

# Planning Justification Report

**37 King Street, Georgetown ON**

**Homeownership Model  
April 22, 2025**

**Habitat for Humanity Halton-Mississauga-Dufferin**  
1800 Appleby Line, Unit 10  
Burlington, Ontario  
L7L 6A1

# Table of Contents

<b>Introduction.....</b>	<b>4</b>
<b>Existing Site and Surroundings.....</b>	<b>6</b>
Site Description.....	7
Surrounding Area.....	8
Noise and Vibration.....	11
Environmental Conditions.....	11
<b>Proposed Development.....</b>	<b>12</b>
Waste Management.....	13
Landscaping & Heritage Interpretation.....	13
<b>Planning Policy and Regulatory Context.....</b>	<b>14</b>
Provincial Policy Statement, 2024.....	14
Key Policies Relevant to the 37 King Street Project:.....	14
Planning Analysis for 37 King Street Project:.....	15
Halton Region Official Plan, 2024.....	15
Key Policies Relevant to the 37 King Street Project:.....	15
Planning Analysis for 37 King Street Project:.....	16
Halton Hills Official Plan, 2025.....	16
Key Policies Relevant to the 37 King Street Project:.....	16
Planning Analysis for 37 King Street Project:.....	16
Zoning and Site-Specific By-law Amendment Request.....	17
Planning Justification & Community Impact.....	17
<b>Planning Applications.....</b>	<b>18</b>
Halton Hills Official Plan Amendment.....	18

Halton Hills Zoning By-law Amendment.....	19
<b>Conclusion.....</b>	<b>20</b>
<b>Appendix A:</b> Draft Official Plan Amendment.....	<b>22</b>
<b>Appendix B:</b> Draft Zoning By-Law Amendment.....	<b>29</b>
<b>Appendix C:</b> Adesso Design - Updated Landscape Plan - 37 King Street, Georgetown (April 11, 2025).....	<b>34</b>
<b>Appendix D:</b> Chamberlain Architect Services Ltd. - Latest Floor Plans & Elevations - 37 King Street, Georgetown (March 21, 2025).....	<b>35</b>
<b>Appendix E:</b> Chamberlain Architect Services Ltd. - Latest Renderings - 37 King Street, Georgetown (March 28, 2025).....	<b>36</b>
<b>Appendix F:</b> Chamberlain Architect Services Ltd. - Latest Updated Site Plan - 37 King Street, Georgetown (April 11, 2025).....	<b>37</b>
<b>Appendix G:</b> Chamberlain Architect Services Ltd. - OBC Data Matrix - 37 King Street, Georgetown (April 4, 2025).....	<b>38</b>
<b>Appendix H:</b> GHD - Updated Traffic Brief and Vehicle Swept Path Designs - 37 King Street, Georgetown (April 15, 2025).....	<b>39</b>
<b>Appendix I:</b> Habitat HMD - Proof of Development Review Fee Payment (9 Units) - 37 King Street, Georgetown (April 17, 2025).....	<b>40</b>
<b>Appendix J:</b> Habitat HMD - Signed Official Plan & Zoning By-law Amendment Application - 37 King Street, Georgetown (March 20, 2025).....	<b>41</b>
<b>Appendix K:</b> Habitat HMD - Updated OPA & ZBLA Application Fees Credit - 37 King Street, Georgetown (April 22, 2025).....	<b>42</b>
<b>Appendix L:</b> Habitat HMD - Updated Site Plan with Waste Removal and Storage Locations & Paths - 37 King Street, Georgetown (April 17, 2025).....	<b>43</b>
<b>Appendix M:</b> Halton Region - Section 59(2b) Notice - 37 King Street, Georgetown (April 18, 2024).....	<b>44</b>
<b>Appendix N:</b> Halton Region - Signed Updated Risk Management Plan (2020-12-08-CTC-RMP-58) - 37 King Street, Georgetown (April 7, 2025).....	<b>45</b>
<b>Appendix O:</b> MTE - Functional Servicing and Stormwater Management Report - 37 King Street, Georgetown (February 14, 2025).....	<b>46</b>
<b>Appendix P:</b> MTE - Updated Functional Site Grading and Esc Plan (C1.1) & Functional Site Servicing Plan (C1.2) - 37 King Street, Georgetown (April 11, 2025).....	<b>47</b>

<b>Appendix Q:</b> MTE - Updated Phase One ESA - 37 King Street, Georgetown (February 20, 2025).....	<b>48</b>
<b>Appendix R:</b> Region of Halton - Completed Supplementary Waste Plan - 37 King Street, Georgetown (April 4, 2025).....	<b>49</b>
<b>Appendix S:</b> Thornton Tomasetti - Updated Noise & Vibration Impact Study - 37 King Street, Georgetown (April 15, 2025).....	<b>50</b>
<b>Appendix T:</b> Town of Halton Hills - Pre-Consultation Notes - 37 King Street (June 6, 2024).....	<b>51</b>
<b>Appendix U:</b> Habitat HMD - Waste Management Plan - 37 King Street, Georgetown (April 22, 2025).....	<b>52</b>

# Introduction

Habitat for Humanity Halton-Mississauga-Dufferin (Habitat HMD) is pleased to submit this Planning Justification Report in support of an Official Plan and Zoning By-law Amendment (OPA/ZBLA) application for the property located at 37 King Street in Georgetown, within the Town of Halton Hills and the Region of Halton (hereafter referred to as ‘the site’). These amendments aim to support the intensification of affordable housing development, building upon the previously approved 2021 applications #D09OPA19.001 (Official Plan amendment, approved May 25, 2021 - bylaw no. 2021-0024) and D14ZBA19.007 (Zoning By-law amendment, approved May 25, 2021 - bylaw no. 2021-0023).

In response to the urgent need for increased affordable housing amidst today’s challenging political and economic climate, Habitat HMD is seeking both an Official Plan and Zoning By-Law amendment to increase the proposed site density from the originally envisioned six units to twelve units, with 13 parking spaces. The changes include revised site, civil, and landscape plans, as well as modifications to building height, setbacks, and a reduction in parking requirements.

The updated proposal envisions the development of 12 stacked townhouse units, arranged in three blocks of four units each, with independent exterior access. This enhanced plan will significantly increase the number of families served by Habitat HMD, contributing vital new units to Halton Hills’ affordable housing stock.

## **Proposed Amendments:**

### **1. Municipal Official Plan Amendment**

- Amend the density calculations in bylaw no. 2021-0024 to accommodate the proposed 88.2 units per residential hectare calculation for 12 units on site. The proposed development:
  - Is located at the corner of King Street and Queen Street in Georgetown, close to several key intersections, including King Street/Mountainview Road North and Queen Street/Guelph Street.
  - Reflects a modern interpretation of traditional homes in the area, with 3 storeys above grade, a clean gable front design, and exterior

colours that complement neighbouring homes.

