

SECTION

SITE LOCATION AND ANALYSIS

2.0



2.1 Existing Conditions

The subject site is located within the Georgetown Urban Area, positioned strategically to the east of 10th Line, north of 10 Side Road, northwest of Adamson Street South, and southwest of Guelph Street. This site is designated as a “Future Residential/Mixed-Use Area” aligning with broader municipal objectives to accommodate a range of uses, including residential, commercial, and community-serving amenities. The surrounding context comprises a diverse mix of established and developing land uses, which inform and influence the urban design considerations for the proposed development.

In its current condition, the subject site primarily consists of undeveloped land and natural landscapes.

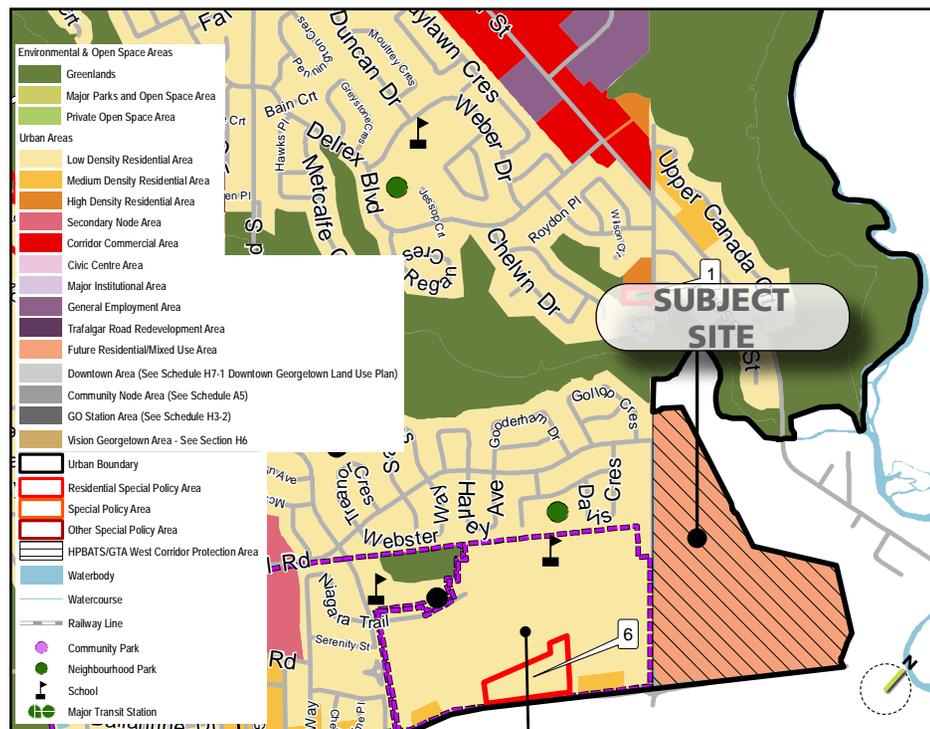


Figure 1: Town of Halton Hills Official Plan - Schedule A3: Georgetown Urban Area Land Use Plan

Surrounding Context

North:

- Guelph Street, a significant corridor featuring commercial establishments that provide a variety of goods and services.
- Existing low-rise residential dwellings, contributing to the area’s established residential character.
- St. Gregorios Indian Orthodox Church Norval Parish
- Parks and an existing Natural Heritage System, providing green infrastructure and passive recreational opportunities.
- The northeast portion of the area transitions into Norval Hamlet, a historic settlement outside of the urban boundary.

South:

- Existing agricultural lands, reflecting the rural character of the area.
- 10 Side Road and 10th Line frame the southeast and southwest edges of the subject site, respectively.
- Southwest features low-rise residential neighborhoods, with parks, schools, and commercial uses supporting the community’s needs.

East:

- Adamson Street South acts as a connector to adjacent neighborhoods and community facilities.
- Hillcrest Cemetery.

West:

- The northwest area includes an existing Natural Heritage System, supporting biodiversity and ecological function.
- Existing trails, offering recreational opportunities and connectivity.
- Low-rise residential dwellings, complementing the established residential context of the surrounding area.

The subject site benefits from its proximity to a diverse array of land uses, including residential neighborhoods, educational institutions, commercial amenities, and green infrastructure. These elements establish a well-rounded urban framework and present an opportunity to create a vibrant, mixed-use community that complements and enhances the surrounding area.

