

# SECTION

BUILT FORM

5.0



The built form of the proposed Russell Pines development is designed to create a cohesive and high-quality community through a variety of housing types and a strategically located commercial/mixed-use area. Built form directly influences the character of the community by shaping its visual identity, establishing a sense of place, and contributing to a vibrant streetscape.

Architectural variety is balanced with consistency in design to ensure compatibility across the development. Careful attention is given to the placement, scale, and massing of buildings, ensuring smooth transitions between different housing types and the commercial area. The built form enhances the livability of the community by defining public spaces, supporting pedestrian activity, and fostering a visually engaging environment.



## 5.1 Housing Forms

The proposed development includes a total of 744 units, featuring a diverse range of housing types, including single-detached homes, dual-frontage homes, on-street townhouses, dual-frontage townhouses, and back-to-back townhouses. The design and placement of these housing types play a key role in shaping the streetscape and defining the overall image of the community.

**The Russell Pines development will provide the following unit mix:**

- 159 single detached units (11.0m min. lot frontage);
- 39 single detached units (9.15m min. lot frontage);
- 76 dual frontage single detached units (9.80m min. lot frontage);
- 169 on-street townhouses (6.1m min. lot frontage);
- 111 dual frontage townhouses (6.1m min. lot frontage); and
- 190 back-to-back townhouses.

This mix of housing types offers a broad range of lifestyle options and helps create a dynamic and diverse community. Architectural design will emphasize variety in styles, materials, and detailing while maintaining cohesion in massing and setbacks. This approach ensures that each street retains a consistent rhythm while offering visual interest and fostering a strong sense of identity.



### Single Detached Homes

Conventional single-detached homes are distributed throughout the proposed development, fronting onto local roads to create a well-defined and pedestrian-friendly streetscape. In areas adjacent to protected open spaces, homes will be situated to take advantage of scenic views, offering residents direct visual connections to natural features while ensuring an appropriate interface with environmentally sensitive areas.

To enhance visual diversity and maintain a dynamic streetscape, a variety of house designs will be introduced. These designs will balance architectural variety with overall cohesion to avoid repetitive patterns and ensure an attractive neighbourhood character. Alternate elevation treatments will be employed to differentiate individual homes, incorporating variations in massing, building forms, rooflines, front entry designs, garage placement and treatments, window arrangements, architectural detailing, and exterior materials.

This approach ensures that while each home offers a unique appearance, the streetscape remains unified, promoting a visually engaging and high-quality built form throughout the community.



## Dual Frontage Single Detached Homes

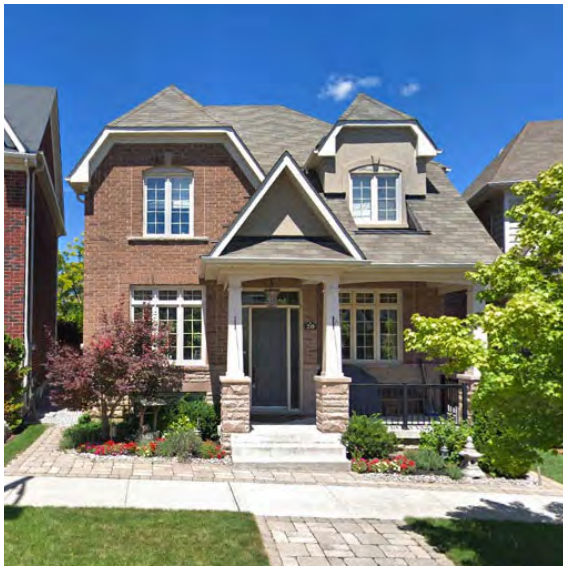
Dual-frontage single-detached homes are proposed along the southern edge of the subject site, adjacent to 10th Line, as well as along the edges of the proposed collector roads, including the western edge of Street 'A', a portion of Street 'C', and both sides of Street 'E'. These homes have been strategically placed to enhance the streetscape and ensure seamless integration with the surrounding road network and community layout.

To create a visually engaging streetscape, these homes will feature distinctive architectural treatments on both frontages. This dual-frontage approach ensures a consistent and attractive appearance, providing an enhanced visual experience from both the primary street and the secondary frontage, where homes face collector roads or open spaces.