- Reduces the number of units from the originally proposed 15 to 12 units, to better serve the community with the available space.
- Has a negligible impact on traffic and parking, generating only 5 trips during the a.m. peak hour and 6 trips during the p.m. peak hour, all of which can be accommodated within the existing road network.
- Is located directly adjacent to the Georgetown GO Station, providing residents with convenient access to sustainable alternative public transit options.
- Is within walking distance of community facilities and local amenities, including parks, schools, and open spaces.
- Incorporates off-street parking for residents, amenity space, a community pollinator garden, and heritage interpretive signage and benches to honour the site's history.
- Includes significant native vegetation and tree planting, designed to complement the existing topography of the neighbourhood.
- Will not create additional shadow impacts on neighbouring homes.
- Will access municipal water and wastewater services, with minimal disruption to traffic during installation.

## **2. Municipal Zoning By-law Amendment**

- Amend the special provisions approved in bylaw no. 2021-0023, including:
  - Increasing the number of parking spaces to 13, including one designated accessible space, and providing a two-way drive aisle.
  - Increasing the building height to 11.804m from grade (from the permitted 10m).

- Adjusting the setbacks to:
  - Front Yard: 4.57m
  - Rear Yard: 8.38m
  - West Side Yard: 1.8m (consistent with the previous site-specific schedule)
  - East Side Yard: 1.87m at the shortest measurement (consistent with the previous site-specific schedule).

As a trusted leader in affordable housing development, Habitat HMD has established itself as the "community builder," an innovative problem-solver, and the preferred non-profit development partner for local social service providers and municipalities. Since its inception in 1999, Habitat HMD has delivered 83 affordable housing units, including 68 family units and 15 tiny homes for Indigenous communities. Our strategic plan aims to create at least 130 additional units by 2032, with this project being a key step toward that goal.

The proposed amendments align with municipal, regional, and provincial planning policies that prioritize intensification, transit-supportive development, and affordable housing initiatives. The property's designation as part of a Major Transit Station Area (MTSA) further strengthens the suitability of the amendments. Ongoing planning and development efforts on this site since the original 2021 amendments ensure the project meets community needs and long-term financial sustainability. With the requested amendments, Habitat HMD is confident in its ability to deliver these units on time and within budget.

## Existing Site and Surroundings

The subject property, located at 37 King Street in Georgetown, is situated within an established urban area and falls within a designated Major Transit Station Area (MTSA). The property is well-served by transit infrastructure and is in close proximity to essential services, making it an ideal location for higher-density residential development.

Currently designated for residential use, the surrounding area features a mix of low-rise and mid-rise residential developments, along with commercial and institutional uses.

The proposed amendments will optimize land use while ensuring compatibility with the existing neighborhood.

## Site Description

The site is located north of King Street and west of Queen Street, situated between Queen Street and Emery Street. The land slopes downward toward King Street and Queen Street, with relatively level grading at the rear, where the site abuts the Georgetown GO Station parking lot.

The site has a frontage of approximately 51.58 meters (169.23 feet) along Queen Street and 21.94 meters (71.98 feet) along King Street. Its depth measures approximately 31.85 meters (104.50 feet), with a total area of 1,358.85 square meters (0.135885 hectares), as shown in Figure 1 below.



**Figure 1: Location Map: 37 King St, Georgetown, Ontario**  
(Town of Halton Hills Interactive Mapping)



**Figure 2: Aerial View of 37 King St, Georgetown, Ontario**  
(Google Maps)

The site has remained vacant since the original Official Plan and Zoning By-law Amendment approvals in 2021. The majority of the site is covered with light vegetation and soil, and there are currently no trees present. Formal parking is not available on-site, and the property is enclosed by construction fencing. The figures below provide views of the site from the street.

## Surrounding Area

The intersection of King Street and Queen Street is located at the southeast corner of the site. This intersection experiences moderate foot and vehicular traffic. However, the traffic brief prepared by GHD concludes the following:

- **Minimal Traffic Impact:** The development will generate only 5 trips during the a.m. peak hour and 6 trips during the p.m. peak hour, which can be easily accommodated within the existing road network.

- **Safe Site Access:** The driveway access on Queen Street meets sightline requirements, ensuring safe entry and exit without significantly impacting nearby intersections.
- **Parking Considerations:** Although the site provides fewer parking spaces than required by the Zoning By-law, it complies with Bill 185, which removes parking minimums in Major Transit Station Areas (MTSA).
- **Adequate Parking Supply:** The 13 proposed parking spaces will meet resident demand, with additional visitor parking available on nearby streets and at the Georgetown GO Station.
- **Transportation Demand Management (TDM) Measures:** The project includes enhanced pedestrian connections, bicycle parking, and transit incentives to promote alternative transportation options.
- **Policy Alignment:** The development integrates well with the surrounding transportation network and aligns with sustainable mobility objectives and municipal planning policies.

To the north, the property abuts the Metrolinx parking lot for the Georgetown GO and Via Rail Station, primarily used by commuters during peak transit times (6 a.m. – 9 a.m. and 4 p.m. – 8 p.m.).

For further details, please refer to Appendix H: GHD - Updated Traffic Brief and Vehicle Swept Path Designs - 37 King Street, Georgetown (April 15, 2025)



**Figure 3: North of Property - Georgetown GO Station**  
(Google Maps, 2018)

On the south side of King Street, low-density residential units exist at a lower elevation than the site at 37 King Street. On the east side of Queen Street, there are additional low-density homes. While the surrounding area is primarily zoned as Low Density Residential 1-2 (LDR1-2), there are medium-density, low-rise apartment buildings in close proximity, located on Union Street and Queen Street. To the southwest, the site backs onto a retaining wall, part of which extends onto an adjacent property that is also designated as low-density residential.



**Figure 4: North-East of Property - King and Queen St**  
(Google Maps, 2018)



**Figure 5: South-West of Property - King and Queen St**  
(Google Maps, 2018)



**Figure 6: South-West of Property - Retaining Wall**  
(Google Maps, 2018)

Queen Street runs from the Georgetown GO Station to Guelph Street, while King Street spans from east to west, intersecting with Mountainview Road to the east. Both streets are two-lane roads, with one lane in each direction, and experience relatively low traffic and foot volumes.