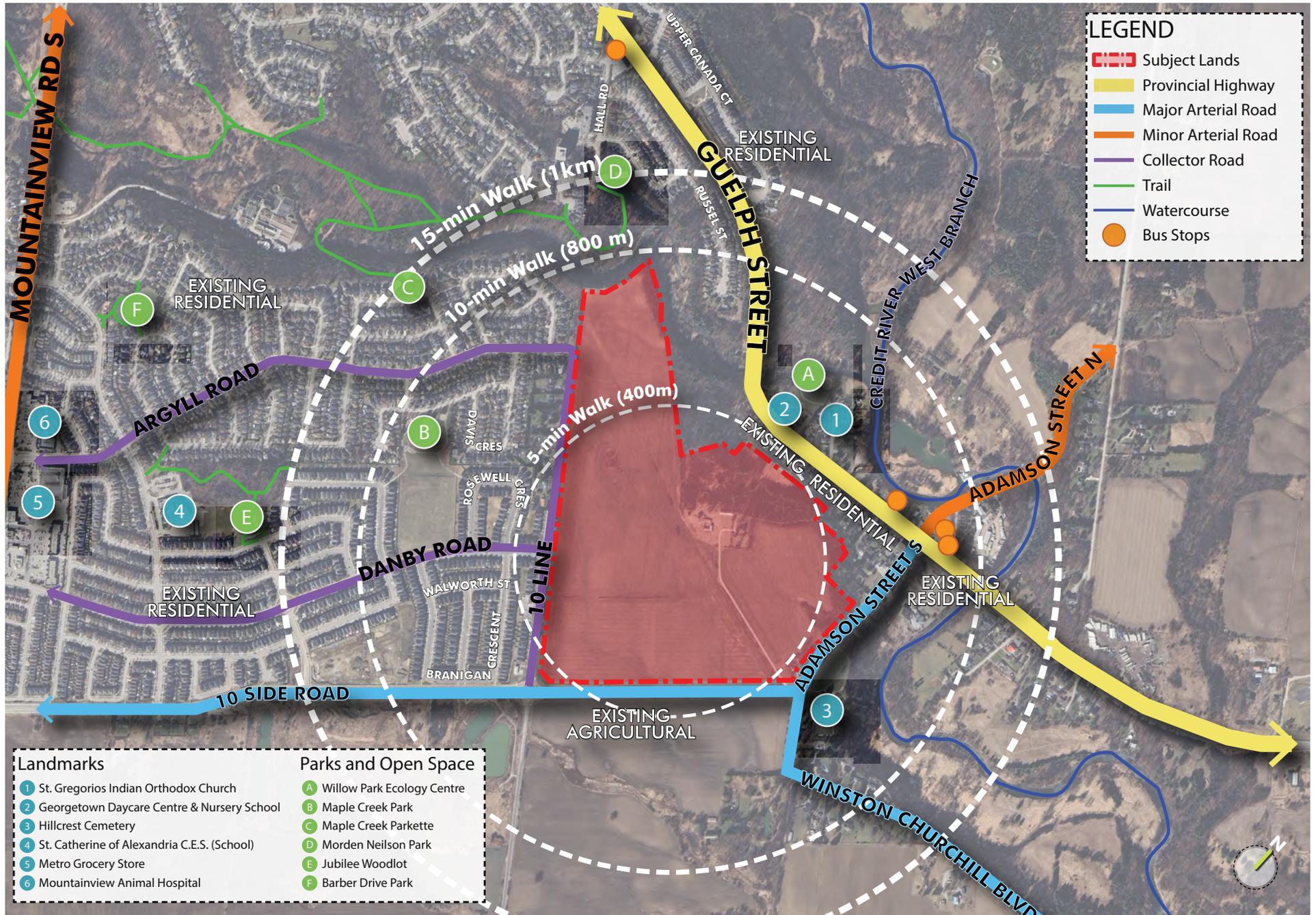


Figure 2: Context Map



Figure 3: Site Context Images - Key Map



1 View of subject site from the south at the intersection of 10 Side Rd and 10 Line



2 View of subject site from 10 Side Rd



3 View of cemetery east of subject site at the intersection of 10 Side Rd and Adamson St. S.



4 View existing residential home east of subject site from Adamson St. S.



5 View existing retail north east of subject site at the intersection of Adamson St. S. and Geulph St.



6 View of St. George Syriac Orthodox Church north of subject from Guelph St.



7 View along Argyll Rd if existing residential community west of subject site from 10 Line



8 View of subject site from the west along 10th Line



9 View west of subject site of Maple Creek Park

2.2 Opportunities and Constraints

The proposed development is designed to support a diverse range of uses, with a primary emphasis on low-rise and mid-rise residential buildings, including single-detached homes and townhouse units. The plan also incorporates a commercial/mixed-use block, a stormwater management block, a neighbourhood park, parkette and vista park block. Thoughtful integration with the existing natural heritage area to the north is a key aspect of the design, complemented by a connected road network linking to neighbouring residential areas to the east and west. The following opportunities and constraints have been identified to ensure seamless integration of the proposed development within its surrounding context:

