjacent to the street line exceeds 2.0 metres in width, street tree planting at the street line should be considered.
- Trees that are planted on the street line should become the maintenance responsibility of the Town or the Region.



Figure X4.24: High branching trees to allow through view to commercial site

#### X4.3.2.2 GUELPH STREET AND MOUNTAINVIEW ROAD NORTH (THE CROSSING)

- The Crossing intersection should act as the main focal area of the Guelph Street Corridor (Figure X4.25).
- The Crossing takes a more urban landscape identity than the East and West Precincts.
- The design of the Crossing should be coordinated with the design of Gateway Nodes. Landscape elements should be repeated and emphasized in the Crossing.
- Accent planting, in the form of small flowering trees, may be used at the four corner areas to distinguish this specific road intersection. These planting should be placed to ensure standard sight line clearances.
- Where appropriate, poles with banners should be incorporated into the landscaped corners of the Crossing. Banners should be colourful and may display a logo for the commercial area, or announce special events (Figure X4.26).
- Feature paving bands should be incorporated at all road crossings.



- Streetscape will consist of trees in oversize in-ground planters set in the concrete sidewalk leaving generous space (more than 3 metres) for pedestrian movement between the storefronts and the trees.
- Annuals may be accommodated in planters above ground.
- Tree grates and guards will be used to protect the trees above ground.
- Streetscape furnishing will be in accordance with standards established herein,



Figure X4.25: Built form addressing the major intersection

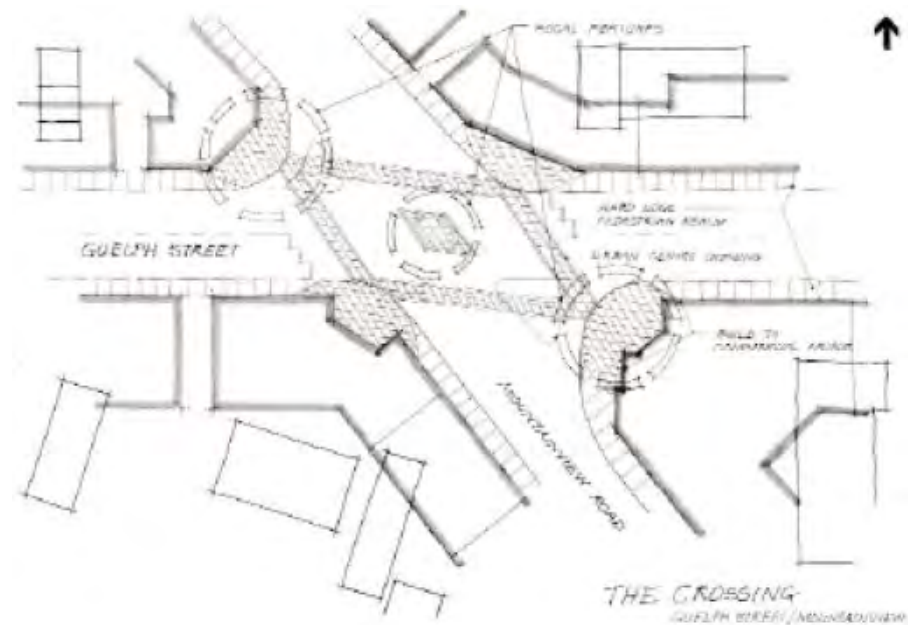


Figure X4.26: Enhanced streetscape at the Crossing

#### X4.3.2.3 GATEWAY FEATURES

- To create a sense of arrival into the commercial area, the entrance points should be visually defined by the use of special streetscape features such as flags, banners, public art, accent planting, floral displays and feature structures. The entrance points occur in the vicinity of the following two intersections:
  - a) Guelph Street and Maple Avenue; and,
  - b) Guelph Street and Hall Road.
- The West Gateway feature should be a masonry structure on two sides of the street (Figure X4.27). Although distinct from the downtown, the element should capture similar materials that are emblematic of Halton Hills. For instance the masonry could be sandblasted concrete to match street furnishings.
- A symbol of the Guelph Street Corridor should be incorporated into the West Gateway feature to distinguish it from the Georgetown Downtown District. The back of the structure should indicate that the traveler is leaving the District.
- The East Gateway feature should be a masonry structure on two sides of the street to match the north gateway and to define its limits.
- Where possible, the logo for the Town of Halton Hills should be incorporated on street furnishings.
- Where a gateway structure is to be located within the jurisdiction of the Ministry of Transportation (MTO) or the Regional/local municipality, design and construction of the structure will require the approval of the respective authority.

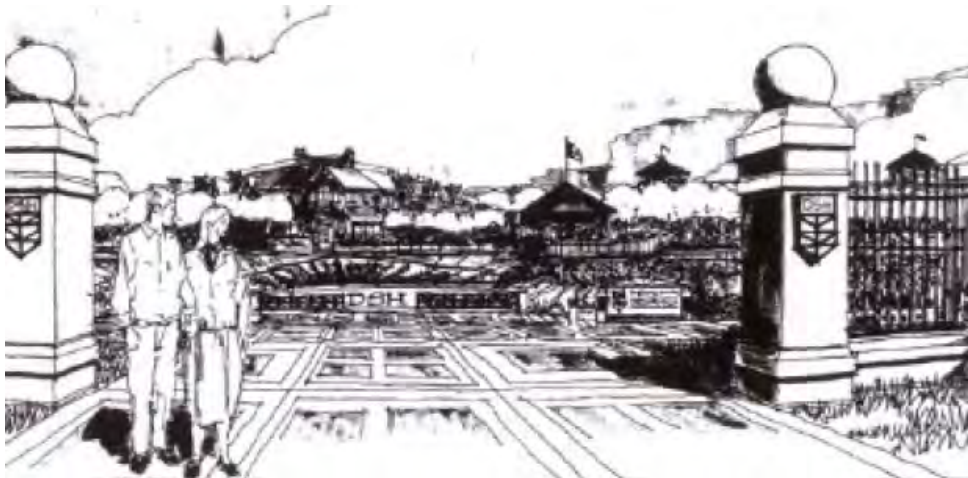


Figure X4.27: West Gateway at Domion Gardens Park

- For pedestrian comfort, safety and visual interest, pedestrian amenities should be provided at the Gateway Nodes, at the Crossing, and along Guelph Street. These pedestrian amenities include paving, lighting, street furnishings and possible future transit shelters.
- Design detailing and scale of pedestrian amenities should be coordinated with the overall design of the streetscape.
- Feature paving may be used along roads at Gateway Nodes, future transit shelters and at the main intersection, such as Guelph Street and Mountainview Road North, and Guelph Street and Sinclair Avenue.
- Feature paving should be consistent in colour and pattern of use throughout the District.
- Pedestrian crosswalks at the Crossing and at the Gateway Nodes should receive special paving treatment to emphasize their significance. The crosswalks may consist of concrete paving or a mixture of concrete and unit pavers on a concrete base.
- To facilitate safe crossing over wide paved streets (such as Guelph Street and Mountainview Road North), barrier-free pedestrian safety islands should be provided (where possible) at the median as a safe refuge for pedestrians.
- Without inhibiting left turn trading, island medians should be installed at signaled intersections and obvious pedestrian crossings.
- Continuous, hard surface paving treatments should be provided between sidewalks and future transit stops and shelters.
- Pedestrian scale lighting should be post-mounted along Guelph Street when there is sufficient pedestrian activity to warrant.
- Benches and waste receptacles should be provided at future transit shelters, at the Gateway Areas and at the Crossing. Furnishings, such as newspaper boxes and mailboxes, should be grouped together with other site amenities.
- Seating areas should be provided in locations adjacent to a barrier-free path of travel. Street furniture design should consider the ease of use by persons with disabilities.
- To enhance the sense of identity of the East and West Precincts, a medallion with the District's name or logo may be designed for each precinct and incorporated into site furnishings.
- A 'banner' program should be developed for the areas along Guelph Street to create a sense of identity and also to identify seasonal events. These banners may differentiate the East and West Precincts to emphasize the two areas and their distinct characteristics, but should be designed so that it doesn't compete with, or detract from banners designed for the Downtown or GO Station Districts within the Town (Figure X4.28).
- The use of these banners should be emphasized at the Gateway Nodes and at the Crossing.



- Curb ramps should be provided at all street corners to ensure barrier-free accessibility. To facilitate barrier-free design, it is recommended that the lip height of curb ramps in the current Town's engineering standards be reviewed.

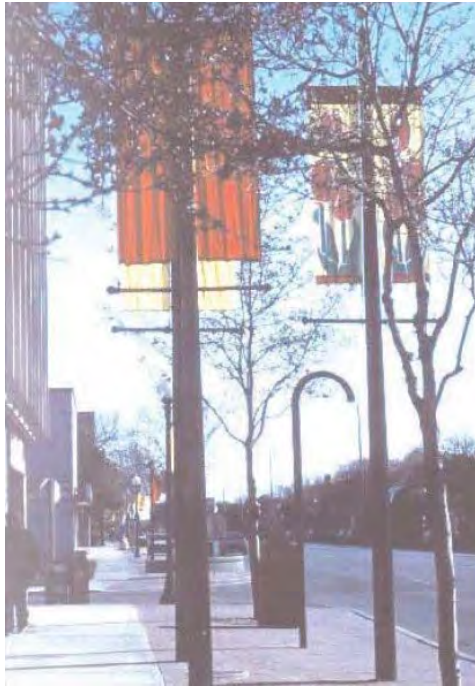


Figure X4.28: Banner as a streetscape element

#### X4.3.3

#### LANDSCAPE

- Landowners should be encouraged to provide landscape buffer or islands within parking lots on private lands.
- Planting strips with minimum widths of 4.5 metres should be provided between the street line and parking lots.
- On sites where buildings are to be located close to the front lot line and no parking in front of the building is proposed, landscape planting will be required in order to frame the building (Figure X4.29). Alternatively, the construction of low metal and masonry fences to define the site will be considered and it should be coordinated with the overall streetscape design.
- Low fencing, combined with low shrubs, maybe used along property lines to screen/protect parked vehicles, and also to provide visual interest (Figure X4.30).
- Decorative fences higher than 1.2 metres, or continuous planting of tall shrubs and coniferous trees, which obscure pedestrian views, should be discouraged.
- Landscape treatments should be provided within medians at major access driveways in the form of high branching street trees and low shrub planting, which do not obstruct vehicular views.

- Where a commercial property abuts a residential zone, a planting strip of 3.0 metres minimum width should be provided for vegetation, fencing, and snow storage.
- Where commercial properties have parking lots which are adjacent to each other, a shared landscape buffer with a minimum total of 3.0 metres should be provided between lots, except where shared driveway access occurs.
- Shrubs should cover a minimum of 50% of the planting strip.
- Screen planting, where provided, should cover a minimum of 50% of the planting strip area and should form a continuous visual screen between properties.
- All landscape plans should be reviewed by Halton Hills staff to ensure that the proposed landscape design is compatible with adjoining land holdings.



Figure X4.29: Enhanced landscaping along sidewalk



Figure X4.30: Low fencing and planting to screen parking lot

#### X4.3.4 OPEN SPACE

##### X4.3.4.1 DOMINION GARDENS PARK

- Dominion Gardens Park, located at the north east corner of Maple Avenue and Guelph Street, is part of the West Gateway and its completion will counter the strip commercial development of the south side of the street.
- Masonry structures in the park near the corner will contribute a visual connection and strength to the North Gateway.

##### X4.3.4.2 MOLD-MASTERS SPORTSPLEX

- A master plan for the entire site should be prepared to meet these guidelines and to suggest a positive integration to this District.

#### X4.3.5 ACCESS AND CIRCULATION

##### X4.3.5.1 VEHICULAR ACCESS AND CIRCULATION

- An access management plan for the District should be initiated by the Town, in conjunction with the Region, to establish an overall strategy for the future consolidation of access points to individual properties (Figure X4.31).
- Where possible, access to parking areas should consolidate access points through shared driveways to minimize disruption of the public sidewalk and to facilitate traffic movement to public roadways.
- Access into, and circulation within, an individual site should provide safe and well-defined routes.
- Proper lighting, landscaping, and pedestrian amenities along the circulation routes will enhance overall site appearance, promote public safety, and encourage the use of public transit in the future. Reference should be made to the relevant Regional or municipal documents in regards to safety.
- Landscaped traffic islands should be used to delineate the main drive aisles, subdivide large parking areas into smaller 'courts', and improve edge conditions between the public street and adjoining properties (Figure X4.32).

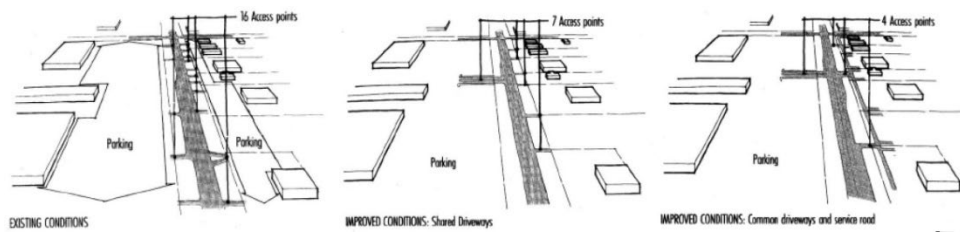


Figure X4.31: Street with and without access management

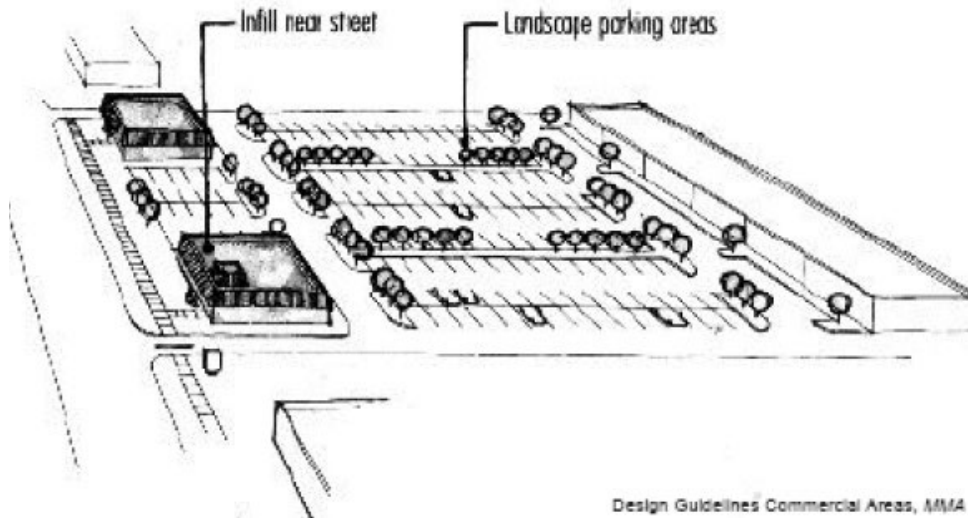


Figure X4.32: Landscaping in parking lot

#### X4.3.5.2 PEDESTRIAN AND CYCLING ROUTES

- Direct, well-lit, and barrier-free pedestrian walkways should be provided between parking facilities and main building entrances, and between future transit stops/shelters and buildings.
- Where possible, conflict between pedestrian and vehicular crossings should be minimized.
- Where possible, pedestrian routes should have priority over vehicular routes where crossings occur, and should be marked accordingly.
- Paving materials other than asphalt should be considered for pedestrian walkways, and they should be continuous across driving aisles (Figure X4.33).
- Lighting levels along pedestrian routes should provide appropriate, continuous illumination.
- Pedestrian-scale light fixtures should be considered either in conjunction with vehicular lighting or as freestanding elements.
- Where appropriate, pedestrian access routes to the adjacent properties should be encouraged.
- To facilitate pedestrian gathering and access, a minimum 3.0 metre walkway and forecourt space should be provided between parking areas and main building entrance(s).

- The minimum pedestrian walkway width should be 2.0 metres.
- Multi-use pathways exist around the Dominion Gardens Park and at the south end of the District leading from the Credit River Valley to the intersection of Delrex Boulevard and Guelph Street. These are the only contacts with the trails system in the Guelph Street Corridor. The crossings of Guelph Street at Delrex and Maple should indicate by signs and striping that the trails cross there. Construct pedestrian refuge medians on Guelph Street at these locations.
- The design of pedestrian and cycling routes should have regard to the relevant Regional or municipal documents in regard to safety.



Figure X4.33: Paved pedestrian walkway across driving aisles

## X4.3.6 PARKING AND SERVICE AREAS

### X4.3.6.1 GRADE PARKING

- Where surface parking for new development is necessary, parking lots should be dispersed around the building. Where possible, parking should be discouraged in a yard fronting on a street.
- In the East and West Precincts, large areas of unbroken surface parking should be avoided.
- Landscaping and landscaped traffic islands (a minimum of 3.0 metres wide) should be used to define smaller parking courts and improve conditions, particularly at the street edge and the principal building façades. If a landscaped traffic island incorporates a pedestrian walkway, the width of the island should be a minimum of 4.5 metres (Figure X4.32).

- In the East Precinct, a maximum of two rows of parking and a drive aisle should occur in a yard fronting on a street. The remainder of the setback depth should be used for landscaping and the forecourt of the building (Figure X4.34).
- For lands on the south side of Guelph Street in the West Precinct, on-site parking at the rear should be strongly encouraged and non-rush hour on-street parking may be considered subject to the implementation of Georgetown Bypass (Figure X4.35).
- For lands on the north side of Guelph Street in the West Precinct, a maximum of two rows of parking and a drive aisle should occur in a yard fronting on a street. The remainder of the setback depth should be used for landscaping and the forecourt of the building (Figure X4.34).
- Each row of parking spaces should terminate with a landscaped island (minimum 3.0 metres wide).
- Parking areas should not infringe on the forecourt of buildings facing the public street (Figure X4.36). A minimum 2.5 metre strip of land in front of the building face should be maintained for pedestrian circulation, paved entry courts, and landscaping.
- Where feasible, shared parking facilities between multiple uses on a single site should be encouraged to optimize daily usage.
- Light standards in the parking lot should be provided both at the pedestrian level along walkways and at higher levels for security and vehicular circulation.
- Pedestrian walkways should be developed between parking lots and the public street. These walkways should be landscaped and lighted to encourage convenient, safe, and frequent public use and should be barrier-free by allowing for unobstructed views from one end to the other.
- Designated barrier-free parking spaces should be located close to the barrier-free building entrances and clearly identified by signs or markings.
- Adequate bike racks should be provided in the parking lot and/or near the building entrances.
- Parking areas adjacent to residential properties should provide landscape planting and opaque fencing to buffer the visual and acoustic impacts of the parking area.
- Lighting for parking areas should not spill over onto adjacent residential properties.
- Parking areas, which abut streets, should be buffered by using attractive low walls, decorative fences, planters, low shrubs, or rows small street trees (Figure X4.37, Figure X4.38).

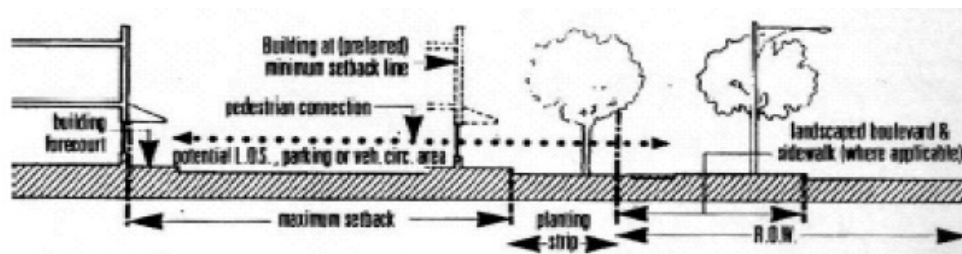








Figure X4.38: Landscape screen for smaller parking lot

#### X4.3.6.2 STRUCTURED PARKING

- Where feasible, parking should be incorporated into proposed buildings in above grade or underground parking structures.
- Above grade parking structures should be screened at grade and designed as part of the overall building. The design guidelines for building details also apply to parking structures.
- Retail or personal service uses should be encouraged at the street level of parking structures, where feasible.
- When existing from the parking structure, vehicles should have a clear view of the pedestrian zone and every entry into and exit from a parking structure should have a minimum stacking area equal to one car length of 6.0 metres behind the property line. The gradient of this stacking area should be less than 5%.
- Parking structures should be well lit to ensure pedestrian safety and provide for ease of identification.
- The exteriors of pedestrian entrances to parking structures should be illuminated with overhead lighting.

#### X4.3.6.3 SERVICE AND LOADING AREAS

- Service (including exterior garbage storage and recycling containers) and loading areas should be located away from the primary building face, preferably in the rear or side yard of the building and integrated within the building, where possible.
- Appropriate buffering and screening of service and loading areas should be incorporated using landscaping or built screens, or a combination of both. Any building materials used to create screens should relate to those of the primary building exterior.



- Adjacent properties should be considered in the treatment of service and loading areas and should be suitable buffered or screened.
- Where appropriate, service area access driveways should be separate from access driveways related to the primary building face.

#### X4.3.7 OPEN STORAGE AND OUTDOOR DISPLAY AREAS

- All inventory merchandise should be placed in the designated open storage area located in side and rear yards.
- The designated open storage area should be screened from view of adjacent streets and properties.
- Tall, opaque fences and/or walls, preferable enhanced with landscaping treatment and integrated with primary building in material and colour, should be used as the screening devices for the open storage area.
- Outdoor display area refers to permanent outside merchandise display area for car dealerships, garden nurseries, home improvement centres and fresh produce markets.
- All merchandise for outdoor display and marketing purposes should be set back from the property line and placed within the designated outdoor display area.
- Along Guelph Street, the frontage of the designated outdoor display area should not exceed the primary building frontage facing the same street. This will result in a balanced contribution to the street edge among the elements of building façade, display area, and landscaping area.
- The designated outdoor display area should be clearly defined on all sides with landscaping and/or architectural edge treatment (Figure X4.39). Landscaping edge treatment may include the combination of low walls, formal hedges, dense planting strips, or architecturally detailed bollards (with heavy duty chain). Architectural treatments may also include independent colonnades or an extension of the building façade.
- The architectural and landscape edge treatments for the outdoor display areas should be integrated by design and also serve as a theft prevention barrier. Chain link and/or barbed wire fences should be prohibited in front yards.
- Lighting for open storage and outdoor display areas should not spill over onto adjacent residential properties.



Figure X4.39: Enhanced landscaping for outdoor display area

#### X4.3.8

#### SIGNAGE

- Street signs within the public right-of-way should consider a unified, thematic, appearance that contributes to the urban character of the Guelph Street Corridor (Figure X4.40).
- For clear orientation, larger-sized street signs should be incorporated within the medians at the Gateway Nodes and the Crossing.
- Signs marking the Gateway Nodes should reinforce a sense of arrival to the District. Gateway sign designs and locations should be clearly visible and relate to both the scale of vehicles and pedestrians.
- Freestanding signs addressing private development are encouraged to be consolidated for each development and located within the property line parallel to the street frontage and mounted in a landscaped setting. Sign materials should be consistent with the building design (Figure X4.41)
- Freestanding signs should be located so that they do not obstruct vehicular and pedestrian views.
- Building identification signs should be incorporated as an integral, coordinated element of the principal building façade and should be compatible with the building design in scale, colour, and materials.
- A single primary identification sign should be allowed per business frontage. Where there is a wider building frontage, an additional, small secondary sign may be allowed.
- Secondary storey business signs should be smaller than the first storey signs.
- Multiple tenant retail developments should encourage a thematic sign design to contribute to a unified building presence.

- Street address signs should be clearly visible from the street curb, located at grade on the principal building façade.
- If alternate signage design demonstrates that the intent of this subsection is achieved, flexibility in the application of these guidelines should be considered.

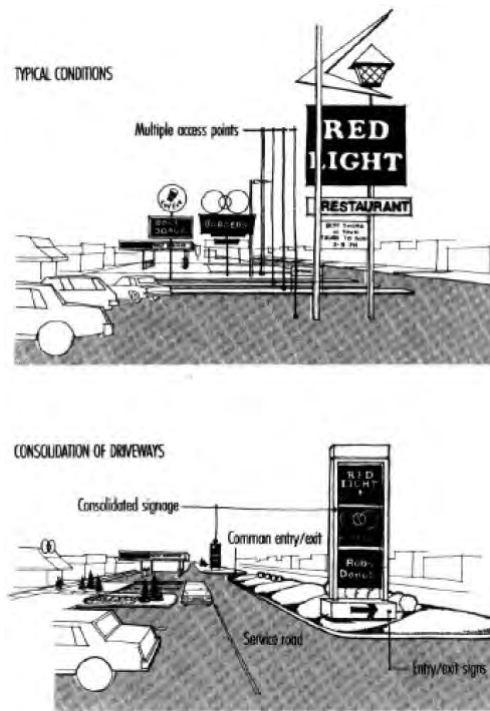


Figure X4.40: Signage consolidation



Figure X4.41: Signage in landscaping

#### X4.4 ACTON DOWNTOWN DISTRICT AND BEYOND

##### X4.4.1 BUILT FORM

##### X4.4.1.1 HISTORICAL PRESERVATION

- Where a new or infill development is to be constructed adjacent to a building of architectural or historical significance, consideration with regard to built form, scale, detailing, colour, and materials should be given to ensure harmony with the features of the existing building (Figure X4.42).

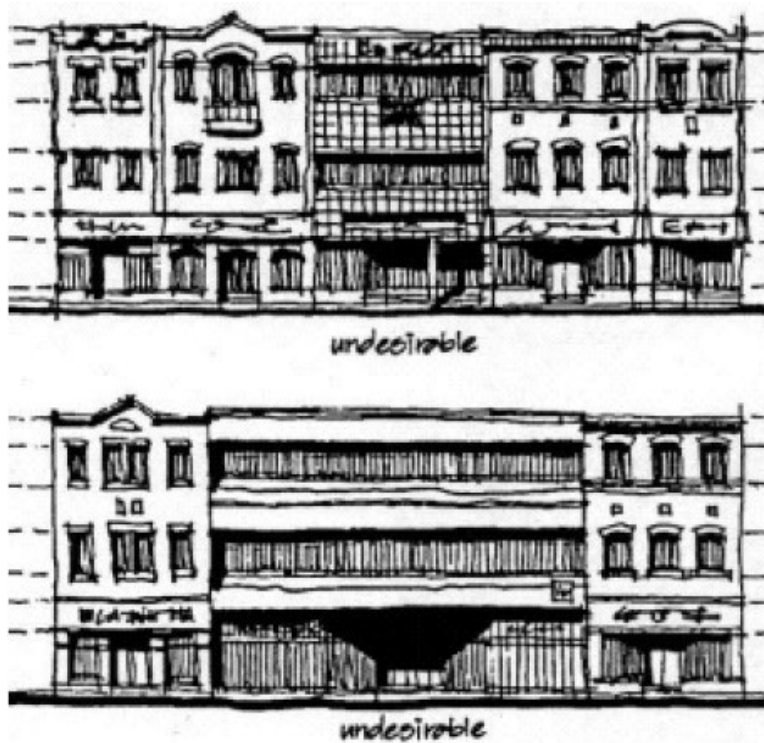


Figure X4.42: Undesirable fenestration design for infill development

#### X4.4.1.2 BUILDING SETBACKS

- Front building walls will be encouraged to align with the front walls of adjacent buildings in order to maintain a consistent building mass along the street line (Figure X4.43).
- In the Mill Street Precinct, front building wall of new infill buildings should occupy majority of the lot frontage to minimize the unwarranted gap in between buildings. It ensures the creation of a consistent street façade (Figure X4.44). Traditional urban form is established by small scale attached buildings with no open space between them. The only exception is well-lit public passageways to the rear parking area.
- For corner lots in the Mill Street Precinct, the design of the side wall of the building should be extended along the exterior side lot line to at least 60% of the lot depth and this side wall should be located within 3m of the side lot line.
- Where breaks in the street building wall occur, the continuation of the linear street form with the use of site elements such as street trees, planters, gates or low walls, will be encouraged.
- The provision of landscaped open space and amenity areas such as entrance plazas, forecourts and outdoor cafes, will be encouraged in locations where buildings are not built to the street line. These open space areas can provide added interest to the street environment.
- Within the Outer Core of the Mill Street Precinct, minimum front yard setbacks for residential buildings should be the established front building line.

- Within the Young and Queen Street Precinct, encourage building placement at the minimum setback required by the Zoning By-law, where feasible, thus avoiding large setbacks and substantial areas of surface parking in the front yard.
- For existing large commercial establishments within the Queen Street Precinct, the placement of out buildings closer to the street is encouraged.
- In the Queen Street Precinct, building placement should respect the minimum front yard setback as prescribed in a new comprehensive Zoning By-law, but also should be restricted in locating to more than 25.0 metres back from the front lot line.
- Building treatments, such as unenclosed entrance structures, colonnades, overhangs, canopies, awnings, and landscape elements should be allowed to encroach into the front yard.



Figure X4.43: Mill Street streetscape



Figure X4.44: Façade defines the street edge

#### X4.4.1.3 BUILDING HEIGHT AND MASSING

- Maintain a consistent 'base building' height of three storeys within the Inner Core. This base height should be equivalent to the heights of façades of historical and significant buildings (Figure X4.45).
- Within the Mill Street Precinct, any portion of the building greater than the specified base building height should be set back or tapered above the base.
- Maximum building heights should not exceed the height limits established in a new comprehensive Zoning By-law.
- Continue the finer grain of architectural massing established in the Mill Street Precinct (Inner Core) by discouraging new buildings with monolithic façades.
- Minimize the use of space between buildings along the Mill Street Precinct (Inner Core) to ensure the creation of a consistent street façade.
- Within the Downtown District, maintain heights of up to three storeys for proposed low density residential buildings, for storeys for medium density residential buildings, and six storeys for high density residential buildings, except as otherwise specified in the Zoning By-law.
- In low density residential areas, proposed building massing and height should be related to the adjacent buildings to enhance the existing neighbourhood character.
- Multiple unit buildings should be designed to retain the general massing character of large detached homes. The exterior walls should not have the appearance of an overly massive building block.
- Row houses should not exceed widths of eight attached units.
- Individual units should have clear identities through varying architectural treatment (i.e. verifying façades, columns, etc.), but should remain consistent in overall theme.
- End units should receive special design treatment to take advantage of their position.
- At Gateway Nodes, taller non-habitable structures (such as clock tower, decorative masts with banners, and/or specialized lighting) should be encouraged in order to contribute to a stronger area identity through inclusion of vertical emphasis at the intersections.
- At the Four Corners, new developments are encouraged to have taller structures, built to the property line to define the intersection (Figure X4.46).
- Within the Queen Street Precinct, the bulk or mass of buildings, in conjunction with setback lines should, where possible, establish a consistent urban streetscape to assist in defining a recognizable street edge.
- Building massing should be designed to minimize impacts on adjacent properties, with respect to privacy, sunlight access, and wind tunneling.



- Building massing should include variations in the building envelope to allow for elements such as entrances, forecourts, or other specific building articulations. These elements should provide interest and detail when viewed from the public street, particularly at corner locations.



Figure X4.45: Consistent building height for the infills



Figure X4.46: Built form defines the intersection

#### X4.4.1.4 BUILDING FRONTAGE

- If alternate site planning demonstrates that the intent of this subsection is achieved, flexibility in the application of these guidelines should be considered. More substantial building façade fronting the public street at the minimum setback line is encouraged, in order to define a more urban street edge.
- Within the Mill Street Precinct, for new infill building, a minimum of 75% of the main building wall should be built at the minimum front yard setback.

- In the Young Street Precinct, for new infill buildings, a minimum of 60% of the main building wall should be built at the minimum front yard setback.
- The following guidelines relate to built form within the Queen Street Precinct:
- To encourage the presence of building façade at the street edge, the minimum building frontage should be in proportion to the front yard setback.
- At the minimum front yard setback, a minimum building frontage should be 50% of lot frontage. As the front yard setback increases, the minimum building frontage should increase proportionately up to 60% at the 25-metre front yard setback.
- For the purpose of building frontage calculation, the length of all major building components (primary building façades or extensions of the façade as decorative screens, and colonnades) could be applied cumulatively as a sum of the total building frontage.
- If alternate site planning demonstrates that the intent of this subsection is achieved, flexibility in the application of these guidelines should be considered.

#### X4.4.1.5

#### BUILDING ELEMENTS

- Accentuate the corners of significant street intersections with the use of prominent building mass, addressing the intersection as well as the street fronts, and building features to provide landmarks within the Town's street system.
- New buildings should be designed and sited to preserve and enhance special street views and significant buildings.
- Discourage the use of blank walls, which are visually prominent. Where such façades exist, encourage the use of murals or landscaping to screen walls and provide visual interest.
- In the Mill Street Precinct, spacing, proportions, dimensions, quantity and general patterning of windows in new buildings should be sympathetic and similar to buildings of historical and architectural significance, and to adjust existing buildings. Rows of windows should be used to define each floor level.
- Continue the rhythm of storefront windows in new development along Mill street as such store fronts traditionally have large window openings to expose goods within shops to passers-by and encourage retail activity.
- Encourage the provision of retail or personal service space along pedestrian routes for pedestrian interest and security.
- The roof forms of proposed buildings should complement those of existing adjacent buildings.
- Parapet walls of complementary design and materials should be used to screen rooftop equipment where flat roofs are proposed.
- Materials and colours for proposed buildings, and for additions or renovations to existing structures, should complement and be compatible with adjacent buildings.



- Encourage the use of fine architectural detailing, compatible and in harmony with adjacent buildings, in the building façades, especially around windows and at the building's cornice lines. A variety of architectural details add richness and interest to façades, and builds on the tradition of craftsmanship established in heritage buildings of Downtown Acton.
- Doorways/entrances to buildings, especially commercial buildings, should be prominent and visible, and emphasized through entrance canopies, awnings, and other architectural elements.
- Pedestrian entrances to both single and multiple residential buildings should be spatially and architecturally prominent and welcoming.
- In multi-tenant development, the use of multiple pedestrian entrances into the building at street level is encouraged.
- The use of canopies should be encouraged to add colour and interest to streetscapes, and they provide weather protection for pedestrians and for merchant's goods. Canopies also reinforce the identity of individual retail and service outlets and may be used to accent entrances to residential buildings located on main streets.
- Canopies should have a minimum 1 metre in depth from the building face and a minimum clearance above sidewalks of 2.4 metres. The encroachment of any canopy is subject to municipal by-law.
- Retail area display openings should be provided along pedestrian routes to maintain interest and improve security.
- In the Queen Street Precinct, windows facing the street frontage, whether display windows for retail use or windows for office space should be large, occupying a minimum of 30% of the street elevation between the ceiling and floor at grade.
- In the Queen Street Precinct, clear glass is preferred for glazing, but some tinting based on functional considerations (building orientation, etc.) is acceptable. Reflective (mirror) glass at grade should not be used and its use at upper level windows/curtain wall should be examined for architectural merit.
- Garages for low density residential dwellings should not form the prominent feature of the dwelling façade. Garages should be well integrated into the overall house design.
- Garages and parking structures should not be located in front of medium and high residential buildings.
- Where steps and ramps provide access, these should be architecturally integrated with the building.

#### X4.4.2

#### STREETSCAPE

##### X4.4.2.1

##### DOWNTOWN DISTRICT (INNER AND OUTER CORE)

- Maintain established sidewalk widths, or increase sidewalk widths where possible (Figure X4.47, Figure X4.48), along retail commercial streets of the Mill Street Precinct. Ease of pedestrian movement is an important factor in retail areas.
- To create the Gateway Node at the Mill Street/CN Rail intersection, any expansion to the 'Olde Hide House' site and the adjacent land should be encouraged to locate close to Mill Street.
- A continuous and consistent streetscape should be developed in the Downtown District by selecting suitable streetscape elements, such as street trees, sidewalk materials, and lighting, and repeating these elements along these main streets.
- Paving materials and patterns for the Main/Mill Street Intersection (the 'Four Corners') should be selected to complement the historic architectural treatment, and these sidewalk patterns and materials should be continued along Mill Street. The same treatment can be used to accent areas along side streets of the Inner Core.
- Street tree species that are low maintenance, pollution- and salt-tolerant, and have good, year-round appearance should be selected.
- Trees should be planted at grade wherever possible for ease of pedestrian movement, and to reduce visual clutter.
- Trees should be planted at grade where there will be greater than 3m clearances from the trunk of the tree to the nearest wall. In narrow street cross-sections, pyramidal or narrowly growing plants suitable to these situations should be used. Where trees are unable to be planted, consideration should be given to the incorporation of hanging planters to provide vegetative relief and colour.
- In the Outer Core, existing sidewalk widths and materials should be maintained and sidewalks should be added to at least one side of the streets where they don't exist, and where possible within the planned street right-of-way.

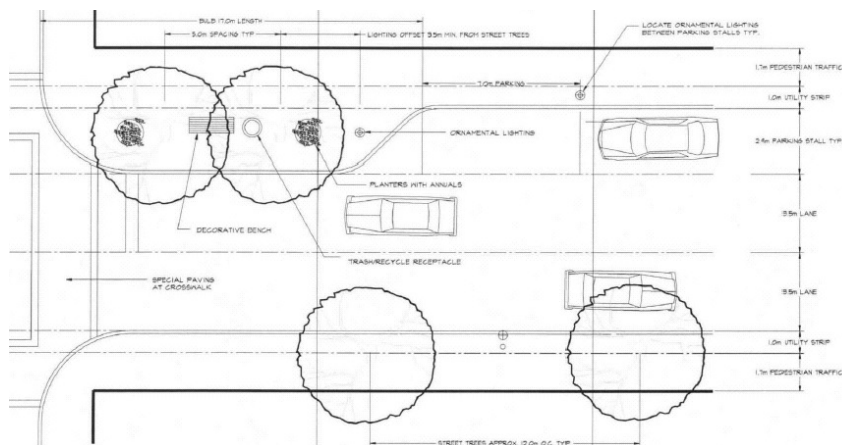


Figure X4.47: Plan view – Parking on one side of Mill Street

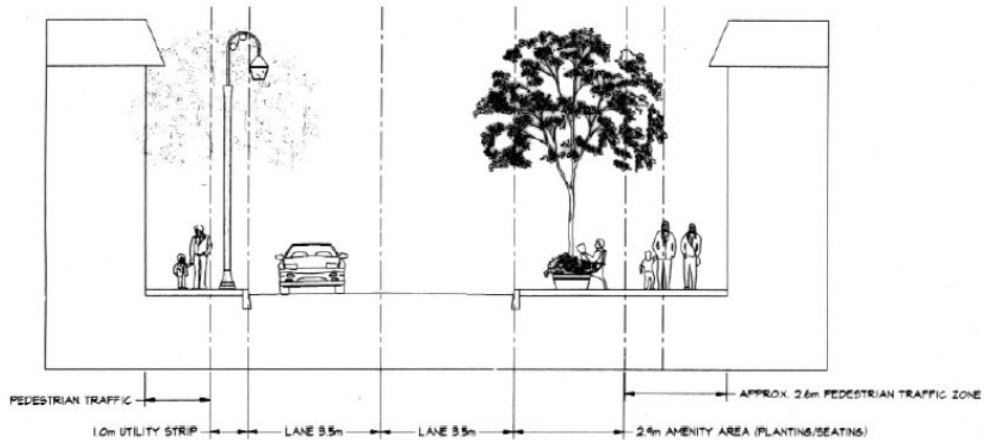


Figure X4.48: Cross section at the bulb out – Parking on one side of Mill Street

#### X4.4.2.2 YOUNG AND QUEEN STREET PRECINCT

- Sidewalks should continue across driveways to indicate pedestrian priority.
- Consistent sidewalk widths and materials should be maintained.
- Landscape strips that are less than 1.0 metre in width and that are adjacent to a hard edge, such as a wall or curb, should be paved with feature paving material.
- Landscape strips between 1.0 and 2.5 metres in width, should consist of grass, ground cover and deciduous/coniferous trees, wherever possible.
- Street trees should be incorporated, to provide a rhythm to the street, to create shade and protection and to provide a dynamic green element.
- Standard shade trees planted within landscape strips should be spaced as recommended above. Small trees (such as Chanticlear Pear, Shubert Cherry, or Japanese Tree Lilac) should be used beneath hydro wires or in narrow (less than 2m) landscape strips, and planted in 10-metre intervals.
- High branching tree species (such as Honey Locust and maiden hair), or columnar trees should be used to allow views through to private commercial development.
- Two or three tree species should be used consistently within landscape strips along Young Street and Queen Street, and should be low maintenance and pollution- and salt-tolerant. Groupings of the same species of tree can be used provided that there are occasional plantings of other species to prevent monocultures and their associated hazards.
- Where tree planting occurs in a paving area, use of tree grates and guards should be provided. All landscape plans should be reviewed by Halton Hills staff to ensure that the proposed landscape design is compatible with adjoining land holdings.