Architectural elements will include varied massing, distinctive rooflines, and diverse entry and garage treatments. Fenestration styles, material palettes,

and decorative details will be varied to reduce repetition and maintain visual interest. While promoting architectural diversity, the designs will adhere to a cohesive architectural theme that complements the broader development.

In addition to enhancing aesthetics, dual-frontage homes provide the benefit of uninterrupted primary streetscapes, as driveways are located on the secondary frontage, resulting in continuous sidewalks and landscaped frontages. This design also minimizes blank walls and enhances natural surveillance, particularly in locations overlooking collector roads and adjacent open spaces. The integration of architectural detail on both frontages creates a welcoming and harmonious streetscape that strengthens the overall character of the community.





### On-Street Townhouses

Conventional on-street townhouses are proposed fronting on local road with garages and driveways accessed at the front of the building. Ample fenestration will provide a great sense of safety with eyes on the street and increased fenestration at the corners will address the intersections. Consideration should be given to breaking up the overall building massing of individual townhouse blocks. Roofscapes within individual townhouse blocks should vary, where possible, to contribute to the creation of interesting streetscapes to avoid large, plain roof masses. Main entrances for exterior end units should be located on the flankage elevation to address both street frontages. The side elevation of exposed corner units shall be specifically designed to respond to public exposure and the additional light source by means of articulated building faces, fenestration, and detailing equal to that of the front elevation.





## Dual Frontage Townhouses

Similar to dual-frontage single-detached homes, dual-frontage townhouses enhance the streetscape by providing an uninterrupted primary frontage, as driveways are located on the secondary frontage. This approach results in continuous sidewalks, landscaped frontages, and a consistent architectural rhythm along the primary streetscape.

Dual-frontage townhouses are proposed along key edges, including 10 Side Road, the south-eastern edge of Street 'A', the south-western edge of Street 'B', and directly adjacent to the proposed Norval West Bypass. Equal attention will be given to architectural detailing on both frontages, ensuring a high-quality design on all visible sides.

Ample fenestration will contribute to safety by providing “eyes on the street,” while increased fenestration at corners will address intersections and enhance the pedestrian experience. The overall building massing of individual townhouse blocks will be carefully considered to avoid monotonous streetscapes and maintain visual interest. Roofscapes within townhouse blocks will vary where possible, contributing to architectural variety and avoiding large, uniform roof masses.

Main entrances for corner units will be located on the flankage elevation, ensuring that both street frontages are well addressed. The side elevations of exposed corner units will feature articulated facades, enhanced fenestration, and detailing consistent with the front elevation, ensuring a well-rounded architectural treatment that responds to public exposure and additional light sources.



### Back-To-Back Townhouses

Back-to-back townhouses are proposed in the southwestern portion of the subject site along local roads, providing a higher-density housing option within the community. These units will feature private amenity spaces, including balconies, lower-level patios, or rooftop terraces, offering outdoor living opportunities for residents.

Each unit will have a covered front entrance oriented toward the street or mews, enhancing the pedestrian experience and fostering a sense of community. Ample fenestration at each floor level will ensure natural light penetration, improving interior living conditions and promoting a sense of safety by maintaining “eyes on the street” through their architectural prominence.