OPPORTUNITIES

- **Enhanced Connectivity:** The site benefits from proximity to key transportation corridors, including Guelph Street (a provincial highway) to the north and major arterial roads (10 Side Road and Adamson Street South) to the east, providing strong regional links for residents and visitors. Additionally, the proposed Norval West Bypass offers an opportunity to improve local traffic flow while alleviating congestion on existing routes. The new bypass and collector roads can also incorporate multi-modal infrastructure, such as bike lanes and pedestrian walkways, to promote active transportation.
- **Integration with Natural Features:** Proximity to Greenbelt lands to the north creates opportunities to incorporate natural elements into the design, fostering green linkages, trails, and open spaces.
- **Community Character:** Being adjacent to Norval Hamlet and an established residential community offers a chance to design a development that complements the local character, fostering a sense of community cohesion.
- **Vibrant Mixed-Use Edge:** The proximity to high-traffic roads supports the inclusion of mixed-use components, contributing to a vibrant and active frontage along 10 Side Road.

CONSTRAINTS

- **Bypass Division:** The proposed Norval West Bypass runs through the site, creating a physical separation between different parts of the development. Careful design will be needed to ensure that the bypass corridor does not hinder the sense of community and that both sides of the development are visually and functionally cohesive.
- **Transit Connectivity Gaps:** Although bus stops exist north of the site, the current transit network requires enhancement to ensure convenient access for future residents. Strengthening transit service will be essential to support mobility and reduce dependence on personal vehicles.

2.3 Policy Context

2.3.1 The Planning Act (RSO; 1990)

The **Planning Act** was established to guide land use development and promote sustainable growth in Ontario. It outlines matters of provincial interest, provides a framework for regulating land uses, and assigns responsibilities to authorities that oversee land use planning.

Part I, Section 2 of the *Planning Act* identifies matters of provincial interest that must be addressed consistently across all municipalities. The following matters are directly relevant to the proposed development:

- (a) the protection of ecological systems, including natural areas, features and functions;
- (e) the supply, efficient use and conservation of energy and water;
- (f) the adequate provision and efficient use of communication, transportation, sewage and water services and waste management systems;
- (h) the orderly development of safe and healthy communities;
- (h.1) the accessibility for persons with disabilities to all facilities, services and matters to which this Act applies;
- (i) the adequate provision of a full range of housing, including affordable housing;
- (o) the protection of public health and safety;
- (p) the appropriate location of growth and development;
- (q) the promotion of development that is designed to be sustainable, to support public transit and to be oriented to pedestrians;
- (r) the promotion of built form that:
 - i. is well-designed,
 - ii. encourages a sense of place, and
 - iii. provides for public spaces that are of high quality, safe, accessible, attractive and vibrant;
- (s) the mitigation of greenhouse gas emissions and adaptation to a changing climate.

The proposed development introduces a pedestrian-friendly residential neighbourhood that aligns with the intent of the Planning Act and addresses key matters of provincial interest. It supports the orderly and sustainable growth of the community by ensuring appropriate transitions in built form and promoting a well-connected street network. Public safety and accessibility are prioritized through safe, vibrant public spaces and design measures that enhance mobility for all, including persons with disabilities.

In addition, the development emphasizes energy and water efficiency, integrates multi-modal transportation options, and provides a diverse range of housing to meet evolving community needs. By incorporating high-quality public spaces and supporting transit-oriented, pedestrian-friendly design, the proposed neighbourhood fosters a sense of place and contributes to the creation of a healthy, inclusive, and sustainable community.