- Trees that are planted on the street line should become the maintenance responsibility of the Town or the Region.

#### X4.4.2.3 GATEWAY FEATURES

- To give a sense of arrival, the entrance points to Downtown Acton should be visually defined by the use of special streetscape features, such as flags, banners, accent planting or feature structures. The entrance points occur at the following intersections:
  - a) Main Street adjacent to Prospect Park;
  - b) Main Street and Brock Street; and
  - c) Mill Street/CN Rail intersection.
- Landmark features should be durable and in keeping with the scale of surroundings and should have regard to the travel speed of the passers-by (i.e. motorists and pedestrians).
- Where a gateway structure is to be located within the jurisdiction of the Ministry of Transportation (MTO) or the Regional/local municipality, design and construction of the structure will require the approval of the respective authority.

#### X4.4.2.4 STREET FURNISHINGS

- As existing sidewalks in the Mill Street Precinct of the Downtown Acton District are fairly narrow (Figure X4.49), planters within sidewalk areas should be discouraged. Planters may be used along the street line and within the development parcel to define the street edge,
- Hanging flower baskets, as a viable alternative, can be incorporated with pedestrian lampposts as an attractive streetscape element (Figure X4.50).
- Wherever sidewalk width permits, streetscape elements such as benches, waste receptacle, newspaper boxes and bus shelters should be grouped together in nodal areas, and located in conjunction with street trees and lights.
- The provision of bicycle racks should be encouraged within major new developments.
- Design detailing and scale of street amenities should be coordinated with the overall streetscape design and architectural character (Figure X4.51).
- Pedestrian streetlights should be located along intensively used areas.
- Light fixtures should be chosen to integrate into the overall existing streetscape design and architectural character. Fixtures and poles should provide visual interest and pedestrian scale during daylight hours.
- A style for Municipal Street Signage should be explored and established, which reflects the traditional image of the Downtown Core.

- Feature paving may be used along roads at Gateway Nodes, future transit shelters and at the main intersection, such as the Mill Street/Young Street/Queen Street intersection.
- Feature paving should be consistent in colour and pattern of use throughout.
- Pedestrian crosswalks at the Four Corners and at the Gateway Nodes should receive special paving treatment to emphasize their significance. The crosswalks may consist of concrete paving or a mixture of concrete and unit pavers on a concrete base.
- To facilitate safe crossing over wide paved streets (such as Queen Street), barrier-free pedestrian safety islands should be provided, where possible, at the median as a safe resting place for pedestrians.
- To enhance the sense of identity of the Mill, Young and Queen Street Precinct, a medallion with the District's name or logo may be designed for each precinct and incorporated into site furnishings and banners.
- The use of these banners should be emphasized at the Gateway Nodes and the Four Corners.



Figure X4.49: Narrow sidewalk on Mill St



Figure X4.50: Example of hanging basket



Figure X4.51: Street furnishings

#### X4.4.3

#### LANDSCAPE

- In the Queen Street Precinct, planting strips with minimum widths of 3.0 metres should be provided between the street line and parking lots. Buffer areas should be planted with a combination of grass or other salt tolerant ground cover, low shrubs and deciduous trees.
- In the Queen Street Precinct, on sites where buildings are to be located within 12 metres of the front lot line and no parking in front of the building is proposed, landscape planting will be required in order to frame the building. Alternatively, the construction of low metal and masonry fences to define the site will be considered and it should be coordinated with the overall streetscape design.
- Low fencing, combined with low shrubs, maybe used along property lines to protect parked vehicles in car dealerships, to contain litter from fast food outlets, and also to provide visual interest. Fencing design should be coordinated with the overall streetscape design.
- Where a commercial property abuts a residential zone, a planting strip of 3.0 metres minimum width should be provided for vegetation, fencing, and snow storage.
- Where commercial properties have parking lots which are adjacent to each other, a shared landscape buffer with a minimum total of 3.0 metres should be provided between lots, except where shared driveway access occurs.
- Shrubs should cover a minimum of 50% of the planting strip.
- Screen planting, where provided, should cover a minimum of 50% of the planting strip area and should form a continuous visual screen between properties.
- Landowners should be encouraged to provide landscape buffer or islands within parking lots on private lands.
- Planting strips with minimum widths of 3.0 metres should be provided between the street line and parking lots.
- Low fencing, combined with low shrubs, may be used along property lines to protect parked vehicles in parking lots, and also to provide visual interest.

- Decorative fences higher than 1.2 metres, or continuous planting of tall shrubs and coniferous trees, which obscure pedestrian views, should be discouraged.
- Landscape treatments should be provided within medians at major access driveways in the form of high branching street trees and low shrub planting, which do not obstruct vehicular views.
- Screen planting, where provided, should cover a minimum of 50% of the planting strip area and should form a continuous visual screen between properties.



#### X4.4.4

#### OPEN SPACE

##### X4.4.4.1

#### PROSPECT PARK (PROSPECT PARK/BOVIS CHANNEL TO ACTON DOWNTOWN)

- An event directory on Main Street to attract patrons to the park should be provided.
- A downtown Acton directory in the park to attract patrons to the downtown should be provided.
- Street furnishings in Prospect Park that reflect the street furnishings of Downtown Acton should be provided.
- Visual impact of the dam and waterfall as a landmark feature at the west terminus of the vista down Mill Street should be promoted (Figure X4.52).



Figure X4.52: View to Fairy Lake from Mill Street West

##### X4.4.4.2

#### FAIRY LAKE

- Fairy Lake should become one of the anchors that draw patrons to the area. Develop windows to the Lake from the downtown (Figure X4.53).
- Downtown should be married with the Lake through a unified signage design and streetscape program.



- A structure or water feature (Figure X4.54) should be developed in Fairy Lake that can be experienced from the Main Street realm. The 'window streets' to the Lake from Main Street should frame views and encourage patrons of the downtown to experience one of Acton's finest assets.



Figure X4.53: Prospect Park and Fairy Lake as the west terminus



Figure X4.54: Example of water feature

#### X4.4.4.3

#### OLDE HIDE HOUSE PARKETTE

- Redevelopment or expansion of the Olde Hide House provides an opportunity to use the proposed buildings or expansion to frame an urban square or parkette at the Mill Street frontage (Figure X4.55). It is intended to form as part of the east gateway at the Mill Street Precinct. A partnership could be formed between the commercial owners and the Town to develop the parkette as a visual terminus at the east end of the downtown (Figure X4.56).

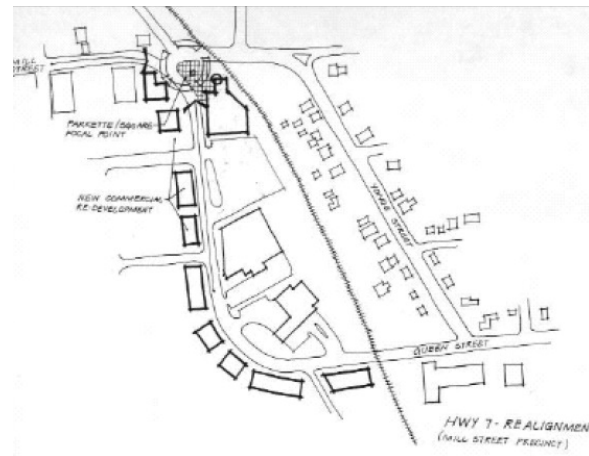


Figure X4.55: Tall element as focal point in urban square Figure X4.56: Highway 7 realignment via Eastern Avenue as an alternative

#### X4.4.5

#### ACCESS AND CIRCULATION

##### X4.4.5.1

##### VEHICULAR ACCESS

- An access management plan for the study area should be initiated by the Town, in conjunction with the Region, to establish an overall strategy for future consolidation of access points to individual properties.
- Encourage an alternate alignment of Highway 7 from Queen Street, Eastern Avenue to Mill Street, subject to the possible extension of Queen Street to Highway 25 (Figure X4.56).
- Where possible, access to parking areas should consolidate access points through shared driveways to minimize disruption of the public sidewalk and to facilitate traffic movement to public roadways.
- Access into, and circulation within, an individual site should provide safe and well-defined routes. Proper lighting, landscaping, and pedestrian amenities along the circulation routes will enhance overall site appearance, promote public safety, and encourage the use of public transit in the future. Reference should be made to the relevant Regional or municipal documents in regard to safety.

- Landscaped traffic islands should be used to delineate the main drive aisles in the off-street parking lot, where possible, subdivide large parking areas into smaller ‘courts’, and improve edge conditions between the public street and adjoining properties.

#### X4.4.5.2 PEDESTRIAN AND CYCLING ROUTES

- Formalized cycling connection between the potential cycling route along Church Street and Downtown Mill Street should be encouraged.
- Formalized pedestrian connection between the proposed multi-use pathway north of the Downtown along creek corridor and Downtown Mill Street should be encouraged.
- The multi-use pathways should be connected through the north part of the downtown along creek corridors.
- The pathways should be connected to the downtown by means of signs and provision of gateways that reflect the street furnishings of Downtown Acton.
- Lighting levels along pedestrian routes should provide appropriate, continuous illumination.
- Pedestrian-scale light fixtures should be considered either in conjunction with vehicular lighting or as freestanding elements.
- Where appropriate, pedestrian access routes to the adjacent properties should be encouraged.
- To facilitate pedestrian gathering and access, a minimum 3.0-metre walkway and forecourt space should be provided between parking areas and main building entrance(s).
- The design of pedestrian and cycling routes should have regard to the relevant Regional or municipal documents in regard to safety.

#### X4.4.6 PARKING AND SERVICE AREAS

##### X4.4.6.1 ON-STREET PARKING

- On-street parking on the north side of Mill Street should be promoted to maximize the width of the pedestrian realm.
- Along Mill Street, pedestrian bulb-outs at intersections to decrease the road crossing and to define the parking stall ends should be considered.
- On-street parking on two sides of Main Street should be encouraged.
- On-street parking on all side streets should be allowed, where pavement width permits.

##### X4.4.6.2 OFF-STREET PARKING

#### X4.4.6.2.1 MILL STREET PRECINCT

- Large expanses of unattractive grade-level parking lots, especially those located in front of buildings, should be discouraged.
- Vehicular entrances to parking structures and areas should be located on side streets, and entrances along Mill Street should be avoided, wherever possible.
- Where surface parking for new development is necessary, parking lots should be located at the rear of buildings.
- Where possible, encourage the use of small, interconnected parking areas distributed throughout the site.

#### X4.4.6.2.2 QUEEN STREET PRECINCT

- Where surface parking for new development is necessary, parking lots should be dispersed around the building. Where possible, parking should be discouraged in a yard fronting on a street.
- Large areas of unbroken surface parking should be avoided. Landscaping and landscaped traffic islands (minimum 3.0 metres wide) should be used to define smaller parking courts and improve conditions, particularly at the street edge and principal building façades. If a landscaped traffic island incorporates a pedestrian walkway, the width of the island should be a minimum of 4.5 metres.
- A maximum of two rows of parking and a drive aisle should occur in a yard fronting on a street. The remainder of the setback depth should be used for landscaping and the forecourt of the building.
- Parking areas should not infringe on the forecourt of buildings facing the public street. A minimum 3.0 metre strip of land in front of the building face should be maintained for pedestrian circulation, paved entry courts, and landscaping.

#### X4.4.6.2.3 YOUNG STREET PRECINCT

- Rear yard parking should be encouraged.

#### X4.4.6.2.4 GENERAL

- Each row of parking spaces should terminate, where possible with a landscaped island (minimum 3.0 metres wide).
- Where feasible, shared parking facilities between multiple uses on a single site should be used to encourage optimal daily usage.
- Light standards in the parking lot should be provided both at the pedestrian level along walkways, and at higher levels for security and vehicular circulation.

- Pedestrian walkways should be landscaped and lit to encourage convenient, safe, and frequent public use.
- Pedestrian walkways should also be barrier-free by allowing for unobstructed views from one end to the other.
- Designated barrier-free parking spaces should be located close to the barrier-free building entrances and clearly identified by signs or markings.
- Adequate bike racks should be provided in the parking lot and/or near the building entrances.
- Parking areas adjacent to residential properties should provide landscape planting and opaque fencing to buffer the visual and acoustic impacts of the parking area.
- Lighting for parking areas should not spill over onto adjacent residential properties.
- Parking areas, which abut streets, should be buffered by using attractive low walls, decorative fences, planters, low shrubs, or rows of small street trees (Figure X4.57).
- The pedestrian zone should continue through all driveways into parking areas and facilities.
- The number of vehicular site entrances should be kept to a minimum.



Figure X4.57: Edge treatment to screen parking area

#### X4.4.6.3 STRUCTURED PARKING

- Above-grade parking structures should be screened at grade and designed as part of the overall building. The design guidelines for building details also apply to parking structures.
- Retail or personal service uses should be encouraged at the street level of parking structures, where feasible.

- When exiting from the parking structure, vehicles should have a clear view of the pedestrian zone. Every entry into and exit from the parking structure should have a minimum stacking area equal to one car length or 6.0 metres behind the property line. The gradient of this stacking area should be less than 5%.
- Parking structures should be well lit to ensure pedestrian safety and provide for ease of identification.
- The exteriors of pedestrian entrances to parking structures should be illuminated with overhead lighting.

#### X4.4.6.4 PARKING STRATEGY PLAN

- Update of the Parking Study (1990) for the Acton Downtown District is recommended. It should be initiated by the local BIA, in conjunction with the Town and the Region (where appropriate), to have an updated analysis of the parking supply and demand and to further develop solutions and implementation strategy for the potential parking shortage, if any.

#### X4.4.6.5 SERVICE AND LOADING AREAS

- Loading, garbage facilities, and other service functions should be screened from the public street. Location of these facilities within or at the rear of buildings is encouraged.
- Co-ordinate the entrances to service areas with those of parking areas to reduce the amount of vehicular interruptions along pedestrian streets.
- The location of service area entrances along Mill Street should be discouraged. It is preferable to locate these off side streets, or rear service laneways.
- Service (including exterior garbage storage and recycling containers) and loading areas should be located away from the primary building face, preferable to the rear of the building.
- Appropriate buffering and screening of service and loading areas should be incorporated using landscaping or built screens, or a combination of both. Any building materials used to create screens should relate to those of the primary building exterior.
- Adjacent properties should be considered in the treatment of service and loading areas and should be suitable buffered or screened.
- Where appropriate, service area access driveways should be separate from access driveways related to the primary building face.

#### X4.4.7 OPEN STORAGE AND OUTDOOR DISPLAY AREAS

- No outdoor storage or outdoor display areas should be allowed in the Mill Street and Young Street Precincts.

#### X4.4.7.1

#### QUEEN STREET PRECINCT

- All inventory merchandise should be placed in the designated open storage area located inside all rear yards.
- The designated open storage should be screened from view of adjacent streets and properties.
- Tall, opaque fences and/or walls, preferably enhanced with landscaping treatment and integrated with primary building in material and colour, should be used as the screening devices for the open storage area.
- All merchandise for outdoor display and marketing purposes should be set back from the property line and placed within the designated outdoor display area.
- Along Queen Street, the frontage of the designated outdoor display area should not exceed the primary building frontage facing the same street. This will result in a balanced contribution to the street edge among the elements of building façade, display area, and landscaping area.
- The designated outdoor display area should be clearly defined on all sides with landscaping and/or architectural edge treatment. Landscaping edge treatment may include the combination of low walls, formal hedges, dense planting strips, or architecturally detailed bollards (with heavy duty chain). Architectural treatments may also include independent colonnades or an extension of the building façade.
- The architectural and landscape edge treatments for the outdoor display areas should be integrated by design and also serve as a theft prevention barrier. Chain link and/or barbed wire fences should be prohibited in front yards.
- Lighting for open storage and outdoor display areas should not spill over onto adjacent residential properties.

#### X4.4.8

#### SIGNAGE

- Street signs within the public right-of-way should consider a unified, thematic appearance that contributes to the urban character of Acton.
- Signs marking the Gateway Nodes should reinforce a sense of arrival to the commercial area. Gateway sign designs and locations should be clearly visible and relate to both the scale of vehicles and pedestrians.
- Freestanding signs addressing private development are encouraged to be consolidated for each development and located within the property line parallel to the street frontage and mounted in a landscaped setting. Sign materials should be consistent with the building design.
- Freestanding signs should be located so that they do not obstruct vehicular and pedestrian views.
- Building identification signs should be incorporated as an integral, coordinated element of the principal building façade and should be compatible with the building design in scale, colour, and materials.



- In the Queen Street Precinct, a single primary identification sign should be allowed per business frontage. Where there is a wider building frontage, an additional, smaller secondary sign may be allowed.
- Second storey business signs should be smaller than the first storey signs.
- Multiple tenant retail developments should encourage a thematic sign design to contribute to a unified building presence.
- Street address signs should be clearly visible from the street curb, located at grade on the principal building façade.
- If alternate signage design demonstrates that the intent of this subsection is achieved, flexibility in the application of these guidelines should be considered.

#### X4.4.9 RAIL CORRIDOR

##### X4.4.9.1 NOISE, VIBRATION AND SAFETY MITIGATION MEASURES

- A berm, at approximately 2.5 metres in height, should be constructed as a safety element along the railway.
- A noise attenuation wall should be provided above the safety berm to protect residential properties.
- Design specifications of the safety berm, noise fence and vibration measure and development setback should conform to the CN's Secondary Mainline requirements.
- The noise wall should have a unique design character. Planting should be provided along the whole north side of the noise attenuation wall to soften the visual impact of the wall, while a low maintenance planting material (such as fescues and wildflowers) should be planted to the rail side to contribute to the ambience of the GO station.
- Tree planting in these areas should be generous, providing high level screening. Shrubs should be planted according to their size to create masses and continuous planting beds on 50% of the land surface of the buffers. Grass maintenance should be minimized or eliminated from areas within the jurisdiction of GO transit.



Much of Norval's character is defined by the heritage architecture displayed by its buildings, as well as their scale and their relationship to each other. An important objective of these guidelines is to ensure new development, both public and private, is compatible with the heritage character of the area.

The following guidelines are designed to protect the unique hamlet character prevalent in the community while still allowing room for individual architectural impression. They are to be used by residents and developers in the design of projects as well as Town staff in reviewing zoning by-law amendments and site plan control applications.

Figure X5.1 identifies “gateways” and “focal points” as important elements towards achieving urban design objectives. Reference should be made to Figure X5.1 when reading the following guidelines.



Figure X5.1: Urban design elements of the Hamlet of Norval.

### X5.1

#### DESIGN GUIDELINE 1 - SUSTAINABILITY

A sustainable community is diverse, well connected and walkable and characterized by a strong respect for local identity and natural heritage.

#### X5.1.1

#### SUSTAINABLE BUILDING DESIGN

- New buildings are encouraged to reduce the energy consumption of building and site systems (HVAC, hot water, lighting) through the use of appropriate mechanical and construction technology (natural cooling, light recovery, passive solar design, etc.);
- Water use reduction technologies are encouraged, including water-efficient appliances, such as aerators, low-flow shower heads, dual-flush toilets, front-loading washers, waterless urinals and high-efficiency dishwashers;
- Waste water technologies, such as rain barrels or cisterns, are encouraged in new buildings to collect and filter rain water to be recycled for non-potable domestic uses; and,
- All buildings should have conveniently located waste management facilities to support the separation of waste into different streams according to reuse and recycling regulation (i.e. compost, paper, plastics, etc.).

#### X5.1.2

#### SUSTAINABLE MATERIAL CHOICE

- Where possible, construction materials should be recycled to reduce the environmental impacts of extracting and manufacturing new materials. If there are no salvageable materials available, efforts should be made to purchase materials from demolition sales, salvage contractors and used materials dealers;
- New construction materials should be locally sourced to reduce the impacts of transportation. Canadian products are generally designed to withstand our climate; and,
- Construction materials should be durable and should be considerate of life cycle costing to avoid premature replacement.

#### X5.1.3

#### SUSTAINABLE LANDSCAPING

- Recommended landscape materials should include non-invasive, non-cultivar species that are native to the area to support sustainable urban biodiversity. Species that are generally drought resistant and require minimal maintenance are encouraged;
- Landscape design should incorporate strategies to minimize water consumption (i.e. use of mulches and compost, alternatives to grass and rainwater collection systems);
- Existing significant trees, tree stands and vegetation should be protected and incorporated into site design where feasible;
- New trees should be planted to contribute to the Town's existing tree canopy. Where the rhythm of existing trees is interrupted, new trees should be planted as infill to maintain a continuous canopy; and,
- Site design should minimize impervious hard surfaces. The surface area of driveways and parking areas should be as small as possible within allowable standards, and porous pavement and landscaped areas should be maximized.

Note: For additional information related to sustainability, please refer to the Halton Hills Green Development Standards.



Figure X5.2: Solar panels, bioswales, permeable pavement and adaptive landscaping are encouraged in Norval.

## X5.2

### DESIGN GUIDELINE 2 – GATEWAYS

Gateways are important features that symbolically define Norval, create identity and help people find their way around. As the basis of first impressions, gateways play an important role in the economic development of a community.

Development at gateways should therefore help shape this sense of identity by the nature and quality of landscaping, built form and urban design features such as public art.

- Gateway features should include taller architectural elements, which symbolize entry into the Hamlet, including customized lighting fixtures, landscape features (i.e. tree plantings), flags, special signage, and banners;
- Where buildings are located at gateways, they should prominently address streets through enhanced design treatments, such as taller corner elements, enhanced entry treatments and large expanses of glazing;
- Buildings within Gateway designations must incorporate streetscape improvements that will serve to provide shelter to pedestrians at these major intersections. For example, this can be accomplished by setting back the building and developing a public space that incorporates, landscaping, public art, lighting and/or shelters;

- In order to strengthen the gateway image, different public art features such as sculptures, fountains, and decorative walls with murals may be used. Gateways should be given first priority when considering the placement of public art features;
- Gateway areas should be centres for information and wayfinding, directing visitors to key destinations within the Hamlet (i.e. Norval Park, Credit River, L.M. Montgomery Garden); and,
- Special ambient lighting and light effects may be employed in order to strengthen the “nightscape” of gateways.



Figure X5.3: Gateways should be defined by neighbourhood features, such as parks and landmark buildings.

### X5.3 DESIGN GUIDELINE 3 - FOCAL POINTS

Built forms at focal points should be of the highest architectural quality to make it memorable and recognizable.

The intersection of Guelph St. (Highway 7) and Adamson St. (Winston Churchill Boulevard) is an important focal point of the community that provides a visual anchor, a point of interest and open space opportunities with access to the Credit River. The strengthening of this intersection through good urban design will improve its attractiveness in terms of tourism as well as general commerce and community pride.

- Built form and development (especially commercial, cultural or entertainment on the ground floor space) should be oriented toward public streets and spaces in order to make public space vibrant and pedestrian-friendly;



- On corner sites, a similar level of architectural expression should be used on both frontages, including enhanced facade articulation, a significant amount of glazing (minimum of 40%), signage etc.;
- Main building entrances of corner buildings should be close to the corner. Alternatively, access can be provided from both frontages;
- Corners should be accentuated by developing to the maximum height limits or with the positioning of entrances;
- Frame prominent intersections by locating new buildings or other structures closer to the street and defining the intersection space; and,
- Activities that attract or generate pedestrian traffic such as cafes, retail functions and public art are highly desirable at the focal points.



Figure X5.4: New buildings at focal points should be vibrant and pedestrian friendly.

#### X5.4 DESIGN GUIDELINE 4 - CROSSWALKS

Clearly marked crosswalks provide safe opportunities for pedestrian movement.

- Crosswalks should be a minimum of 3.0 metres wide, with visible edge bands to identify them as a continuation of the pedestrian surface;
- In certain locations, crosswalks may be completely raised to accommodate easier access for seniors and children as well as to serve as a traffic calming measure;
- Additional mid-block pedestrian signals and courtesy crossings with specialized markings and signage should be considered within the Community Core; and,

- Within the focal area, opportunities to integrate public art into crosswalk design should be explored.



Figure X5.5: The design of crosswalks may be used to celebrate the history of Norval through artistic interpretations.

## X5.5

### DESIGN GUIDELINE 5 - SURFACE PARKING AND SITE CIRCULATION

Surface parking lots should be appropriately located, well landscaped and visually divided into smaller courts to minimize their impact on the public streetscape.

Access to parking should be from rear lanes and side streets. Shared entrances to parking areas and loading areas (for 2 or more properties) are encouraged, in order to minimize the number of curb-cuts and to minimize impact on street and pedestrian traffic.

- Parking lots be oriented to the rear or side lot areas of the building site. Parking lots should not dominate the frontage of streets. Where parking areas must be situated adjacent to the sidewalk, a landscaped buffer should be located between parked vehicles and the sidewalk. This buffer should be located within the private realm to not reduce the total sidewalk width;
- Planting strips, landscaped traffic islands and/ or paving articulation should be used to define smaller parking 'courts' that provide pedestrian walkways, improve edge conditions and minimize the aesthetic impact of surface parking; Surface parking lots should be appropriately located, well landscaped and visually divided into smaller courts to minimize their impact on the public streetscape.

- The amount of landscaping should be proportionate to the overall parking lot size, but generally, 1 tree for every 8 parking spaces is recommended. These can be clustered to facilitate snow clearing;
- Pedestrian-scaled lighting should be provided along pathways to enhance visibility and security; and,
- Where appropriate, permeable paving should be considered to promote drainage. Well-drained snow storage areas should be provided or snow should be removed off-site.

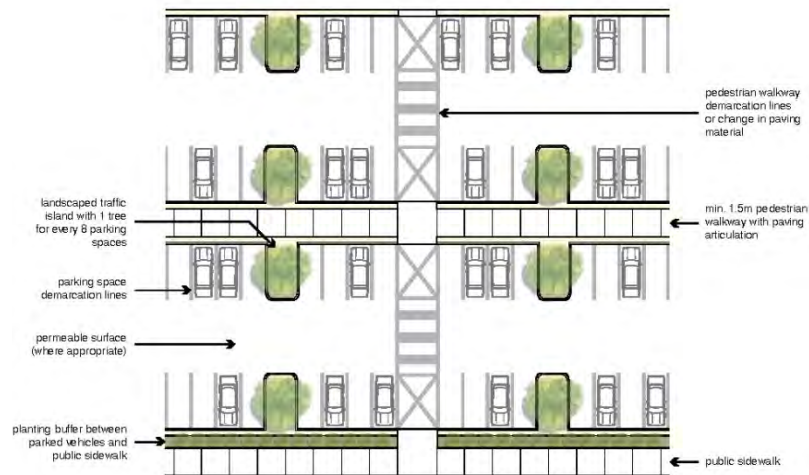


Figure X5.6: The illustration above demonstrates the key components used to mitigate the negative impacts of surface parking lots.



Figure X5.7: To minimize their impact on the public realm, surface parking lots should be located behind buildings, and should be designed as more intimate parking 'courts'.

Create boulevards that combine safe, unobstructed pedestrian travel routes with places to stop and socialize.

Boulevards are the interface between private and public spaces and other circulation systems. They represent one of the most important elements of the streetscape performing functional, aesthetic and social roles in the daily lives of the residents of Norval.

- Street and boulevard grades must be designed not to obstruct the movement of pedestrians;
- Within the Community Core, new development should maintain continuous sidewalks on both sides of Highway 7 and Adamson Street. Outside of the Community Core, sidewalks on one side of the street is acceptable.
- At points of congestion (focal points with outdoor patios, entrances to civic or entertainment buildings), sidewalks should be at least 1.85 metres wide to accommodate increased numbers of pedestrians and activities;
- Wherever possible, sidewalks should have elements for weather protection (permanent porticos or arcades, fixed or removable hanging canopies, permanent or temporary awnings);
- On-street parking may be situated within bulbouts, where appropriate. The bump-outs should be landscaped with mature street trees or low level ground cover;
- On-street parking should not conflict with bicycle/ pedestrian travel;
- Street furniture and landscaping should be located between the sidewalk and vehicle traffic. This zone may contain landscaped areas with site furnishings and infrastructure facilities such as benches, bicycle locks, transit stops, and utilities;
- In the long-term, where sidewalks are located directly adjacent to vehicle travel lanes, they should be relocated to accommodate the above furniture and landscape zone (please refer to Page 15 for example street sections).
- Street trees should be offset a minimum of 1.5 metres from the curb to accommodate snow storage, large vehicle movements and to minimize salt damage. Trees should be spaced consistently at 6.0-9.0 metre intervals;
- A transition zone between the sidewalk and the building or property line provides a dedicated area for window shopping, spill-out retail, building entrances, street furniture and signage. In areas not bounded by buildings, this transition zone may include landscaping or a second row of trees;
- Sidewalk surface textures should be designed to be sufficiently smooth and flat to accommodate safe and pleasant use for people of all ages and abilities. Similarly, surface textures should provide additional visual qualities through use of different colors, shapes or materials;



- Pedestrian-scaled boulevard lighting should be provided in areas of high use, such as focal points, and where the future tree canopy may impact light levels; and,
- All boulevards should be designed to accommodate snow storage.



Figure X5.8: Street furniture and landscaping should be located between the sidewalk and vehicle traffic and parking. Street furniture and bicycle parking are encouraged throughout Norval, particularly at key destinations (i.e. Community Core) and should be situated where they will not interfere with pedestrian circulation.

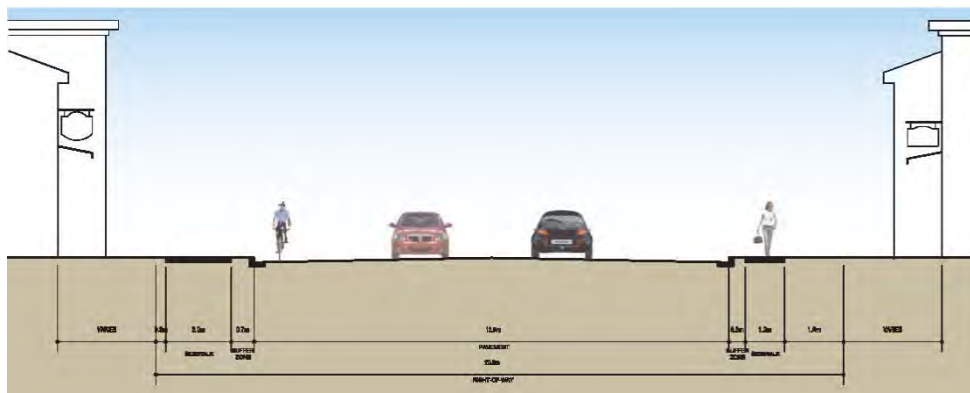


Figure X5.9: Existing Condition: Narrow boulevard widths limit the potential for enhanced boulevard treatment.

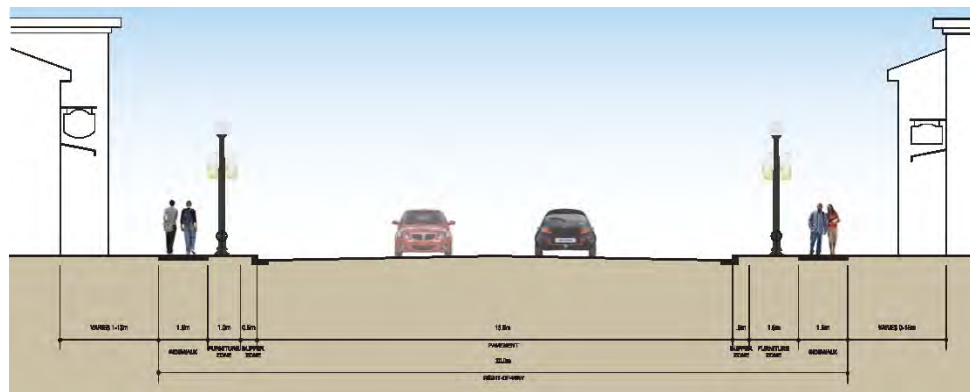


Figure X5.10: Alternative 1 (Short-term) - The relocation and narrowing of the sidewalk provides a buffer between pedestrian and vehicle traffic and accommodates basic pedestrian amenities (i.e. lighting and banners).

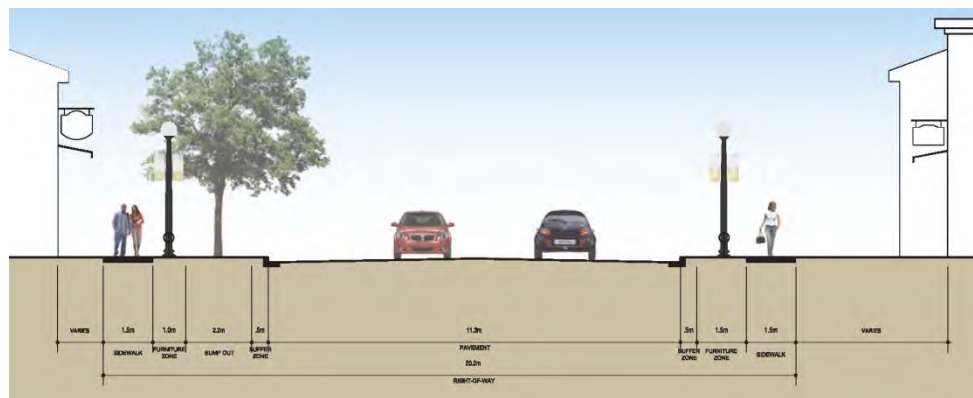


Figure X5.11: Alternative 2 - Providing bulbouts along Guelph Street provides the opportunity for on-street parking and the addition of street trees, seating, and pedestrian amenities.

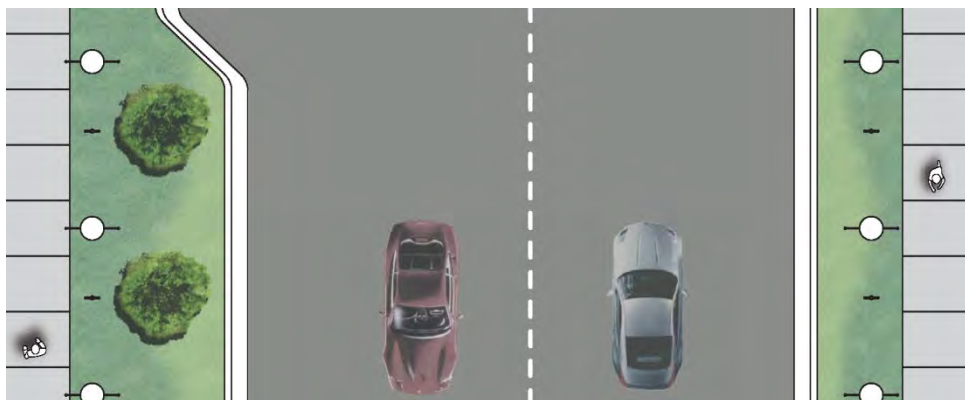


Figure X5.12: Landscaped bulbouts provide space for additional boulevard treatments.



Figure X5.13: Alternative 3 (Long-term) - As part of the reconstruction of the Guelph Street right-of-way (in combination with the Norval interchange), a wider right-of-way width could allow the proper boulevard width to accommodate street trees, lighting and banners, and pedestrian amenities on both sides.

## X5.7 DESIGN GUIDELINE 7 - LIGHTING FIXTURES

Well-placed lighting standards create safe, active streetscapes.

Exterior lighting is an important and relatively inexpensive way to improve streetscapes and open spaces. This is particularly true during winter periods where daylight is reduced. Lighting, properly employed, attracts people, and provides safety and comfort.

- At gateways and focal areas, the Town Standard for decorative lighting should be applied to reinforce the cultural character of the Hamlet;
- In the Community Core where buildings are built to the edge of the sidewalk fixtures may be mounted directly on buildings;
- Alternatively, light fixtures should be placed regularly between sidewalk and curb cuts to allow unobstructed pedestrian movement;
- Lighting fixtures should be no more than 0.6 metres from the curb;
- Spacing of lighting fixtures should vary according to the intensity of pedestrian use. For the typical situation, spacing will be approximately 10 metres;



- An average luminary mounting height should be 3.6 metres (4.2 metre maximum);
- Downcast, pedestrian-scaled lighting enhances safety and visibility on streets. At gateways and focal points, lighting can be used to accent special features, such as heritage properties, landscaping and signage;
- Private property lighting should ensure safe and well lit pedestrian areas, including parking areas and building entrances; and,
- All pedestrian and street lighting should be “dark sky” friendly to minimize light pollution.



Figure X5.14: Appropriately designed lighting fixtures add to the façade’s aesthetic while offering nighttime visibility. Light standards throughout Norval should reflect the historic character of the community.

The Hamlet of Norval has a rich and diverse cultural heritage, which should be enhanced by appropriate signage.

In most cases, communities lack proper community identification, which can increase civic pride and awareness of the community. Such identification provides direction to visitors and through traffic and provides basic information on local history and architectural heritage.



Figure X5.15: Ensuring signage is high quality and is appropriately located within the Hamlet will enhance wayfinding, celebrate the history of Norval, and help to facilitate tourism. Portable signs (like “sandwich boards”) provide interest and add vitality to the streetscape, though their use should be regulated.

- Signs which obscure architectural elements on heritage building are not permitted;
- Traditional materials such as wood, brass, or bronze are the most appropriate materials for signage within the *Hamlet Community Core*. Some modern materials may be considered if they blend with the material of built structure upon which the sign is to be located;
- Utilization of symbols, and historic lettering is encouraged;
- In historical areas, in general, each building is permitted one ground sign, canopy sign, one projecting sign, one soffit sign, one wall sign and window sign;
- In a case where the operations of a store have expanded into a number of adjoining storefronts, individual repeating signboards should be considered for each of the original storefronts;
- Pedestrian scale signs (window, hanging, awning signs) should be small and positioned to interfere as little as possible with neighbouring signs;

- Free-standing signage should not interfere with pedestrian circulation or accessibility;
- Within the *Hamlet Community Core Area* animated, portable or roof signs are discouraged, as well as billboards and internally illuminated signs;
- The amount of information on signs should be limited - the shortest message has the greatest impact; and,
- Historical photographs may be used to establish the styles and types of signage appropriate to a building within its district during the era of its construction and early life and use these models for contemporary signs.