## Noise and Vibration

Due to the site's proximity to the Georgetown GO Station, a Noise and Vibration Study has been completed and updated based on the revised design, as requested by Development Engineering. All recommended mitigations from the report will be implemented to ensure that the property's acoustic environment is suitable for residential use. For further details, please refer to Appendix S: Thornton Tomasetti - Updated Noise & Vibration Impact Study - 37 King Street, Georgetown (April 15, 2025)

The updated Noise and Vibration Impact Study has been circulated to CN Railway's legal team through Habitat HMD's legal team to facilitate the registration of the required Environmental Easement on the property title.

## Environmental Conditions

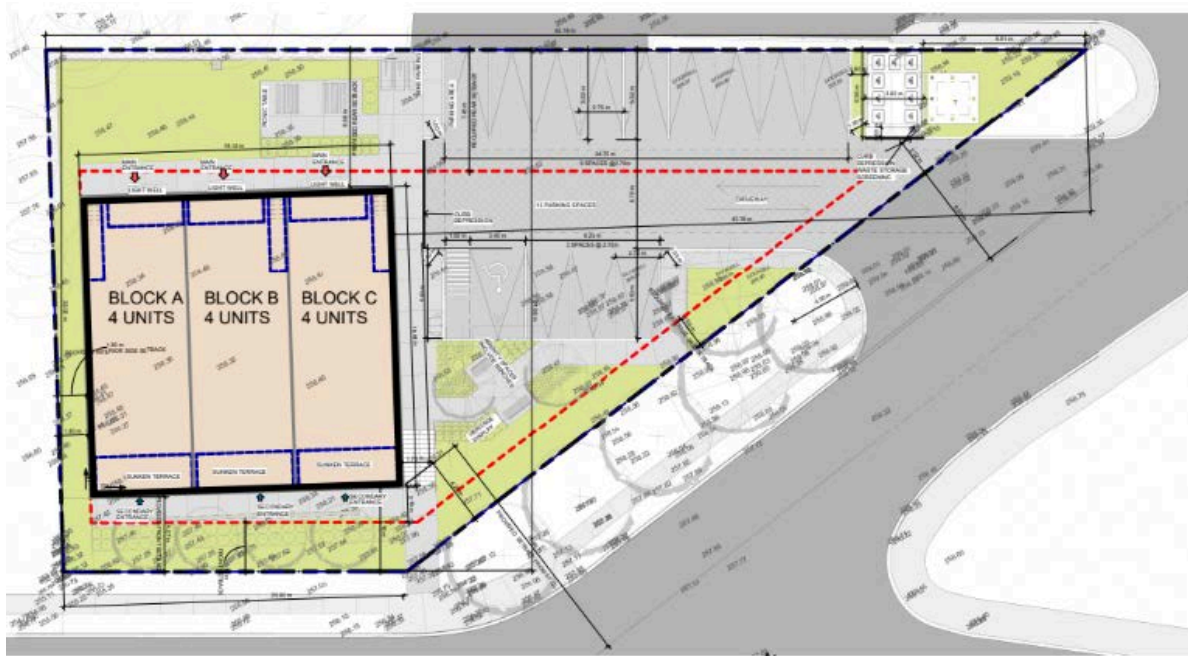
At the request of the Town of Halton Hills, MTE conducted a review of the Phase One (I) and Phase Two (II) Environmental Site Assessments (ESAs) and the Record of Site Condition (RSC). An updated Phase One Environmental Site Assessment report has

been included in the application submission. The report concludes that no new Potentially Contaminating Activities (PCAs) or Areas of Potential Environmental Concern (APECs) were identified, no additional Phase Two ESA sampling is recommended, and the RSC filed in October 2019 remains valid. For more details, please refer to Appendix Q: MTE - Updated Phase One ESA - 37 King Street, Georgetown (February 20, 2025)

## Proposed Development

The proposed development consists of three (3) blocks of stacked townhouses, each three (3) stories with walk-out basements, totaling four (4) units per block. The development will include outdoor parking.

The stacked townhouses will primarily have their main entrances facing the parking lot on the north side, which overlooks the amenity space. The basement units will have entrances facing King Street, accessible via stairs to the parking lot or through the sidewalk entrance. A new sidewalk will be constructed along the west side of Queen Street, from King Street to the Georgetown GO Station, providing a safe and accessible pedestrian route (located on public property). Additionally, a driveway entrance from Queen Street will run along the north side of the site to a small parking lot, which will include 13 parking spaces, including one accessible parking space (type A).



**Figure 7: Proposed Site Plan for 37 King St**  
(Chamberlain Architect Services Limited)

The Gross Floor Area (GFA) for all three blocks is 352 m<sup>2</sup> (3,788.9 sq. ft.), with a mix of 2- and 3-bedroom units, each offering living space.

## Waste Management

In consultation with the Region of Halton, a dedicated waste cart storage area will be located in a small, screened section at the northeast corner of the site. This communal cart system is designed to support Halton Region's municipal waste collection program and will accommodate garbage, recycling, and organic (green bin) waste. The storage area will feature a concrete pad and enclosure, with capacity for nine (9) carts: five (5) for garbage, three (3) for recycling, and one (1) for organics. Residents will deposit their waste directly into the appropriate carts. On collection days, Miller Waste staff will access the site on foot from Queen Street, transport the carts to the collection vehicle parked curbside, and return them to the storage area after servicing.

A Waste Management Plan, Supplementary Waste Plan, and Updated Site Plan - including waste storage locations, resident access paths, and collection routes - have been submitted as part of the planning application for review. For more details, please refer to Appendices U, R & L.

## Landscaping & Heritage Interpretation

Following our pre-consultation meeting with Town staff on April 25, 2024, we are collaborating with the Town's Public Art Coordinator to develop a Heritage Interpretation Plan, which will be included in our Site Plan application. In the meantime, Habitat HMD has incorporated an area on the site plan for interpretive signage, two benches, and a pollinator garden to honor the history of the site at 37 King Street and its significance to the Georgetown community. We are excited to continue refining these plans and will provide further details in the upcoming planning submission.

The site's landscape will feature a diverse range of native perennials and trees, enhancing the streetscape and serving as a gateway to the Georgetown GO Station. Plant selections will align with the Town of Halton Hills Green Development Standards, focusing on native and drought-resistant species that require no potable water for irrigation. Additionally, a sidewalk will be installed connecting to the GO parking lot, and fencing will be placed along the north and west sides of the property, in accordance with the Town's design guidelines to complement the aesthetic of the area.