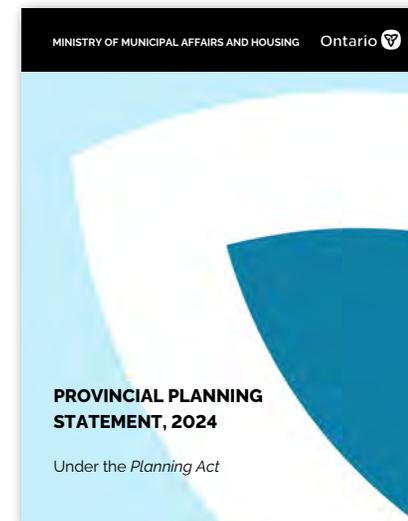
2.3.2 Provincial Policy Statement (2024)

The 2024 Provincial Policy Statement (PPS) outlines Ontario's land use vision while also providing a framework for growth and resource management. The objective of the PPS is to promote the effective use of land and resource conservation methods to support the development of vibrant communities, a clean and healthy environment, and a strong economy. The proposed development works to support the following principles:

- Growth and development within urban and rural settlement areas while supporting the viability of rural areas.
- Wise management of land use change may involve directing, promoting, or sustaining development.
- Land use must be carefully managed to accommodate appropriate development to meet the full range of current and future needs, while achieving efficient development patterns and avoiding significant or sensitive resources and areas which may pose a risk to public health and safety.
- Efficient development patterns optimize the use of land, resources and public investment in infrastructure and public service facilities.
- Promote a mix of housing, including affordable housing, employment, recreation, parks and open spaces, and transportation choices that increase the use of active transportation and transit before other modes of travel.
- Strong, liveable, and healthy communities promote and enhance human health and social well-being, are economically and environmentally sound, and are resilient to climate change.
- The wise use and management of these resources over the long term is a key provincial interest.
- Conserve land and resources avoids the need for costly remedial measures to correct problems and supports economic and environmental principles.
- Long-term prosperity, human and environmental health and social well-being should take precedence over short-term considerations.

The proposed development introduces a range of uses, including single-detached and townhouse dwellings, a commercial/mixed-use block, parks, open spaces, and a stormwater management block. These elements have been efficiently planned and appropriately located to optimize land use and infrastructure within the subject site. The design ensures that land is used thoughtfully by integrating various uses and open spaces, promoting a complete community that supports diverse living, working, and recreational opportunities.

Situated within the broader context of the Georgetown Urban Area, the proposed development aligns with the Provincial Policy Statement (PPS) by fostering a well-designed community with a strong sense of place. It builds upon existing natural resources and incorporates thoughtfully designed public spaces, contributing to a high quality of life for future residents. The development also supports long-term prosperity and environmental sustainability by offering a mix of housing options, commercial uses, and recreational amenities, all of which complement the surrounding land uses within the Town of Halton Hills.



2.3.3 Halton Region Official Plan (May 16, 2024)

The Halton Region Official Plan guides land use and growth through a comprehensive framework of social, economic, and environmental policies. It outlines the Region's development vision by establishing clear goals, objectives, and policies to shape sustainable communities.

The subject site is located within the approved Urban Area, as identified in Map 1 – Regional Structure of the Regional Official Plan. Policy 74 defines the Urban Area as “where urban services are or will be made available to accommodate existing and future urban development and amenities.”

Section 31 of the Halton Region Official Plan outlines the vision for Halton's future, focusing on building healthy communities. The following policies are relevant to the proposed development:

(1) that fosters among the residents a state of physical, mental, social and economic well-being;

(3) that is physically so designed to minimize the stress of daily living and meet the life-long needs of its residents;

(4) where a full range of housing, employment, social, health, educational, recreational and cultural opportunities are accessible for all segments of the community;

(5) where mobility is provided primarily through an affordable, convenient, safe and efficient public transportation system and non-motorized travel modes; and

Policy 72 of the Regional Official Plan outlines the objectives for Urban Areas, several of which apply directly to the subject lands:

(1) To accommodate growth in accordance with the Region's desire to improve and maintain regional unity, retain local community identity, create healthy communities, promote economic prosperity, maintain a high quality, sustainable natural environment, and preserve certain landscapes permanently.

(2) To support a form of growth that is compact and supportive of transit usage and non-motorized modes of travel, reduces the dependence on the automobile, makes efficient use of space and services, promotes live-work relationships and fosters a strong and competitive economy.

(3) To provide a range of identifiable, inter-connected and complete communities of various sizes, types and characters, which afford maximum choices for residence, work and leisure.

(4) To ensure that growth takes place commensurately both within and outside the Built Boundary.

(5) To establish a rate and phasing of growth that ensures the logical and orderly progression of development, supports sustainable and cost-effective growth, encourages complete communities, and is consistent with the policies of this Plan.

(7) To plan and invest for a balance of jobs and housing in communities across the Region to reduce the need for long distance commuting and to increase the modal share for transit and active transportation.