Note: Notwithstanding the above guidelines, all signage along Guelph Street/Highway 7 must conform to the Ministry of Transportation's Corridor Signage Policy.

## X5.9

### DESIGN GUIDELINE 9 - TRAILS

Provide alternative transportation options, and recreational and tourism opportunities through a well-connected trail network.

Trail development is an important component of providing non-vehicular access through the Hamlet of Norval and along the Credit River Valley. Trail development in the Norval Secondary Plan Area must be consistent with the Halton Hills Trails and Cycling Master Plan and the following policies.

- Trails within Norval should have minimum widths of 3.0-4.0 metres to accommodate pedestrians, bicyclists, and other types of recreational users;
- Trails should be designed, wherever possible, as separate linkages from other vehicular traffic;
- Development of trails that link existing natural areas, parks and open spaces, particularly along the Credit River Valley, should be encouraged;
- Design of trails should allow easy access for every user group. Surfaces of the trails should be carefully graded and finished to allow full accessibility except where such finishes have an impact on Greenlands areas. In these sensitive areas, the trails should be constructed of low impact materials that are porous and stable, such as crushed rock, wood chip paths or board walks;
- Design of trails should consider elements of public safety, avoiding creation of entrapment spots by non-transparent landscaping or through creation of walls or similar built features and should be designed to have frequent, clearly-marked exits to areas of high pedestrian and car traffic;
- Trails should be clearly marked with attractive way-finding (signage) systems;
- Trail development should explore opportunities to introduce environmental or cultural / historical learning experience and other educational experience (flora, fauna, local history) such as those that have developed at the Willow Park Ecology Centre; and,

- Trails should generally have lighting, except in Environmentally Sensitive Areas where light could negatively impact on natural habitat areas. The necessity for and level of lighting, as well as the type of light fixtures will depend on the size and character of the trail.



Figure X5.16: Trails should provide links throughout the Hamlet and Region with easy access for a range of users.

#### X5.10 DESIGN GUIDELINE 10 - BUILDING CHARACTER

Create an active, attractive public realm through buildings that have a distinct image and quality.

The Hamlet of Norval has a wide variety of building styles. These styles, while different, have a variety of similar elements that should be reflected through high quality building design that supports the unique character and scale of the Hamlet. Uniqueness, achieved through creative use of forms, details and colours should enhance pedestrian enjoyment along the street.





Figure X5.17: Typical heritage façade articulation elements in the Hamlet.

- New buildings, particularly in the Community Core, should reflect the scale, and common elements that define the heritage building character (please refer to the diagram on the right);
- Character should be achieved through creative and sensitive architectural design utilizing:
  - building silhouette;
  - spacing between buildings;
  - setbacks from street property line;
  - massing of building form;
  - location and treatment of entrances;
  - surface materials, textures and finishes;
  - shadow patterns from massing and decorative features;
  - style of architecture; and,
  - landscaping on the site;



- Existing buildings within the Hamlet reflect a variety of building styles, including Post-War American, smaller “cottage” character, and a more traditional Victorian style (predominantly found on the Hamlet’s heritage churches). These styles have a number of key elements that should be reflected in new development, including:
  - gabled roofs;
  - protruding eavestroughs;
  - façades with vertically oriented windows and a wide variety of wall detailing;
  - double-hung windows;
  - stone lintels;
  - columns;
  - bending and arches of same or contrasting colour;
  - ground arches;
  - wide front porches; and,
  - wood detailing, etc.;
- In addition, door lintels, window lintels and sills, window shutters, horizontal bands and cornices, different types of decorations (tiles or sculptural elements), light lamps, fences or balustrades should be taken into consideration during architectural design of new buildings in Norval;
- Set-backs should enhance the streetscape if they denote an important/public building or should create a well-defined public realm which is highly usable and pedestrian friendly; and,
- All front yard and side property lines in residential areas should be delineated with low hedges or similar plantings (rows of shrubs, linear flower beds). High, solid, fencing (over 2.0 metres) in front yards is strongly discouraged unless the property is adjacent to an industrial or commercial use that requires visual buffering.



Figure X5.18: If intensification occurs within the Commercial Core and new commercial buildings are developed, they should reflect the key elements that define the character of the existing buildings within

the Hamlet.



Figure X5.19: Buildings in the Community Core should reflect the heritage character of Norval. Window lintels and sills, window shutters, wood and stone detailing and façades with vertically oriented windows are important character components of residential buildings in the Hamlet.

#### X5.11 DESIGN GUIDELINE 11 - BUILDING HEIGHT

Create a strong street edge and a human scaled environment through appropriate built form, height and massing.

Building heights are regulated in the Official Plan and should not exceed 2 storeys. Within these restrictions, the following guidelines should be considered.



Figure X5.20: Buildings should be uniform height to create a defined street ceiling. Renovations and additions to existing buildings should not be more than 1/3 of the existing building volume. The illustrations above demonstrate an example of an appropriate addition that is consistent with the existing character of the building.

- Buildings should generally be of uniform height that does not vary more than 25 % from each other to define a street “ceiling”;
- Infill buildings abutting existing structures at the building line should generally match the adjacent building height, or provide a clear offset in height so as to maintain the visual integrity of the existing structure;
- Buildings abutting lower scale buildings should ensure a transition in scale. The location of windows, horizontal lines and cornices, gables and roofs can be used to scale and proportion buildings and create transitions;
- Additions and renovations to existing buildings should ensure a final building that reflects the height, scale and massing of adjacent buildings;
- Additions/renovations to existing buildings should not be greater than 1/3 of the total building volume, and should be limited to no more than 1 storey above the existing height of the building (to a maximum of 2 storeys);
- Within the Community Core, vertical additions should result in building heights no greater than 2 storeys (7.5 metres);
- The height of planned buildings should comply with the permitted number of stories allowed by these design guidelines. Since the Secondary Plan proposes mixed use development in the *Hamlet Community Core Area* it is advised that the approximate maximum allowed heights per individual storey be:
  - for commercial ground floors: 4.2m;
  - for office use second floors: 3.6m;
  - for standard residential floors: 3.3m; and,
  - for non-standard residential lofts (mansards, attics): 3.6m max, although all abovementioned heights may be doubled if the architectural concept proposes loft-type of space;
- Mechanical penthouses, clock towers or similar architectural features shall not be subject to these height restrictions, but their massing and proportions should be well integrated and in direct relation to the building;
- Building heights should be used as a tool for assuring a minimum of 5 hours of daily sunlight in the public realm. Building envelope and height should be derived from the sun angle (on the shortest day in the year - Dec. 21) desired for a particular part of the street; and,
- Building height should be utilized in conjunction with setback control in order to establish proper sun radiation to public spaces.

Finishing materials should be of a high quality and should extend to all sides of the building, including projections.

Building materials are one of the predominant factors which determine character and quality of the building exterior. Careful consideration of materials, especially with respect to colour and texture, will make a significant contribution to the overall streetscape.

- Extensive mixtures of different materials should be discouraged. Exterior materials should be limited to no more than two complimentary materials, to avoid cluttering or an overly complex appearance;
- Acceptable exterior materials are: solid brick, cut stone, stucco, wood shingles, wood siding, glass, ceramic tiles or adequate combinations of them;
- Use of building materials should complement each other and complement traditional materials in the Hamlet Community Core. Although, there are many new materials on the market, more natural, traditional materials (brick, stone, wood) are encouraged in Norval;
- Imitation materials are discouraged. Building materials should not be used to replicate other materials (i.e. shingles that resemble bricks, etc.); and,
- Contemporary materials, such as aluminum, steel panels, coloured glass, ceramic tiles, etc. may be considered for use in future developments in conjunction with traditional materials. They should be used as accents for no more than 30% of front facades.

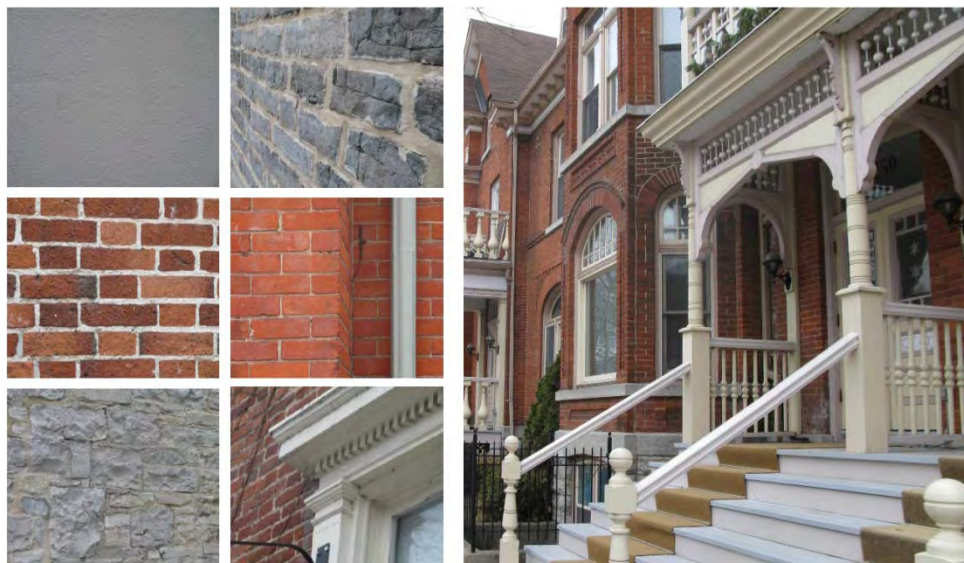


Figure X5.21: Preferred cladding materials include brick, stone, metal, glass, in-situ concrete, pre-cast concrete and stucco. Building materials should be complementary to the heritage and character of the Hamlet, especially in the Community Core.

## X5.13 DESIGN GUIDELINE 13 - RESIDENTIAL BUILDINGS

The built form, height and massing of residential buildings should be sensitive to adjoining areas, and the Hamlet of Norval as a whole.

### X5.13.1 GENERAL

- The mass and scale of residential buildings should be sensitive to adjoining areas. Currently, the majority of residential dwellings in Norval are between 1 and 2 storeys and future buildings should respect this 2 storey height limit;
- Dwellings should be oriented towards streets and/ or open spaces to provide a sense of enclosure and enhanced safety through “eyes on the street”;
- Primary building facades, particularly those which face streets, parks, and open spaces, should exhibit increased architectural detailing and generous amount of window openings to give attention to the prominence of these building faces and encourage strong visual connections between the private dwelling and public street; and,
- Flanking façades should have a design and materials standard equal to the front façade through the use of wrap-around porches, sun rooms, bay windows and side entrances.

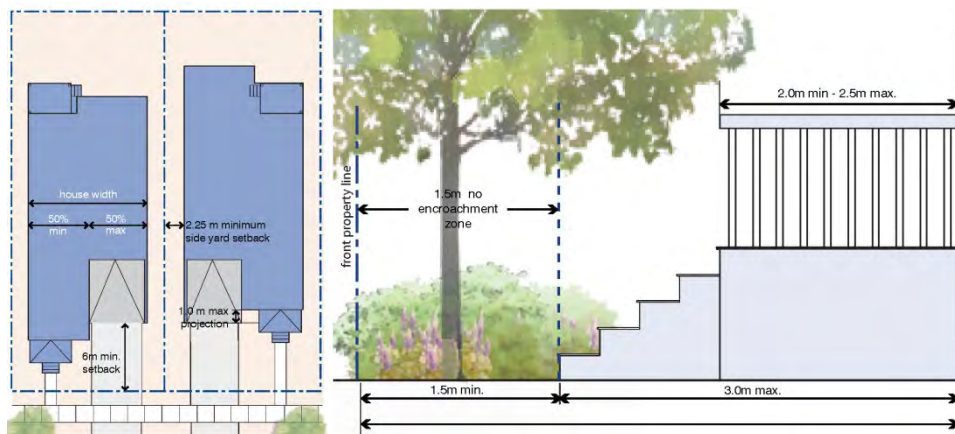


Figure X5.22

### X5.13.2 SINGLE DETACHED DWELLINGS

- A range of front yard setbacks currently exists within Norval, and should continue to create a diversity of setbacks on the streetscape. However, front yard setbacks should range between 4.5 to 7.5 metres;
- Within the Community Core, front yard setbacks should range between 3.0 to 5.0 metres to create an appropriate transition between the public and private realm, while still maintaining a connection to the street;



- 1.5 metres of this minimum setback, from the front property line, should be a “no encroachment” zone. The remaining setback may contain uninhabitable building elements (e.g. porches, steps, roof elements, etc.);
- Where dwellings have a front yard garage, a minimum 6.0 metre setback is recommended between the front of the garage and the front property line to accommodate one vehicle without disrupting the sidewalk;
- Interior side yard setbacks should be a minimum of 2.25 metres (including roof overhangs), or 3.0 metres where a garage is accessed by a side-yard driveway. Exterior side yard setbacks should be a minimum of 4.5 metres;
- There should be a minimum rear yard setback of 7.5m measured either to the rear property line or, in instances where a garage is present in the rear yard, to the face of the garage which is closest to the residential dwelling;
- Garage design should be complementary in character and quality of detail to the principal dwelling. To ensure garages are not a dominant feature of the community, they should be no wider than one half the width of the house;
- The minimum depth for porches and decks should be 2.0 metres; and,
- The top of the front porch should not be higher than 1/2 a storey above grade.

#### X5.14

#### DESIGN GUIDELINE 14 - COMMERCIAL BUILDINGS

Commercial buildings should promote attractive, pedestrian-supportive streetscape with a variety of public amenities.

Commercial buildings should have a high quality of architectural design to enhance and activate the streetscape.

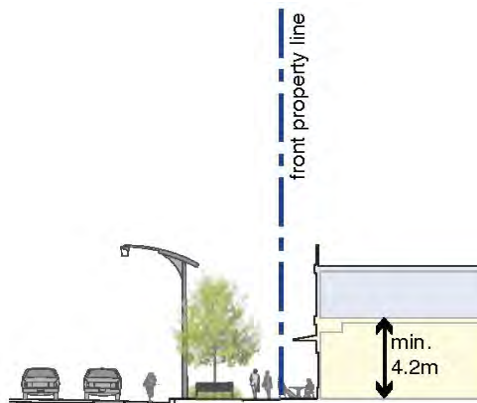


Figure X5.23: Buildings in the Community Core with retail at-grade should have a minimum ground floor height of 4.2m.

- Commercial-related features that detract from the streetscape, such as excessive or illuminated signage, are discouraged. Pedestrian amenities, including walkways that connect entries, seating landscaping and human scaled lighting are encouraged wherever possible;
- Buildings should incorporate architectural detail such as vestibules, recessed entrances and covered walkways, canopies and awnings to reflect the heritage character of the Hamlet, and to provide weather protection;
- Commercial buildings should not have blank façades facing the street. The facades should have distinct architectural detailing, including entrance and window design;
- A significant amount of the building frontage on the ground floor and at the building base levels should be glass to allow views of the indoor uses and create visual interest for pedestrians;
- Landmark buildings are encouraged at the intersection of Guelph Street and Adamson Street, and should reinforce the prominence of this location through appropriate massing, building projections, recesses at-grade, lower storey design and open space treatments;
- A 4.2 metre floor-to-ceiling height is recommended at-grade to create a strong street presence and allow for flexible commercial space; and,
- Where setbacks vary on both sides of a proposed commercial building, the average of the two setbacks should be used.

#### X5.15

#### DESIGN GUIDELINE 15 - INSTITUTIONAL BUILDINGS

Institutional buildings should be designed to reflect their civic role through prominent, high quality architecture.

Existing institutional uses, such as places of worship and educational facilities, are focal points in the Hamlet. As required, opportunities for additional institutional uses (i.e. community centre, museum, etc.) should be explored, and can be attractive destinations within the Hamlet.



Figure X5.24: New institutional buildings should be attractive landmark sites within the Hamlet.

- Institutional buildings should be located at gateways and focal points, and should be highly visible;
- Building design should promote safety and ease of access through well-defined entrances and windows facing the public street and primary walkways;
- Main entrances should be highly visible and distinguished through the building's architecture and detailing (i.e. door size, entry and windows); and,
- Façades should maximize the use of operable windows for natural illumination and ventilation.



The hamlet design analysis has revealed that, despite the strong impact of heritage buildings in the hamlet centre, the overall architectural character of Glen Williams is a variety of building forms and styles, representative of Glen Williams' organic pattern of growth over the last century.

The guidelines below describe how the heritage character of the community should be retained both in its infrastructure and its building forms, both existing and future

## X6.1 STREET TYPE AND PATTERN

- Consider the use of rural road or rolled curb sections to promote the rural character of the hamlet;
- Consider modifying road engineering and lot grading standards, without compromising safety standards, where new streets encounter topographical features, woodlots, single trees, and other natural features, to preserve the natural character of streets;
- Utilize a range of street/block types including irregular blocks, short orthogonal blocks, winding streets, and rural cul-de-sacs to preserve the varied character of the hamlet street system; and,
- Limit block lengths to 175m, in keeping with the smaller block lengths of the hamlet centre.

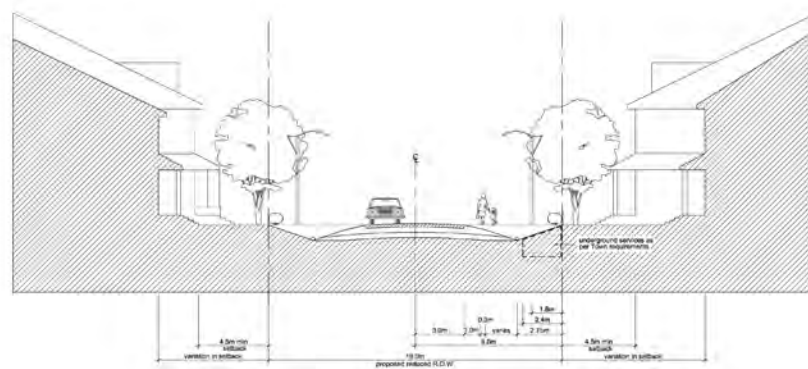


Figure X6.1: Proposed Rural Cross Section. Note: This sketch is for illustrative purposes, and does not replace Town standards.

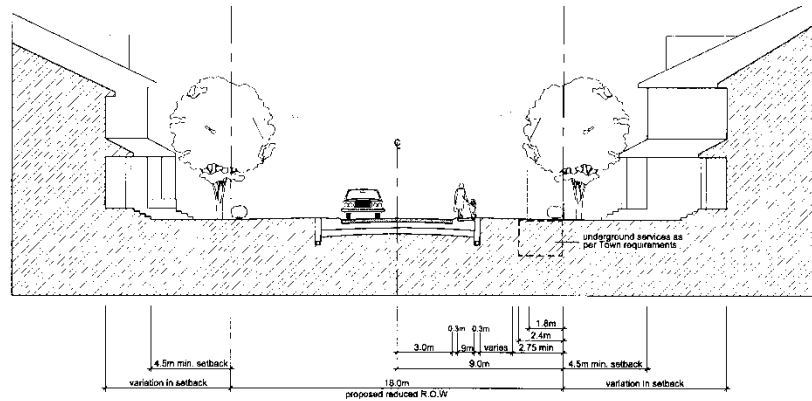


Figure X6.2: Proposed Rolled Curb Street Section. Note: This sketch is for illustrative purposes, and does not replace Town standards.

## X6.2 LOT CONFIGURATION

- Vary lot frontages and depths within each streetscape to maintain the hamlet's random lot pattern. Allow adjacent lots to vary in lot configuration.
- Ensure that lot sizes allow for the safe and effective installation/connection of sanitary services (private, communal or municipal), per regulatory requirements.
- Avoid streetscapes with uniform lot frontages. Permit adjacent lot frontages to vary up to 50%. It is recommended that no more than four consecutive lots shall have the same frontage. Beyond a maximum of four lots, allow adjacent lot frontages to vary by 50%.

## X6.3 SETBACKS

### X6.3.1 FRONT YARD

- Encourage flexibility of front yard setbacks to maintain the variety of setbacks found on hamlet streetscapes. It is recommended that no more than four consecutive lots shall have the same front yard setback.
- Ensure that no front wall of a house shall be set further back than half the length of the adjacent house to maintain privacy of rear yards.
- On one streetscape, ensure that a minimum of 30% of the front wall of houses are located at the minimum setback to provide a sense of enclosure to the street and a pedestrian oriented environment. Consider revising the current 50ft (15m) minimum front yard requirement to 4.5m.

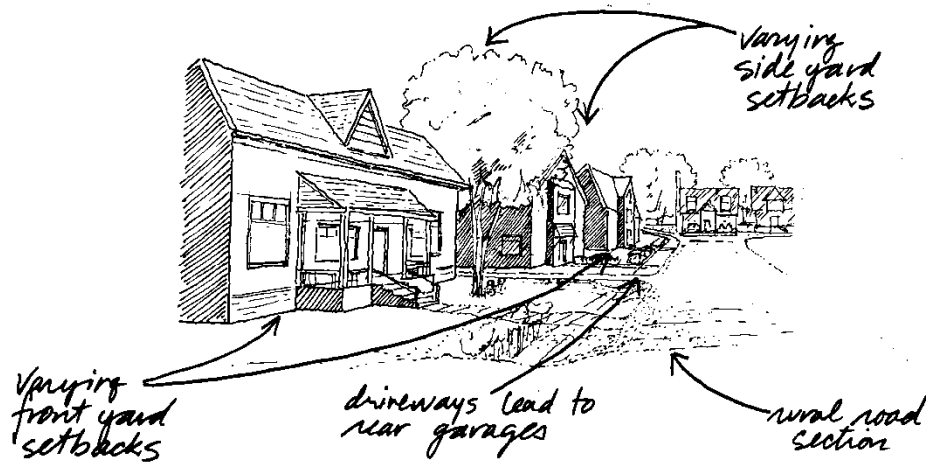


Figure X6.3: Setbacks

### X6.3.2 SIDE YARD

- Side yard setbacks should allow for access, servicing requirements, variations in grading and natural features.
- Side yard setbacks in the hamlet vary from as low as 2m up to 35m. Consider revising the current 15ft (4.5m) interior setback to 2.25m to allow for flexibility of siting of the main house. Refer to “Houses at Corner Lots and Pedestrian/Open Space Links” for reference to exterior side yard setbacks.

### X6.3.3 REAR YARD

- The current 25ft (7.6m) rear yard setbacks can be maintained for all lot depths.

## X6.4 HOUSES AT FOCAL LOCATIONS

### X6.4.1 CORNER LOTS

- Houses at corner lots are important within a streetscape as they are visible from both streets and create the entrance condition or a “gate” to the street. Flanking elevations, garages and private yard enclosures are exposed to the public realm at these locations. The design of these buildings and elevations should have special consideration.
- Exposed elevations should have equal importance with respect to openings and attention to detail. The use of wrap-around porches and corner bay windows is encouraged to link the two facades and to accentuate the corner condition. The main entrance should be located on the long frontage to avoid blank sections of walls.

### X6.4.2 HOUSES AT PEDESTRIAN TRAILS/LINKS AND OPEN SPACE AREAS

- Houses that border upon open space are also visible from two sides, similar to corner lots. Both elevations should have equal importance with respect to openings and attention to detail.

- The main entrance should face the street. The use of wrap-around porches and corner bay windows is encouraged to link the two facades, to accentuate the corner condition; to link the two elevations; and, to provide a visual connection from the house to these public areas.
- For both corner and pedestrian link locations, consider a reduction of the current exterior side yard setback of 30ft (9.1m) to 4.5m to increase the sense of community supervision at these public space connections.

Note: A 10 metre setback will be required from valley top of bank to lot lines to allow adequate space for pedestrian trails.

## X6.5

### GARAGES AND AUXILIARY BUILDINGS

- Encourage the use of detached garages that are located at the rear of the lot. Many garages in the hamlet are detached and to the rear and/or side of the lot. To encourage this design strategy, consider exemption of the area of rear yard garages from calculations for maximum coverage, under the zoning by-law.
- Where garages are attached, they shall be recessed a minimum of 1.0 m from the face of the house. Avoid garages that project forward from the front wall of the house.

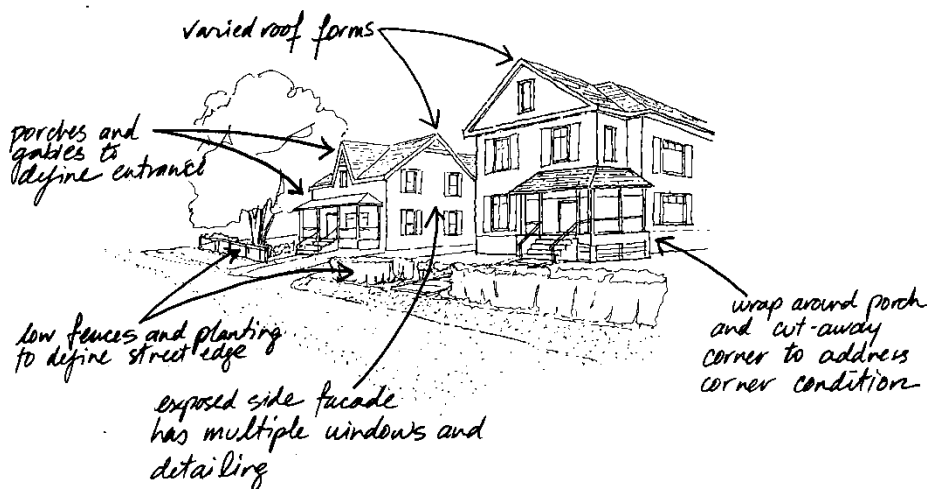


Figure X6.4: Urban Design Principles

## X6.6

### ARCHITECTURAL DESIGN PRINCIPLES FOR NEW DEVELOPMENT

The following guidelines are intended to aid developers and builders to create buildings and streets that contribute to the quality of intimate, random and individual qualities of streetscapes that can be found in the heritage district and that could foster the kind of atmosphere that can be defined by “hamlet character”.

#### X6.6.1 STREET ADDRESS

The main elevation of houses shall address the lot frontage to provide a clear identification of the street address. Architectural elements such as the front entrance; habitable spaces with windows to the street; porches and stairs; and, terraces and balconies, convey the sense of houses “looking out onto the street”.

#### X6.6.2 ENTRANCE ARCHITECTURE

- The design of houses should accentuate the main entrance. Attention should be given to the architectural detailing of entrances and their importance in setting the character, or “identity” of the streetscape.
- The use of porches is encouraged as a means to define the entrance and create well-proportioned front elevations. The detailing of porches should be integrated with that of the house as a whole.
- Porches foster social activity between the house and the street, which is very common along the streets of Glen Williams. Porches should be generous enough in depth and length to allow for furniture and planting. A minimum depth of 1.8m is recommended.
- Walkways from the entrance to the street are encouraged as a means of linking street and property at a pedestrian scale.



Figure X6.5: Entrance Architecture

#### X6.6.3 RELATIONSHIP TO GRADE

The relationship of the house to grade is important in the streetscape. The main floors of houses in the hamlet tend to be at grade or close to grade. In cases of strong topography, entrance levels are related to grade through terracing. Basement garages or high service floors do not appear in the hamlet and should be avoided.

#### X6.6.4 WINDOWS AND PROJECTING ELEMENTS

- The design, placement and size of windows are important in creating architecturally well-proportioned streetscapes and affect the sense of privacy between properties. Special attention should be given to the location and detailing of windows.
- Projections such as bay windows and balconies, chimney elements, projecting cornices and roof eaves are encouraged to create variety along the streetscape.
- Bay windows, balconies, porches and porticoes may project up to 1.8m from the main building face into the front yard setback. This is intended to encourage houses to have these elements and be located close to the street edge.
- Bay windows may be single or double storey in height. Their proportions should be appropriate to the building from which they project.

#### X6.6.5 ROOFS

Because of the various ages of houses in Glen Williams, no single roof type or pitch is prevalent. Heritage homes typically have steeply-pitched roofs with a variety of roof forms such as dormers and gables, while bungalows have shallower hip roofs. A variety of roof forms appropriate to the scale and architecture of the built form is encouraged.

#### X6.6.6 CONSTRUCTION MATERIALS

- A variety of building materials is used throughout Glen Williams. To promote the character of the hamlet, the use of materials found in heritage buildings, such as brick, stone and wood is encouraged.
- Some houses and porches in the hamlet, mostly those of wood siding, are painted light colours. This creates an attractive, lively streetscape. The use of colour is encouraged for building facades and/or for architectural details to create streetscapes that are in keeping with those of the hamlet centre.
- Materials for garages and outbuildings should be similar to those used for the main house.

#### X6.6.7 LANDSCAPING

- The use of fences and landscaped elements, used in combination, is encouraged to delineate between properties.
- Where an existing natural feature is located within a property, such as a woodlot or single tree, it should be integrated into the landscaping design as a means to promote and preserve Glen Williams' natural setting.
- Many paths to houses in the hamlet are identified with planted features. Where walkways extend to the street, they should be augmented with planting both to provide an alternate means of street address and to bring natural elements to the street edge.

## X6.7 IMPLEMENTATION

### X6.7.1 DESIGN REVIEW

The Design Review process shall occur in conjunction with applications for Draft Plan Approval and prior to application for building permits. The Design Review process shall monitor the realization of the vision for Glen Williams including:

- preservation and promotion of the character of Glen William's built form;
- protection and enhancement of Glen William's open space network and natural environment;
- improvement of pedestrian connections to the commercial and community facilities of the hamlet core and to Glen William's open space system; and,
- preservation of the balance between development in the hamlet and adjacent natural lands.

The Design Review process shall determine how new development fulfills the hamlet design recommendations for:

#### X6.7.1.1 STREET SYSTEM

- A street pattern that responds to significant natural features and / or topography.
- Street sections and streetscape elements.
- Connection of parcel to other neighbourhoods and/or open space with pedestrian paths, where possible.

#### X6.7.1.2 LOT CONFIGURATION, SETBACKS, HOUSING TYPES, AND GARAGES

- Variation of lot sizes.
- Setbacks.
- Proposed housing types and garage locations.

#### X6.7.1.3 HOUSES AT FOCAL LOCATIONS

- Treatment of houses at corner lots, those abutting pedestrian trails and open spaces and at other focal locations.

#### X6.7.1.4 ARCHITECTURAL PRINCIPLES

- Response of new development to heritage character, where new development is in proximity to, or includes within the site, buildings of heritage character.
- Intended architectural character of built form.



Figure X6.6: Landscaping

#### X6.7.1.5 LANDSCAPING PRINCIPLES

- Integration of natural features on the site.
- Character of new landscaping and streetscape features.

#### X6.8 GATEWAYS

Gateway features should be established at key entry points within the community to help strengthen the character and identity of Glen Williams Development at gateways should therefore help shape this sense of identity by the nature and quality of landscaping, built forms and design features such as public art.

Depending on location and available space, gateway features may include taller architectural elements that symbolize entry-like gateposts such as columns and customized lighting fixtures. Landscape features (such as plantings, flags, special signage, and banners) may also accentuate gateways.

In order to strengthen the gateway image, different public art features such as sculptures, fountains, and decorative walls with murals may be used. Gateways should be given first priority when considering the placement public art features.

Building developments within Gateway areas must incorporate streetscape improvements that will serve to provide shelter to pedestrians at these major intersections. For example, this can be accomplished by setting back the building and developing a public space that incorporates, landscaping, public art, lighting and/or shelters.

#### X6.9 DOCUMENTATION

The information required for the Design Review process, in addition to normal requirements of draft plan application, shall be prepared by a qualified architect and or landscape architect and include:

- a) Site Plan(s) indicating:



- i) setbacks, heights and housing locations;
  - ii) clear location and site dimensioning of septic beds, if privately serviced;
  - iii) indication of pedestrian connections and access to open space;
  - iv) topography and new grading;
  - v) existing vegetation and proposed character of landscaping; and,
  - vi) roads, walkways, driveways, terraces and other impervious surfaces location of public features such as postal kiosks and any above grade utilities; and,
- b) cross sections of each street type showing:
- i) width of right-of-way;
  - ii) type of road section being used with all services located as per the Town's requirements;
  - iii) location of street lighting and furniture;
  - iv) boulevards; and,
  - v) streetscape elevations illustrating proposed residential character, including:
    - entrances and porch locations;
    - heights;
    - roof form; and,
    - fenestration.

## X7

### HAMLET OF GLEN WILLIAMS TERMS OF REFERENCE FOR ENVIRONMENTAL IMPLEMENTATION REPORTS

An Environmental Implementation Report (EIR) must be prepared according to the policies of the Glen Williams Secondary Plan, on a tributary basis. The complexity of the EIR will be dependent on the environmental sensitivity of the subject tributary and adjacent watercourses. It is suggested that the proponent consult with the Town of Halton Hills, Credit Valley Conservation (CVC) and the Region of Halton prior to beginning to prepare the EIR. The EIR can be prepared concurrently with any other studies that are required.

#### X7.1

##### PURPOSE

The purpose of the EIR is to:

- ensure the goals and objectives set out in the Scoped Subwatershed Plan for Glen Williams are met when land use changes are planned;
- develop an appropriate plan that will achieve the targets that have been set for individual environmental resources;
- streamline the review and approval processes; and,
- collect and provide sufficient detailed data so that the proposed implementation reports and stormwater management reports can be developed.

#### X7.2

##### PROCESS

The EIR consists of fourteen steps divided into three parts:

- Part A – Background Review;
- Part B – Detailed Studies/Impact Assessment; and,
- Part C – Implementation.

The EIR process is summarized on Table X7 and the steps in each of the three parts are detailed below.

TABLE X7: ENVIRONMENTAL IMPLEMENTATION REPORT PROCESS

Part	Description	Output	Step
X7.3	<b>Background Review</b> <ul style="list-style-type: none"><li>• Brief description of the proposed land use change</li><li>• Describe and implement monitoring to collect baseline conditions</li></ul>	<b>Background Report that includes:</b> <ul style="list-style-type: none"><li>• General site map</li><li>• Development schedule</li><li>• Literature cited</li></ul>	1-5

Part	Description	Output	Step
	<ul style="list-style-type: none"> <li>Field work carried out to better define the environmental resources</li> </ul>	<ul style="list-style-type: none"> <li>List of background data consulted</li> <li>Constraint map that includes all environmental and water-related features, in and adjacent to the proposed area of land use change</li> <li>List of detailed studies needed for Part B</li> </ul>	
X7.4	<p>Detailed Studies and Impact Assessment</p> <ul style="list-style-type: none"> <li>Describe the purpose of the land use change</li> <li>Describe the activities associated with the land use change</li> <li>Describe possible alternatives to the proposal</li> <li>If insufficient information exists to adequately assess the impact of the proposal, then detailed studies need to be completed</li> <li>The hydrogeology study requirements are more detailed for extraction developments that plan to go below the water table</li> <li>An impact assessment will be included in this phase – a statement that describes the potential impacts that the land use</li> </ul>	<p>Impact Assessment Report that includes:</p> <ul style="list-style-type: none"> <li>Detailed location map</li> <li>Site plan</li> <li>Surface water and groundwater resources study</li> <li>Geomorphology study</li> <li>Aquatic resources and water quality study</li> <li>Terrestrial resources study</li> <li>Hydrogeology study to assess areas of potentially higher recharge</li> <li>Hydrogeology impact associated with aggregate extraction</li> <li>Summary of alternative scenarios, the associated impacts, and a list of mitigative measures</li> <li>Revised constraint mapping</li> </ul>	6-9

Part	Description	Output	Step
	<p>change will have on environmental resources</p> <ul style="list-style-type: none"> <li>Show how the proposed land use change has considered the environmental resources in planning/design proposal</li> <li>Set environmental targets</li> </ul>	<ul style="list-style-type: none"> <li>Forecasted changes in all environmental resources</li> <li>Note: Acceptable mitigation must conform to the goals and objectives, and must meet targets.</li> </ul>	
X7.5	<p>Implementation</p> <ul style="list-style-type: none"> <li>Will include a long-term monitoring plan to determine if and where change is occurring</li> <li>Includes a recommended plan that shows how goals and objectives are achieved</li> <li>Includes recommendations for implementation</li> </ul>	<p>Implementation Report that includes:</p> <ul style="list-style-type: none"> <li>Stormwater Management Plan</li> <li>Long-term monitoring plan</li> <li>Conclusions</li> <li>Recommendations</li> <li>Mitigating measures</li> <li>Executive Summary of Parts A, B and C</li> <li>Note: Monitoring plan must be designed to evaluate if the targets set are being met.</li> </ul>	5-6, 10-13

### X7.3 PART A - BACKGROUND INFORMATION

#### X7.3.1 STEP 1 INTRODUCTION TO PROPOSED LAND USE CHANGES

The purpose of this component is to focus on the tributary of concern and translate pertinent known information to establish initial constraints and baseline conditions. The end product is to be able to identify the needed detailed studies to be done in Part B.

This section should include:

- A brief description of the proposed land use change.
- A general map of the area.

#### X7.3.2 STEP 2 BASELINE MONITORING

Monitoring is completed to check the impact that the proposed land use change has on the environment. The purpose of the baseline monitoring is to establish the baseline conditions and existing environmental trends to which future monitoring results will be compared.

Information should be collected on (but not limited to):

- water quality;
- fisheries;
- hydrology;
- groundwater quality and quantity;
- stream morphology; and,
- terrestrial resources – woodlots, wetlands, wildlife habitat, Environmentally Sensitive Areas, Areas of Natural and Scientific Interest.

When preparing a baseline monitoring plan, it is important to ensure that many different disciplines are being monitored at the same site when possible and appropriate. For example, fisheries and water quality monitoring should take place at the same site.

An explanation of how indicator parameters were established, for example, what criteria were used when deciding what to monitor, will be included in this section.

It is essential that baseline monitoring be included in the project work plan, and that associated costs are included in the project budget.

#### X7.3.3 STEP 3 BACKGROUND INFORMATION

Include literature cited, all background data, a listing of information sources contacted during the study, and a listing of the professionals on the study team.

#### X7.3.4 STEP 4 EXISTING CONDITIONS AND INITIAL CONSTRAINT MAPPING

Fieldwork should be carried out to better define the existing functions, linkages and limits of the natural environmental resources. Detailed constraint mapping at the tributary scale will be prepared that highlights the environmental resources detailed in the appropriate Tributary Fact Sheet in the Scoped Subwatershed Plan for Glen Williams, as well as agency and municipal setback requirements (i.e. the Environmental Protection Strategy, the Fisheries Act, valleyland setbacks). This part of the submission

will show how the proposed land use change has been designed in conformity with the initial constraint mapping and will show how the EIR conforms to the general concepts of the Scoped Subwatershed Plan for Glen Williams. The mapping specifications are outlined under Item 14.