For more details, please refer to Appendix C: Adesso Design - Updated Landscape Plan - 37 King Street, Georgetown (April 11, 2025)

## Planning Policy and Regulatory Context

The following policy documents guide the determination of the most appropriate land use for the site. The key elements of the requested amendments include:

- **Provincial Policy Statement (PPS, 2024):** The proposal aligns with the PPS objectives of promoting efficient land use, intensification, and housing diversity.
- **Halton Region Official Plan:** The development conforms to regional policies supporting urban intensification and the efficient use of land.
- **Town of Halton Hills Official Plan:** The property is designated Medium Density Residential in the Official Plan, supporting stacked townhomes and intensification.
- **Zoning By-law Compliance:** The property was rezoned in 2022 from Low Density to Medium Density to accommodate the increased unit count and optimize site utilization.

## Provincial Policy Statement, 2024

The Provincial Policy Statement (PPS) has undergone several significant updates, with the latest PPS 2024 coming into effect on October 20, 2024. This update consolidates previous policies to streamline land use planning across Ontario.

### Key Policies Relevant to the 37 King Street Project:

- **Efficient Development and Land Use Patterns:** The PPS 2024 encourages land use patterns that promote long-term municipal financial sustainability. The development fosters the growth of diverse housing options, including multi-unit housing, to meet both affordable and market-based housing needs.
- **Housing Policies:** Planning authorities are required to provide a range of housing options to meet the needs of current and future residents, including

social, health, economic, and well-being requirements.

- **Settlement Areas:** The PPS emphasizes promoting growth within settlement areas, advocating for land use patterns that are efficient and resource-conserving.

#### Planning Analysis for 37 King Street Project:

The proposed development at 37 King Street aligns with PPS 2024 in several key ways:

- **Location within a Settlement Area:** The site is located within Georgetown's urban area, a designated settlement area that encourages vitality and regeneration.
- **Efficient Use of Land and Resources:** The development promotes the efficient use of land and existing infrastructure, as it consists of multi-unit residential units.
- **Diverse Housing Options:** The project provides a mix of housing types, including affordable units, in line with the PPS's directive for diverse housing options.
- **Proximity to Amenities and Services:** The site's central location offers residents easy access to public transportation, employment, and community services.

## Halton Region Official Plan, 2024

As of July 1, 2024, the Halton Region Official Plan transitioned to serve as a guiding document for local municipalities. This update prioritizes intensification within urban boundaries to promote sustainable growth.

#### Key Policies Relevant to the 37 King Street Project:

- **Urban Area and Built-Up Area:** The site falls within a designated Urban Area and Built-Up Area, which encourages growth within existing urban boundaries to use infrastructure efficiently.

- **Intensification and Redevelopment:** The Official Plan supports intensification within the Built-Up Area to foster the creation of complete communities.
- **Housing Diversity and Affordability:** The Plan stresses the importance of a diverse range of housing options, including affordable housing.

#### Planning Analysis for 37 King Street Project:

The project aligns with the Official Plan's goals by:

- **Efficient Use of Infrastructure:** The site utilizes existing infrastructure and public services, supporting cost-effective growth.
- **Contribution to Housing Targets:** The development introduces multi-storey residential units, contributing to the Plan's goals for housing diversity.
- **Compatibility with Surrounding Uses:** The development is compatible with neighboring uses, with noise and vibration impacts mitigated through a specialized study.

## Halton Hills Official Plan, 2025

The 2025 update of the Halton Hills Official Plan addresses growth pressures and intensification opportunities, particularly in the urban areas, due to the area's proximity to the Greater Toronto Area (GTA).

#### Key Policies Relevant to the 37 King Street Project:

- **Site Location within the Georgetown GO Station Secondary Plan:** The site is located within the Georgetown GO Station Secondary Plan Area, which is designated as a Major Transit Station Area (MTSA). This encourages medium- to high-density development that supports transit and active transportation.
- **Intensification and Mixed-Use Development:** The OPA promotes higher-density residential, retail, and commercial spaces near major transit hubs.

#### Planning Analysis for 37 King Street Project:

The proposed development fits the Town's vision for the MTSA, promoting transit-oriented intensification and increasing housing diversity.

## **Zoning and Site-Specific By-law Amendment Request**

To align the project with the 2025 Official Plan Amendment, Habitat HMD is requesting several amendments to the zoning by-laws:

- **Density Adjustment:** Increase the density calculation to 88.2 units per hectare, which would allow for 12 residential units, aligning with community growth while maximizing the site's potential for affordable housing.
- **Requested Zoning By-law Amendments and Special Provisions:**
  - **Parking Reduction:** The development requests 13 parking spaces (including one accessible space) based on Bill 185, which removes parking minimums in Major Transit Station Areas.
  - **Building Height Adjustment:** The height limit will increase to 11.804 meters (from 10 meters) to accommodate modern design features, while ensuring compatibility with the surrounding neighborhood.
  - **Setback Adjustments:**
    - **Front Yard:** 4.57 meters
    - **Rear Yard:** 8.38 meters
    - **West Side Yard:** 1.8 meters
    - **East Side Yard:** 1.87 meters at its shortest point

## **Planning Justification & Community Impact**

The requested amendments are consistent with the 2025 Official Plan Amendment and contribute to the Town's urban growth strategy:

- **Affordable Housing:** The development supports affordable housing initiatives and offers a balanced growth approach that aligns with the region's needs.
- **Transit-Oriented Development:** The site's proximity to the Georgetown GO Station strengthens the project's alignment with the Town's strategy for transit-oriented development.
- **Community Enhancement:** The development will contribute to the community by improving green spaces, sustainable landscaping, and public amenities.
- **Minimized Environmental Impact:** Environmental and traffic impacts have been mitigated, ensuring a sustainable and efficient use of land.

The project at 37 King Street presents a thoughtful approach to sustainable growth, affordable housing, and community development. It is aligned with the Town of Halton Hills' planning objectives and contributes positively to the region's long-term development goals.

## Planning Applications

In order to permit the proposed residential development on this site, both an Official Plan Amendment and Zoning By-law Amendment are required. Draft amendments have been prepared and are appended to this Report (see Appendix A and B).

## Halton Hills Official Plan Amendment

Appendix A includes a draft proposed Amendment to the Halton Hills Official Plan.

The Medium Density Residential designation permits a density range of 21 to 50 units per hectare. The Proposed Development density for the Site is for 88.2 units per hectare.

DETAILS OF DEVELOPMENT	
Designation	Medium Density Residential Two Exemption 107 (MDR2)(107)(H1)
Lot Area	1357.06 m <sup>2</sup> (0.135706 ha)

Proposed Building Coverage (max)	27.00%		
Gross Floor Area (max)	352 m²		
No. of Residential Units (max)	12 units		
SITE STATISTICS PERTAINING OFFICIAL PLAN – (Halton Hills Official Plan, 2017)			
MDR1 Zone	Required	Provided	Requested Site Specific
Density	Min: 21 Units/ha Max: 50 Units/ha	88.2 Units/ha	Variance for 88.2 Units/ha to accommodate 12 units
Building Height	3 Storey	3 Storey	N/A

## Halton Hills Zoning By-law Amendment

Appendix B includes a draft proposed Amendment to the Halton Hills Zoning By-law.

