

LEGAL DESCRIPTION

CREDIT NOTES:
THIS SITE PLAN IS BASED UPON AND MUST BE READ IN CONJUNCTION WITH THE SURVEY PLAN PREPARED BY SKIRA & ASSOCIATES LTD., DATED AUGUST, 2017, DWG NUMBER 217-066.

CHAMBERLAIN ARCHITECT SERVICES LIMITED ACCEPTS NO RESPONSIBILITY FOR THE ACCURACY OR COMPLETENESS OF THE DATA SUPPLIED AND SUCH DATA IS NOT INCLUDED UNDER SEALS OF CERTIFICATION, IF ANY.

REGISTERED PLAN 37 LOTS 8,9 AND 10
TOWN OF HALTON HILLS (GEORGETOWN), ONTARIO



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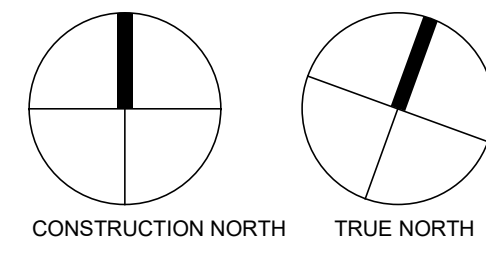
www.chamberlainIPD.com

NO.	ISSUED	DATE
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DO NOT SCALE DRAWINGS. USE ONLY DRAWINGS MARKED "ISSUED FOR CONSTRUCTION". VERIFY CONFIGURATIONS AND DIMENSIONS ON SITE BEFORE BEGINNING WORK. NOTIFY ARCHITECT IMMEDIATELY OF ANY ERRORS, OMISSIONS OR DISCREPANCIES.

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HABITAT FOR HUMANITY

37 King Street, Georgetown,
ON

SHEET NAME

SITE PLAN

START DATE
APRIL 2024

DRAWN BY
KO

CHECKED BY
SM

SCALE
As indicated

PROJECT NO.
123012

DRAWING

A001

- (a) "All surface drainage will be self-contained, collected and discharged at a location to be approved prior to the issuance of a building permit."
(b) "The portions of the driveway within the municipal boulevard will be paved by the applicant."
(c) "At the entrances to the site, the municipal curb and sidewalk will be continuous through the driveway and a curb depression will be provided for each entrance."
(d) "All proposed curbing within the municipal boulevard area for the site is to suit as follows:
i) For all single family residential properties including on street townhouses, all curbing is to stop at the property limit or the back of the municipal sidewalk, whichever is applicable; or
ii) For all other proposals including Industrial, Commercial and Condominium developments, all entrances to the site are to be in accordance with OPSP 350.010 (save and except Detail 'A' which shall match City of Mississauga Standard 2240.031).
(e) "All excess excavated material will be removed from the site."
(f) "The existing drainage pattern will be maintained except where noted."
(g) "The applicant will be required to contact all utility companies to obtain all required locates prior to the installation of hoarding within the municipal right-of-way."
(h) "The applicant will be responsible for the cost of any utility relocations necessitated by the site plan."
(i) "All internal curbs are to be standard 2-stage curb and gutter as per O.P.S.D. 600.070."
(j) "Prior to commencing construction, all required hoarding in accordance with the Ontario Occupational Health & Safety Act and regulations for construction projects, must be erected and then maintained throughout all phases of construction."
(k) "Should any works be required within the municipal right-of-way, a Road Occupancy Permit will be required. P.U.C.C. approval will be required. For further information please contact the P.U.C.C./Permit Technologist, located at 3185 Mavis Road.
(l) "All asphalt and base materials of the existing driveway to be removed are to be reinstated with topsoil and sod."
(m) "Refer to the Site Grading Plan prepared by (Jain Infrastructure Consultants Ltd.), Drawing (C102-SITE GRADING PLAN), Issued for approval MARCH 30/23 and FEB 07/24 for the purposes of obtaining site grading information."