The mapping may include, but is not limited to:

- all hydrologic features including watercourses, swales, ponds, depression areas, springs, seepage areas and existing stormwater management facilities;
- Regulatory Floodplain as per the CVC Flood Plain Management Policies;
- present day land use;
- wetlands, woodlots and other terrestrial and riparian communities;
- terrestrial corridors (existing and potential);
- water quality;
- aquatic communities and habitat, and appropriate setbacks;
- fill lines, valley slopes, top of bank, ecological considerations and geotechnical hazard areas as per the CVC Watercourse and Valleyland Protection Policies;
- groundwater recharge and discharge areas, and the linkages between them;
- groundwatersheds (extending outside the study area – if applicable);
- stream morphology, channel sensitivity, and setbacks required to allow natural channel functions (migration, meander belt width, flooding); and,
- limit of extractable resource, and grade and type of extractable resource.

#### X7.3.5 STEP 5 REPORT

Once the requirements of Steps 1 to 4 have been met, a Background Report will be prepared and submitted for review and approval prior to proceeding to the following steps.

### X7.4 PART B - DETAILED STUDIES AND IMPACT ASSESSMENT

#### X7.4.1 STEP 6 DETAILED STUDIES

If found through completing Part A that insufficient information exists to complete the constraint mapping or develop protection, restoration and enhancement plans for the subject area, then detailed studies must be prepared. The need for, and scope of, the detailed studies should be confirmed with the Town of Halton Hills, the CVC, and the Region of Halton at the conclusion of Part A. Detailed studies may include, but are not limited to:

- surface water and groundwater resources study;

- aquatic resources and water quality study;
- geomorphology study;
- terrestrial resources study;
- hydrogeological analyses, including field investigations, to assess identified areas of potentially higher recharge; and,
- hydrogeological impact associated with aggregate extraction.

#### X7.4.1.1 SURFACE WATER AND GROUNDWATER RESOURCES STUDY

Note: The groundwater detailed study is more extensive if the proposed land use change is an aggregate resource area – additional study components are required. If this is the case see the section on Hydrogeological Impacts Associated with Aggregate Extraction.

The constraint mapping will have identified hydrologic features within the study area, however, the overall hydrologic system must be described. The components of the system to be addressed by the detailed studies include:

- a) identification of flow characteristics in on-site watercourses and swales, and a general description of the water balance on the site;
- b) characterization of all hydrologic features (watercourse, swales, natural areas providing flood storage/attenuation, depression storage areas, recharge areas, seepage areas and springs);
- c) determination of the volume and distribution patterns of the major discharge areas and a representative location used for monitoring; and,
- d) description of the relationship and dependencies between these features and the surrounding terrestrial, wetland and aquatic resources.

The above analyses should be carried out using technical methods and procedures that are in conformity with the protocols and requirements of the Town of Halton Hills, the CVC, and the Region of Halton. These agencies should be consulted prior to initiating the studies to confirm computer modelling techniques and watershed parameters to be employed (e.g. rainfall distribution, recession constants, curve numbers, etc.) as well as the scope and format of the supporting documentation.

#### X7.4.1.2 AQUATIC RESOURCES AND WATER QUALITY STUDY

The constraint mapping will have identified fish habitat and water quality classification for the tributaries. The detailed study is to provide the following information in support of the habitat classifications and planned land use change conditions:

- a) The direct and indirect physical impacts on water bodies and water quality from the activity;
- b) The fish species present, and the direct and indirect biological impacts of the physical impacts in (a); and,

- c) An assessment of whether the impacted habitat represents a limiting factor for the proposed land use change.

#### X7.4.1.3 STREAM MORPHOLOGY STUDY

The study will describe the physical form of the watercourse. The following information will be included:

- a) Characterization of geomorphic features including sensitive reaches, areas of erosion and aggravation, meander belt width, and channel migration; and,
- b) Description of the relationship between the geomorphologic and hydrologic characteristics of the system.

#### X7.4.1.4 TERRESTRIAL RESOURCES STUDY

The study will describe the physical form and function of the ecological features of the area, any functional relationships to adjacent areas, define what additional issues must be examined (e.g. buffers) and demonstrate how the proposed land use change will maintain or compensate for the areas existing ecological and hydrological functions. Reference to Section 5 of the Natural Heritage Reference Manual (MNR, 1999) will be of some assistance.

The terrestrial resources study will contain, but not be limited to:

- a) maps illustrating land ownership, and land use zoning;
- b) maps illustrating any of the following features that occur in or within 120 metres of the proposed land use change:
  - i) significant wetlands;
  - ii) significant portions of the habitat of threatened or endangered species;
  - iii) significant woodlands or valleylands;
  - iv) significant wildlife habitat;
  - v) areas of Natural and Scientific Interest (ANSIs); and,
  - vi) Environmentally Sensitive Areas (ESAs); and,
- c) a biological description of the natural environment that might reasonably be expected to be affected by the proposal for land use change.

#### X7.4.1.5 HYDROGEOLOGY ANALYSES - RECHARGE CONSIDERATIONS

The Scoped Subwatershed Plan for Glen Williams identified several areas within the Glen Williams Planning Area that represent potential areas with higher infiltration characteristics. This conclusion was



reached primarily from available information produced as part of the Regional-based groundwater studies prepared by the Region of Halton.

Detailed hydrogeology investigations should be conducted on a tributary or site-specific basis to determine and quantify the following:

- The nature and composition of the native soils;
- The infiltration capacity of the native materials;
- An estimate of the groundwater recharge volume that is being achieved, and its significance to the receiving watercourse; and,
- If warranted, identify appropriate Best Management Practices that are in accordance with the direction provided by the Scoped Subwatershed Plan for Glen Williams, and which should be employed to maintain current levels and/or minimize potential impacts.

#### X7.4.1.6 HYDROGEOLOGY IMPACT ASSOCIATED WITH AGGREGATE EXTRACTION

If required, the proponent is expected to follow the Provincial standards set out in the document Aggregate Resources for Ontario and the following more detailed requirements:

If extracting above the water table:

- a) establish the water table elevation before extraction occurs; and,
- b) determine how hydrology and hydrogeology will change, and the effect of the change on fisheries, terrestrial resources, geomorphology, surface water, water quality, etc.

If extracting below the water table, the study should, but not be limited to:

- a) the general groundwater setting and linkages between the local and surrounding groundwater system;
- b) approximate high water table location;
- c) groundwater flow and direction, and the general geologic setting;
- d) potential recharge and discharge areas on, and adjacent to, the site;
- e) local groundwater resource usage;
- f) location and usage of water wells within 1 km of the site;
- g) detailed description of the local geologic conditions and the function of the geologic units from a hydrogeologic perspective;
- h) detailed assessment of the groundwater flow system, local flow direction, linkages to surface water and the regional groundwater flow system;

- i) detailed local and Regional water budget related to the groundwater system. The emphasis should be on estimating recharge to the site, groundwater flux off-site to the local system, regional system and local and regional discharge. The water budget should be evaluated from a sensitivity perspective to potential change in groundwater movement on a subcatchment and subwatershed scale;
- j) delineate major and local aquifers in the area and interpret the connection to the site;
- k) studies on springs, surface watercourses or discharge to surface water that focus on groundwater/surface water interaction, determining linkages to recharge and discharge areas through baseflow assessment, vertical gradients and water table location. This information should be incorporated into the water balance; and,
- l) proposed water diversions or storage and drainage facility studies should focus on the linkage of the surface water to the groundwater.

#### X7.4.2 STEP 7 DESCRIPTION OF PROPOSED LAND USE CHANGE

Section 7.0 should include, but not be limited to:

- a) the purpose of the proposed land use change;
- b) a detailed location map and site plan;
- c) activities associated with the proposed land use change both during the construction phase and the post-development phase that may have an impact on the natural environment;
- d) a schedule of the proposed land use change, including any phasing for the development;
- e) a discussion of the “do nothing” alternative and other alternatives to the proposal; and
- f) an explanation of how the proposed land use change has incorporated the environmental resources into planning/ designing the proposed land use change.

#### X7.4.3 STEP 8 IMPACT ASSESSMENT

A concise description of potential impacts for each study completed will be included. This will include how linkages between environmental resources and the functions of the resources, will be affected. Predicted changes in all natural features will be included.

A revised or updated Constraint Map will be required to reflect the results of the Impact Assessment.

#### X7.4.4 STEP 9 REPORT

Once the requirements of Steps 6 to 8 have been fulfilled, a report on Part B (Impact Assessment Report) will be prepared and submitted to the Town of Halton Hills, the Region of Halton and the CVC for review and approval prior to proceeding to the following steps. The report will include environmental targets for the area should the land use change be approved.

#### X7.5 PART C - IMPLEMENTATION

The implementation section should focus on how the recommendations and findings will be addressed. There may be several recommendations related to various aspects of the natural environment. Stormwater management is a consistent requirement, and as such is outlined here.

#### X7.5.1 STEP 10 STORMWATER MANAGEMENT PLAN

Stormwater management facilities and enhancement techniques will be required to ensure that hydrologic characteristics of the subwatershed will be maintained and ecological resources will be protected.

A fundamental goal of the stormwater management plan will be to determine the most appropriate type of control and enhancement measures and facilities that should be incorporated into the development proposal. Particular attention will be required in dealing with Tributary E, as identified in the Scoped Subwatershed Study for Glen Williams, which is a sensitive cold water watercourse. Accordingly, consideration must be given to incorporating management measures that will avoid the creation of potential thermal impacts that would be harmful to the environment.

Accordingly, the stormwater management study will define and provide the following:

- a) Description of existing and proposed runoff conditions by subcatchment;
- b) Identification of significant watercourses;
- c) Identification of management practices and design considerations necessary to ensure that the stormwater management plan conforms to the Scoped Subwatershed Plan for Glen Williams and the Credit River Water Quality Strategy;
- d) Identification of downstream problems and methods to mitigate or eliminate them;
- e) Identification, screening and design of alternative management practices based on guidelines provided in the Scoped Subwatershed Plan for Glen Williams, the Stormwater Management Practices Planning and Design Manual (MOEE, 1994) and the CVC Stormwater Management Guidelines (CVC, 1996);
- f) Where basins are proposed, confirmation of location, catchment area, functional considerations, outlet characteristics and preliminary design elements;
- g) Documentation and adherence to criteria related to water quality, water quantity and base flow protection;
- h) Location, size, type and design of all stormwater management facilities. Details to be provided include: determination of whether an on-line or off-line facility should be used; selection of wet pond or wetland; forebay dimensions; operating characteristics; targets that will be achieved; integration with existing natural features, etc. Reference should be made to the Scoped Subwatershed Plan for Glen Williams for guidance in regard to the screening and selection of the stormwater management measures, and the initial listing of the preferred alternatives;
- i) Detailed implementation steps and programs; and,
- j) A summary of the technical findings and recommendations.

#### X7.5.2

#### STEP 11 LONG-TERM MONITORING PLAN

Monitoring should continue after baseline conditions are established. The monitoring plan should be designed in order that impacts can be distinguished from natural trends at an early stage.

If impacts are detected:

- a) A more aggressive type of monitoring should take place that determines where, why and how fast the change is occurring;
- b) cause-effect relationships between environmental resources and land use change should be determined;
- c) appropriate mitigative measures should be proposed to deal with change; and,
- d) a focus should be placed on evaluating ongoing or proposed management practices.

Areas that should be monitored over the longer-term include, but are not limited to:

- a) water quality;
- b) fisheries;
- c) hydrology;
- d) groundwater quality and quantity;
- e) stream morphology; and,
- f) terrestrial resources – woodlots, wetlands, wildlife habitat.

It is essential that long-term monitoring is included in the project work plan, and that monitoring costs are included in the project budget.

#### X7.5.3

#### STEP 12 IMPLEMENTATION

This section will include, but not be limited to:

- a) a comparative evaluation of alternative management options leading to the selection of the preferred option;
- b) conclusions and recommendations – including the advisability of proceeding; and,
- c) mitigation measures – if impacts are expected, what plans are in place to maintain functions within the natural system.

#### X7.5.4

#### STEP 13 EXECUTIVE SUMMARY

Include a summary at the front of the report that contains a description of the land use change, the effects on the environment, and all recommendations. The summary should include information from Parts A, B and C.

#### X7.5.5 STEP 14 REPORTING FORMAT

A complete description of all the work and conclusions involved in the EIR (Parts A, B and C) needs to be included here.

Reports should be submitted in hard copy along with an electronic copy in Microsoft Word on a labeled 3.5 inch floppy disk, CD or via e-mail. Diskettes should be scanned for viruses and corruption prior to delivery. Three copies of the report, each containing a full set of maps must be prepared, and one copy submitted to the Town of Halton Hills, Credit Valley Conservation, and the Region of Halton.

##### X7.5.5.1 GRAPHICS

All graphics will be submitted in Microsoft PowerPoint format. The graphic should be delivered on disk separately from the main document, although it should also be incorporated into the main document file.

##### X7.5.5.2 ARTWORK

All artwork should be provided in Corel Draw format and should be delivered on 3.5 inch floppy disk separately from the main document, and also incorporated into the main document. If the artwork has to be scanned, it may be delivered in GIF or TIF bitmap format.

##### X7.5.5.3 MAPPING

Mapping should be at a scale of 1:5000 or less. It should be noted that ArcView 3.0 and PC ArcInfo comprise the software currently in use at the Town of Halton Hills and CVC. As such, ArcInfo format coverages are considered to be standard for the organization. In general, digital graphic data:

- must be georeferenced in either UTM (preferred, using NAD 83) or latitude-longitude coordinates;
- must be clean (i.e. polygons are closed, dangles eliminated, polygons with common borders should not overlap, etc.);
- should be packaged/organized into logical layers (e.g. soils layer, wetlands layer, etc.);
- must be in vector format, unless otherwise specified; and,
- should be either uncompressed ASCII ArcInfo Export (.E00) format, ArcView Shapefile or AutoCAD exchange DXF format.

If provided in DXF format, the elements necessary to produce correct topology in ArcInfo should be included, such as feature codes and attributes. Peripheral graphics such as page borders and title boxes should be included only if they are stored in layers distinct and separate from the actual map graphic. All features should be in separate named layers, and layers should be colour-coded. A single unique numeric label feature ID should be placed inside of polygons. Attributes may be provided in separate

tables, although it should be ensured that a common variable exists between the attribute table and the map features, such as feature ID, to allow their association and/or joining.

#### X7.5.5.4 TABULAR ATTRIBUTE DATA

Attribute data should be provided in Microsoft Access 97 format files (preferred), dBase IV format files, or in formatted (i.e. with defined columns) ASCII files.

#### X7.5.5.5 TEXTUAL DATA FOR GRAPHICS

Text should be provided in Microsoft Word or ASCII format. Please be aware that any tabular data to be referenced to actual map features should not be provided as tables in a Word document.

#### X7.5.5.6 DIGITAL PHOTOS

Digital photos, whether they are scanned photographs or computer-generated artwork, should be provided in Corel Draw 5.0 (i.e. for vector graphics), encapsulated postscript (EPS), or bitmap (either TIF or PCX) format for scanned graphics.

For more information on delivering digital data, refer to the document Specifications for the

## PREMIER GATEWAY PHASE 1B SECONDARY PLAN SUBWATERSHED IMPACT STUDY MATTERS TO BE ADDRESSED

### Subwatershed Impact Study

The Subwatershed Impact Study will:


- a) Update the characterization of features that are recommended to be integrated in the Natural Heritage System;
- b) Refine the natural heritage and natural hazard limits reflecting the Natural Heritage System objectives and other intentions of the subwatershed study (i.e. final staking of Natural Heritage System features and buffers, calculation of riparian storage volumes, etc.);
- c) Delineate the location of key Natural Heritage System features and Conservation Halton regulated areas based on site specific topographic survey information and detailed technical studies regarding watercourses, flooding hazards (eg. floodplain analysis), erosion hazards (e.g. physical top of bank staking, geotechnical slope stability and/or meander belt erosion assessments) and wetlands (wetland limit staking and Ontario Wetland Evaluation System);
- d) Conduct feature-based wetland and pond water balance assessments for those features that have been identified as Significant Wildlife Habitat and/or associated with watercourses or “protection” or “conservation” rated headwater drainage features;
- e) Confirm presence of turtle nesting habitat and amphibian breeding habitat in areas not surveyed in the Scoped Subwatershed Study;
- f) Conduct Species at Risk surveys and follow Endangered Species Act regulations in consultation with the Ministry of Natural Resources and Forestry, where required;
- g) Confirm the watercourse constraint ranking and Headwater Drainage Feature classification;
- h) Determine the preferred servicing plan;
- i) Determine the road layout;
- j) Develop and define integration of stormwater management facilities;
- k) Define phasing in areas of multiple ownership;
- l) Establish and fulfill terrestrial and aquatic natural heritage and groundwater monitoring requirements for pre, during and post development and define cost sharing for monitoring programs;
- m) Refine the meander belt width delineation, hazard setbacks and regulatory setbacks;

- n) Refine the limits of the Natural Heritage System with respect to updated watercourse corridor widths;
- o) Develop further characterization of the groundwater resources associated protection measures, and mitigation techniques;
- p) Verify the appropriate management of the water budget based on the proposed development and stormwater management plan;
- q) Determine the detailed road alignment and configuration of watercourse and valley crossings;
- r) Prescribe site specific standards and preliminary design for landscaping, implementation and the management of corridors, wetland creation areas, buffers, and restoration areas;
- s) Develop strategies to enable construction phasing while allowing rescue of biota from small isolated habitats, and maintenance of the Natural Heritage System resources and functioning through the construction period;
- t) Develop the use of Low Impact Development measures;
- u) Undertake a preliminary analysis of the stream corridor dimensions required to maintain the pre-development riparian strategy; and
- v) Determine the location and design of any trail system







A photograph of a modern, multi-story building with a glass curtain wall, viewed through the branches and leaves of large trees. The scene is bright and sunny. In the foreground, there is a paved area with some greenery and a blurred figure of a person walking. A semi-transparent white box contains text in the lower-left corner.

The Town of Halton Hills

# Premier Gateway Employment Area Urban Design Guidelines

Brook McIlroy/  
November, 2020





Steeles Avenue and Hornby Road, Aug 2015

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# 1.0 Introduction



The Halton Hills Premier Gateway Employment Area Urban Design Guidelines (2020) reflect the results of a comprehensive review of the applicable zoning standards in Zoning By-Law 00-138 and the 401 Corridor Integrated Planning Project Design Guidelines (2000). The Premier Gateway Employment Area is located between Highway 401 and Steeles Avenue, as well as along the north side of Steeles Avenue; and between the Town of Milton in the vicinity of James Snow Parkway, and the City of Brampton at Winston Churchill Boulevard.

The guidelines are applicable to the Phase 1A, 1B and 2A areas.

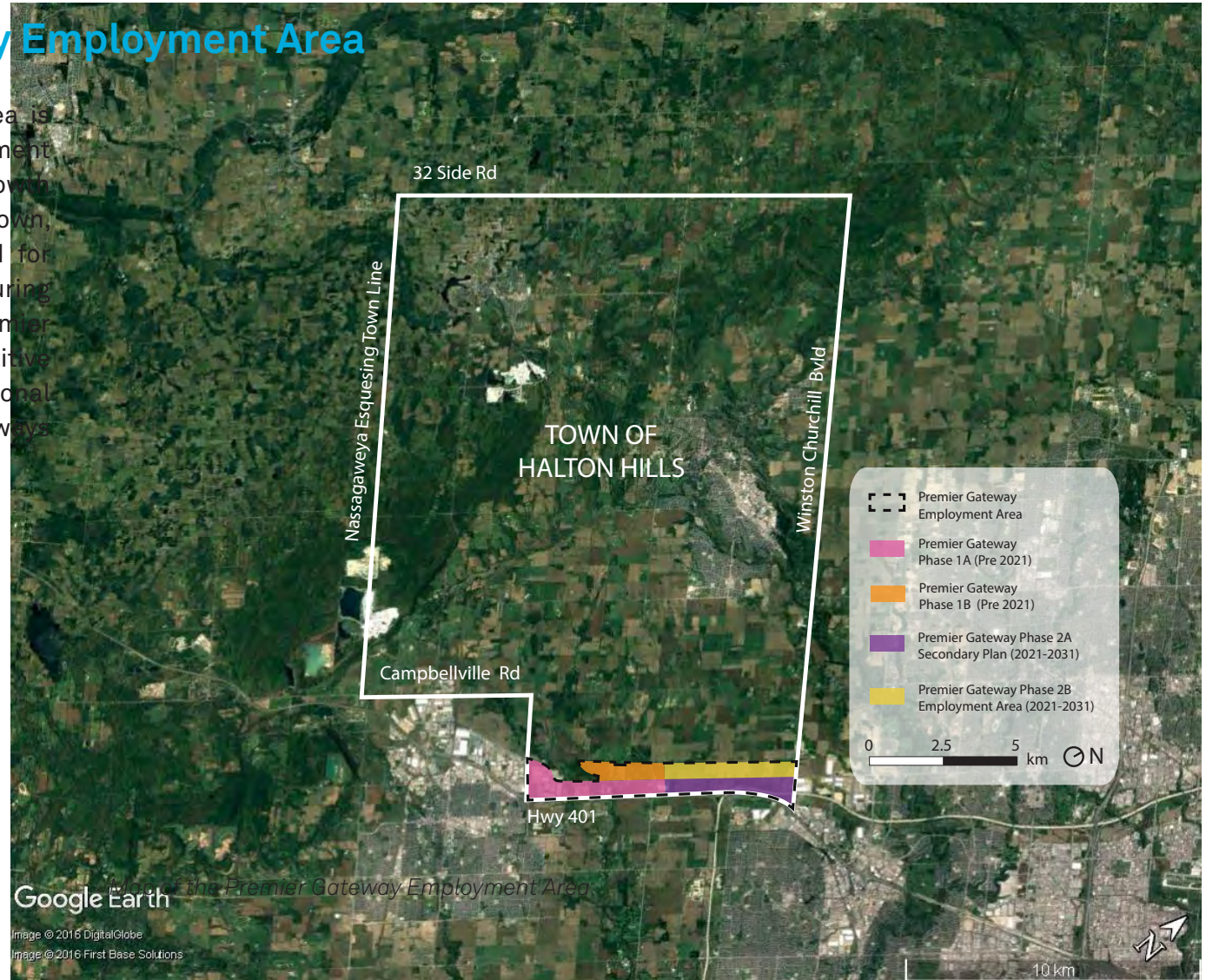
These urban design guidelines support the vision and objectives in the Halton Region Official Plan, and the Halton Hills Official Plan, in particular the policies related to the Premier Gateway Employment Area, as well as the regulations of the Premier Gateway Zoning By-Law. The urban design guidelines aim to fulfill the Town's vision and objectives for the Premier Gateway Employment Area, in order to ensure high quality development

with a strong visual identity. The intent of the guidelines is to establish a planning framework that will help the area thrive as a dynamic, accessible, and economically competitive employment area that reflects high-quality urban design aesthetics and sustainable development practices appropriate for this area.



## 1.1 The Premier Gateway Employment Area

The Premier Gateway Employment Area is identified as the Town's prestige employment area where large scale employment growth is directed. As the "gateway" to the Town, a high standard of design is required for new office, warehousing, manufacturing and commercial development. The Premier Gateway Employment Area is a competitive employment area based on its locational advantages, including access to Highways 401 and 407, rail, and the airport.



## General Policy and Regulatory Direction

### Halton Region Official Plan (ROPA 38)

The Halton Region Official Plan presents a vision for Halton Region's growth and development to the year 2031. The document includes policies that enforce the Region's desire for economic competitiveness, protection and preservation of natural and agricultural areas, sustainable development, and the creation of healthy communities.

The Official Plan primarily designates the Premier Gateway Employment Area as "Urban Area" with an "Employment Area" overlay designation. The key objective of the Employment Area designation is to provide "opportunities for a fully-diversified economic base, including maintaining a range and choice of suitable sites for employment uses which support a wide range of economic activities and ancillary uses, and take into account the needs of existing and future businesses" (Section 77.1(2)). In addition, the three main tributaries of the Sixteen Mile Creek that cross the area are designated as "Regional Natural Heritage System".

### Halton Hills Official Plan

The Halton Hills Official Plan, as amended by Official Plan Amendment Nos. 10, 21, and 30 and 31 A & B<sup>1</sup> establish a vision for the land use and development of the Town, for the planning horizon of 2031. The Official Plan provides direction on managing growth and development in the Town of Halton Hills, and states that development should demonstrate high-quality built form, site design, and landscape design. The document identifies the Premier Gateway Employment Area as the only employment area that can accommodate large-scale employment growth (Section D3.3). Sections D3.5 and D6.4 and Schedule A8 of the Official Plan provide detailed policy direction and designations with respect to the Premier Gateway Employment Area.

<sup>1</sup>Official Plan Amendment Nos. 10, 21, 30 and 31 A & B have been adopted by the Town of Halton Hills. As of November 2020, OPAs 21, 30 and 31 A & B have not yet been approved by the Region of Halton.



**TOWN OF HALTON HILLS  
OFFICIAL PLAN**

### Premier Gateway Phase 1B Secondary Plan

The Premier Gateway Phase 1B Employment Area Secondary Plan is a comprehensive secondary plan that provides land use designations and policies for certain lands on the north side of Steeles Avenue. The Secondary Plan area includes Lot 1, bounded by Steeles Avenue, Sixth Line, Eighth Line, and a line parallel to, and approximately 1.2 kilometres north of, Steeles Avenue. North of this, the Plan identifies an additional Lot 2 which includes approximately 75 hectares of land for employment uses that form part of the Premier Gateway Employment Area.

### Premier Gateway Zoning By-Law Review

The Zoning By-Law 00-138 for the Premier Gateway Employment Area was created in 2000 as a standalone by-law to implement the direction in the Official Plan regarding the design of new development, and to ensure consistency with the area's urban design guidelines. The 2020 update of this document updates several standards related to setbacks, employee parking, commercial vehicle waiting spaces, loading docks, and minimum landscaping requirements, and will integrate these policies into a comprehensive zoning by-law.



## 1.2 Purpose of the Guidelines

The Premier Gateway Employment Area Urban Design Guidelines establish the physical design framework for the area, to ensure that new built form demonstrates sustainability and high-quality design that reflects building practices that are appropriate for the area. These guidelines contain an Urban Design Vision, a detailed set of Guiding Principles, and guidelines that will establish urban design parameters for the Premier Gateway Employment Area. The guidelines have been prepared to provide guidance to developers, Town staff, and the public on the desired physical development.

## 1.3 How to Use the Guidelines

The Premier Gateway Employment Area Urban Design Guidelines provide guidance for consideration by developers and land owners designing developments within the Premier Gateway Employment Area. The Guidelines will also be used by Town staff to evaluate development applications. The objective of the guidelines is to provide direction related to site plan design, built form design, sustainability, Greenlands, Major Parks & Open Space, and to provide strategies for implementation. These aspects are interrelated and should be referenced in their entirety in the design and review of all projects.

The document contains general guidelines that apply to Phase 1A, 1B and 2A lands within the Premier Gateway Employment Area. Additional directions for Steeles Avenue, Trafalgar Road and the Gateway Areas are found within relevant sections to provide further insight on the design of these key areas.



*Existing greenfield lands*



# 2.0 Vision and Guiding Principles



## 2.1 Land Use Plan

Phases 1A and 2A of the Premier Gateway Employment Area includes four land use designations: Prestige Industrial Area; Gateway Area; Greenlands; and Major Parks & Open Space. Phase 1B includes four land use designations: Prestige Industrial Area; Business Commercial Area; Residential Special Policy Area, and Natural Heritage System

The Official Plan designates most of the Premier Gateway Employment Area as “Prestige Industrial Area”. The other major designation is “Gateway Area”. Lands in this designation are found south of Steeles Avenue on the west side of Winston Churchill Boulevard, north of

Highway 407; surrounding the intersection of Trafalgar Road and Steeles Avenue north of Highway 401; and on the east side of the Town boundary, east of James Snow Parkway, north of Highway 401.

Within Phase 1B, the other major destination is the “Business Commercial Area,” which is found north of Steeles Avenue surrounding its intersections with Hornby Road and Trafalgar Road.

The three main tributaries of the Sixteen Mile Creek that cross the area in a north-south direction are designated as “Greenlands” or

“Natural Heritage System”. In addition, Hornby Park, a Community Park at Sixth Line and Steeles Avenue is designated “Major Parks & Open Space.”

A number of buildings along both sides of Steeles Avenue are identified as “Building with Historic Significance,” or “Cultural Heritage Resource,” as well as along Hornby Road. In addition, a listed heritage cemetery is located at the northwest corner of Steeles Avenue and Eighth Line.

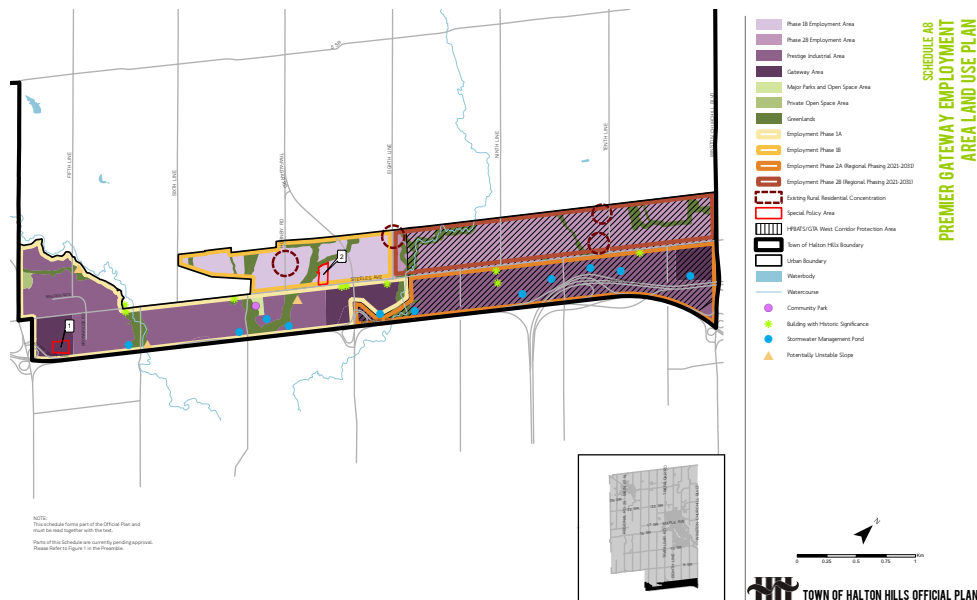
## 2.2 Urban Design Vision

The Premier Gateway Employment Area should reflect the vision and policy framework established in the Halton Hills Official Plan. The Urban Design Vision, as identified in Section D3.5 of the Official Plan is focused on accommodating “a range of industrial, office, commercial and institutional uses on full municipal services.” With respect to design, the development is to be “comprised of visually attractive buildings in aesthetically pleasing and sustainable environments....natural systems are a central element in helping to define the

Corridor.” In addition, design of development is to be representative of the area’s location as one of the gateways into the Town of Halton Hills and is to be “sensitive to the character of the area.”(Section D3.5.1).

Building on the specific directions in the Official Plan, as well as Provincial and Regional policy, the Premier Gateway Employment Area should reflect an urban character, including animated and comfortable streetscapes, support for public transit and alternative transportation, and

buildings that address primary streets. New developments should aim to incorporate cultural and natural heritage, and should provide physical and visual connections to Greenlands and Major Parks & Open Space. Gateway Areas should be given special architectural and landscaping treatments to create aesthetically pleasing and prominent entrances into the Premier Gateway Employment Area and the Town as a whole.



Premier Gateway Employment Area Land Use Plan, As Amended by OPA's 10 and 21



Draft Land Use Plan for Phase 1B Secondary Plan, Premier Gateway Employment Area



## 2.3 Guiding Principles

The Premier Gateway Employment Area Urban Design Guidelines are premised on the following Guiding Principles, which reflect the Urban Design Vision for the area:

### 1. Enhance Natural Features

- a. Preserve and enhance natural features through sensitive planning and design.
- b. Provide physical and visual connections to Greenlands and Major Parks & Open Space.
- c. Integrate stormwater facilities into site design.
- d. Use Low Impact Development (LID) techniques in the site design process.

### 2. Achieve a Strong Visual Identity for Trafalgar Road and Steeles Avenue

- a. Encourage a strong visual identity for Trafalgar Road and Steeles Avenue, through urban design, site planning, building design and landscaping.
- b. Design buildings to address Trafalgar Road and Steeles Avenue, with a strong secondary elevation on Highways 401 and 407 (where appropriate).
- c. Design Trafalgar Road and Steeles Avenue with a comfortable pedestrian-focused streetscape design that supports a multi-modal environment accommodating pedestrians, cyclists, and vehicles, with appropriate sidewalks or multi-use pathways, street furniture, lighting, and landscaping.

### 3. Develop Nodes at the Three Gateway Areas

- a. Focus the highest-quality development within the three Gateway Areas, located along Steeles Avenue where it intersects with Winston Churchill Boulevard, Trafalgar Road, and James Snow Parkway.
- b. Enhance development within the three Gateway Areas through features such as:
  - Taller, articulated building elements including towers and other features to create a prominent architectural presence;
  - Enhanced softscape and hardscape treatments, including street trees, vegetation, and unique paving;
  - Coordinated street furniture such as benches, lighting, waste receptacles, and bicycle racks;
  - Wayfinding and branding elements including banners, flags, and signage to reinforce entrances into the Premier Gateway Employment Area; and
  - Public art in areas with the highest visibility.



#### 4. Ensure a High Quality of Built Form and Landscape Development

- a. Encourage high-quality built form and landscape development that reflects sustainable building practices including:
  - Transit-supportive design that results in more efficient development, with access to transit and active transportation;
  - Streetscape design that facilitates goods movement while at the same time being pedestrian-oriented, which includes buildings that address main streets, reduced building setbacks, and active uses at-grade in Gateway Areas; and
  - Enhanced softscape and hardscape landscaping treatments along major streets.
- b. Provide for the provision of buffering and other measures to allow for a high quality environment for existing transitional uses.

#### 5. Provide Flexible Development to Accommodate Evolving Needs

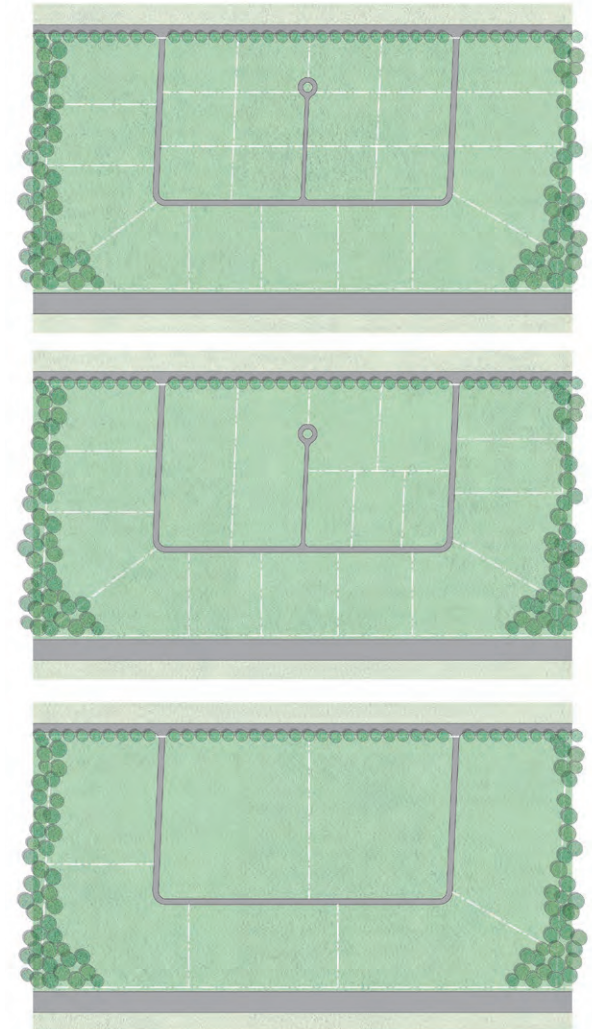
- a. Provide a framework of roads, including private roads, and infrastructure that facilitates goods movement and allows for maximum flexibility of development lot sizes, thereby accommodating the evolving needs of employment developments.

#### 6. Encourage the Protection and Adaptive Reuse of Heritage Elements

- a. Encourage the protection of identified heritage structures and their grounds, where feasible.
- b. Provide for the interface of new development and heritage elements with appropriate buffers and other transitional elements between them.
- c. Encourage the appropriate adaptive re-use of heritage structures within new developments.

#### 7. Encourage the Construction of Sustainable Buildings

- a. Promote the integration of Leadership in Energy and Environmental Design (LEED).
- b. Use Low Impact Development (LID) techniques in the site design process.
- c. Adhere to the Town of Halton Hills' Green Development Standards Study.
- d. Integrate white roof practices into the design of built form.





# 3.0 Site Plan Design Guidelines



*The design and organization of sites within the Premier Gateway Employment Area should promote safe and efficient circulation for multimodal transportation, including vehicles, pedestrians, and cyclists. The design of front yards and boulevards should be high-quality and pedestrian-supportive, resulting in comfortable, accessible, and attractive streets.*

*Buildings and main entrances should be located to frame and address public streets and should provide access to public open spaces. Pedestrian pathways should be clearly demarcated through landscaping, lighting, and signage, and should facilitate safe movement.*

## 3.1 Block Layout and Street Design

### Special Streets and Gateway Areas

The Premier Gateway Employment Area contains several streets that should be given the highest design attention due to their role as major corridors and gateways. These include:

#### Special Streets

**Steeles Avenue:** The main (east-west) corridor within the Premier Gateway Employment Area.

**Trafalgar Road:** The major (north-south) corridor through the Premier Gateway Employment Area and the major Gateway Area to the Employment Area and the Town from Highway 401.

**Highway 401 Frontage:** The lands located at the southern boundary of the Premier Gateway Employment Area along Highway 401, south of Steeles Avenue.

#### Gateway Areas

**Steeles Avenue, at Winston Churchill Boulevard:** A major Gateway Area located at the eastern boundary of the Employment Area, north of Highway 407.

**Steeles Avenue, at Trafalgar Road:** A major Gateway Area located in the centre of the Employment Area, with access from Highway 401.

**Steeles Avenue, at James Snow Parkway:** A major Gateway Area located at the western boundary of the Employment Area, with access from Highway 401.

These streets will significantly contribute to the character of the Premier Gateway Employment Area. Where appropriate, the guidelines provide specific direction for these areas.

## 3.1.1 Boulevards, Sidewalks, and Pedestrian Crossings

### Design Guidelines

#### General

- Public boulevards for future local streets other than Steeles Avenue and Trafalgar Road and within Gateway Areas, should be a minimum of 4.5 metres wide including a minimum 2.1 metre sidewalk, and should be provided on both sides of arterial and collector streets (existing and proposed).
- Sidewalks and walkways should be continuous, universally accessible, barrier-free and clearly designated.
- Sidewalk surfaces should be constructed of poured, brushed concrete, in accordance with Town standards.
- A minimum landscape strip of 3.0 metres should be provided within the public boulevard to accommodate street trees and landscaping.
- A continuous row of street trees is encouraged within the minimum landscape strip. All trees should have access to a minimum of 15 cubed metres of good quality soil (can be shared).
- Amenities like street furniture, wayfinding signage, lighting fixtures, and street trees should be incorporated in public boulevards throughout the Premier Gateway Employment Area.
- Curb ramps must provide barrier-free connections between the roadway and the boulevard.
- Mid-block pedestrian crossings with pedestrian activated signals should be included on long blocks.