The Medium Density Residential designation permits site-specific amendments based on the previous design and has been updated to reflect the new unit count and building dimensions.

DETAILS OF DEVELOPMENT			
Zoning	Medium Density Residential Two Exemption 107 (MDR2)(107)(H1)		
Lot Area	1357.06 m <sup>2</sup> (0.135706 ha)		
Proposed Building Coverage (max)	27.00%		
Gross Floor Area (max)	352 m <sup>2</sup>		
No. of Residential Units (max)	12 units		
SITE STATISTICS – (Halton Hills Zoning By-law 2010-0050)			
MDR1 Zone	Required	Provided	Requested Site Specific
Lot Width (max)	N/A	62.77m	N/A
Lot Area		0.135885 ha	N/A
Front Yard	4.5 m for Semi-Detached. 3.0 Metres for Townhomes (with motor vehicle access via a lane).	4.57 m	N/A

Rear Yard	7.5 m for Semi-Detached. 14.8 m for Townhomes (with motor vehicle access via a lane).	8.38m	N/A
Lot Frontage	7.0 m for Semi-Detached. 5.5 m for Townhomes.	19.12m	N/A
Interior Side Yard (only required on one side)	1.2 m for Semi-Detached. 1.2 m for Townhomes.	1.87m	N/A
Exterior Side Yard	3.0 m for Semi-Detached. 3.0 m for Townhomes.	1.8m (consistent with prior approvals)	Variance for Min 1.8m on west side
Density	N/A	88.2 Units/ha	Variance for 88.2 Units/ha to accommodate 12 units
Floor Area Ratio	N/A	0.273658	N/A
Amenity Area		339 m <sup>2</sup>	N/A
Privacy Area			N/A
Building Height	11.0 m	Max 11.804 m	Variance for 11.804 m Building Height
Porch Area	1.5 m Depth.	Min 1.8m Depth	N/A
Landscape Area and Buffer			
Landscape Area Abutting a Street	1.5 m	Min 2 m	N/A
Landscape Buffer Abutting Residential Zones	1 m	1.8 m on west property line	N/A
Parking			
Total Parking Spots	28	12	Variance for 12 parking spaces
Barrier Free Parking Spots	2	1	Variance for 1 type a accessible parking space

## Conclusion

In conclusion, the proposed development at 37 King Street represents a thoughtful and strategic response to the Town of Halton Hills' growth objectives and urban intensification goals. The development aligns with provincial, regional, and municipal policies, particularly the Provincial Policy Statement (PPS 2024), the Halton Region

Official Plan, and the Halton Hills Official Plan (2025), which prioritize sustainable, efficient land use and the creation of diverse housing options.

By introducing medium-density stacked townhomes with a mix of 2- and 3-bedroom units, the development offers a range of housing types that contribute to the local housing supply, including affordable units. The development's location near the Georgetown GO Station further supports transit-oriented growth, promoting pedestrian accessibility and reducing reliance on cars.

The proposal also emphasizes sustainability through the inclusion of native and drought-resistant landscaping, green space, and design elements that enhance the community's aesthetic and environmental value. Furthermore, efforts to mitigate noise and vibration impacts, alongside the provision of a waste management plan, reflect a commitment to ensuring a quality living environment for future residents.

While the proposed density of 88.2 units per hectare exceeds the current limit for Medium Density Residential areas, the amendment aligns with the Town's vision for intensification within transit-oriented locations. This proposal seeks to maximize the potential of the site while respecting the surrounding context, contributing positively to the character and growth of Halton Hills.

We look forward to working closely with Town staff, stakeholders, and the community to ensure the success of this project and its integration into the vibrant Georgetown community.

# Appendix A: Draft Official Plan Amendment

## By-Law No. 20XX-XXXX

A By-law to Amend NO. XX to the Official Plan of the  
Town of Halton Hills, 37 King Street (Georgetown)  
Lots 8, 9 and 10, Plan 37  
Town of Halton Hills, Regional Municipality of Halton  
(File:\_\_\_\_\_)

**WHEREAS** the Council of the Corporation of the Town of Halton Hills, is empowered to enact this By-law by virtue of the provisions of the Planning Act, 1990, R.S.O., c.p. 13, as amended;

**AND WHEREAS** the Regional Municipality of Halton, as the approval authority, has exempt this Official Plan Amendment from approval;

**AND WHEREAS** on \_\_\_\_\_, 202\_, Council for the Town of Halton Hills approved Report No. PD-202\_-\_\_, dated \_\_\_\_\_, 202\_, in which certain recommendations were made regarding amending the Official Plan for the Town of Halton Hills to permit the development of 12 stacked-townhouse units on the lands municipally known as 37 King Street, Georgetown.

### **NOW, THEREFORE, BE IT RESOLVED THAT THE COUNCIL OF THE CORPORATION OF THE TOWN OF HALTON HILLS ENACTS AS FOLLOWS:**

1. That Amendment No. XX to the Official Plan for the Town of Halton Hills, being the attached Schedule and text, is hereby approved;
2. That the Town Clerk is hereby authorized to circulate the Official Plan Amendment as provided for by the Planning Act, R.S.O. 1990, c.P.13, as amended, regarding the appeal process.

BY-LAW read and passed by the Council for Town of Halton Hills this \_\_\_\_th day of \_\_\_\_\_, 20XX.

\_\_\_\_\_  
MAYOR – ANN LAWLOR

\_\_\_\_\_  
TOWN CLERK – VALERIE PETRYNIAK

**OFFICIAL PLAN AMENDMENT NO. XX  
TO THE OFFICIAL PLAN FOR THE TOWN OF HALTON HILLS**

**PART A:**     **THE PREAMBLE** does not constitute part of this Amendment

**PART B:**     **THE AMENDMENT** consisting of the following Schedule and text  
constitutes Amendment No. XX to the Official Plan of the Town of Halton  
Hills

**AMENDMENT NO. XX TO THE OFFICIAL PLAN  
OF THE TOWN OF HALTON HILLS**

The attached text and schedules constitute Amendment No. XX to the Official Plan of the Town of Halton Hills, which was adopted by the Council of the Town of Halton Hills by By-law 20XX-XXXX in accordance with the provisions of the Planning Act, 1990, R.S.O., c.P. 13, as amended;

THE CORPORATION OF THE TOWN OF HALTON HILLS

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MAYOR - ANN LAWLOR

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CLERK - VALERIE PETRYNIAK

## **PART A - THE PREAMBLE**

### **PURPOSE OF THE AMENDMENT**

This Amendment is intended to allow the development of three (3) blocks of four (4) stacked-townhouse units for a total of twelve (12) stacked-townhouse units. Units will have a proposed height of no more than 3 storeys on the lands municipally known as 37 King Street (“subject lands”) within the Georgetown/Urban Area of the Town of Halton Hills.