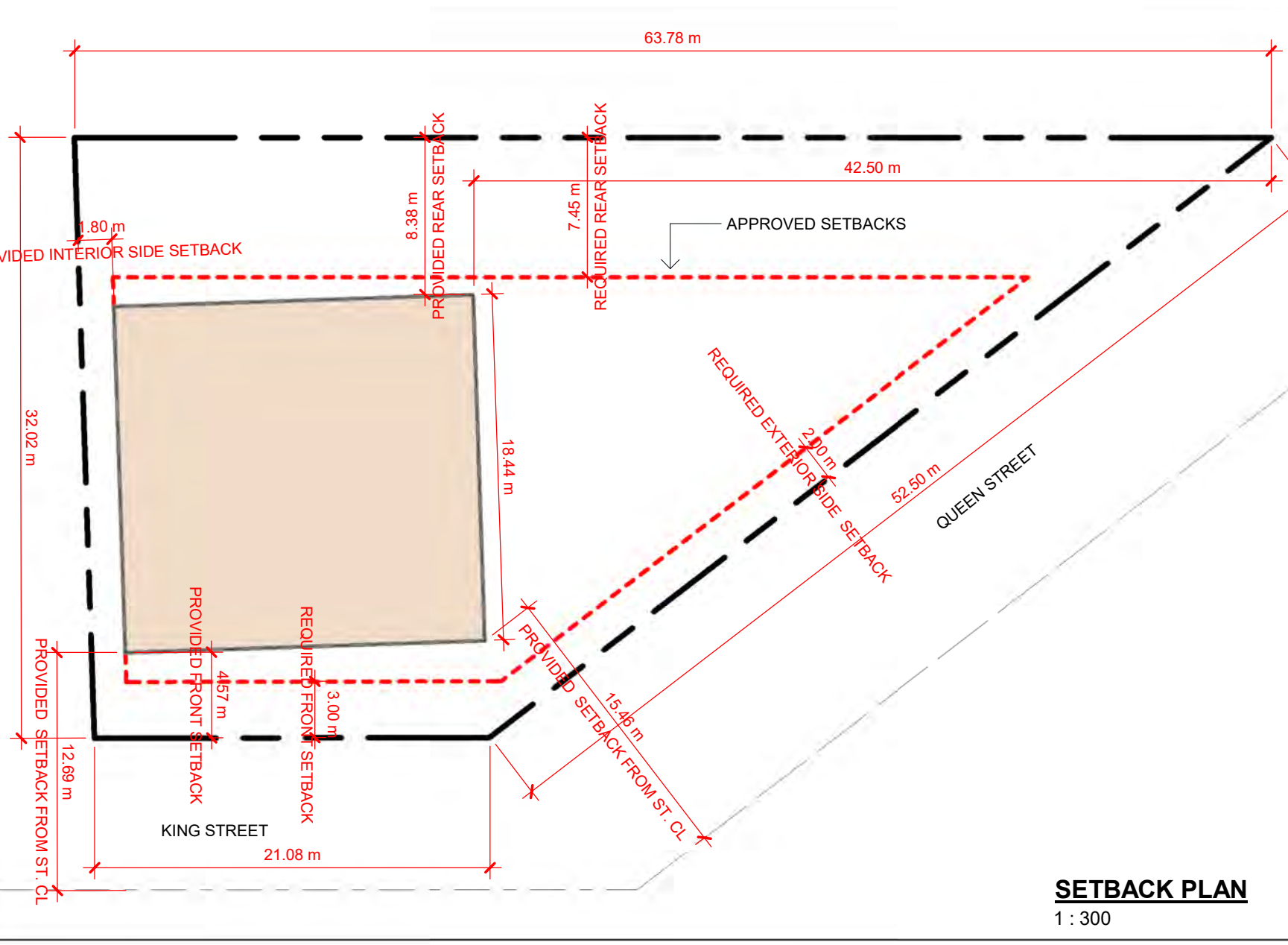
(TRAFFIC NOTES)
(i) All damaged or disturbed areas within the municipal right-of-way are to be reinstated at the Owner's expense; (ii) All landscaping and grading within close proximity to the proposed access points is to be designed to ensure that adequate sight distances are available for all approaching and exiting motorists and pedestrians; (iii) The portion of the driveway within the municipal boulevard is to be paved by the Owner; (iv) Driveway accesses shall maintain a 1.5m setback from aboveground features such as utilities and trees; (v) Any above ground utilities located within 1.5m of a proposed access are to be relocated at the Owner's expense; (vi) The cost for any/all road improvements required in support of this development application will be borne by the Owner; (vii) The Owner shall make satisfactory arrangements with the Transportation and Works Department for the design, construction and payment of all costs associated with works necessary to support access to this site; (viii) Any access to internal servicing shall be provided internally through the site; (ix) Details of the site specific access configurations will be finalized in conjunction with the Site Plan review/approval process.