#### *Steeles Avenue, Trafalgar Road, and Gateway Areas*

- Public boulevards should be a minimum of 6 metres with a minimum 2.1 metre sidewalk, and should be provided on both sides of Steeles Avenue and Trafalgar Road.
- Boulevard materials should incorporate higher-quality materials to demarcate these important locations.
- Gateway Areas should include distinct wayfinding and branding elements including banners, flags, and signage, as well as lighting standards and other street furniture, to reinforce their role as the entrances into the Premier Gateway Employment Area, and to create a distinct visual presence from Highways 401 and 407.
- Gateway Areas should include public art installations in the areas with the highest visibility.
- Special paving treatments for pedestrian crossings should be used in Gateway Areas to demarcate these important locations, and to facilitate safe pedestrian movement.



## 3.2 Building Location and Orientation

### Design Guidelines

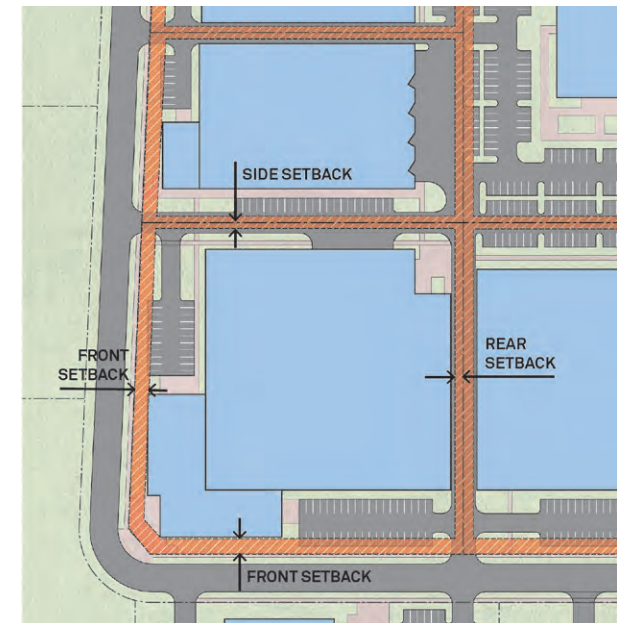
#### General

- Buildings should generally be located close to the front property line to define the street edge.
- Buildings should generally occupy a minimum of 60% of the total lot frontage.
- Industrial buildings should locate offices and entrance elements towards streets and intersections.
- Site development should be as efficient as possible including minimizing setbacks and maximizing building coverage. The minimum required development setback from Highways 401 and 407 should reflect the Ministry of Transportation (MTO) minimum requirements.
- Site layout and building orientation should be designed to maximize solar gain, the capture of solar energy through building elements like windows, as a form of passive heating for built form.
- Building orientation should respect existing sensitive land uses.

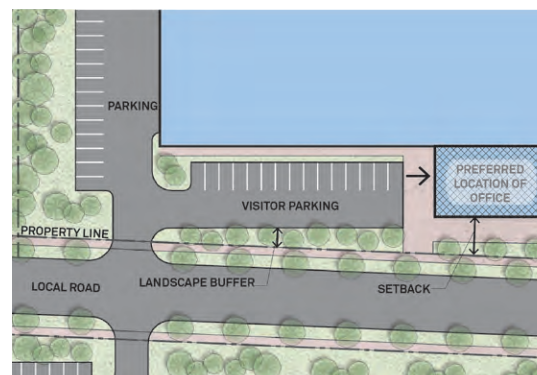
#### *Steeles Avenue, Trafalgar Road, and Gateway Areas*

- Buildings on lots at the intersection of Steeles Avenue and Trafalgar Road shall be oriented to the intersection, and entrance elements shall be encouraged to front on both Steeles Avenue and Trafalgar Road to enhance this major Gateway Area.
- Where a building has a frontage on Steeles Avenue or Trafalgar Road and a local street, office and entrance elements are encouraged to front both on Steeles Avenue or Trafalgar Road and the adjacent local streets to engage the corner.

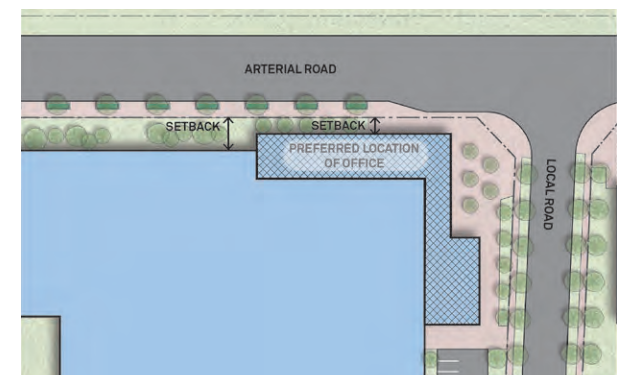
#### *Typical Street Setbacks*



#### *Orientation of Entrance Towards the Street*



#### *Setbacks from Arterial Roads*





## 3.3 Open Space and Landscaping

### Design Guidelines

#### General

- Where on-site open spaces are provided, or where development abuts public open space, buildings should be located to frame and address these open spaces. At least 50% of open spaces should be fronted by public streets and/or active building edges.
- High standards for planting density, quality and variety at main building facades on public street frontages, and in landscape buffers shall be applied, meeting or exceeding the applicable Halton Hills standards.
- Use of low maintenance, drought resistant, non invasive plants and trees native to Halton Region shall be encouraged for at least 50% of landscaped areas. For a list of native species refer to *Conservation Halton Landscaping and Tree Preservation Guidelines, Appendix 1* or the *Credit Valley Conservation Plant Selection Guideline*.
- On-site open spaces are encouraged to provide outdoor amenity space for employees in accessible areas.
- The design of parking areas for employee and customer parking should include landscaped islands and strips to screen the parking and mitigate the impact of paved areas.
- There should be a significant minimum landscape buffer provided where industrial uses abut residential uses. This buffer shall consist of plantings such as high shrubs and dense tree coverage that obscure views of industrial developments from the adjacent residential development.
- The perimeter of parking lots should be heavily landscaped with a minimum 3.0 metre buffer that includes a range of trees and vegetation.
- A landscape buffer of 15 metres is required adjacent to the historic sites of St. Stephen's Church and the historic house south of the church.
- The setback required by the Ministry of Transportation (from Highways 401 and 407) should be landscaped.
- Open space and landscape design should adhere to the principles of Low Impact Development (LID).

#### Steeles Avenue, Trafalgar Road, and Gateway Areas

- Steeles Avenue, Trafalgar Road and Gateway Areas should incorporate unique softscape and hardscape landscaping treatments that distinguish these important areas, including feature paving, planters, trees, shrubs, and other vegetation.



## 3.4 Access and Circulation

### Design Guidelines

#### General

- Access to development sites on major arterial roads such as Steeles Avenue and Trafalgar Road should be provided from local streets and/or private streets to minimize intersections and entrances.
- Joint access driveways between adjacent development lots should be encouraged where possible.
- Access for trucks (i.e. for servicing and loading) should not conflict with general passenger vehicle circulation. Separate entrances are encouraged where possible.
- Provide pedestrian connections from buildings to parking, public transit, and public spaces.
- Pedestrian walkway paving treatments should be clearly articulated and should differ in material and appearance from vehicular routes.
- The implementation of clear signage will assist orientation on public walkways and through public spaces.
- Tree planting and other landscaping

must not be an obstacle to the barrier-free path of travel.

#### *Steeles Avenue, Trafalgar Road, and Gateway Areas*

- New street connections to Steeles Avenue and Trafalgar Road shall generally be limited to a minimum interval between intersections of 120 metres and shall utilize existing or relocated streets where possible. This requires approval of the Region and must be supported by a traffic study.



*Typical Individual Access Driveways*



*Recommended Driveway with Joint Access Easement*



## 3.5 On-Site Parking

### 3.5.1 Surface Parking

#### Design Guidelines

- Parking should generally be located at the rear or in the interior side yard of the building. However, parking in yards abutting Highway 401 and 407 shall be limited with a significant landscape buffer between the parking area and the highway right-of-way, a portion of which can be in the required minimum MTO setback.
- Parking between the primary building elevation and the public street is discouraged.
- Opportunities to reduce the total number of parking spaces required within each site should be explored including shared parking between users and provision of formal parking to serve identified initial employees with reserve unpaved areas available, should the nature of the use change in the future.
- Large areas of surface parking for employees and customers should be broken-up through landscaping islands and pedestrian pathways to minimize their visual impacts.
- Dedicated pedestrian walkways should be incorporated through surface parking areas for employees and customers and defined through differentiated paving materials and landscaping. The amount of landscaping should be proportionate to the overall parking lot size.
- Where large surface parking areas are provided, including parking areas for trucks, opportunities to integrate Low Impact Development (LID) technologies should be implemented, including permeable paving and bioswales.
- Parking and access design should mitigate potential pedestrian and vehicular conflicts through clearly demarcated circulation routes, pavement marking, and signage.
- Pick-up and drop-off areas should be designed to not interfere with pedestrian circulation.
- Site and parking area design should address hydrant location and snow storage.
- On larger sites, way-finding signage should be included in all parking areas.





## 3.5.2 Structured Parking

### Design Guidelines

- Where a significant amount of parking is required for employees and customers, structured parking is preferable to surface parking.
- Above-grade parking structures fronting onto public streets and public open space should avoid the presence of blank facades, including providing active uses along the facades in those areas where feasible, and a range of approaches including landscaping, public art and architectural detailing.
- Access to structured parking should be from secondary streets or the interior of blocks. Pedestrian entrances should be visible and accessible, accessed from public streets or near main building entrances.
- Street trees and pedestrian-scaled lighting should be provided on all boulevards flanking a parking structure.



## 3.5.3 Bicycle Parking

### Design Guidelines

- Locate visitor bicycle spaces in visible and accessible locations at-grade, near building entrances and pedestrian walkways.
- Locate employee bicycle parking in a weather protected secure area with controlled access.
- The design of bicycle racks should not impede pedestrian movement and snow clearing.
- Bicycle lockers are encouraged, particularly for large office or industrial developments.

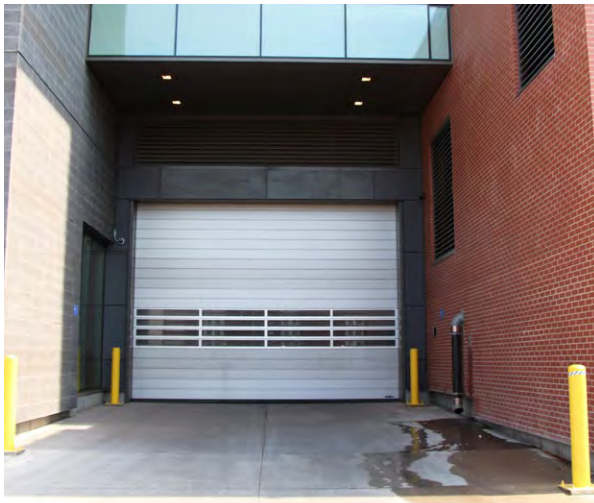




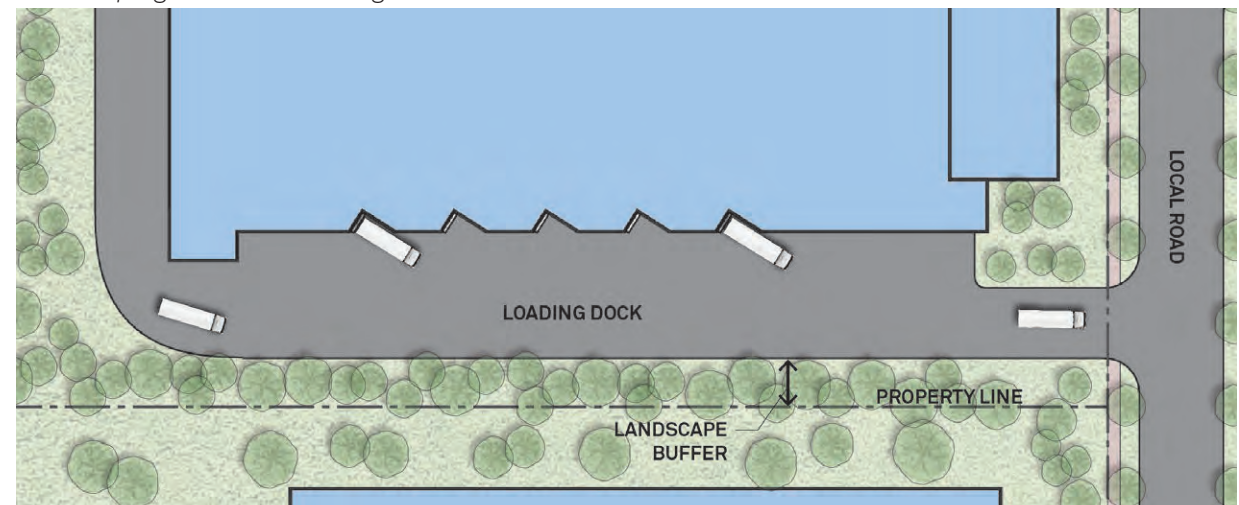
## 3.6 Servicing, Loading and Storage

### Design Guidelines

- Service, loading, and garbage areas should be located within main building structures, where possible, or in separate enclosed buildings where it is not possible to locate them in the main building.
- Loading, service, and garbage storage areas should not face Steeles Avenue or Highway 401, and should be in areas of low visibility such as the interior side or the rear of buildings. Wherever site planning constraints necessitate loading areas in visually prominent locations, they shall be screened with architectural elements.
- The coordination of service areas for delivery, loading and garbage pick-up is encouraged, to reduce the number of entrances along public streets and within parking areas.
- Outdoor garbage containers should be limited and screened by wall features or incorporated into built form, as should feature significant landscaping. These areas must be compatible with building design and materials.
- Service areas should be separated from pedestrian amenity areas, open spaces, and walkways.
- When occupied, loading areas should not impede on-site vehicular circulation.



*Landscaping Buffer for Loading Areas*



## 3.7 Stand-Alone Signage

### Design Guidelines

#### General

- Permitted signage types must comply with the Halton Hills Sign By-Law.
- Signage along Highway 401 must comply with MTO standards.
- Free standing signs and site signs should generally be limited to a single free standing monument sign with graphic consistency.
- For free standing and site signs, where the principal frontage of a site exceeds 100 metres, one additional site sign should be permitted for every additional 80 metres of frontage in excess of 100 metres.
- Front lit signage, back lit individual block letter signs and/or logo signage is required, particularly those that face the public street or are parallel to a pedestrian walkway.
- Electronic messages on digital signage with visible effects during the message transition, including fading, flashing, or motion are discouraged for safety reasons.
- To reduce the impacts of light pollution, the illumination of signage is discouraged during off-peak hours.

#### *Steeles Avenue, Trafalgar Road, and Gateway Areas*

- As part of a broader gateway design strategy, a signage feature may be erected to identify Gateway Areas as entry points to the Town of Halton Hills. The feature should be well-integrated into a broader gateway feature, including landscaping, seating and/or public art, and be of a scale that is visible from Trafalgar Road, Winston Churchill Boulevard, James Snow Parkway, or Steeles Avenue.



## 3.8 On-Site Lighting

### Design Guidelines

- All pedestrian and parking areas shall be well lit to promote safety, security and comfort during hours of office use.
- Pedestrian-scale light standards or illuminated bollards should be used in employee and customer parking areas, along pedestrian walkways, and other pedestrian zones.
- Accent lighting to emphasize built forms and landscape elements is encouraged.
- The design and location of lighting must consider the impacts of light pollution, energy efficiency, and other potential negative impacts. The mitigation of light pollution is particularly important where commercial and industrial uses abut residential areas.
- Light emitting diodes (LEDs), solar power, road reflectors and other alternative lighting and energy sources should be encouraged for energy efficiency and should be dark sky compliant.
- Lighting shall comply with applicable Town standards.





## 4.0 Built Form Design Guidelines



*Built form within the Premier Gateway Employment Area should frame public streets and open spaces and create a well defined street edge. Buildings located within Gateway Areas should be distinguished by special architectural treatments.*

## 4.1 Height and Massing

### Design Guidelines

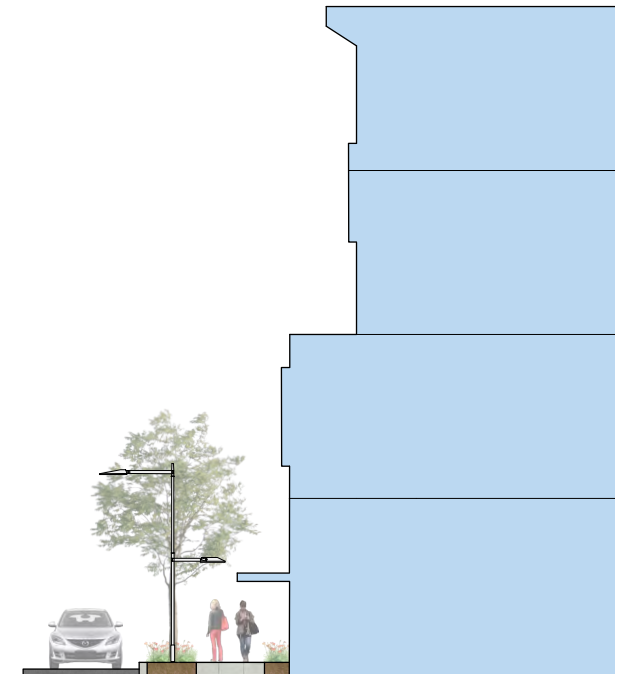
#### General

- The maximum building height permitted within the Premier Gateway Employment Area ranges from 2 to 8 storeys.
- Building massing within, and between, developments should be varied and may incorporate stepbacks, changes in height, or strategic setbacks of sections of the building.
- All roof top mechanical units are to be screened in all directions. Such screening shall be compatible with building design in form, materials and colour.

#### Steeles Avenue, Trafalgar Road, and Gateway Areas

- Where taller buildings are proposed in the Premier Gateway Employment Area, they should be concentrated at the key Gateway Areas, as well as along Highway 401, Highway 407 and Trafalgar Road.
- Consideration may be given to increased height limits beyond 6 storeys for buildings within Gateway Areas based on review of a specific development.
- Gateway Areas should feature appropriate massing, building projections, recesses at grade, lower storey design and open space treatments to reinforce the prominence of these locations.

*Taller buildings should be varied through the use of stepbacks and changes of height, and should feature open space treatments at grade.*



## 4.2 Street-Oriented Design

### Design Guidelines

#### General

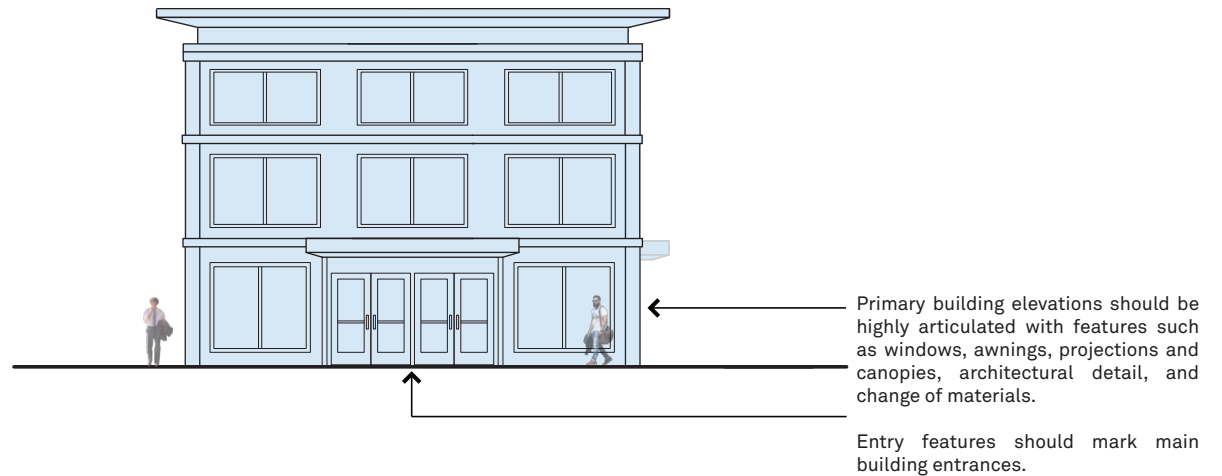
- Buildings should generally be designed and oriented to address Steeles Avenue and other local roads. Where a building has frontage on Highways 401 or 407, it should be treated as a highly-visible secondary elevation as outlined in Section 4.3.
- Buildings and main entrances should be designed to create a consistent street edge and frame adjacent streets, as well as amenity spaces, and parking areas.
- Primary building elevations are those elevations that face a public street. Blank facades along the length of a public street are to be broken up. Primary building elevations should be designed with significant architectural features that articulate horizontal and vertical elements of the building face, including windows, awnings and canopies, projections and recesses, architectural detail, and change of materials.

- Buildings that have frontage on public streets should emphasize architectural elements that promote pedestrian comfort. These may include the use of canopy structures and arcades.
- Exterior building materials should be low maintenance and visually appealing.

#### *Steeles Avenue, Trafalgar Road, and Gateway Areas*

- Buildings fronting on Steeles Avenue and Trafalgar Road that include office uses should be encouraged to locate such uses at-grade adjacent to the street, and should provide a significant number of windows in order to establish a strong connection to the street.

#### *Primary Building Elevations*

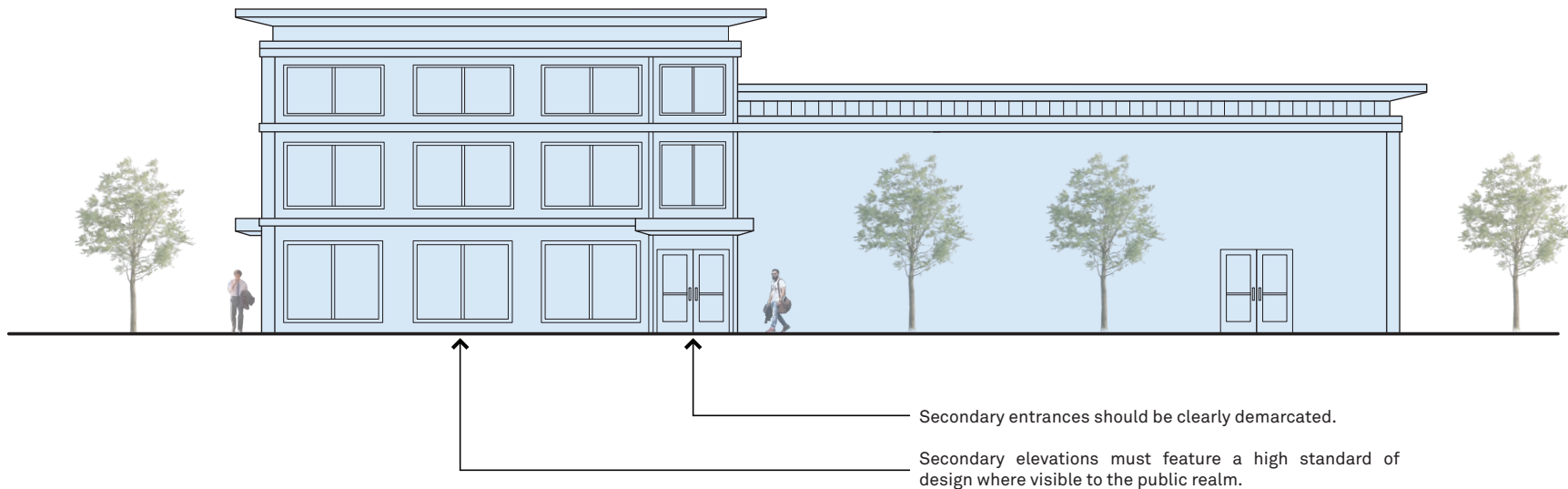


## 4.3 Secondary Elevations

### Design Guidelines

- Secondary building elevations are those elevations that do not face a public street. Such elevations should complement the primary building elevation, and where visible to the public, should feature a comparable level of design and be articulated by horizontal and vertical building elements including windows, projections and recesses, and architectural detail.
- Where a building has a secondary frontage on Highways 401 or 407, a significant amount of articulation is encouraged to create visual interest, and may include building projections, material variations, windows, signage, and interior active uses (i.e. offices, common space).
- Where secondary entrances are provided within secondary elevations, they should be clearly demarcated.
- Exterior building materials should be low maintenance and visually appealing.

#### *Secondary Building Elevations*





## 4.4 Heritage

### Design Guidelines

- Where feasible, the preservation and imaginative integration of historic structures within the context of commercial and industrial development is encouraged.
- The design of new structures should be compatible with neighbouring heritage sites and structures. However, the artificial application of historical building styles to modern construction methods is to be discouraged.

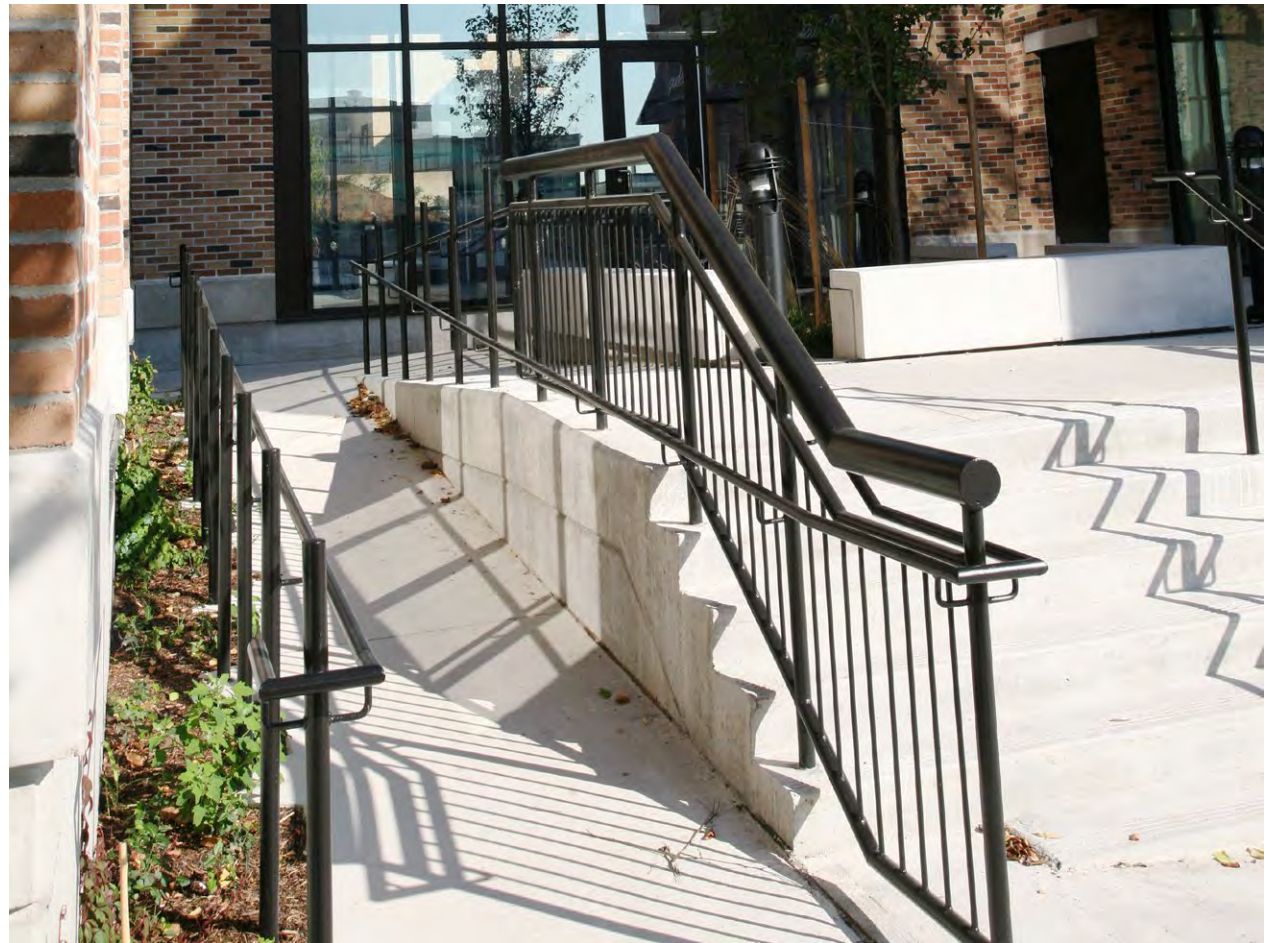


*St. Stephen's Church*

## 4.5 Access and Entrances

### Design Guidelines

- Main building entrances should be located along the primary building elevation.
- Architectural features to emphasize entry areas and other special building areas and relieve large expanses of solid wall are encouraged. Considerations may include canopies, awnings, double-height windows, or taller architectural elements. Entrances shall be appropriate to building scale and form.
- Building entrances should be barrier-free, and should incorporate ramps, automatic doors, and other elements that contribute to accessibility.
- Main building entrances must be accessible by public walkways and/or pedestrian sidewalks.



## 4.6 Signage and Lighting

### Design Guidelines

- Signage and lighting design should adhere to the relevant by-laws for the Town of Halton Hills and MTO.
- Signage should be integrated into building design to reduce clutter.
- Accent lighting to emphasize built forms and landscape elements is encouraged.
- Rooftop signs are not encouraged.
- Signage on sites adjacent to a highway should be of a scale appropriate to the speed and scale of the highway environment. On sites adjacent to Highways 401 and 407, the size limits of signage on building facades facing the highways may be increased to a total surface area of no more than 20% of the building wall face.
- Electronic messages on digital building signage with visible effects during the message transition, including fading, flashing, or motion are discouraged for safety reasons.
- To reduce the impacts of light pollution, the illumination of digital signage is discouraged between off-peak hours.
- Light emitting diodes (LEDs), solar power, road reflectors and other alternative lighting and energy sources should be encouraged for energy efficiency.







Existing Building Signage within the Premier Gateway Employment Area



## 5.0 Environment and Open Space Guidelines

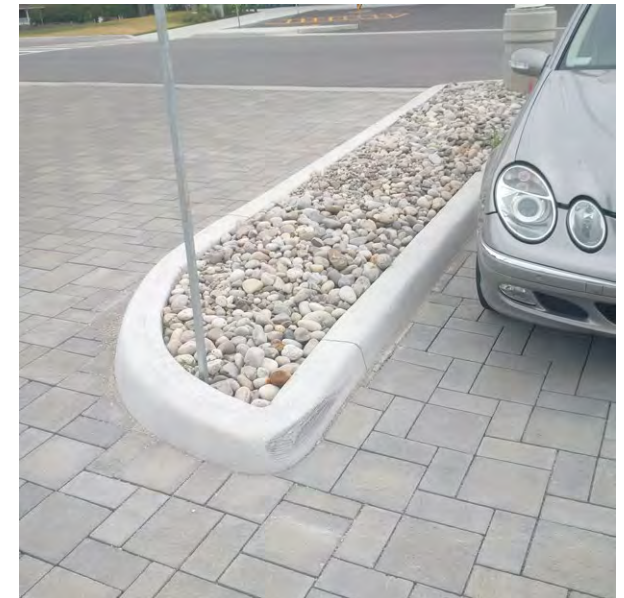


*The protection and regeneration of the natural landscape should be encouraged through site planning and design.*

# 5.1 Sustainability

## Design Guidelines

- Sustainable site and building design and construction techniques in new development that reduce energy and water consumption, and improve air quality, water quality, and waste management should be encouraged and promoted.
- In addition to the protection and enhancement of trees and other natural features in the Greenlands and Major Parks & Open Space designations, additional trees should be planted on sites and public streets as part of the development process.
- Pedestrian, cycling, and transit should be promoted to encourage alternatives to automobile use.
- The use of recycled materials for building construction is encouraged.
- Construction of solar-ready buildings should be encouraged including consideration of required roof loads, as appropriate.
- Use Low Impact Development (LID) techniques in the site design process to reduce impermeable surfaces and stormwater runoff.
- Integrate white roof practices into the site and built form design to contribute to reduced energy use.
- Built form should be oriented to maximize exposure to natural light.

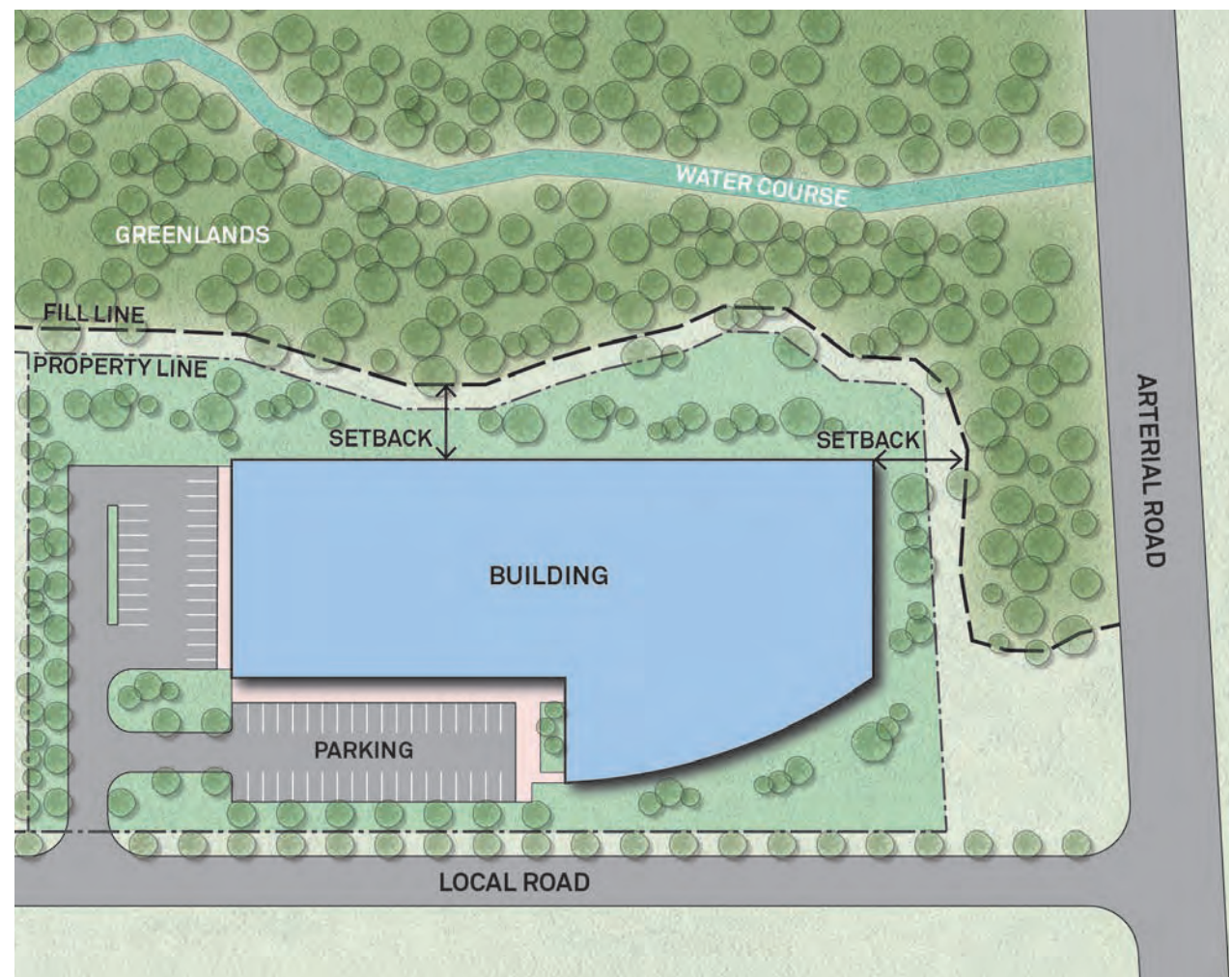


## 5.2 Greenlands

### Design Guidelines

- Contribute to the preservation and enhancement of lands in the Greenlands designation through sensitive site planning and design including:
  - Prohibiting the planting of non-native and invasive species particularly adjacent to the top of bank of valleys and ravines or natural features.
  - Providing naturalized buffers between development and Greenlands.
  - Providing physical and visual connections to lands in the Greenlands designation.
  - Encouraging outdoor uses and facilities such as picnic areas, patios, employee recreation facilities and pedestrian areas in locations adjacent to Greenlands so that these natural features may act as amenities for users.

*Site Planning Near Greenlands*





## 5.3 Major Parks & Open Space

### Design Guidelines

- Provide strong pedestrian and recreational pathways to connect parks and open spaces to the natural lands, as part of a pathway system that connects to the Sixteen Mile Creek system. In addition, provide physical and visual connections from parks to adjacent public streets and other features.
- Provide built elements such as trellises or pavilions to act as landscape markers that define the focal importance of parks and provide passive recreational elements.



View of Hornby Park

## 5.4 Stormwater Management Ponds

### Design Guidelines

- Stormwater management ponds should be integrated into developments using sustainable design strategies.
- Where appropriate, stormwater management ponds should be considered important site amenities and should be accessible and visible to employees and site visitors.
- Edges of stormwater management ponds abutting Greenlands should remain naturalized.



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# 6.0 Implementation



The Premier Gateway Employment Area Urban Design Guidelines are to be utilized in conjunction with the Official Plan and Zoning By-law to act as a framework for development implementation.

These guidelines should be considered a flexible tool to illustrate both the general and site specific framework. The urban design guidelines aim to fulfill the Town's vision and objectives for the Premier Gateway Employment Area, in order to ensure high-quality development with a strong visual identity.

The intent of the guidelines is to establish a planning framework that will help the area thrive as a dynamic, accessible, and economically competitive employment area and that reflects urban design aesthetics and sustainable development practices appropriate for this area. They are not to be viewed as requirements forcing adherence to all guidelines. Specific plans for site development and individual buildings will vary, and this diversity is encouraged.

## 6.1 Review and Approvals Process

The guidelines will be utilized at the various review stages of development approvals, from Draft Plan of Subdivision through to Site Plan Approvals. In addition, they offer a review function for design input from the municipality into detailed design at the building permit stage.

Applications for approvals for specific properties within the Premier Gateway Employment Area must demonstrate how their planning and design fulfills the Urban Design Vision and Guiding Principles, and the specific guidelines applicable to the application. Third party peer review may be required for large projects.

The process involves the following stages:

### 1. Draft Plan of Subdivision

Applications requesting approval for Draft Plan of Subdivision should be evaluated in the context of these guidelines. Specific review should be given to the applicable guidelines within Site Plan Design Guidelines, Built Form Design Guidelines, and Environment and Open Space Guidelines.

Applications should provide a description of how the Draft Plan of Subdivision implements the applicable guidelines for the site in question.

### 2. Site Plan Review

Applications for Site Plan Approval will be subject to review of these guidelines and the Town's applicable planning, construction and engineering requirements.

Applications should provide a description of how the development implements the guidelines within both their general and site specific context. Applications shall be assessed with respect to the Site Plan Design Guidelines, Built Form Design Guidelines, and Environment and Open Space Guidelines.

### 3. Building Permit

Building permits will be reviewed for confirmation that individual developments will be implemented in accordance with Site Plan Approvals and with the design intent of the guidelines for built form and landscaping.