Under the Town of Halton Hills Official Plan, the subject lands are currently designated as Medium Density Residential Two, Exemption 107 (MDR2)(107)(H1).

The approved policies for the Official Plan provide for permitted uses that include semi-detached dwellings and multiple unit buildings, but do not accommodate a units per residential hectare calculation beyond the maximum 50 Units/ha. The proposed development of 12 stacked-townhouse units has a units per residential hectare calculation of 88.2 Units/ha.

The Amendment proposes to change the Medium Density Residential units per residential hectare calculation to 88.2 Units/ha, which would permit the proposed development.

### **LOCATION AND SITE DESCRIPTION**

The subject site is described as Part of North Lot 18, Concession 9, Town of Halton Hills, Regional Municipality of Halton, 37 King Street (Georgetown). The 0.135885 hectare property is located on the North side of King Street and West side of Queen Street in the Community of Georgetown.

Surrounding land uses to the property include:

- To the North: Georgetown GO Station
- To the East: Single detached dwellings
- To the South: Single detached dwellings
- To the west: Single detached dwellings

The subject lands are currently vacant.

## **BASIS FOR THE AMENDMENT**

The following planning and land use considerations have been identified and are considered appropriate to provide for the change to the units per residential hectare calculation for a Medium Density Residential Area:

1. The proposed medium density development abutting to the Georgetown GO Station conforms to Provincial Policy which encourages intensification adjacent to major transit stations;
2. The proposed development reinforces the streetscape edge, reflects the important historical reference of the previous Exchange Hotel through interpretation signage, provides for an upgraded gateway feature for the GO Station entrance and should easily be able to integrate into the character of the surrounding neighbourhood;
3. The conceptual submitted Site Plan submitted drawing demonstrates that the site can be developed with adequate on-site parking, a private road be developed with a private road and private amenity areas for each unit; and,
4. Council is satisfied that the proposed development is consistent with the criteria set out in the Official Plan that provides guidance for developments seeking to establish Medium Density Residential units by way of an Amendment to the Plan.

## **PART B - THE AMENDMENT**

All of this part of the documents consisting of the following Schedule and Text constitutes Amendment No. XX of the Official Plan for the town of Halton Hills.

### **DETAILS OF THE AMENDMENT**

The Official Plan for the Town of Halton Hills is amended as follows:

1. That Schedule H3 – Georgetown GO Station Area Land Use Plan is hereby amended by marking with a number “X” and adding a solid red line around the lands shown in Schedule ‘1’ to this Amendment, municipally known as 37 King Street, Georgetown.
2. That Section H3.9 of the Georgetown GO Station Secondary Plan for the Town of Halton Hills is hereby amended by adding a new subsection, which shall read as follows:

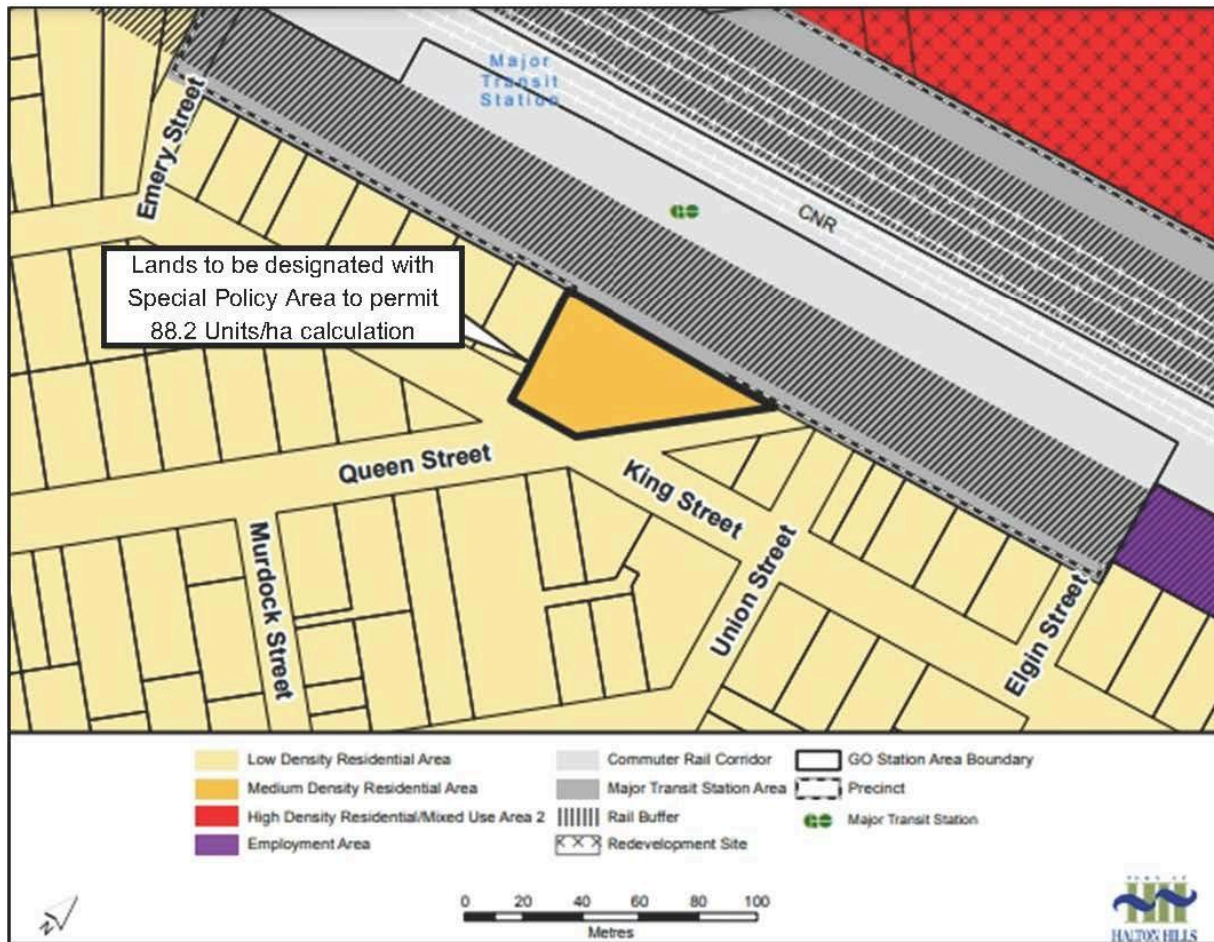
#### **H3.9.X Special Policy Area X**

The following policies apply to lands designated as Medium Density Residential and identified as Special Policy Area X, as shown on Schedule H3 of this Plan:

##### **a) Unit per Residential Hectare Calculation**

For the site as a whole, a maximum unit per residential hectare calculation of 88.2 Units/ha is permitted.