(9) To facilitate and promote intensification and increased densities.

(10) To provide for an appropriate range and balance of employment uses including industrial, office and retail and institutional uses to meet long-term needs.

The proposed development introduces a well-balanced mix of residential, commercial, and recreational uses that enhance local community identity, promote economic vitality, and maintain a high-quality natural environment. By incorporating single-detached and townhouse dwellings, a commercial/mixed-use block, parks, open spaces, and a stormwater management area, the design supports compact growth, reduces automobile dependence, and encourages active transportation. Thoughtfully planned streetscapes and open spaces promote social interaction, physical activity, and a sense of place, contributing to a vibrant and cohesive community.

The development creates a complete and interconnected neighbourhood by offering a diverse range of housing types, employment opportunities, and amenities, ensuring that it meets both current and future community needs. The efficient use of land and infrastructure supports long-term prosperity while fostering sustainability through energy-conscious building design, integrated green infrastructure, and environmental stewardship.

By prioritizing mobility and incorporating active transportation routes alongside its proximity to transit services, the proposal ensures convenient, safe, and efficient movement within the neighbourhood, reducing reliance on personal vehicles. Overall, the development aligns with regional growth and planning objectives by creating a livable, resilient community that promotes well-being, sustainability, and a high quality of life for future residents

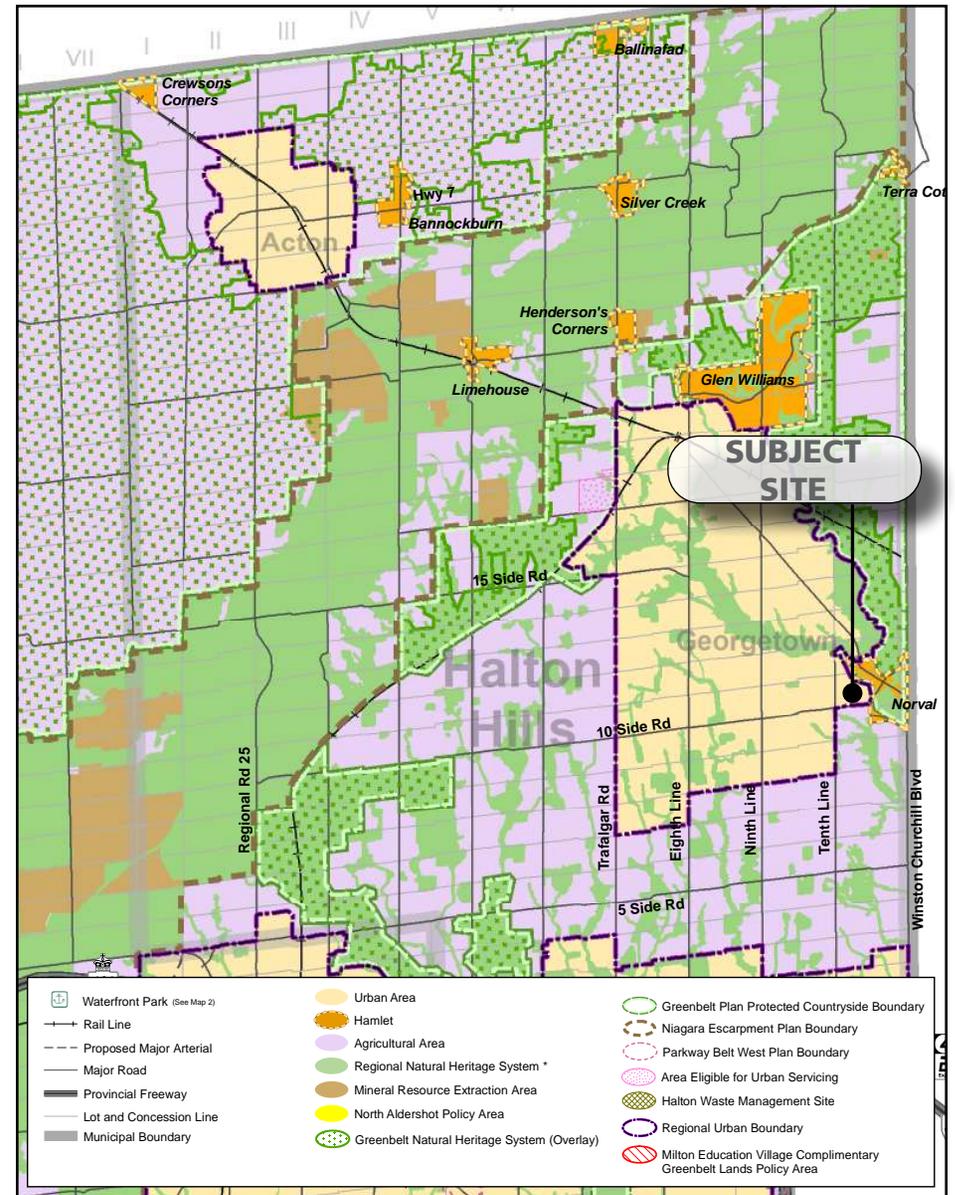


Figure 4: Town of Halton Hills Official Plan - Schedule A3: Georgetown Urban Area Land Use Plan

2.3.4 Town of Halton Hills Official Plan

The Halton Hills Official Plan establishes a vision for the future land use structure of the Town and, as a result, is intended to serve as the basis for managing change until 2031.

The Official Plan was adopted by Town Council in September 2006, approved by Halton Region in March 2008, and most recently consolidated on April 30, 2024. The subject site is designated as “Future Residential/Mixed Use Area” and “HPBATS/GTA West Corridor Protection Area”, as indicated in Schedule A3 – Georgetown Urban Design Area Land Use Plan.

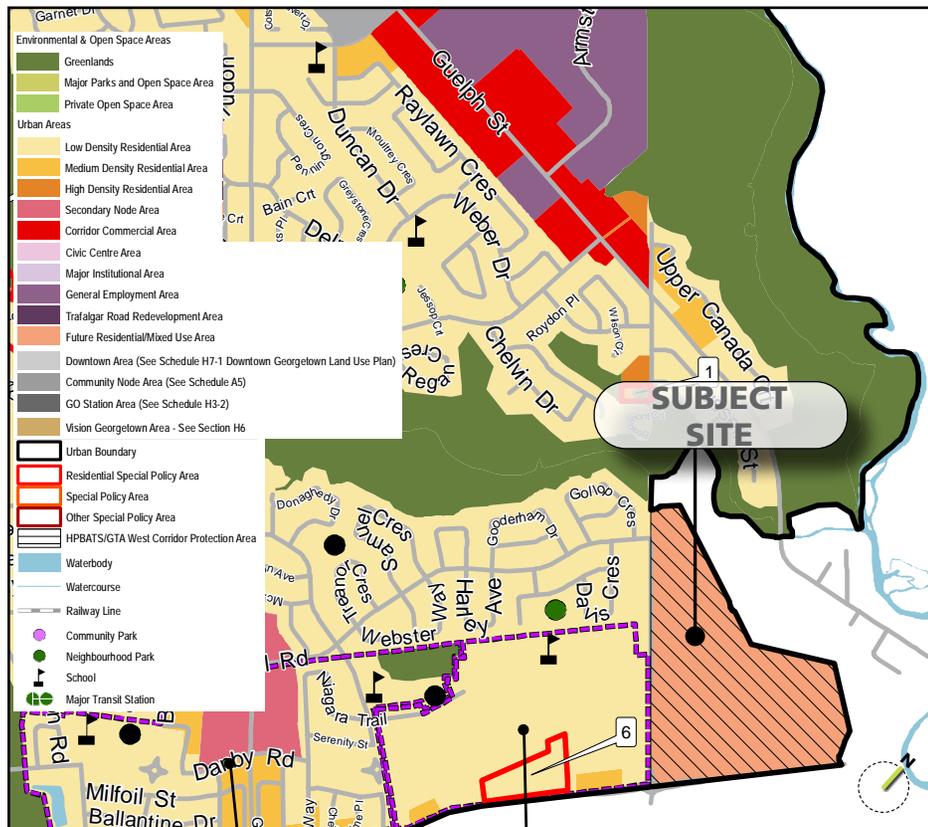


Figure 5: Town of Halton Hills Official Plan - Schedule A3: Georgetown Urban Area Land Use Plan

With respect to Future Residential/Mixed Use Area, **Policy D6.3** of the Official Plan outlines key objectives aimed at creating a well-integrated and sustainable community. These objectives form the foundation for the proposed development and will be addressed accordingly to demonstrate alignment with the Town’s long-term planning vision.

- b) ensure that the new development area is integrated into the fabric of the existing community of Georgetown;
- c) ensure that the new development area is developed as a complete community, with compact pedestrian-friendly neighbourhoods, a mix of housing types, community facilities, commercial centres, and open spaces;
- d) ensure a high standard of urban design; and,
- e) adhere to the community design principles for new communities contained in Section F3 of this Plan.