## 6.2 Partnership Opportunities

To assist in the implementation of the guidelines, the Town of Halton Hills should consider potential partnership opportunities with other governmental bodies, agencies, and key stakeholders to realize shared goals and to uphold the document's Urban Design Vision. Partnership opportunities may include:

### The Ministry of Transportation:

- The Ministry of Transportation (MTO) is responsible for establishing setbacks from Highway 401 and 407. Continued cooperation and engagement between the Town of Halton Hills and MTO is required to ensure that all design guidelines reflect a shared vision for these highways and abutting land uses.

### Developers, land owners, business owners, employees, and residents:

- The Town should monitor the implementation of the urban design guidelines on an ongoing basis to ensure that it results in development that implements the Urban Design Vision and reflects a high quality of design.



## 6.3 Urban Design Review Process

This document **should be reviewed** every five years and updated where required to reflect changes to building design standards, sustainable design, and the Premier Gateway Employment Area's existing and emerging context.

Updates to the guidelines must reflect the intent of the Official Plan and the Zoning By-Law, and should reflect the overall Urban Design Vision and Guiding Principles established for the area.



*Steeles Avenue and Sixth Line South*





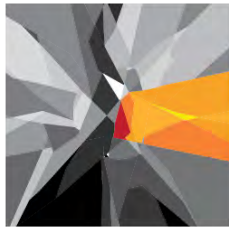
# DxG

*Destination*  
DOWNTOWN

## APPENDIX II DESIGN GUIDELINES



**The Planning Partnership**  
PLAN B Natural Heritage  
SCS Consulting Group  
NBLC  
Cole Engineering  
BRAY Heritage  
Meridian Planning Consultants



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## 1.0 Introduction

The Downtown Georgetown Design Guidelines work together with the policies of the Downtown Georgetown Secondary Plan to provide the Town with a sound and rational framework for public investments and coordinated assessment and regulation of private development proposals.

The Design Guidelines will direct and shape the ongoing development of the Downtown in a balanced manner and according to the principles of good urbanism and high-quality architectural design. In particular, the Design Guidelines seek to protect and reinforce the Downtown's distinct history and built characteristics, while enabling investment and revitalization opportunities through appropriate development, as market and demographic conditions evolve.

The Design Guidelines will help to shape the public and private realms, including streets, public spaces and buildings, as they change and evolve, to achieve the vision of the Downtown Georgetown Secondary Plan. These Guidelines are meant to achieve continuity and compatibility with the existing historical fabric while, at the same time, enable individual expression in new developments.



## 2.0 Design Objectives

The following objectives guide the physical form and character of development of the Public and Private Realms in Downtown Georgetown.

- Ensure an attractive and distinctive Downtown that has outstanding architecture that both respects the heritage character and sits well within the context of visual diversity, interest and beauty;
- Promote sensitive integration of new development with existing development through the requirement for compatible development;
- Establish a variety of beautiful public gathering spaces to support cultural events, festivals and community life throughout the year including interconnected park spaces, trails and sidewalk networks;
- Create vibrant, safe and comfortable pedestrian-oriented streets that enhance mobility for pedestrians, cyclists and drivers and will support existing and future transit;
- Promote a mix of uses in a variety of building forms, including a range of housing types and opportunities for retail, commercial and community uses;
- Protect and enhance natural features while broadening opportunities for public access, enjoyment, education and stewardship;
- Demonstrate high-quality design in new development and incorporate best practices that respect and complement the character of Downtown Georgetown and its adjacent neighbourhoods;
- Incorporate sustainable development and construction practices to maximize resource conservation; and,
- Create a fine-grained, human scaled urban fabric.

Each of the sites that make up the Downtown are unique – from their physical location, size and shape to their surrounding context of existing streets, buildings, uses and heritage considerations, and will have their own unique set of requirements for development. As such, some of these guidelines will be more important / applicable than others, depending on the specific context, scale and use of each site. This will be assessed on a site-by-site basis.







## 3.0 Design Guidelines for the Public Realm

As the population of Downtown Georgetown continues to grow, it will require a public realm that continues to support and enhance the needs of its existing residents, new residents and visitors. This includes a variety and hierarchy of spaces for special events, community gathering and every day activities such as walking, sitting, socializing and engaging in street life, actively and passively. As such, the Public Realm Network for Downtown Georgetown is comprised of a diversity of Park Spaces and Streetscapes.

A comprehensive understanding of how the components of the Public Realm Network work together and complement each other, and their adjacent uses, will lead to a more connected, accessible and logical network of pedestrian friendly spaces throughout Downtown Georgetown. Moving people into, out of, and through the community easily and safely, and providing a variety of spaces for socializing, special events and recreation, is a priority.



### 3.1 Streetscapes

Collectively, the Streetscapes in Downtown Georgetown comprise a substantial percentage of community open space – which solidifies their importance as a defining feature of the Public Realm Network. Streets and lanes can be engaging and safe outdoor places with beautiful trees and plants, seating, shade and Public Art for everyone to enjoy.

#### The Streetscape Hierarchy

The Streetscape Hierarchy in Downtown Georgetown is comprised of the following:

- Main Street;
- Guelph Street;
- Mill Street;
- Church Street; and,
- Local Streets.



## Guidelines for all Streetscapes

The following Design Guidelines apply to all streetscapes within Downtown Georgetown:

- AS1. All Streets will be safe, accessible, secure and shall implement the relevant policies of the Ontarians with Disabilities Act;
- AS2. The design of all streets shall include defined and, wherever possible, continuous zones for plantings, street furnishings, utilities, pedestrian sidewalks, bicycle paths and vehicular pavements;
- AS3. All streets shall include sidewalks on both sides of the street. Sidewalks on Main Street and on Guelph Street shall be a minimum of 2.1 metres in width, wherever possible. Sidewalks on all other streets shall be a minimum of 1.8 metres in width;
- AS4. A comprehensive and consistent approach to the design of the elements that make up the street environment is essential to enhance the character of the downtown and contribute to creating a sense of place. These elements include street furnishings and plantings;
- AS5. A coordinated family of street furnishings shall include street lights, seating, waste and recycling receptacles, community information boards, bollards, bicycle lock-ups, paving, and planters;
- AS6. Plantings include street trees, shrubs, annuals and perennials; street trees shall be included, wherever possible, acknowledging the potential constraints posed by below and above ground utilities. Where street trees are not possible, opportunities for other place-making, place enhancing elements such as public art, shall be explored;
- AS7. A comprehensive planting and soils strategy will be based upon species diversity, resiliency and urban tolerance; and,





- AS8. Public art opportunities shall be integrated in the downtown, refer to Town of Halton Hill Public Art Strategy.



## Additional Guidelines for Main Street

Main Street, from Maple Avenue to Guelph Street, is characterized by different land uses, building forms, setbacks and street conditions. The generous residential lots with stately homes and large setbacks at the south end transition through a traditional Main Street with buildings and storefronts located close to the sidewalk to an eclectic mix of buildings and lot pattern towards the north end. As such, special consideration will be given to these contexts with respect to design of the street, the public realm and the adjacent built form.

MS1. Within the South Portion of Main Street, the current condition and approach to the streetscape is to be continued and enhanced:

- On-street parking shall continue to be accommodated along the north side of the street; consideration shall be given to demarcating these areas through decorative paving;
- A 2-way bicycle path shall be created through widening of the existing sidewalk;
- Large canopy street trees shall be planted between the sidewalk and the property line;

MS2. Within the Central Portion of Main Street, the current condition and approach to the streetscape is to be continued and enhanced:

- On-street parking shall continue to be accommodated along both sides of the street; consideration shall be given to demarcating these areas through decorative paving;
- Pedestrian-scaled street lights are encouraged;
- A 2-way bicycle path may be created through;
  - Removal of on-street parking from one side of the street; or,
  - Removal of the centre landscaped median;

MS3. Within the North Portion of Main Street, the current condition and approach to the streetscape is to be improved and enhanced:

- On-street parking shall continue to be accommodated along one side of the street; consideration shall be given to demarcating these areas through decorative paving;
- Pedestrian-scaled street lights are encouraged; and,
- A 2-way bicycle path shall be created.



# MAIN STREET - SOUTH

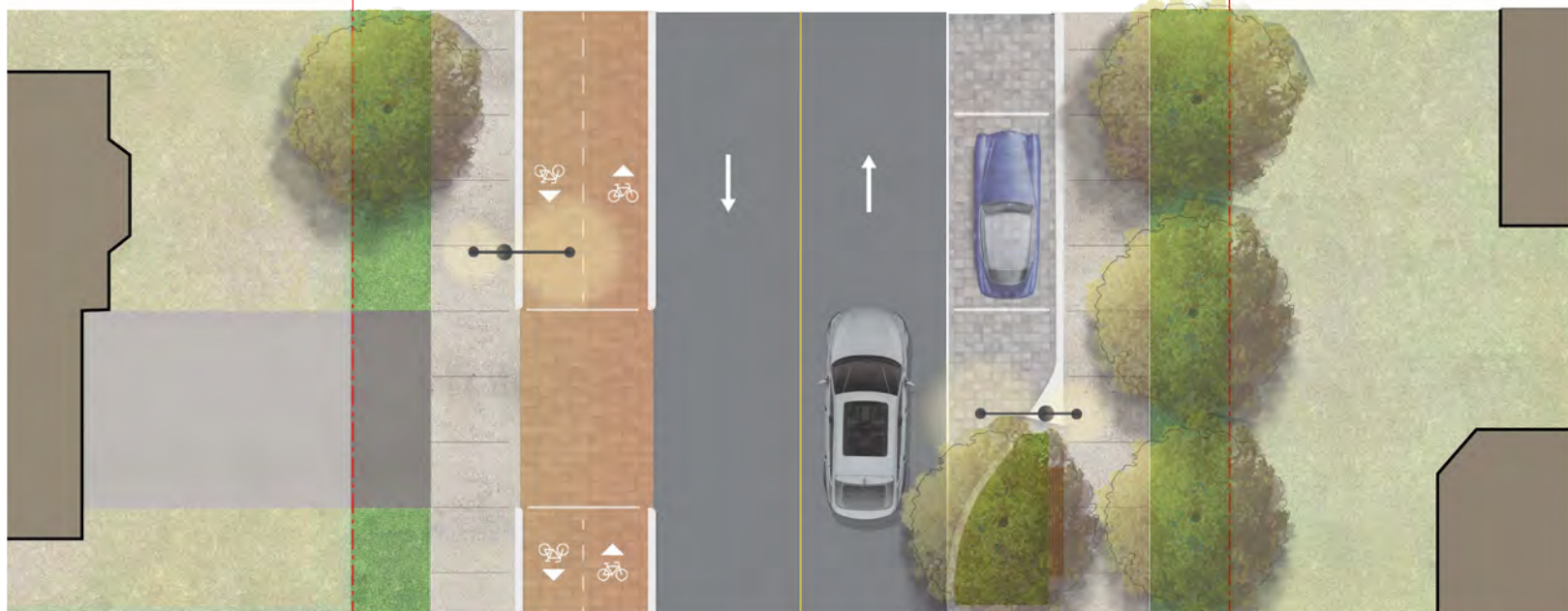
20.0 m ROW

PROPERTY LINE

PROPERTY LINE



1.8 m	2.1 m	3 m	3.35 m	3.35 m	2.5 m	2.1 m	1.8 m
PLANTING ZONE	SIDEWALK	BIKE LANES	TRAVEL LANES		PLANTING / PARKING	SIDEWALK	PLANTING ZONE



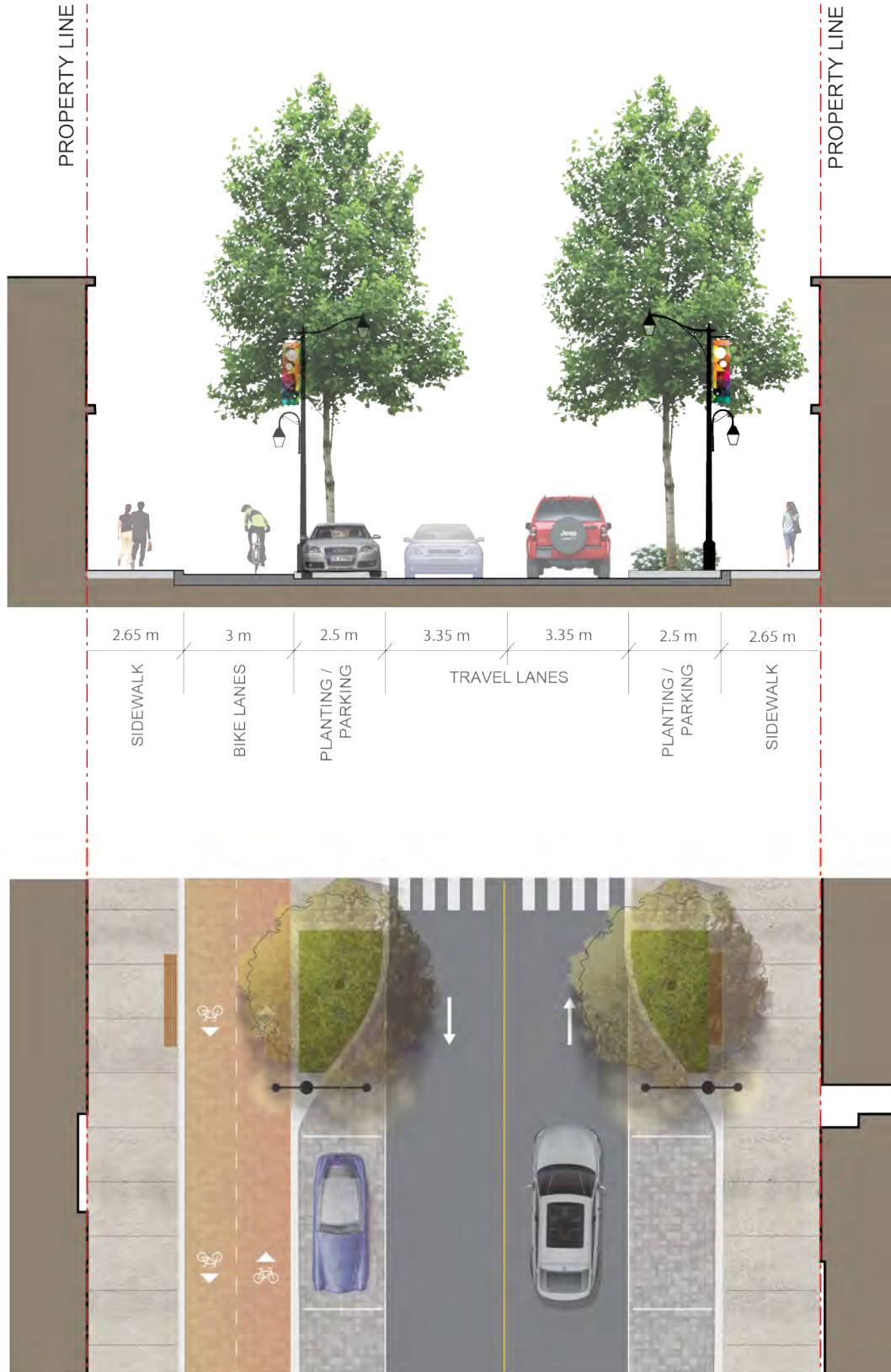
# MAIN STREET - CENTRAL

with median 20.0 m ROW



# MAIN STREET - CENTRAL

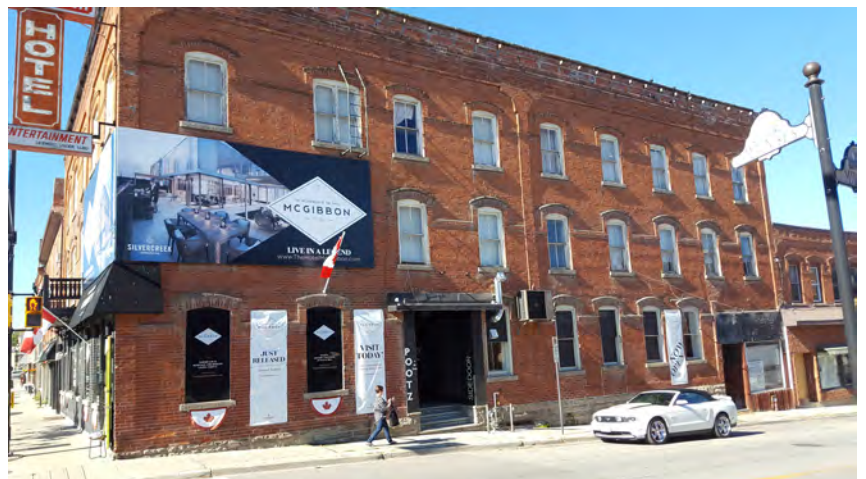
without median 20.0 m ROW





MAIN STREET - NORTH  
20.0 m ROW







## Additional Guidelines for Guelph Street

Guelph Street (Highway 7) is a major road that bounds the Downtown and connects it to the rest of the community and beyond. As such, it has a distinct identity and character that can be enhanced with the following improvements:

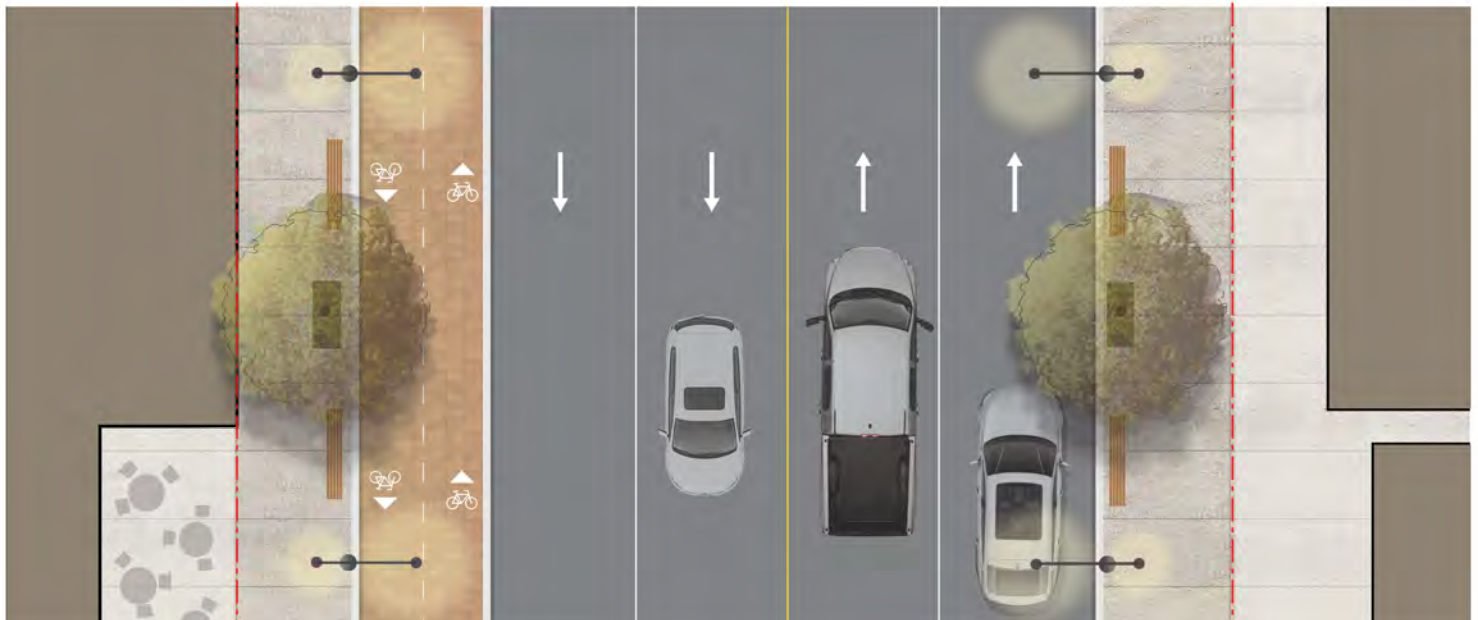
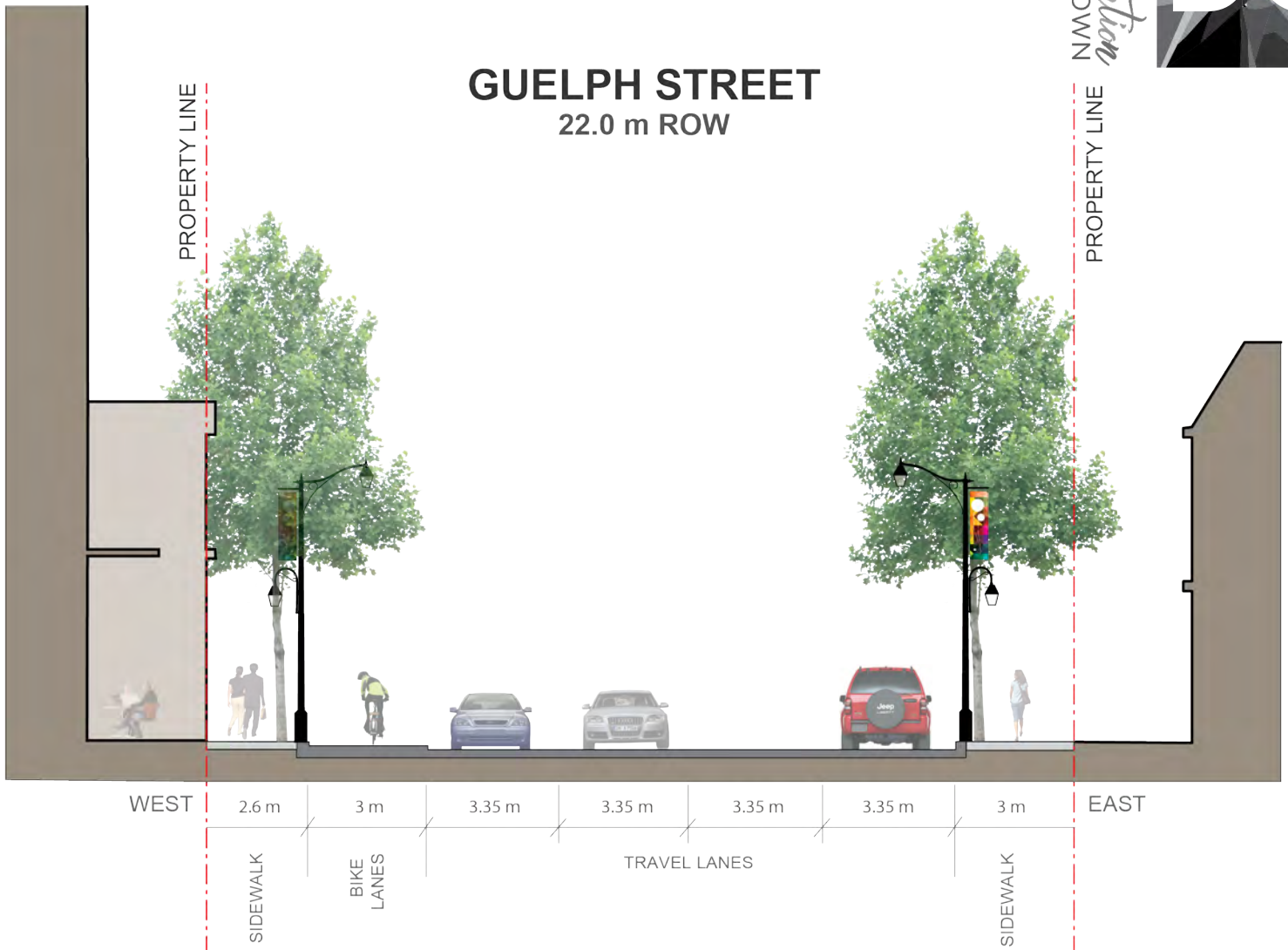
- GS1. Sidewalks shall provide space for outdoor cafes and retail uses. Visual and textural materials are to be considered for pedestrian paving to clearly delineate the pedestrian thoroughfare for the visually impaired and to help limit encroachments;
- GS2. Trees are to be located along the curb edge and shall be protected by tree grates and guards;
- GS3. Pedestrian scale decorative lighting shall illuminate the sidewalks. Where space permits, hardscaped boulevards shall line the street, separating pedestrians from vehicles to facilitate safe pedestrian movement;
- GS4. On-street parking, where possible and appropriate, shall be accommodated on Guelph Street;
- GS5. Where pedestrians and cyclists are meant to cross Guelph Street, the street environment shall provide safety features, which may include pedestrian islands, lit crosswalks, clear signage and/or well-marked routes;
- GS6. Intersections along Guelph Street shall be considered as high priorities for intersection improvements that increase the safety for pedestrians and cyclists; and,
- GS7. Through redevelopment of the parcels south of Guelph St, the Town shall consider a road widening of approximately 3.0 metres.



extensive sidewalks

# GUELPH STREET

22.0 m ROW





## Additional Guidelines for Mill Street

Mill Street is a local road that connects the residential neighbourhoods west of Main Street to the Downtown and beyond, to the GO Station area. As such it has a distinct function and character that can be enhanced with the following improvements:

- mS1. The sidewalk on the north side shall be expanded to incorporate street trees and a raised bike lane; and,
- mS2. The sidewalk on the south side shall incorporate street trees.



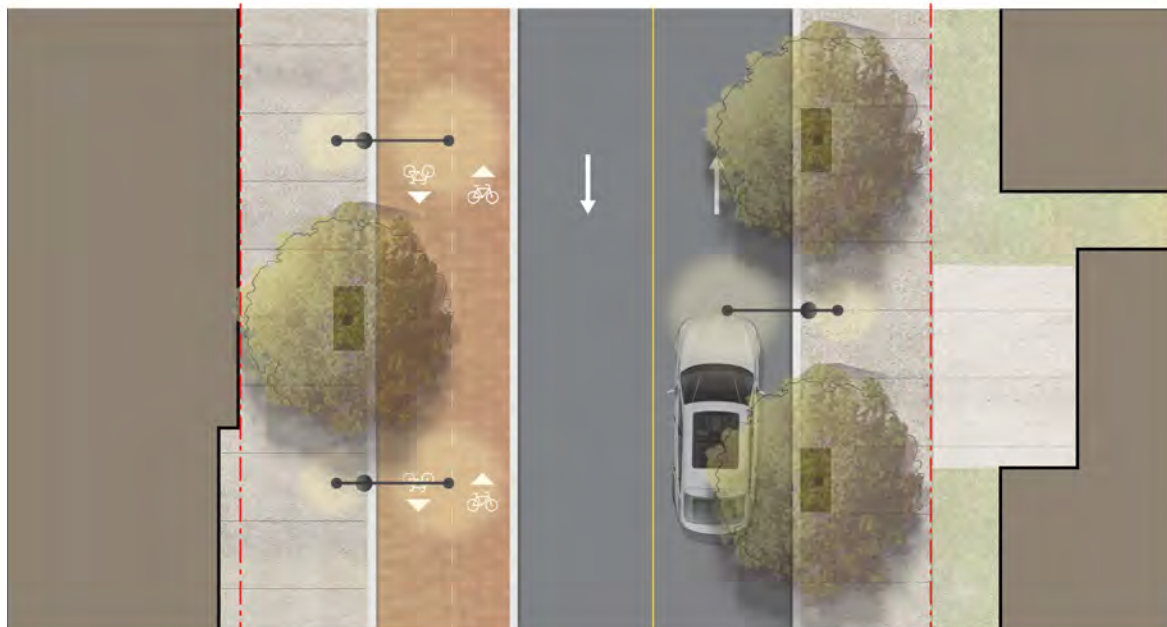
street trees



raised bike lane

# MILL STREET

15.0 m ROW





## Additional Guidelines for Church Street

The section of Church Street, between Main and Market Streets, has a unique character that results from the mix of community uses and buildings that are located on the south side of the street. The concentration of these uses, in combination with the townhouses located close to the street, on the north side, provides the opportunity to create a “shared street” that supports the library/cultural centre functions/events and an enhanced streetscape transition between the townhouse units and the public realm. On this basis, the following improvements shall be considered:

- CS1. Reduction of the roadway pavement width and removal of the lay-by parking;
- CS2. Relocation of the sidewalk on the north side of the street to allow for a landscaped zone between the Townhouse front doors and the roadway;

- CS3. Replacement of the barrier curbs with either flush or rolled curbs, in combination with decorative bollards to delineate the vehicular travel lanes;
- CS4. Decorative pavement across the public walkways, roadway and private areas in front of the library/cultural centre;
- CS5. Decorative street lights/fixtures that address roadway and pedestrian zones; and,
- CS6. An allee of canopy trees along street line in front of the church and library/culture centre.



# CHURCH STREET 'SHARED STREET'

12.0 m ROW

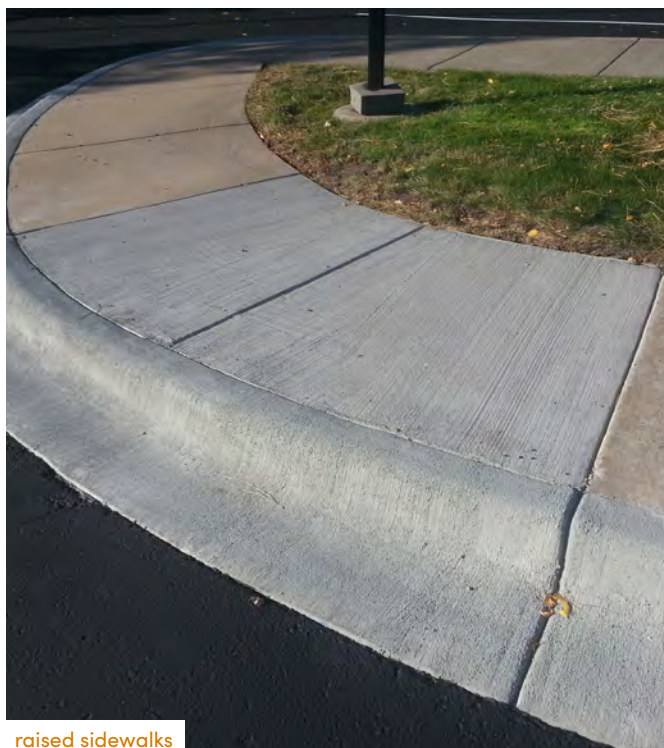




## Additional Guidelines for Local Streets

Local Streets are the smaller scaled, quieter public spaces where daily life happens - children play, neighbours meet, dogs are walked and, in some cases, street parties can take place. On this basis, the following improvements shall be considered:

- LS1. Where space permits, hardscaped boulevards shall line the roads, separating pedestrians from vehicles to facilitate safe pedestrian movement;
- LS2. Street lighting is to illuminate both the pedestrian and vehicular realm; and,
- LS3. Sidewalks are to be provided on both sides of all Local Streets.



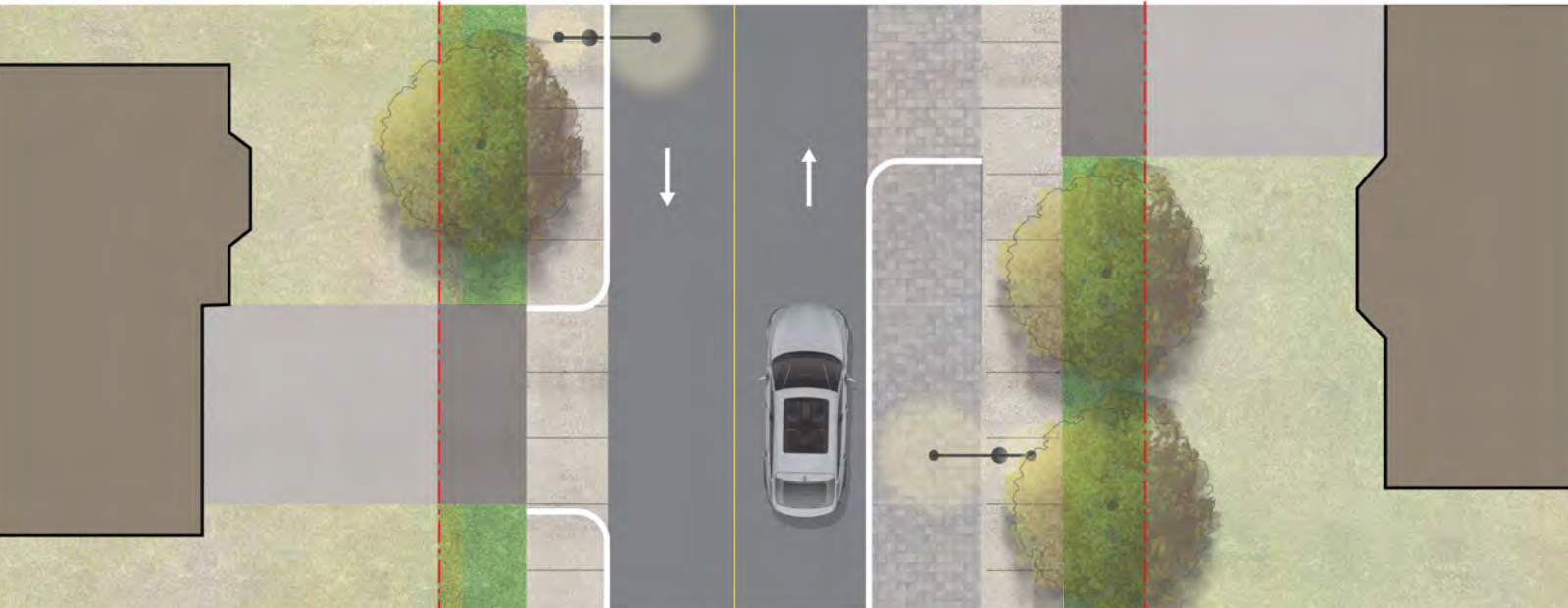
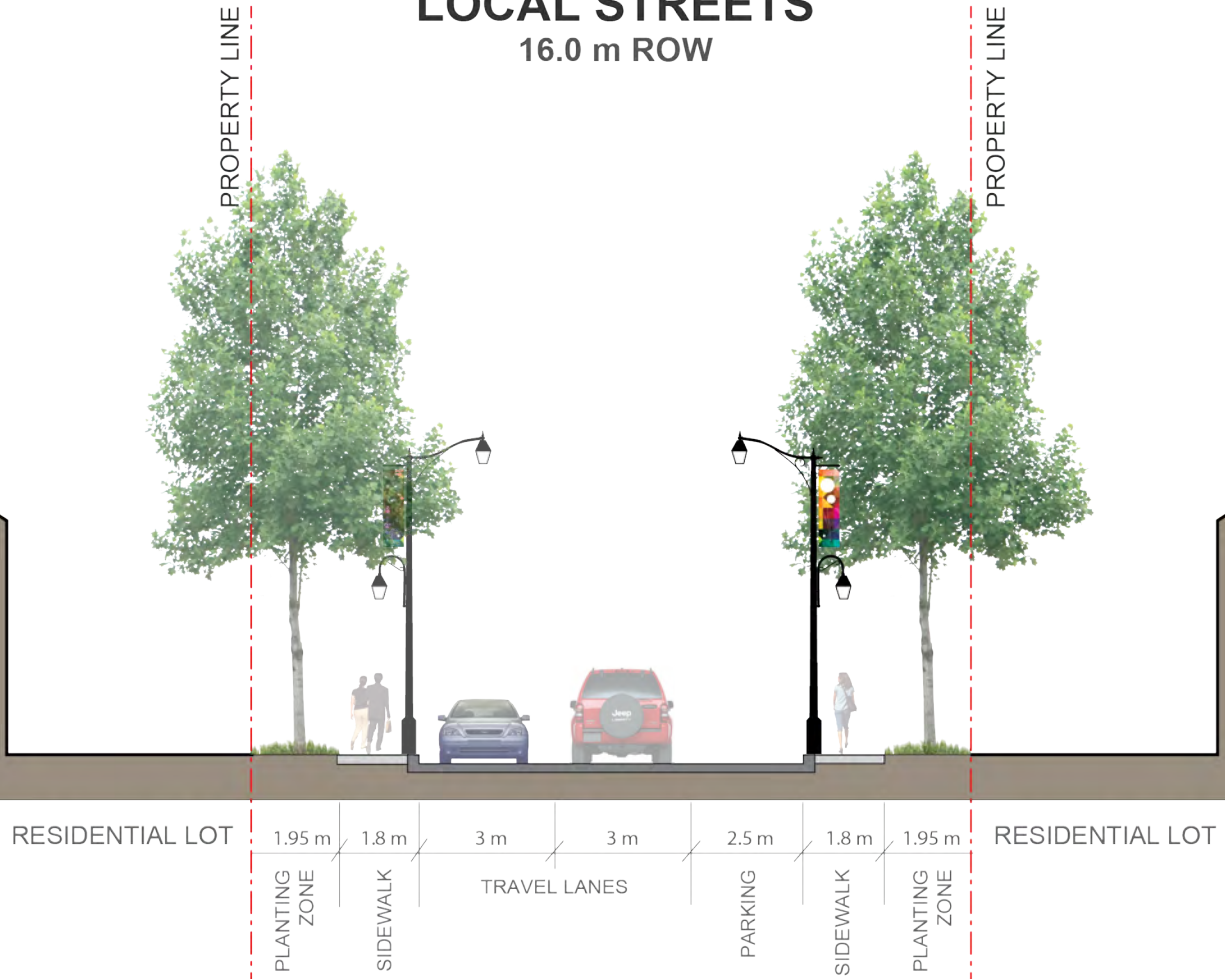
raised sidewalks



pedestrian-scaled lighting

## LOCAL STREETS

16.0 m ROW





## 3.2 Park Spaces

An accessible, animated and varied hierarchy of Park Spaces is vital to promoting a healthy community and important to fostering a strong sense of place within Downtown Georgetown. Well designed, interesting and unique Park Spaces where people go to rest, relax, play, walk their dogs, eat lunch, enjoy the landscape and access community amenities, become the jewels of the community and are crucial components of the Public Realm Network. These spaces must support a variety of special, seasonal and daily activities, their design must reflect their context and enhance the character of the Downtown and they should promote best practices in urban design, landscape architecture and landscape ecology. Most importantly, Park Spaces shall reflect their urban context and be planned and designed as urban parks - smaller in scale, more passive and containing more features and elements than their suburban counterparts.