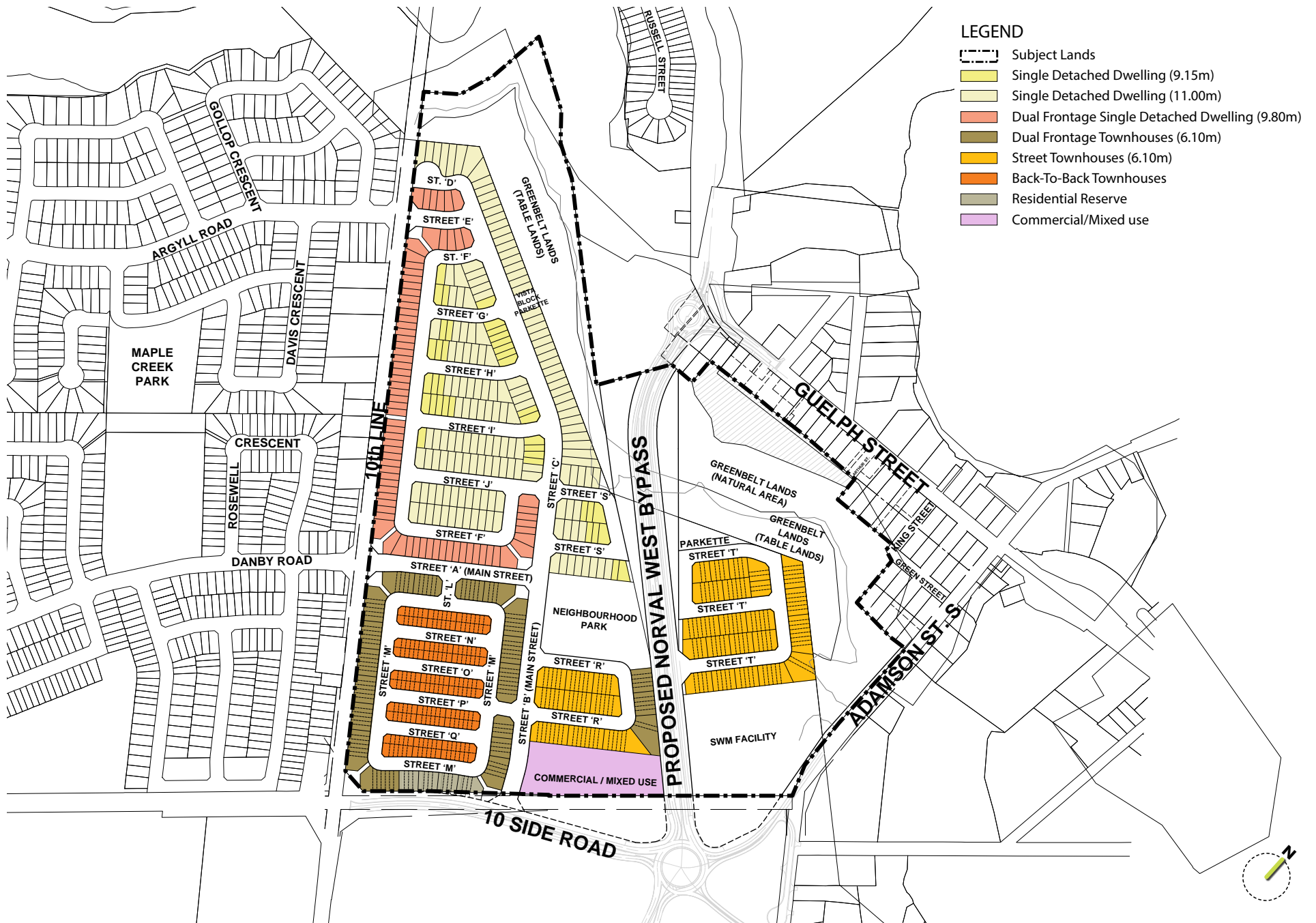


Figure 15: Built Form Diagram



## 5.2 Commercial / Mixed Use

The commercial and mixed-use block is situated between the northwest corner of the Norval West Bypass and 10 Side Road and the northeast corner of 10 Side Road and proposed Street B, serving as a key focal point within the development. This block is designed to accommodate a variety of commercial and office uses, supporting local services and providing opportunities for community interaction and employment.

The built form in this area will promote an active and engaging street edge by orienting buildings toward 10 Side Road and proposed Street B, ensuring a strong relationship between the public realm and private spaces. Architectural design will prioritize high-quality materials and cohesive treatments across all visible elevations, contributing to a visually appealing streetscape. Opportunities for landmark architectural elements will be explored to emphasize this block's gateway function and create a distinct identity for the community.

To maintain a pedestrian-friendly environment, parking will be positioned behind the buildings, minimizing its presence along the main streets. The design will encourage ground-floor activity through large storefront windows, where appropriate, to enhance street-level interaction and passive surveillance.