The proposed development aligns with the above-mentioned policies by fostering a complete, integrated, and high-quality community. The mix of housing types—including detached homes and a variety of townhouse types such as traditional, back-to-back, and dual-frontage units—addresses a broad range of housing needs and promotes inclusivity. The commercial/mixed-use block provides convenient access to amenities, supporting daily needs and enhancing neighbourhood vibrancy. Parks, open spaces, and the integration of adjacent Greenbelt lands contribute to recreational opportunities and create a pedestrian-friendly environment that encourages active lifestyles.

The well-planned street layout promotes connectivity and integrates seamlessly with the existing urban fabric of Georgetown, ensuring the development complements and enhances the surrounding community.

Part F of the Official Plan outlines the general development policies for Halton Hills, with **Section F2** specifically addressing urban design policies. The following urban design policies in Section F2 are applicable to the proposed development:

F2.2.1.1 Streetscape

a) An integrated design and treatment of streetscape features shall be promoted throughout the municipality. Specialized streetscape designs and treatments may be adopted for particular areas of the Town in accordance with area, or site-specific, Council-adopted Urban Design Guidelines.

b) Streetscape features located within public rights-of-way, such as lighting fixtures, directional and street signs, parking meters, transit shelters, and street furniture shall be complementary in their design and located in an integrated manner, so as to avoid visual clutter.

c) Gateway features shall be established at strategic locations within the municipality, as identified in Council-adopted Secondary Plan policies and/or Urban Design Guidelines and may include specialized boulevards, landscape medians, decorative street lightings, and/or decorative signage treatments.

d) Planned road reconstruction shall be encouraged to include improvements to the existing streetscape consistent with the policies of this Plan or any Council-adopted Urban Design Guidelines.

The proposed development will incorporate cohesive streetscape features across the planned local and collector roads. Streetscapes are enhanced with street trees, lighting, and landscaped boulevards, fostering a pedestrian-friendly environment. Key streets, such as the proposed Main Streets (Street 'A' and Street 'B'), are designed as central spines with potential for decorative lighting and signage, reinforcing the identity of the community. Gateway features at key intersections, provide a sense of arrival and distinctiveness. The streetscape incorporates landscaping, including street trees and pedestrian-scale lighting, to enhance walkability and visual appeal .

F2.2.1.2 Roads

a) road rights-of-way shall be designed to secure a separation of vehicles and pedestrians and should provide an appropriate sidewalk for pedestrian use;

b) on collector and arterial roads within Urban Areas and Hamlet Areas, a suitable boulevard shall be provided to separate the road curb from the sidewalk and such boulevard shall include hard and/or soft landscape materials, street trees and pedestrian-level street lights, where appropriate;

d) a regularized pattern of street tree planting shall be encouraged along roads within Urban Areas and Hamlet Areas, and the Town shall establish minimum planting standards and species types in the Site Plan Manual and/or Council-adopted Urban Design Guidelines;

e) the number and location of access points onto the public road system shall be minimized by encouraging common access points to be shared by adjacent development;

f) street lighting shall, where appropriate, incorporate pedestrian-level lighting to maintain pedestrian safety; and;

g) street lighting that reduces energy consumption and directs light away from the night sky and adjacent uses shall be encouraged.

The road network is designed with a clear hierarchy of major, collector, and local roads to enhance connectivity and support efficient traffic flow. Landscaped boulevards along collector roads provide a buffer between sidewalks and vehicular lanes, prioritizing pedestrian safety and comfort. Local roads serve the internal residential areas and feature pedestrian pathways that connect seamlessly to parks, open spaces, and mixed-use areas, fostering walkability throughout the community. The alignment and layout of roads are carefully planned to minimize access points onto major streets, ensuring smooth traffic transitions and reducing potential conflicts.

F2.2.2.1 Site Design

a) Site design incorporates the built form of structures, landscaping, services and the layout of all amenities. Site design shall promote an appropriate design relationship between the public realm, adjacent land uses, on-site operations and visual aesthetics, in order to promote an environment that is pleasant and attractive to the community.

b) Site design shall address compatibility between differing adjacent land uses in context of density, height and massing through appropriate site layout, building locations and landscape treatments.

c) The design of sites adjacent to parks, woodlots and watercourses shall be sensitive to these features. In these instances, appropriate setbacks shall be maintained between buildings and sensitive natural areas, while on-site landscaping shall be well integrated with natural areas.

d) Continuous, highly visible, well-articulated and landscaped connections between building(s) and the street should be provided to establish appropriate pedestrian linkages between the sidewalk and building entrances, and generally improve access for public transit users.

e) Site design along major roads should also consider the integration of future public transit access.

f) Along collector and arterial roads within Urban Areas and Hamlet Areas, reverse frontage residential lots shall be minimized through techniques such as window streets and where reverse frontage lots are provided, shall incorporate a substantial landscape buffer to improve the visual amenity of such areas.

g) On large sites, efforts shall be made to encourage pedestrian linkages between uses and adjacent sites.