### The Park Space Hierarchy

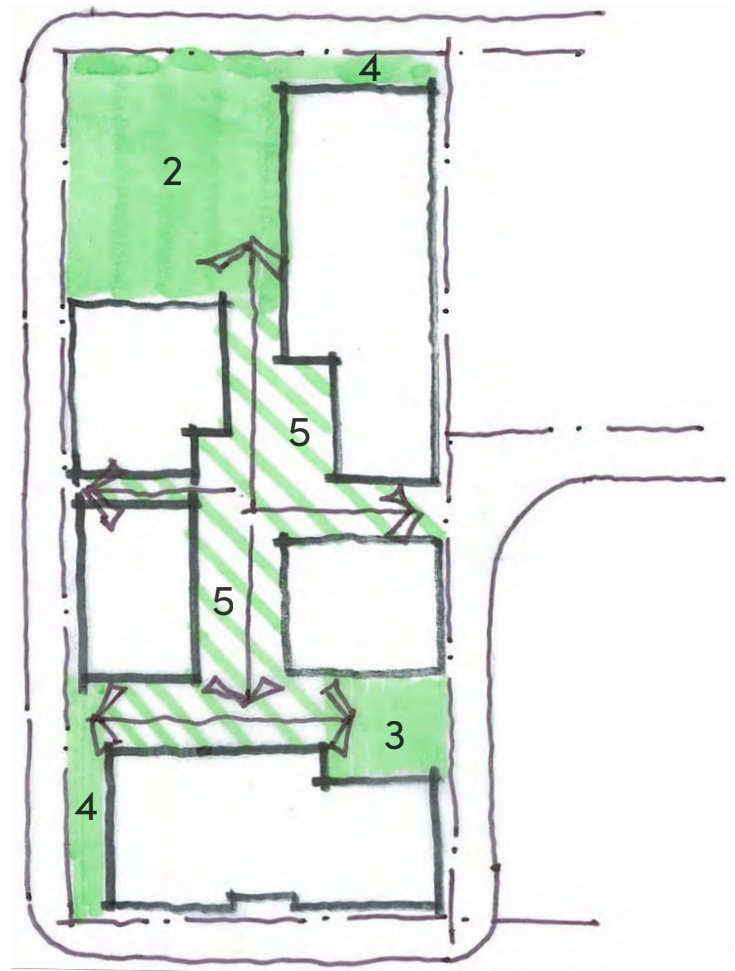
The Secondary Plan identifies the existing Remembrance Park, as well as a number of Potential Parks. The Park Space Hierarchy, including Potential Parks, appropriate for Downtown Georgetown is comprised of the following components:

1. The Existing Park (ie Remembrance Park);
2. Urban Squares;
3. Pocket Parks;
4. Sliver Parks; and,
5. Connecting Links.

### Guidelines for all Park Spaces

The following guidelines apply to all Park Spaces in Downtown Georgetown:

- APS1. All public Park Spaces, will be safe, secure and accessible to the public;
- APS2. All Privately Owned Public Spaces (POPS) will be safe, secure and accessible to the public for a minimum of 14 hours per day, unless otherwise established through required legal agreements.





These POPS will include adequate signage that indicates when it is open and accessible to the public;

APS3. All Park Spaces, including existing, and new, will display signage outlining the 'Park Manifesto' that outlines appropriate conduct for Park Space users and encourages respect for the Park and all users and adjacent spaces;

APS4. All Park Spaces shall be well maintained; Comprehensive maintenance schedules will be developed for all Park Spaces, including existing and new, to ensure safe, accessible and healthy landscapes;



APS5. All new Park Spaces will achieve a minimum of 40 percent tree canopy cover by the end of the 10th year after it is constructed, or redesigned;

APS6. The design of new Park Spaces will prioritize pedestrian comfort, by maximizing the duration of daily sunlight and protecting pedestrians from wind and other elements to support year-round use of the Park Space;

APS7. Amenities, such as seating, tables, washrooms, water fountains and waste receptacles shall be of a high quality and readily available within all Park Spaces;

APS8. New Park Spaces will be designed to the highest standards; high-quality building materials, informed planting choices and environmental sustainability are priorities in the design of new Park Spaces;

APS9. New Park Spaces will include opportunities for Public Art;

APS10. New Park Spaces will include high quality, barrier free programmable space that can accommodate the needs of Park Space users and facilitate children's play, socializing, special events and recreation; and,



various park designs



APS11. New Park Spaces will create and enhance opportunities for greening, and shall be appropriately linked with other elements of the Public Realm.

## Additional Guidelines for Urban Squares

In addition to the guidelines for all Park Spaces, the following guidelines apply to Urban Squares:

US1. Urban Squares are small Park Spaces that accommodate passive activities in a dense urban area. An Urban Square is defined as a pedestrian space larger than 1,000 square metres and smaller than 8,000 square metres in size with street frontage. Urban Squares include both hard and softscaped elements and are equipped with seating opportunities and ample pedestrian amenities;

US2. Urban Squares shall be designed as follows:

- Have a minimum frontage on a public street of at least 40 percent of the depth of the Urban Square;
- May have multiple public street frontages;
- Adjacent built form shall have active, at-grade frontages abutting the Urban Square;
- Shall include community and civic event spaces;
- Shall include seating and a full furniture program, including lighting, opportunities for outdoor cafés and restaurants, facilities for seniors, children and youth; and,

US3. It is not acceptable to place garbage storage facilities, loading docks or utilities in or directly adjacent to Urban Squares.



urban squares



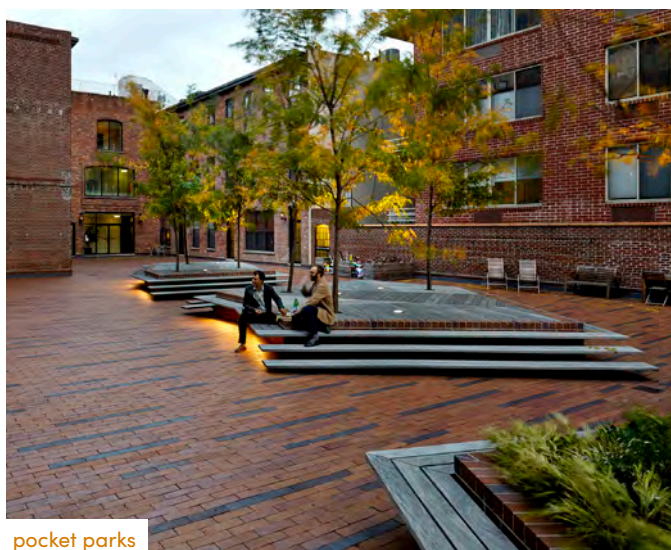
## Additional Guidelines for Pocket Parks

In addition to the guidelines for all Park Spaces, the following guidelines apply to Pocket Parks:

PP1. Pocket Parks are small-scaled components of the Public Realm Network and have an important role to play in enhancing the pedestrian connectivity in the Downtown. They are expected to be less than 1,000 square metres in size, but generally greater than 75 square metres. Pocket Parks shall be primarily hard surfaced, with limited soft surface elements;

PP2. Pocket Parks are expected to develop with the following criteria in mind:

- Have frontage on at least one public street;
- Require that adjacent built form have primary and active frontages facing the Pocket Park, where appropriate; and,
- Facilities shall include a full site furniture program, opportunities for outdoor cafés and restaurants, and facilities that support diverse and passive uses/activities.



pocket parks

## Additional Guidelines for Sliver Parks

In addition to the guidelines for all Park Spaces, the following guidelines apply to Sliver Parks:

SP1. Sliver Parks are small scale components of the Public Realm Network that add to the width of the public sidewalk system, and create plazas or forecourts between the face of the adjacent building and the street. Sliver Parks shall be primarily hard surfaced, with limited planting and soft surface elements;

SP2. Sliver Parks are expected to develop with the following criteria in mind:

- Require that adjacent built form have primary and active frontages facing the Sliver Park; and,



sliver park

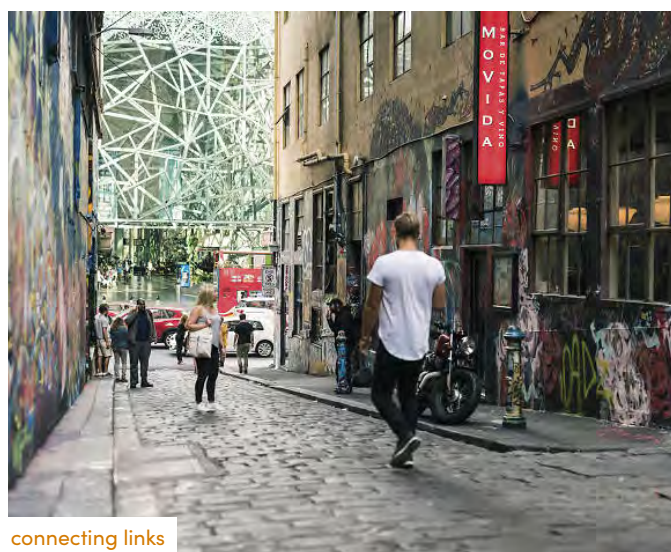
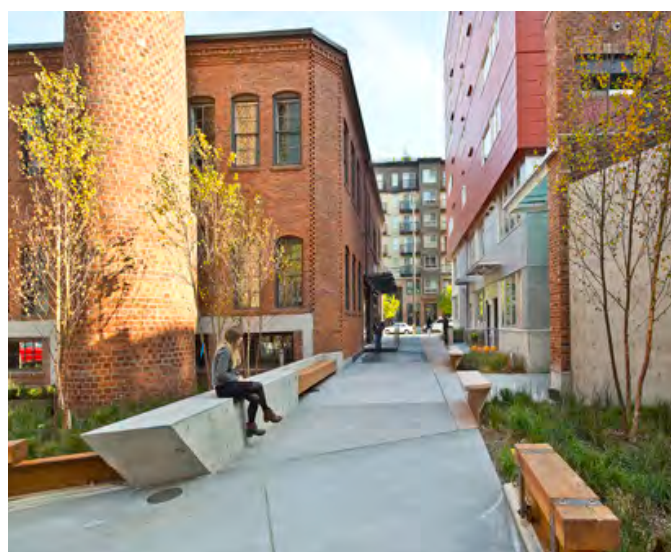


- Be flexible to accommodate spill out retail space, and/or outdoor cafés and restaurants.

## Additional Guidelines for Connecting Links

In addition to the guidelines for all Park Spaces, the following guidelines apply to all Connecting Links:

- CL1. Connecting Links are outdoor walkways through a development site, connecting two streets together. They shall be provided in areas with high volumes of pedestrian traffic, for ease of movement as well as the creation of unique urban spaces;
- CL2. Connecting Links shall contribute to the logical wayfinding system and help to establish a network of publicly accessible spaces within Downtown Georgetown; and,
- CL3. Connecting Links are expected to develop with the following criteria in mind:
  - Width will take into account scale of adjacent buildings, but shall be a minimum of width of 2.5 metres;
  - Shall include several egress opportunities to the public sidewalk system;
  - Will be located between pedestrian destinations and may become destinations themselves;
  - Require that adjacent built form have primary and active frontages facing the Sliver Park;
  - Be primarily hard surfaced, with limited planting and soft surface elements; and,
  - Be flexible to accommodate spill out retail space, and/or outdoor cafés and restaurants.



connecting links



## 4.0 Design Guidelines for the Private Realm

An array of building types is encouraged throughout the Downtown. All development within Downtown Georgetown shall be compatible with development on adjacent properties and shall be consistent with these Design Guidelines.

### 4.1 Guidelines for all Buildings

The intent of these Design Guidelines is to achieve a balance between consistency of design as well as individual expression in new developments. Innovation is encouraged.



### Guidelines for Architectural Variety/Quality

Architectural variety is crucial in creating a visually stimulating urban environment. Streetscapes composed of buildings of similar style and form can succeed through variations in the façade treatment and building mass in order to improve the overall architectural richness, variety, and building articulation in the community. In addition, Downtown Georgetown has a proud history of well designed buildings. All development shall ensure excellence in design and demonstrate high quality architectural detailing, including high quality materials, in accordance with the following guidelines:

AV/Q1. In Downtown Georgetown, new building design shall:

- Be barrier free;
- Have a textured architectural quality that can be achieved by introducing variation in certain elements of the façade treatment such as balconies, bay windows and porches, cornices, window trim, entrances, canopies and the articulation of the building mass;
- Promote street space that is scaled to the pedestrian and organized to present an appropriate façade to all adjacent public





streets. Primary pedestrian entrances shall provide direct and universal access to the public sidewalk;

- Include pedestrian weather and sun protection systems including awnings, canopies, colonnades, or front porches along the sidewalk edge of important pedestrian streets, adjacent to Park Spaces and at entrances to buildings;
- Where feasible, have all transformers and other above ground utilities located within the building, or on private property located away, and/or screened, from public view;



AV/Q2. Design Excellence – All development shall demonstrate design excellence and compatibility with its surrounding context. Architectural detailing, landscape treatments, colour and building materials shall be representative of the highest quality possible;

AV/Q3. Identity – Development shall achieve a unique expressive identity respectful of context. Where applicable, the ground floor of buildings shall be designed to express the individuality of the commercial or residential unit through architectural expression and the inclusion of entrance doors and windows addressing the street. In addition, development shall respect the existing physical character of its adjacent and surrounding context. Development shall:



- Respect the prevailing existing building height, and, if taller, be stepped-back from the prevailing existing building height;
- Respect and reinforce the general physical character, pattern, scale and massing of prevailing development context;
- Require that the prevailing patterns of landscaped open space within the Public Realm are maintained or enhanced;







**AV/Q4. Expressive Forms** - Development shall clearly express a base at the street level, the main body of the building, and a roof form. This will be achieved through various means including setbacks, step backs, textures and materials and other architectural treatments;

**AV/Q5. Flexibility** - Provide sufficient flexibility in considering architectural design proposals to support and accommodate variety, without any strict imposition on building style, or the poorly interpreted replication of historical elements in new building design, except where Designated or Listed Heritage Buildings are affected;



**AV/Q6. Exterior Materials and Colours** - Exterior building colours and materials are selected from a traditional palette. Cladding materials shall be high quality and appropriate for the building type proposed and in recognition of the development context in proximity. Use building materials, such as red or yellow brick, stone, wood and glass, that are true to their nature and do not mimic other materials. Vinyl siding, faux brick/stucco panels, plastic, plywood, concrete block, darkly tinted and mirrored glass and metal siding are not permitted;

**AV/Q7. All publicly visible façades** at the side and rear of buildings should have a compatible architectural expression as the primary façade through consistency in materials and expression of datum lines;



**AV/Q8. Balconies** - Balconies shall be designed as an integral part of the building rather than appearing to be 'tacked-on';

**AV/Q9. Roof Top Gardens** - Where appropriate, roofs and terraces shall be usable for private and communal outdoor patios, decks and gardens. Green roofs are encouraged as a means of retaining storm water, improving air quality and to add visual interest. Roof top gardens may also offer opportunities as dog stations; and,



AV/Q10. Roof top mechanical equipment shall be screened from public view with materials that are complementary to the building or through architectural features. Roof top penthouses are to be integrated with the primary architectural expression and/or roofline.

## Guidelines for Compatible Development and Transition

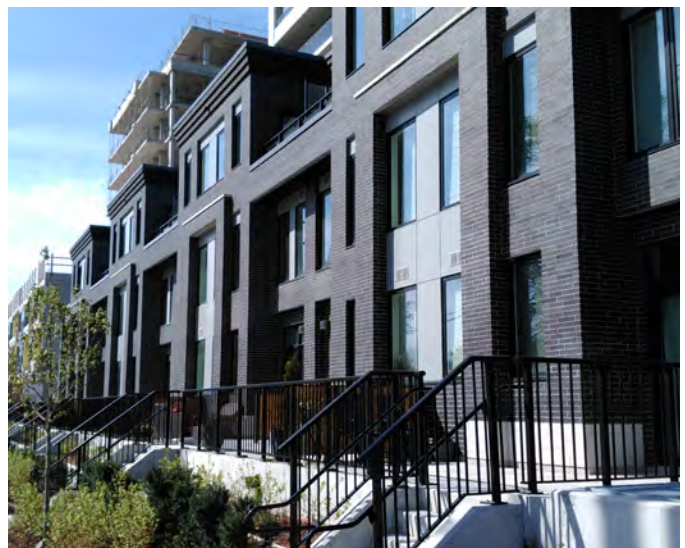
All development applications within Downtown Georgetown will be required to demonstrate compatibility with their surrounding context. Compatible development is central to the vision of preserving and enhancing the unique character of the Downtown. A key element of compatible development is related to the defined transition in development height on adjacent properties.

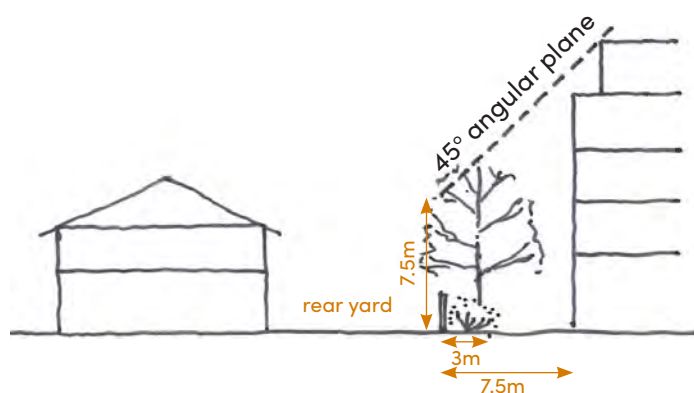
CD/T1. For any proposed development, building mass, lot coverage, scale and building setbacks shall be compatible with buildings in the immediate vicinity

CD/T2. For additions or renovations to an existing building shall be compatible with the architectural style, exterior colour and materials of the primary building;

CD/T3. The transition of building types and between abutting Areas and properties shall be established through the application of some combination of:

- The transition in permitted heights established in the policies of the Secondary Plan;
- Setbacks, step backs and the application of angular planes;
- Façade articulation, enhanced landscape treatments, tree planting and fencing;
- Any other applicable development transition approaches acceptable to the Town;





Downtown Neighbourhood  
Designation

New Development

Setback and angular plane requirements for new development abutting the rear yard of lands within the Downtown Neighbourhood designation



Downtown Neighbourhood  
Designation

New Development

Setback and angular plane requirements for new development abutting the side yard of lands within the Downtown Neighbourhood designation

CD/T4. In addition to those tools, transition requirements shall also consider and mitigate impacts related to overlook and loss of privacy, to the satisfaction of the Town;

CD/T5. Where any proposed development abuts the Downtown Neighbourhood Area, as defined in the Secondary Plan:

- An appropriate building setback shall be established from any abutting rear or interior side lot line;
- A minimum landscape buffer strip of 3 metres is encouraged within the setback, abutting any abutting rear or side lot line. The 3 metre landscape buffer provides the opportunity for substantial tree, shrub and groundcover planting and appropriate privacy fencing;
- An angular plane measured from 7.5 metres above the abutting lot line shall be used to establish the maximum height of any new development. The angular planes shall be applied as follows:
  - A 45 degree angular plane where new development abuts a rear yard condition; or,
  - A 60 degree angular plane where new development abuts an interior side yard condition;
- Front lot lines and exterior side lot lines are specifically exempt from the need to apply an angular plane; and,

CD/T6. Where the transition techniques are to be applied, they shall be applied only to those developments that are expected to accommodate Mid-Rise I and Mid-Rise II Buildings. It is the responsibility of the taller and more massive buildings to establish appropriate transition to the abutting smaller scale development.



## Additional Policies For Landmark Sites

- LS7. Taller building elements at Landmark Sites shall be considered, if compatible with and appropriately transitions to the surrounding context; additionally, these elements must:
- Include distinct architecture and/or architectural features;
  - Strengthen the identity of the Downtown;
  - Be proportionately scaled, relative to its location, context and visibility;
  - Reflect design excellence;
- LS8. On Landmark Sites, distinct/special building height, massing and articulation shall be visible and legible from greater distances and close up;
- LS9. Built form and landscaping at Landmarks sites must be comprehensively considered and include coordination of the private and public realm, with respect to layout, configuration, materials, design features and design details; and,
- LS10. Built form design criteria shall be based upon the proposed building typology – Mid-Rise I and Mid-Rise II.



landmark buildings



## Additional Policies For Gateways

- GS1. A series of Gateway Locations mark the entry points to the Downtown; these locations, which may include existing built form and/or proposed Landmark Sites, shall be the focus of public realm development;
- GS2. Gateway Locations shall be comprehensively considered and include coordination of the private and public realm, with respect to layout, configuration, materials, design features and design details;

GS1. Gateway Locations shall be designed to:

- Enhance the sense of arrival / wayfinding;
- Strengthen the identity of the Downtown;
- Be proportionately scaled, relative to its location, context and visibility;
- Complement the character of the Downtown;
- Reflect design excellence; and,

GS3. Design at Gateway Locations should incorporate a combination of built features and landscape elements; these may include public art, signage, architectural features, landscape structures, street furnishings, special pavements and plantings (both permanent and seasonal).



gateway features



## Additional Guidelines for Low-Rise Buildings

LRB1. Low-Rise Buildings, including Live-Work Units, shall be a maximum of 3 storeys in height. The Town may establish more specific height limits on a site by site basis, based on the scale of adjacent buildings and the character and context of the surrounding community.

## Additional Guidelines for Mid-Rise I Buildings

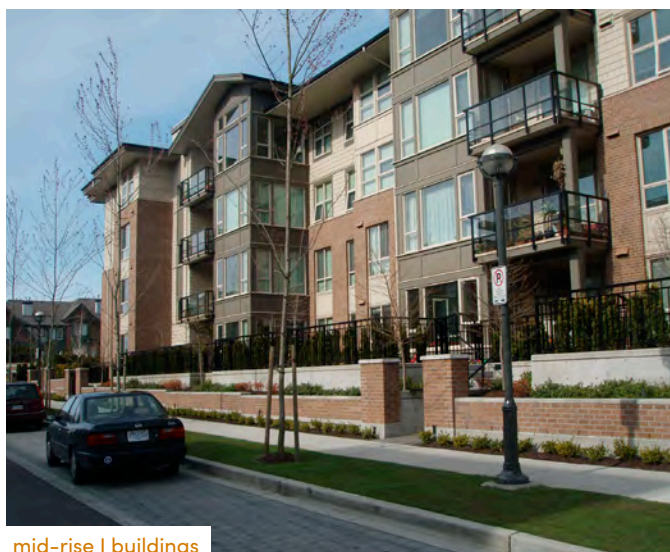
MRI1. Mid-Rise I Buildings are to be a minimum of 3 storeys and a maximum of 6 storeys. The building height for Mid-Rise I Buildings shall be exclusive of mechanical penthouses and roof ornamentation.

MRI2. Mid-Rise I Buildings shall:

- Be compatible with and provide appropriate transitions to the surrounding context;
- Respect the character and be suitably integrated with adjacent existing and planning developments;
- Be developed on sites that are suitable for the proposed development, with adequate land area for required parking, site landscaping and landscape buffering;
- Be arranged/sited to align with and frame streets and park spaces;
- Incorporate highly articulated primary façades that face a public street and include main entrances to the building;
- Incorporate at-grade main entrances with a maximum of one step above grade; and,
- Incorporate a podium element that is 2 or 3 storeys in height, or the same height as immediately adjacent buildings; the tower portion above the podium shall be stepped back a minimum of 2.0 metres from the podium façade that faces a public street.



low-rise building



mid-rise I buildings

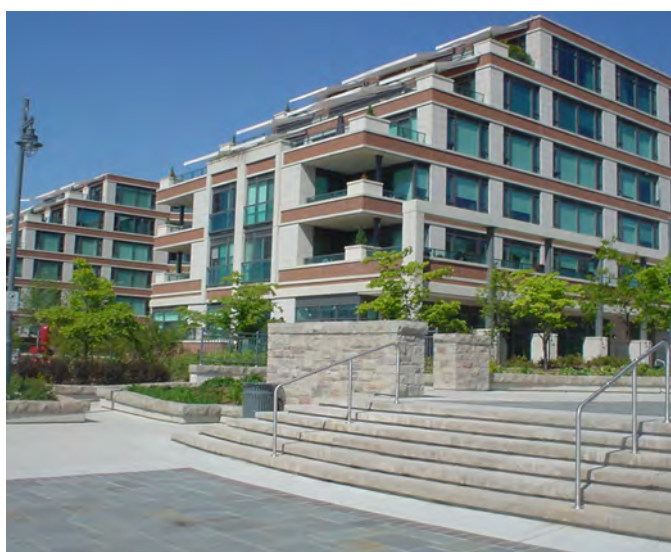


## Additional Guidelines for Mid-Rise II Buildings

MR11. Mid-Rise II Buildings are a minimum of 6 storeys, or 25 metres in height and a maximum of 12 storeys. The building height for Mid-Rise II Buildings shall be exclusive of mechanical penthouses and roof ornamentation.

MR12. Mid-Rise II Buildings shall:

- Be compatible with and provide appropriate transitions to the surrounding context;
- Respect the character and be suitably integrated with adjacent existing and planning developments;
- Be developed on sites that are suitable for the proposed development, with adequate land area for required parking, site landscaping and landscape buffering;
- Be arranged/sited to align with and frame streets and park spaces;
- Incorporate highly articulated primary façades that face a public street and include main entrances to the building;
- Incorporate at-grade main entrances with a maximum of one step above grade; and,
- Incorporate a podium element that is 2 or 3 storeys in height, or the same height as immediately adjacent buildings; the tower portion above the podium shall be stepped back a minimum of 2.0 metres from the podium façade that faces a public street.



mid-rise I buildings



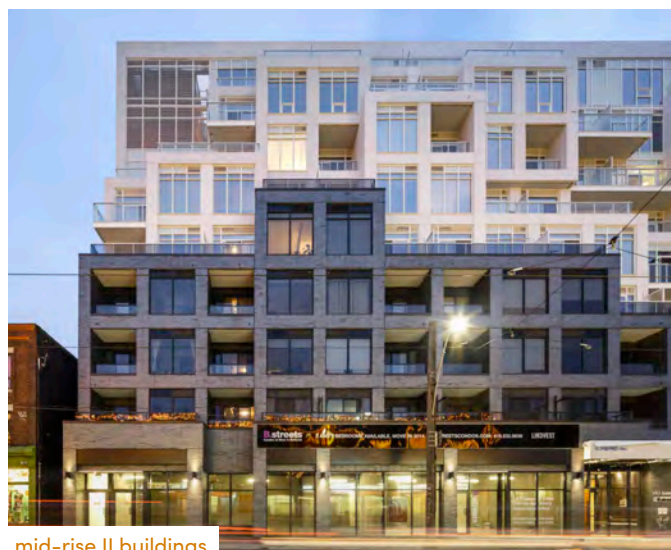
## Additional Guidelines for Buildings in the Historic Main Street Area

The Secondary Plan includes a number of Land Use Designations that have specific requirements for design, that are not necessarily related to the building form that is permitted. Within the Historic Main Street Area, as defined within the Secondary Plan, the following additional guidelines apply:

HMSA1. High activity uses that animate the streetscape and encourage foot traffic are required at-grade abutting Main Street, and along the side streets within the Historic Main Street Designation, with uses such as offices and residential uses on second floors and above. At-grade façades shall incorporate primary building entrances and broad window treatments at street level;

HMSA2. All development proposals within the Historic Main Street Area shall incorporate the following requirements for the design of front and exterior side façades:

- The minimum building height shall be 2 storeys;
- Buildings above 2 storeys shall be required to articulate the historic 2 storey building height, both through architectural detailing and with a building step-back above the 2nd storey;
- The floor-to-ceiling height of ground floors for all new buildings shall be a minimum of 4.25 metres; and,
- The elevation of the front door shall preferably flush with grade, and shall be no more than 1 step above grade.



mid-rise II buildings



## Additional Guidelines for Buildings in the Downtown Regeneration Area

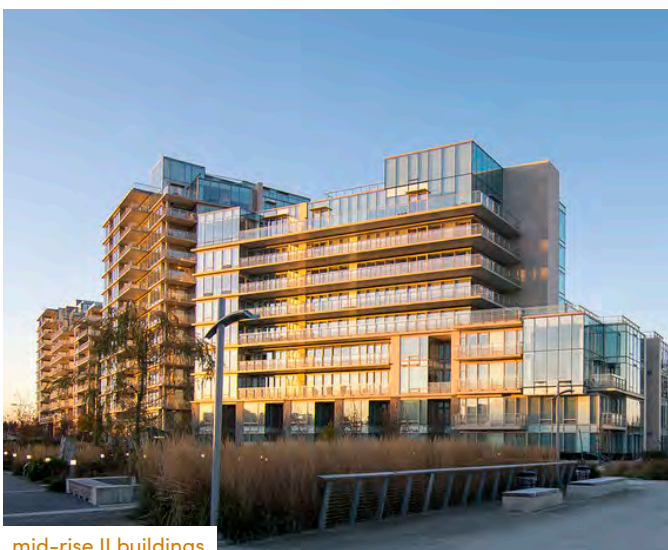
The Secondary Plan includes a number of Land Use Designations that have specific requirements for design, that are not necessarily related to the building form that is permitted. Within the Downtown Regeneration Area, as defined within the Secondary Plan, the following additional guidelines apply:

**DRA1.** High activity uses that animate the streetscape and encourage foot traffic are required at-grade abutting Main Street and Mill Street within the Downtown Regeneration Area, with uses such as offices and residential uses on second floors and above. At-grade façades shall incorporate primary building entrances and broad window treatments at street level;

**DRA2.** Abutting other streets within the Downtown Regeneration Area, high activity uses that animate the streetscape and encourage foot traffic, are encouraged at-grade. At-grade façades shall incorporate primary building entrances and broad window treatments at street level, where appropriate;

**DRA3.** Development within the Downtown Regeneration Area shall:

- Ensure that enhanced connections to the Town's parks and trails are provided;
- Provide enhanced on-site pedestrian elements;
- Consider the topography of the area in determining appropriate building design and massing;
- Incorporate appropriate conservation and integration of any on-site Designated or Listed Cultural Heritage Resource.



mid-rise II buildings



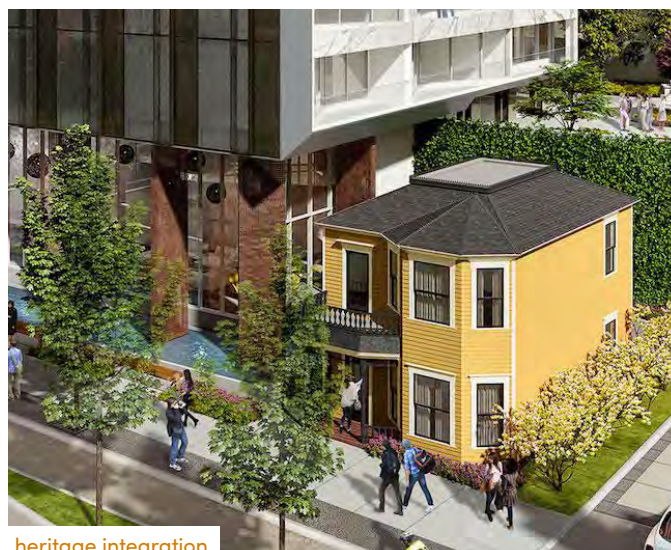
DRA4. All development proposals shall incorporate the following requirements for the design of front and exterior side façades within the Downtown Regeneration Area:

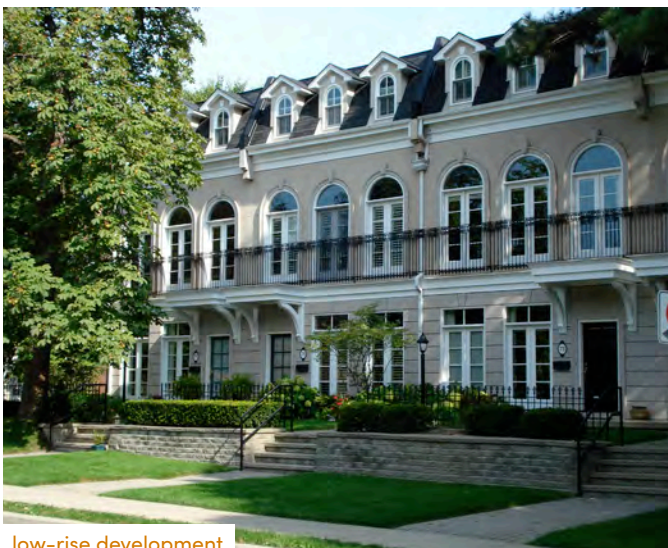
- The minimum building height shall be 3 storeys;
- The floor-to-ceiling height of ground floors for all new buildings shall be a minimum of 4.25 metres;
- The maximum floor plate for any building component above the sixth floor shall be 750 square metres; and,
- The minimum separation distance between adjacent buildings above the sixth floor shall be 25 metres.

### Additional Guidelines for Buildings in the Downtown Neighbourhood Area

DNA1. In reviewing applications for development within the Downtown Neighbourhood Area, the Town shall be satisfied that:

- The residential character of the neighbourhood is maintained;
- The proposed development complements, and is similar to other existing residential buildings in proximity in terms of:
  - Landscape treatments;
  - Building height, scale, lot coverage and massing;
  - Roof line;
  - The location and size of parking facilities/garages;
  - Architectural detail;
  - Building materials and colour; and,





low-rise development

— Front and side-yard setbacks;

- The façade of any building facing a street shall feature a porch, prominent entrance or other architectural gestures that provides a 'public face'. The main front door must be clearly visible from the street;
- The elevation of the front door shall be no more than 1.2 metres above grade;
- Appropriate buffering such as landscaping and fencing will be provided on-site to ensure the compatibility of the use with adjacent uses; and,
- Appropriate on-site parking is available. Driveways and/or garage doors shall not dominate the front façade of the primary building or the view from the street. All on-site parking shall be located within the interior side and/or rear yard.



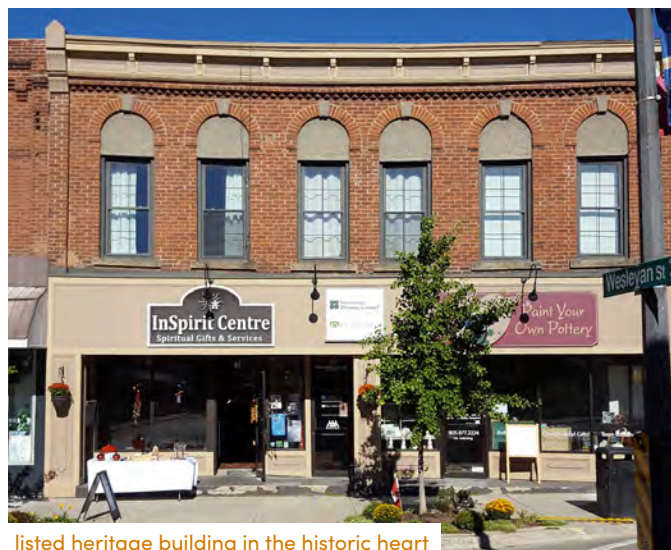
## 4.2 Additional Guidelines for Gateways and Landmark Sites

Highly visible and prominent locations in the Downtown provide excellent opportunities for place-making and promoting the identity of the Downtown. For these reasons, Gateway Locations and Landmark Sites warrant special consideration and shall include a combination of building and landscape features that enhance the quality of the Public Realm and contribute to the character of the Downtown.

## 4.3 Heritage Design

Appendix III to the Secondary Plan includes an inventory of both Designated and Listed Cultural Heritage Resources. Within Downtown Georgetown, there are three distinct areas exhibiting unique heritage characteristics, as follows:

1. The **Historic Heart**, centred on Main Street, where many of Georgetown's most recognizable historic commercial and institutional buildings are found. Collectively, they define a relatively intact, authentic, and historic streetscape with a strong sense of architectural continuity. Within this approximately two-block zone, the intent of these guidelines is to preserve the existing character, and ensure that new development conforms to the primary character-defining architectural elements of the Main Street streetscape;
2. A **Heritage Character Area**, roughly corresponding to the Downtown Neighbourhood area and those areas of Main Street outside of the Historic Heart. In this area, there is a recognizable pattern of streets, lots, landscapes and buildings with historic origins, but with a more eclectic architectural and spatial character. Within this area, the intent of these guidelines is to ensure new development is compatible with the character-defining elements, but with more latitude for individual expression; and,
3. The **Balance of the Downtown**, where some heritage buildings are found, but where there is no prevalent historic character across the area. In



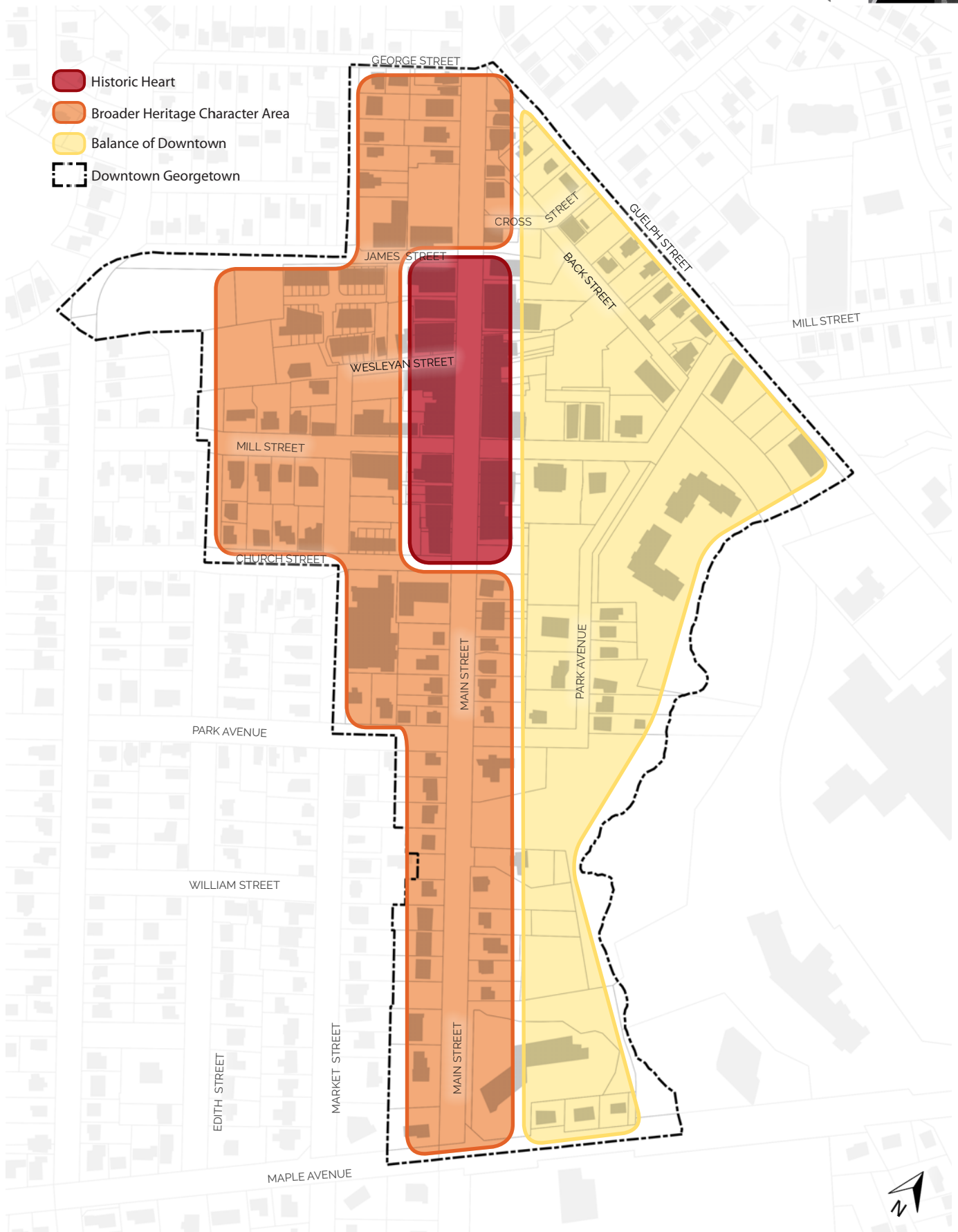
listed heritage building in the historic heart



listed heritage building in the heritage character area



listed heritage building in the balance of the downtown



this area, the intent of these guidelines is to ensure that heritage resources are conserved and featured appropriately within new development, but with the most latitude for individual expression. Refer to 7.1 Design Guidelines for All New Development.