The commercial mixed-use block will be subject to a future site plan application process, during which detailed design elements, including massing, materials, and architectural articulation, will be further refined to ensure alignment with the development's overall vision and urban design objectives.

The design of this block will integrate seamlessly with adjacent residential areas by employing appropriate massing and transition strategies. This ensures compatibility with surrounding housing while establishing a vibrant and accessible community hub that meets local service and amenity needs.



### 5.3 Priority Lot Plan

Priority lots are strategically located in prominent and publicly visible areas within the community and are designed to contribute to the overall visual identity and character of the streetscape. The built form on these lots plays a crucial role in enhancing the aesthetic appeal of the development, ensuring that key community features and focal points are well-defined through thoughtful architectural design.

The proposed Russell Pines development includes a variety of priority lot types, each carefully positioned to respond to unique site conditions and design objectives. These priority lots include:

- **Gateway lots**
- **Corner lots**
- **Elbow lots**
- **T-junction lots**
- **Side upgrade lots**
- **Lots fronting or backing onto open spaces**

To ensure these lots contribute meaningfully to the community's design, their placement and architectural treatment will be guided by specific design strategies, such as enhanced massing, articulation, fenestration, and material detailing. The intent is to create visually engaging streetscapes, promote natural surveillance, and establish a cohesive neighbourhood character.

The locations of these priority lots are illustrated in Figure 16.

#### 5.3.1 Gateway Lots

Special opportunities exist at entrances into the community to create gateway buildings. Gateway houses should be considered on the special corner lots. The design of these gateways may be handled through a variation in height or built form, building detailing, building color, landscape elements and plantings. Gateway buildings located on opposite corners should be similar or compatible in architectural style, elements and details to ensure visual harmony.

The design of gateway buildings should incorporate traditional architectural elements such as wrap-around porches, turrets and bay windows or sun terraces, as appropriate to the architectural style of the unit and the architectural theme of the community. A special building model should be designed for these locations.





### 5.3.2 Corner Lot Architecture

Corner lots are characterized by their exposure to two street frontages, which permits a variety of main entry and garage access configurations. Designs for corner lots are to take advantage of the opportunities for the community plan and the level of exposure. The design of corner lot houses is to provide a consistent level of detailing on all publicly exposed elevations. The flankage and rear elevations should introduce sufficient fenestration displaying balanced proportions.

### 5.3.3 T-Intersection Lots

T-Intersection lots are located at the end of a view corridor, and are framed by two corner lots flanking the terminated street. These dwellings are viewed more frequently and for prolonged periods while traveling through community streets. Careful considerations should be given to the selection of houses that de-emphasize the presence of the garage, and driveway locations that favor a larger area for landscaped treatment in the front yard.



### 5.3.4 Elbow Lots

On curved, elbowed streets, special attention should be given to those dwellings where the bend of the street can partially expose the interior side elevation, as they are viewed from along the length of the street. On elbowed streets special opportunities exist on the outside or visually highlighted side of the road-bend to create a special grouping of buildings. The grouping of buildings occurring at the end of a view corridor should consider the selection of houses that de-emphasize the presence of the garage.

### 5.3.5 Lots Fronting onto Parks & Open Spaces

Any buildings fronting onto parks and open spaces should make full use of the opportunities presented by these special locations and reinforce their significance. Buildings at this priority location shall present a consistent level of architectural detailing and fenestration in the design of all publicly exposed elevations.



## 5.3.6 Buildings Adjacent to Open Spaces or Vista Blocks (Side & Rear Upgrade)

Any buildings flanking or backing onto open spaces, walkways or vista blocks shall present a consistent level of architectural detailing and fenestration, in the design of all publicly exposed elevations. These publicly exposed elevations should introduce sufficient fenestration displaying balanced proportions, wall plane changes or projecting bays along with gable features to break up the roofline.

To make full use of the opportunities presented by these special locations and to reinforce their significance, these buildings will address the following guidelines:

- Special consideration shall be given to the quality of the architectural design, the amount and quality of detailing, and the type and quality of materials and finish on all exposed elevations;
- Designs are encouraged to provide an architectural feature using elements such as traditional details including wrap-around porches, or bay windows; and
- Flankage and rear yard fencing should be designed to reinforce the visual importance of these lots and be coordinated with the whole community.