## SCHEDULE 1 to OPA No. XX



# Appendix B: Draft Zoning By-Law Amendment

## By-Law No. 20XX-XXXX

Being a By-law to Amend Zoning By-Law 2010-0050, as amended,  
Lots 8, 9 and 10, Plan 37  
Town of Halton Hills, Regional Municipality of Halton  
Municipally known as 37 King Street (Georgetown)

**WHEREAS** Council is empowered to enact this By-law by virtue of the [provisions of Section 34 of the Planning Act, R.S.O. 1990, as amended;

**AND WHEREAS** on \_\_\_\_\_, 20XX, Council for the Town of Halton Hills approved Report No. PD-20XX-00XX, dated \_\_\_\_\_, 20XX, in which certain recommendations were made relating to amending Zoning By-law 2010-0050;

**AND WHEREAS** Council has recommended that Zoning By-law 2010-0050 be amended as hereinafter set out;

**AND WHEREAS** said recommendation conforms to the Official Plan for the Town of Halton Hills, as amended by Official Plan Amendment No. 43.

**NOW, THEREFORE, BE IT RESOLVED THAT THE COUNCIL OF THE CORPORATION OF THE TOWN OF HALTON HILLS ENACTS AS FOLLOWS:**

3. That Table 13.1: Exceptions of Zoning By-law 2010-0050 is hereby amended by adding the Exception Provision contained in Schedule “2” and forming part of this By-law.

**BY-LAW** read and passed by the Council for the Town of Halton Hills this \_\_\_\_th day of \_\_\_\_\_, 20XX.

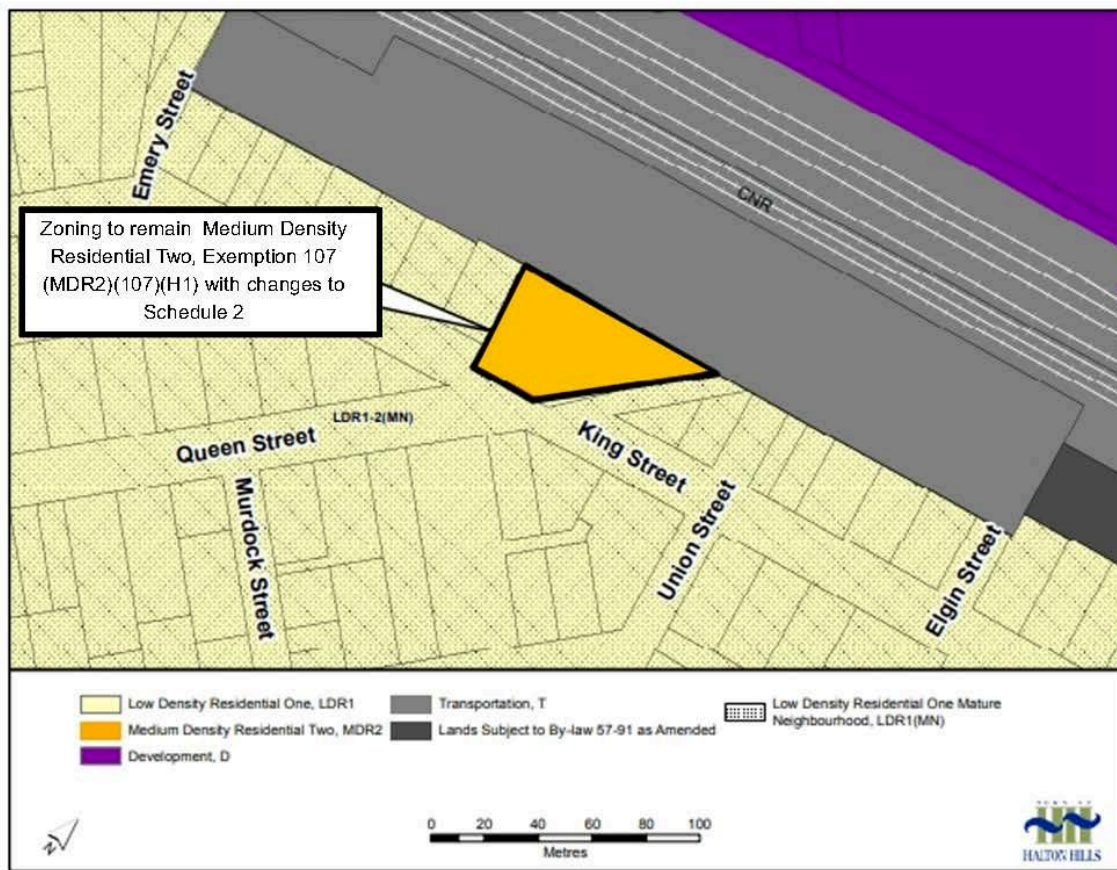
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MAYOR - ANN LAWLOR