This Heritage Guideline Section is organized in 2 parts. First, to provide general guidance for development on, or adjacent to the identified heritage resources in the Downtown, and second, to provide specific guidance for the identified Historic Heart and the Heritage Character Area.

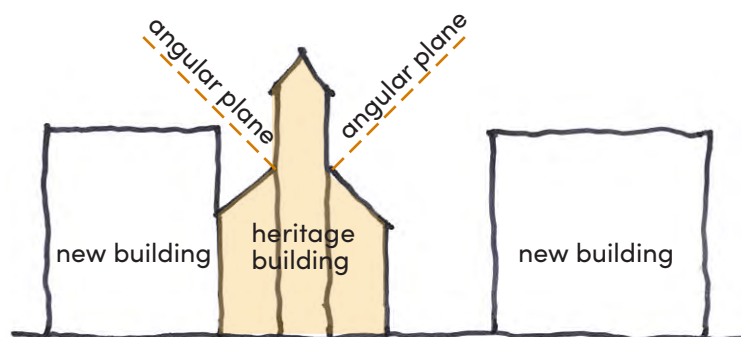
## Heritage Guidelines for all Development

HG1. All development proposals within Downtown Georgetown shall design, restore or enhance their building façades in order to maintain the historic architectural character and identity of the Downtown. Where development is proposed that includes a Designated or Listed Heritage Building (as identified in Appendix III), the original architectural details and features shall be restored and appropriately incorporated into the development, including requirements for:

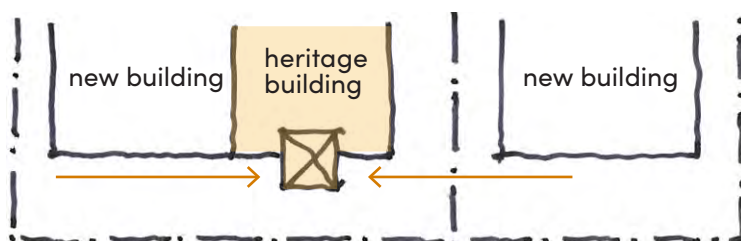
- Decorative details and façade articulation that respects, or is consistent with the horizontal architectural elements of the building and/or neighbouring buildings with historic features;
- Significant vertical elements that maintain the traditional vertical pattern of building façade design for development sites that exceed 12.0 metre frontages; and,
- Architectural features such as awnings, canopies, and building cantilevers/overhangs on the building's front and exterior façades, where appropriate;

HG2. Changes to a building of heritage value must:

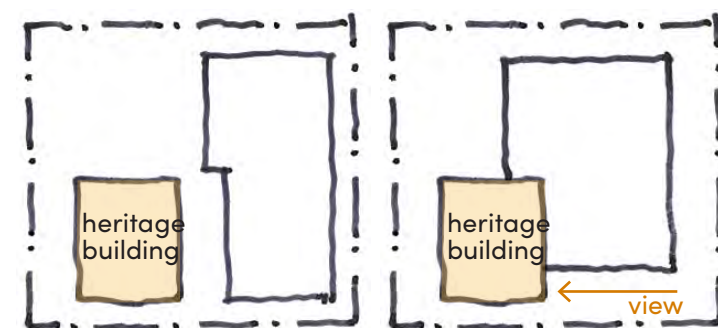
- Treat distinctive stylistic features or examples of skilled craftsmanship with sensitivity;



angular plane from roofline of architectural feature

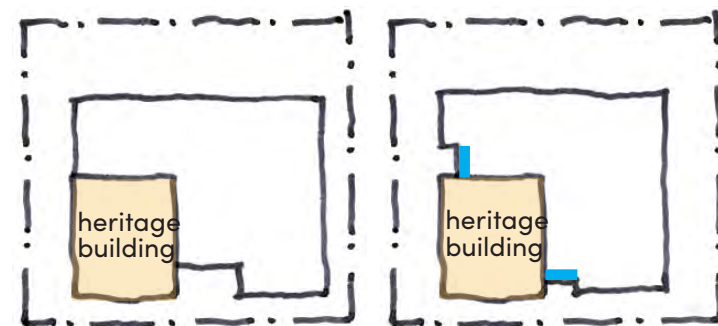


building setbacks to maintain view of architectural feature



separation

setback



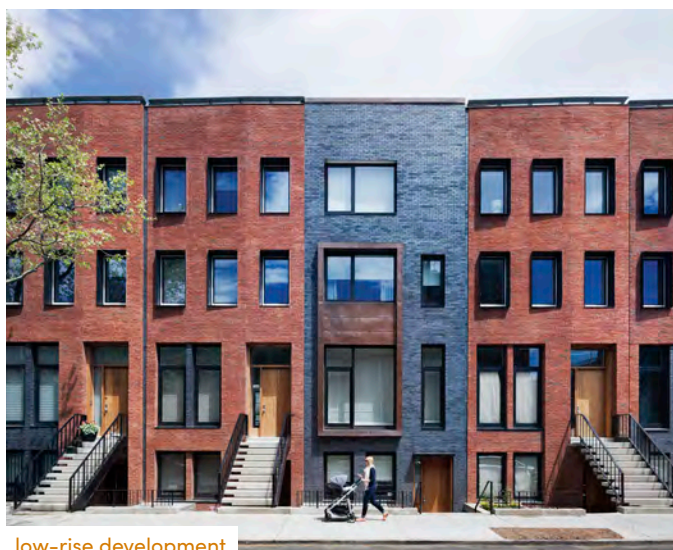
massing & shadow

transparency





maintain vertical and horizontal rhythms of heritage buildings in new construction



low-rise development

- Repair or replace missing architectural features with an accurate duplication of features, substantiated by historic, physical or pictorial evidence, rather than on conjectural design of different architectural elements from other buildings or structures;
- In instances where removal is required due to damage or deterioration, the materials shall be replaced with the same material or with materials that are compatible and/ or complementary to the original architectural character:
  - Respect changes to a historic building or resource which may have taken place over the course of time and may have acquired significance in their own right. The valid contributions of all periods to a historic building or resource should be respected;
  - Avoid historical misrepresentation. Buildings tell the story of historical development of the area. It is important that the historical record does not get confused through the mimicry of past architectural styles;
  - Be complementary to a building's original materials, as well as to those of adjacent buildings;
  - Ensure the selection of windows be undertaken in a manner that the materials and surrounds be based on those of the original building. The type of window (double hung etc.) and the resulting profile should reflect that of the original building;
  - Where an existing building lacks significant architectural detail, or a new building is proposed, the façade shall be representative of, or consistent with the historic architectural character and identity of the Designated or Listed Heritage Buildings in proximity;

HG3. Views of prominent architectural features of heritage buildings such as towers, turrets, spires or landmark façades will be maintained by all new development, including:

- For development on sites adjacent to the architectural feature, buildings shall be set back to maintain views of the feature from the ground level;
- For development on the same site, a 45 degree angular plane established from the roofline abutting the architectural feature shall be established where new development is not permitted, in addition to setbacks as above;

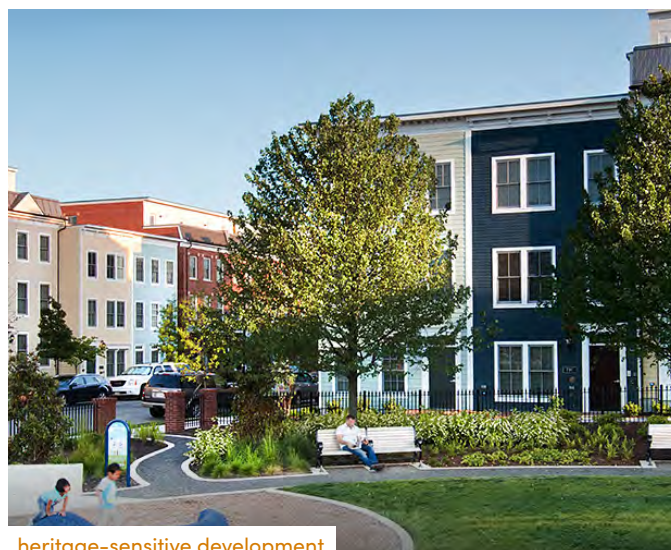


HG4. New construction on the site of existing heritage buildings shall feature the heritage resources within the architectural composition and provide a distinct visual separation between heritage features and new construction, using one or more of the following:

- A physical separation of 2 metres or more;
- Set back of new buildings to maintain views of the heritage building;
- A distinct massing change to create a strong shadow line, 3 metres wide and 2 metres deep;
- The use of transparency (glass) in joining the new construction to the heritage building. The transparency should be recessed a minimum of 0.5 metres; and,



HG5. New construction on the site of existing heritage buildings shall maintain similar horizontal and vertical rhythms through massing, datum lines, floor heights, bays, and architectural detailing. New construction should be visually distinct from the heritage building, using compatible materials.



heritage-sensitive development





## Additional Guidelines for the Historic Heart

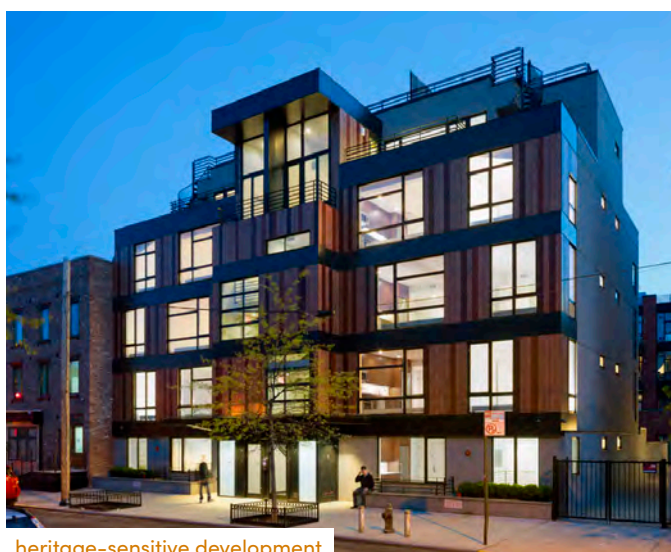
HH1. For sites with existing heritage buildings, the existing building shall form the podium element of the building, subject to the following:

- For new construction, buildings shall have a two storey podium at the street edge;
- The primary façade material shall be masonry, preferably yellow and/or red brick, wood, or natural stone. Aluminum siding, shingles, precast concrete panels, curtain wall glass, or stucco are not appropriate;
- An articulated cornice (minimum 0.5m height) shall be provided at the second storey. It shall respond to the cornice line of heritage buildings on abutting properties through alignment, height, proportion, rhythm, materials and/or architectural expression;
- Second storey windows shall have a vertical proportion and be vertically divided. They shall have articulated lintels and sills through elements such as projecting masonry, soldier courses, arched headers, change of material and/or decorative trims.

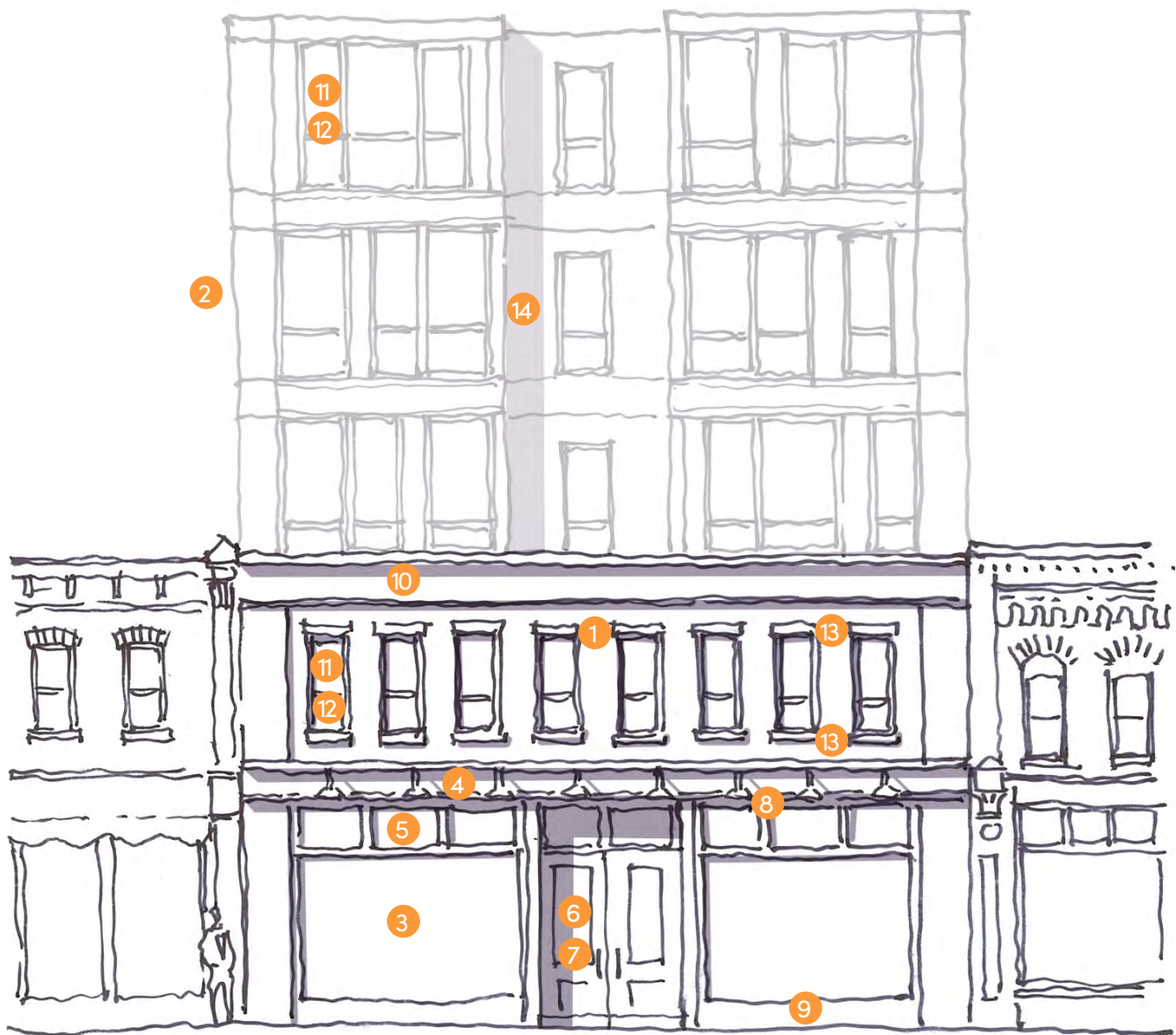


HH2. To facilitate a variety of retail expressions that are compatible with the context of the Historic Heart, the ground level façade shall incorporate a minimum of 5 (and preferably more) of the following:

- Transom windows;
- Recessed main store entrance with retail display along the sides forming the recessed entry;
- Paired, narrow store entry doors;
- Clear sidelights or doorway surrounds;
- A secondary cornice above the first storey;



heritage-sensitive development



adjacent heritage building	new construction	adjacent heritage building
-------------------------------	---------------------	-------------------------------

- |  |  |  |
|--|--|--|
| 1 two storey podium building                         | 7 recessed retail entry doors                  | 12 upper storey windows divided  |
| 2 upper storeys set back                             | 8 projecting (e.g. gooseneck style) lighting   | 13 prominent lintels and sills on second storey windows  |
| 3 ground floor 75% transparent                       | 9 window treatment within 0.5m of grade        | 14 vertical division to upper storey façade through datum lines, change in plane, and/or materials |
| 4 sign band or datum line distinguishes ground floor | 10 prominent cornice at second storey          |  |
| 5 transom windows at ground level                    | 11 vertical proportion to upper storey windows |  |
| 6 narrow, vertical proportion entry doors            |  |  |

Demonstration of design guidelines for a building site within the Historic Heart. Architectural elements of new construction may be simplified or contemporary if desired. However, those elements must have significant conformity with historic styles for the podium building that defines the street edge. Above the podium, there is greater latitude for architectural expression.





mcgibbon hotel old proposal rendition

- Decorative brackets or trim;
- Projecting lighting that illuminates the sign band and storefront, such as gooseneck lighting;
- A dedicated sign band with individual cut-out lettering and graphics (three dimensional);
- Decorative columns or pilasters;

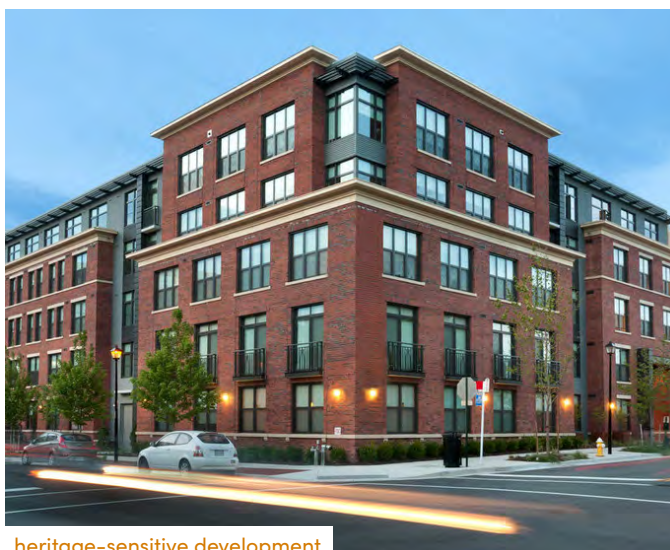
HH3. In addition to the optional choices above, the following guidelines apply to all ground level façades:

- The bottom of the window frame shall be within 0.5m of grade;
- The plane of the window shall be within 0.25m of the main front plane of the podium;
- The expression of retail may be accomplished with a wide variety of materials including glass, metal, wood, and masonry;



HH4. If the building is taller than the traditional podium along Main Street, additional building elements above the podium shall be stepped back from the front face of the podium. In addition, the building elements above the podium shall:

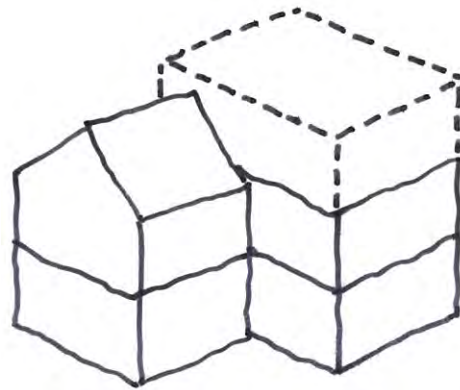
- Establish a vertical rhythm by incorporating datum lines, recesses, vertical breaks, pilasters and/or changes in material in the facade; and,
- While materials may differ from the podium, but must be complementary.



heritage-sensitive development

## Additional Guidelines for Heritage Character Area

- HCA1. New buildings are to be located consistently with other buildings on the same block. For commercial blocks, setbacks are minimal. For residential blocks, setback varies but typically includes a front yard landscape zone;
- HCA2. Live work units and buildings with commercial on the ground floor are preferred to have a flat roof design;
- HCA3. For single-unit new residential construction located near the street edge of a predominantly residential block, building height should be a maximum of two storeys and incorporate a sloped roof design or profile. New construction taller than two storeys are to be located behind the rear half of the building;
- HCA4. Garages will not be a prominent visual feature of new development. If garages are provided, they are preferred to be located in the rear yard and accessed by a single lane driveway (3 metres maximum width). If integral garages are desired they may not project beyond the main front façade of the building:
- Single car garages are to be flush with the main front façade of the building, or set back;
  - Double car garages shall be set back a minimum of 6 metres from the main front façade of the building.



2 storey peaked roof building volume at street edge, taller portions set back



sensitive infill town houses

## 4.3 Other Key Design Elements

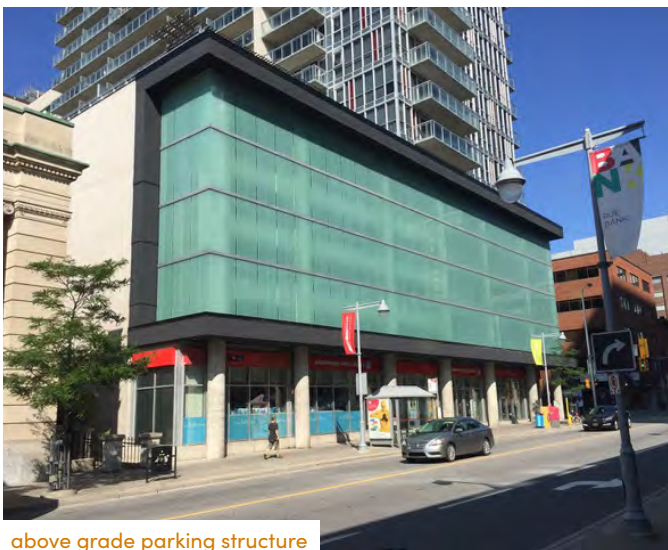
### Guidelines for Parking, Loading and Service Facilities

Parking, loading and service facilities are important functional design elements that support a successful downtown and have a visual impact on the community. The sensitive location and design treatment of these

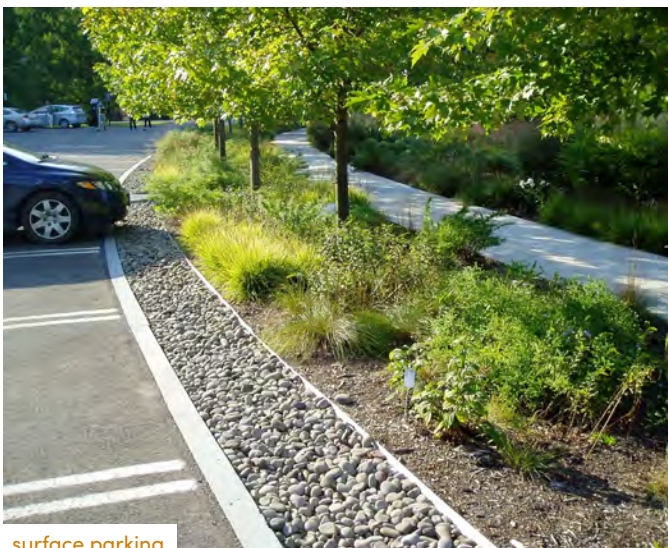




surface parking



above grade parking structure



surface parking

facilities is important in the evolution of a beautiful historic downtown. The Town shall consider the following guidelines in their review of development applications:

- PLSF1. Access points shall be minimized and consolidated to optimize the use of driveways. An interconnected system of rear laneways and drive aisles across adjoining properties shall be encouraged;
- PLSF2. Connectivity between parking facilities and Main Street is required through the establishment of pedestrian linkages.
- PLSF3. Surface parking, loading and service facilities shall not be permitted in the front yard of any building within Downtown Georgetown. Parking, loading and/or servicing facilities may be permitted in an interior side yard and are permitted within the rear yard. On-street loading will be discouraged. Common laneways at the rear of commercial/mixed-use buildings shall be provided, where possible;
- PLSF4. Where parking, loading and/or servicing facilities are provided, their visual impact shall be mitigated by a combination of setbacks and landscaping including: pavement treatments, low walls or decorative fencing, landscaping and trees throughout parking lots and along the edges. Surface parking lots shall be lit with pedestrian-scale lighting;
- PLSF5. Parking is encouraged to be provided in structures, either above, or where possible, below grade. Where a parking structure is above grade, permitted commercial uses shall be provided within the façade at-grade and the façade shall include appropriate architectural articulation; and,
- PLSF6. Where surface parking is proposed, it shall be located within the rear yard or the interior side yard. Where any parking facility is located adjacent to a public street, it shall be designed to complement and integrate with adjacent buildings and the streetscape;



## Guidelines for Lighting

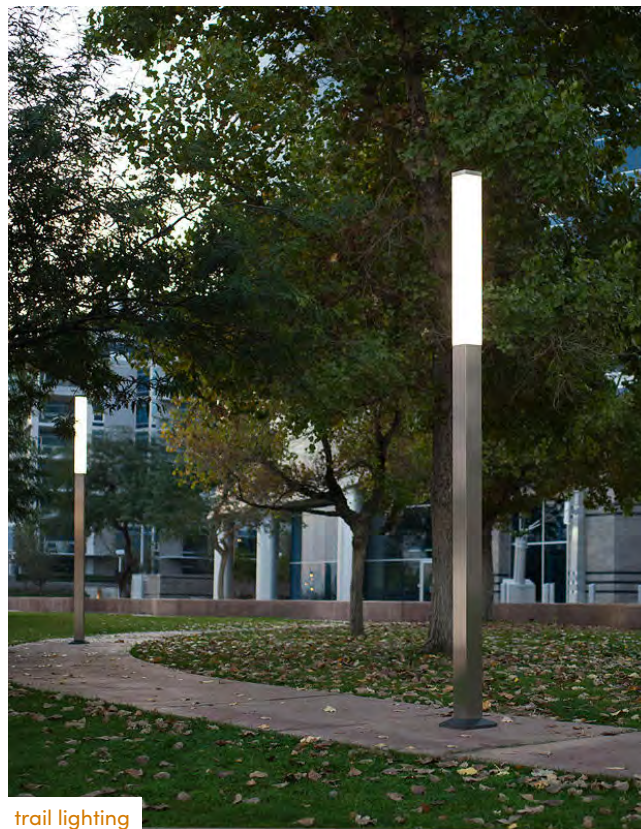
Lighting is an essential consideration to ensure safe pedestrian, cyclist and vehicular movement. The following guidelines shall apply to the Downtown:

- L1. Exterior lighting shall be designed to promote pedestrian comfort, safety and provide a high quality ambiance;
- L2. Only Dark Skies friendly lighting will be permitted in the Downtown Area. In addition, accent lighting is encouraged where it can emphasize built form and landscape elements; and,
- L3. Pedestrian scale lighting shall be provided adjacent to streets, walkways, urban squares, pedestrian routes and within parks and along trails.

## Guidelines for Signage

Signs should contribute to the quality of individual buildings and the overall streetscape, and should reflect the unique characteristic of their context. High quality, imaginative, and innovative signs are encouraged. Commercial storefront signage should be subject to the following guidelines, in addition to the Town's Sign By-law:

- S1. Signage shall be designed to be complementary to the character of the Downtown;
- S2. Backlit signage, fluorescent sign boxes corporate signage, billboards and large pylon signs shall be prohibited;
- S3. Signage within the Downtown Neighbourhood Designation shall be restricted in size and closely related to the principle building entrance;



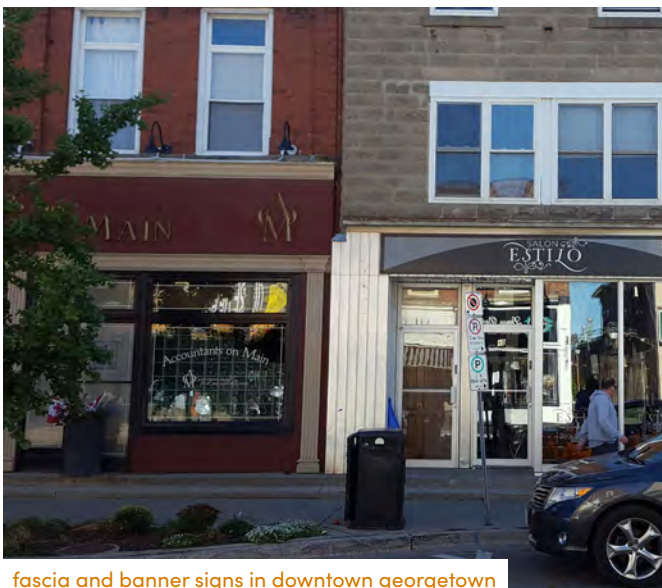
trail lighting



pedestrian lighting

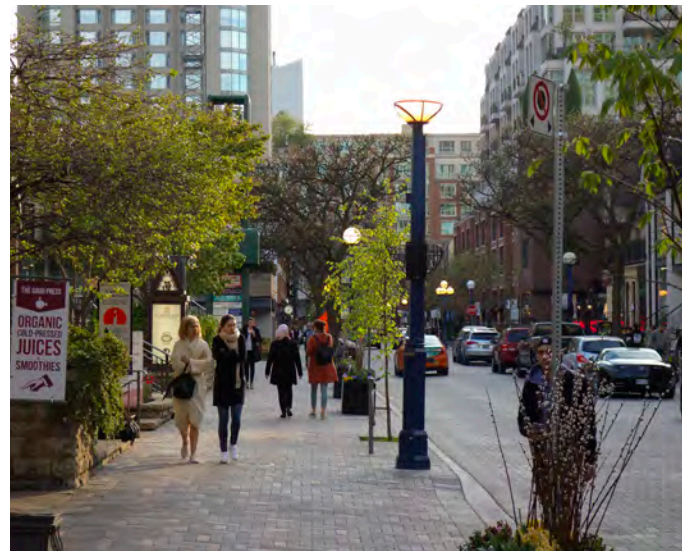


- S4. Signs should be constructed of durable, high-quality materials and well maintained;
- S5. Street addresses should be clearly visible for every building;
- S6. Signage shall be integrated into the organization and design of building façades and located within architectural bays, friezes and datum lines;
- S7. Signs should not obscure windows, cornices, or other architectural elements;
- S8. Signs on heritage buildings should be compatible in terms of heritage character, colour, and material, and should not obscure architectural details;
- S9. The Town shall establish a Sign By-law specific to the various conditions and contexts found throughout the Downtown; and,
- S10. The Town shall consider an enhanced public wayfinding program throughout Downtown Georgetown.



fascia and banner signs in downtown georgetown









## 5.0 Implementation

These Design Guidelines establish a framework that guides the physical design of the Public and Private Realms within Downtown Georgetown. Each of the identified guidelines are expected to give the Town direction in the review and approval of various forms of development over time. The Design Guidelines for the Public Realm are expected to be implemented by the Town over time.

The Design Guidelines for the Private Realm are expected to be implemented through the statutory planning tools that are provided to the Town through the Planning Act. Key implementing tools and processes will allow the Town to provide a number of key opportunities and public processes to consider development applications – these tools and processes include the Secondary Plan, the Zoning By-Law and Site Plan Approval.



The wording in this Design Guideline is substantially definitive. Notwithstanding the use of definitive language, this document is truly a Design Guideline, which is an Appendix to the Downtown Georgetown Secondary Plan. As a guideline document, flexibility in interpretation is implied, to the satisfaction of the Town. The statutory effect given to these Design Guidelines will only be achieved as they are implemented in the Secondary Plan and Zoning By-Law, or through the Site Plan Approval process.

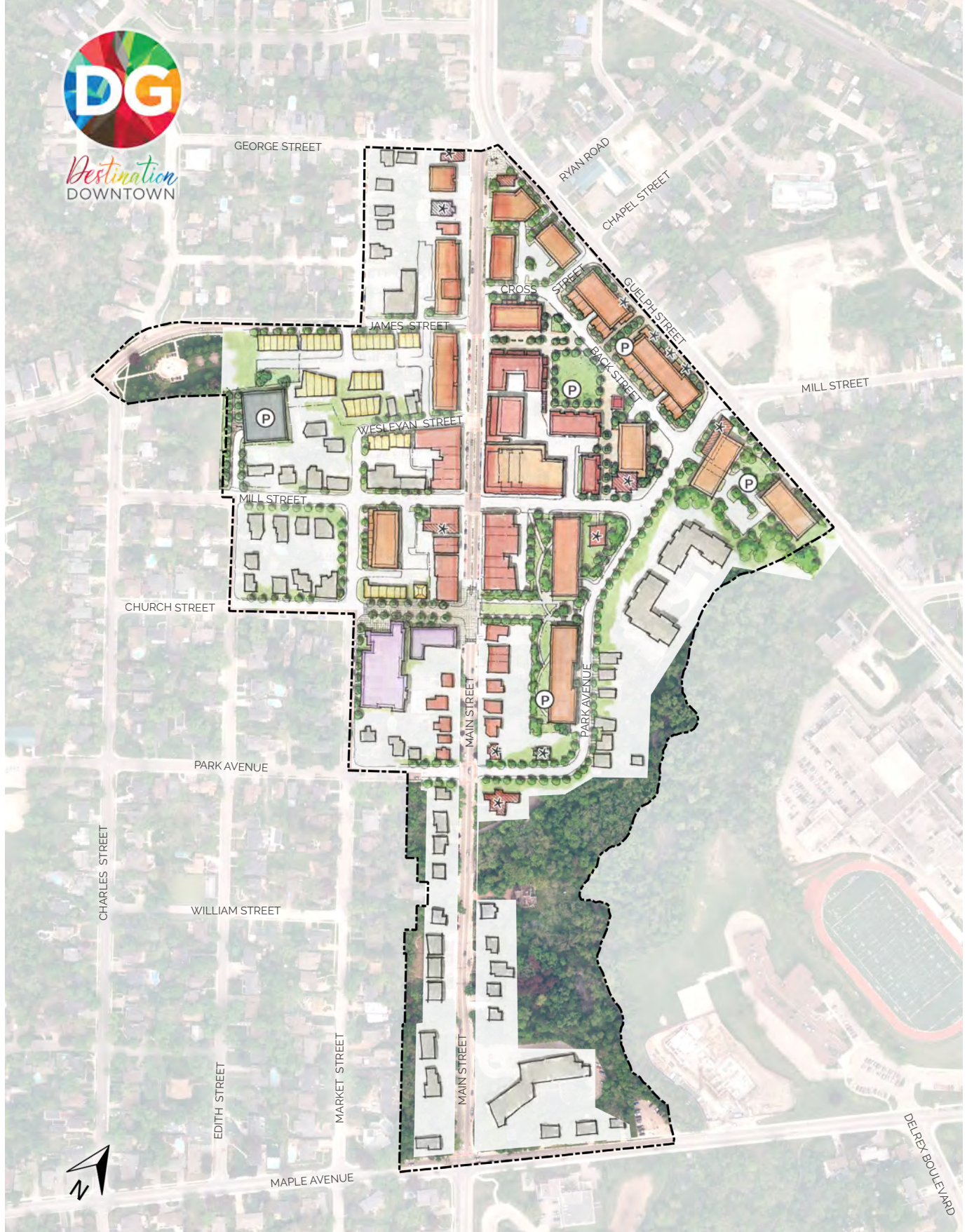








*Destination*  
DOWNTOWN



## APPENDIX I DOWNTOWN GEORGETOWN DEMONSTRATION PLAN



Residential / Mixed Use



Institutional



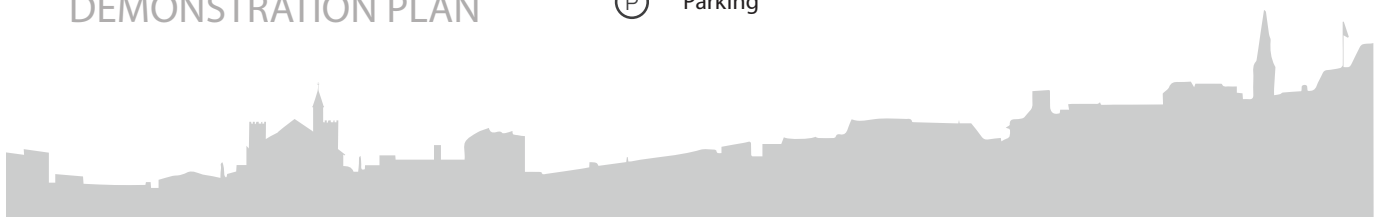
Parking



Heritage Significance



Downtown Georgetown

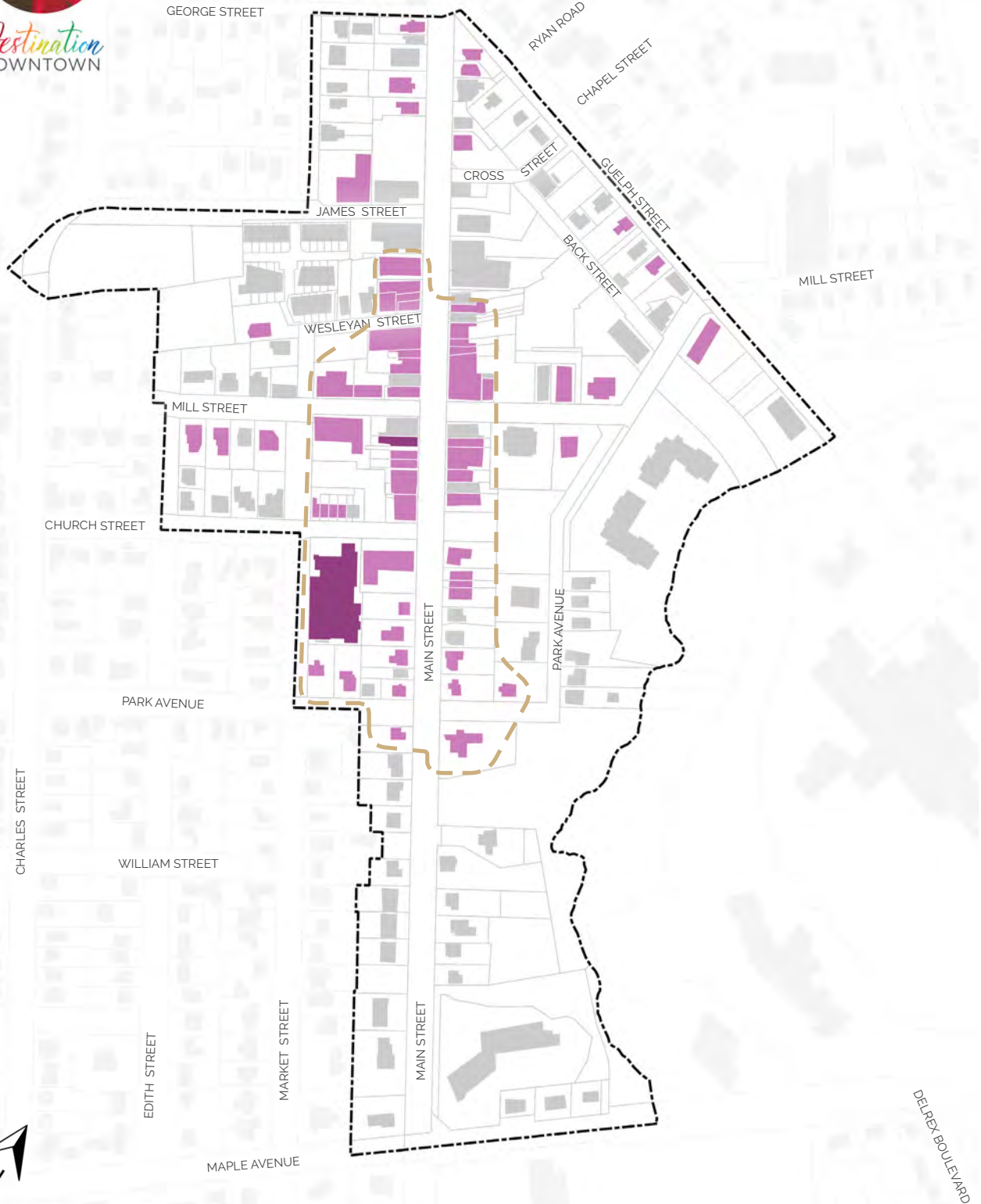








*Destination*  
DOWNTOWN



### APPENDIX III DOWNTOWN GEORGETOWN CULTURAL HERITAGE

- Designated Heritage Property
- Listed Heritage Property
- Potential Heritage Conservation District
- Downtown Georgetown