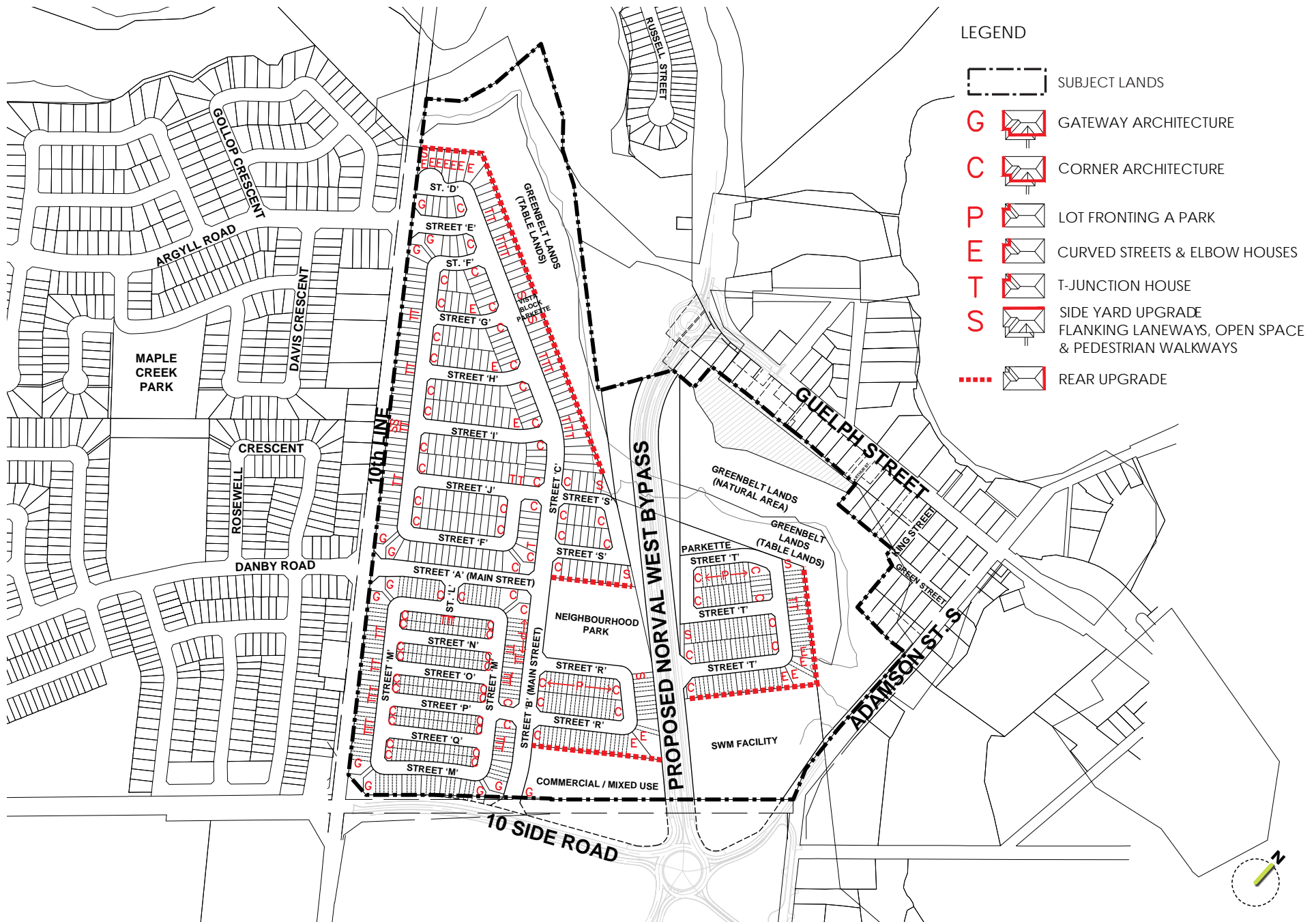


Figure 16: Priority Lot Plan

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