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CLERK - VALERIE PETRYNIAK

## SCHEDULE 1 to By-law 20XX-XXXX



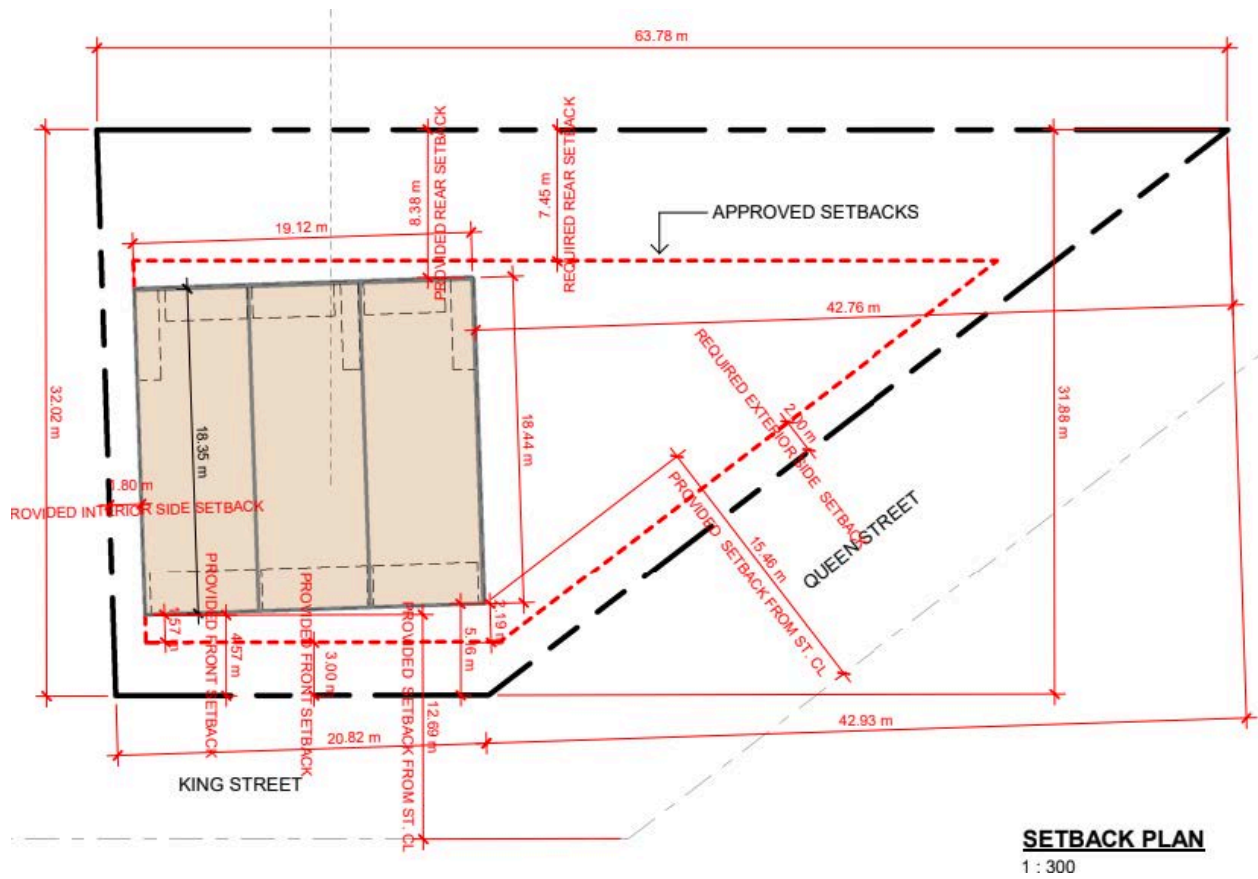
## SCHEDULE 2 to By-law 20XX-XXXX

### 13.1 EXCEPTIONS

1	2	3	4	5	6	7
Exemption Number	Zone	Municipal Address	Additional Permitted Uses	Only Permitted Uses	Uses Prohibited	Special Provisions
107	MDR2	37 King Street (Georgetown) Lots 8, 9 and 10, Plan 37	(i) Semi-Detached Dwellings			<p>(i) Minimum required number of parking spaces – 13 parking spaces;</p> <p>(i) For the purpose of this zone “Multiple Unit Building” means a building that is vertically divided into a maximum of 12 units, each of which has an independent entrance at grade to the front and rear of the building and each of which shares a common wall that has a minimum height of 2.4 metres and a depth of 6.0 metres above grade.</p> <p>(ii) Minimum required front yard setback – as shown on Schedule 3 to this By-law;</p> <p>(iii) Minimum required interior side yard setback – as shown on Schedule 3 to this By-law;</p> <p>(iv) Minimum required exterior side yard setback – as shown on Schedule 3 to this By-law;</p> <p>(v) Minimum required</p>

						<p>rear yard setback – as shown on Schedule 3 to this By-law;</p> <p>(vi) Porches, decks and/or stairs may encroach into any required yard setback no more than 2 metres;</p> <p>(vii) Features including but not limited to eaves or gutters, chimney breasts, pilasters and roof overhangs may encroach into any required yard setback no more than 1.5 metres.</p> <p>(viii) Maximum Height –</p> <p>11.804 metres</p>
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# Schedule 3 to By-law 2021-\_\_\_\_\_



## **Appendix C: Adesso Design - Updated Landscape Plan - 37 King Street, Georgetown (April 11, 2025)**

## **Appendix D: Chamberlain Architect Services Ltd. - Latest Floor Plans & Elevations - 37 King Street, Georgetown (March 21, 2025)**

## **Appendix E: Chamberlain Architect Services Ltd. - Latest Renderings - 37 King Street, Georgetown (March 28, 2025)**

# **Appendix F: Chamberlain Architect Services Ltd. - Latest Updated Site Plan - 37 King Street, Georgetown (April 11, 2025)**

# **Appendix G: Chamberlain Architect Services Ltd. - OBC Data Matrix - 37 King Street, Georgetown (April 4. 2025)**

## **Appendix H: GHD - Updated Traffic Brief and Vehicle Swept Path Designs - 37 King Street, Georgetown (April 15, 2025)**

# **Appendix I: Habitat HMD - Proof of Development Review Fee Payment (9 Units) - 37 King Street, Georgetown (April 17, 2025)**

**Appendix J: Habitat HMD - Signed Official  
Plan & Zoning By-law Amendment  
Application - 37 King Street, Georgetown  
(March 20, 2025)**

## **Appendix K: Habitat HMD - Updated OPA & ZBLA Application Fees Credit - 37 King Street, Georgetown (April 22, 2025)**

# **Appendix L: Habitat HMD - Updated Site Plan with Waste Removal and Storage Locations & Paths - 37 King Street, Georgetown (April 17, 2025)**

# **Appendix M: Halton Region - Section 59(2b) Notice - 37 King Street, Georgetown (April 18, 2024)**

**Appendix N: Halton Region - Signed  
Updated Risk Management Plan  
(2020-12-08-CTC-RMP-58) - 37 King Street,  
Georgetown (April 7, 2025)**

## **Appendix O: MTE - Functional Servicing and Stormwater Management Report - 37 King Street, Georgetown (February 14, 2025)**

**Appendix P: MTE - Updated Functional Site  
Grading and Esc Plan (C1.1) & Functional  
Site Servicing Plan (C1.2) - 37 King Street,  
Georgetown (April 11, 2025)**

## **Appendix Q: MTE - Updated Phase One ESA - 37 King Street, Georgetown (February 20, 2025)**

## **Appendix R: Region of Halton - Completed Supplementary Waste Plan - 37 King Street, Georgetown (April 4, 2025)**

# **Appendix S: Thornton Tomasetti - Updated Noise & Vibration Impact Study - 37 King Street, Georgetown (April 15, 2025)**

# **Appendix T: Town of Halton Hills - Pre-Consultation Notes - 37 King Street (June 6, 2024)**

## **Appendix U: Habitat HMD - Waste Management Plan - 37 King Street, Georgetown (April 22, 2025)**