GENERAL PROVISIONS		
Zone Code : MDR2-107-H1 City : Town of Halton Hills Property Address : 37 King Street, Georgetown, ON	Zone Description : TWIN HOUSES Zoning By-law : 2010-0050 Lot Area : 1357.6m ²	
ZONING DATA		
	REQUIRED	PROVIDED
- MINIMUM LOT FRONTAGE	3.06M	4.57 M
- LOT COVERAGE	3.06M	4.57M
- MINIMUM FRONT YARD	1.80M	1.8M
- MINIMUM INTERIOR SIDE YARD	1.98M	4.24 M
- MINIMUM EXTERIOR SIDE YARD	7.43M	8.38 M
- MINIMUM REAR YARD		
PARKING REQUIREMENT		
2 spaces per dwelling unit plus 0.3 spaces per dwelling unit for visitor parking. If each individual dwelling unit includes an attached garage and does not front on a public street, the street townhouse		
RESIDENTIAL : 28 PARKING SPACE 2 ACC PARKING, 26 TYPICAL PARKING	RESIDENTIAL : 12 PARKING SPACE 1 ACC PARKING, 10 TYPICAL PARKING	
TOTAL REQUIREMENT = 28	TOTAL PROPOSED = 13	
ACCESSIBLE PARKING		
ACCESSIBLE PARKING: 1 SPACE PLUS 2% OF TOTAL REQUIRED = 1 + (28 x 2%) = 2 TOTAL REQUIRED ACCESSIBLE PARKING=1SPACES (2 TYPE A)		
PARKING SCHEDULE		
TYPE	DESCRIPTION	COUNT
VEHICLE ACC PARKING	TYPE A - 3.4mX5.5m	1
VEHICLE PARKING	2.75m x 5.5m	12
		13
BICYCLE PARKING SCHEDULE		
TYPE	DESCRIPTION	COUNT
BIKE PARKING	UG PARKING	3
		3

SITE STATISTICS			
Site Category	Area	Area SF	PERCENTAGE
BUILDING FOOTPRINT	352 m ²	3790 ft ²	26%
HARD LANDSCAPE	224 m ²	2410 ft ²	17%
HARDSCAPE	16 m ²	173 ft ²	1%
HARDSCAPE	18 m ²	191 ft ²	1%
HARDSCAPE	403 m ²	4341 ft ²	30%
HARDSCAPE	9 m ²	102 ft ²	1%
SOFT LANDSCAPE	334 m ²	3598 ft ²	25%
Grand total: 17	1357 m ²	14606 ft ²	
OVERALL SITE			
DESCRIPTION	AREA (SM)	AREA (SF)	PERCENTAGE
OVERALL SITE	1357.09 m ²	14608 ft ²	100.0%
ZONING INFO			
ZONE 2010-0050 Medium Density Residential Two Exemption 07 MDR2(107)(H1)			
MIN. NUMBER OF PARKING SPACES		REQUIRED	PROVIDED
		28	13
MAX. HEIGHT		10.0m	11.6m
REFER TO SETBACK PLAN FOR AS APPROVED SETBACKS VS PROPOSED BUILDING			
PARKING PROVIDED NUMBER AS FROM THE APPROVED LANDPLOT MIN PARKING NUMBER			

KEY PLAN		
GFA		
GFA / FSI		
ML	Area	FSI
GROUND FLOOR	316.15 m ²	0.072152
T/O BASEMENT	293.90 m ²	0.067075
T/O SECOND FLOOR	296.28 m ²	0.067617
T/O THIRD FLOOR	292.76 m ²	0.066814
Grand total	1199.08 m ²	0.273658

ESTABLISHED GRADE CALCULATION				
length no.	Begin elev	end elev	length	((el1+el2)/2)XL
L1	256.80	256.68	9.67	2482.68
L2	256.68	258.24	9.52	2451.02
L3	258.24	258.75	18.35	4743.38
L4	258.75	258.75	19.19	4965.41
L5	258.75	258.76	16.36	4233.23
L6	258.76	256.80	1.99	512.98
TOTAL			75.08	19388.70
EG			L SUM/P	258.24
SITE PLAN - ESTABLISHED GRADE				
1: 500				



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