The site design integrates open spaces, parks, parkettes, and residential blocks to create a cohesive and well-connected community. Buildings are strategically oriented to establish a strong relationship with the street, enhancing the public realm and promoting a pedestrian-friendly environment. Landscaping buffers are incorporated to mitigate potential impacts on adjacent uses and ensure compatibility with surrounding areas. Clearly defined pedestrian pathways and green linkages provide seamless

connections between residential areas and open spaces, enhancing both accessibility and visual continuity. The layout accommodates a diverse mix of housing types, promoting density, variety, and a vibrant community character.

F2.2.2.2 Building Design and Siting

a) The design of new buildings should achieve a complementary design relationship to existing buildings, while accommodating a diversity of architectural styles, building materials and colours, energy conservation techniques and innovative built forms.

b) The design of all buildings shall have regard to pedestrian safety and direct street access. Buildings should be massed to recognize pedestrian scale and provide an appropriate street wall height at the street line and be architecturally articulated to provide visual variety and interest, yet be sensitive to high wind speeds, and long periods of shadowing. Generally, building articulation features such as canopies, cornice lines and varying façade materials should be used to reinforce a pedestrian scale.

c) Buildings shall be encouraged to present their principal building facades with an appropriate building design and fenestration to the public street. The design of corner buildings shall take into account exposure to multiple street frontages and high public exposure; as well as incorporate elements such as increased height, fenestration and roof features, and well-articulated entrances.

d) Buildings located at major vista terminations in their urban setting, or on view corridors within the development site, shall be given special treatment through the use of massing and building articulation strategies, such as added height, special roof treatments, and use of special cladding materials.

e) The protrusion of garages on residential buildings into the front yard, beyond the main front building wall, shall be discouraged.

f) Building functions that do not directly serve the public, such as loading areas, shall not face a public street and should be located away from noise sensitive land uses, such as residential areas, and buffered as necessary.

g) Buildings should employ devices such as awnings, canopies, building cantilevers / overhangs to minimize uncomfortable high winds which may be associated with the height or placement of buildings, and generally improve the level of pedestrian comfort. Sheltered building entrances should be provided at primary building entrances to high-density residential, public, recreational, industrial, office and commercial buildings, where necessary.

h) When a development is located adjacent to existing, or planned residential areas, sufficient building setbacks should be provided to minimize potential height and massing impacts such as overlook, shadowing and high wind speeds. Massing strategies such as stepping down towards buildings of lower height should be employed to minimize impacts. Site Plan applications may be required to submit wind and/or shadow studies to address such potential conditions.

i) Roofscapes shall be an integral part of the design of a building and harmonize with the design of the rest of the building. On this basis, roof top mechanical units shall be organized and screened with complementary materials, colours and finishes as necessary to provide a skyline with desirable visual attributes.

j) Freestanding buildings for retail, restaurants and services, as well as office and residential buildings, shall be located at the street edge where possible, to encourage their use by pedestrians.

k) Buildings on corner lots shall be located in close proximity to the street rights of way. Corner lots should emphasize their important urban presence by employing appropriate strategies for major landscape treatments as well as building massing and articulation that emphasize the corner condition.

l) Building entrances shall be located to be visible from the adjoining street(s) and, where possible, directly linked to the sidewalks through appropriately articulated walkways.

The design of residential buildings and the future design of the commercial/mixed-use block ensures compatibility with the character of the community by incorporating a variety of architectural styles, materials, and colours. Special attention is given to buildings located at intersections and vista

terminations, with enhanced articulation, added height, and distinctive architectural features to create prominent focal points within the neighbourhood.

To maintain an active and engaging streetscape, principal facades are oriented toward the streets, while garages and functional elements, such as loading areas, are carefully positioned to minimize visibility from public rights-of-way. This approach promotes a visually appealing streetscape while ensuring pedestrian comfort and safety.

F2.2.2.4 Landscaping

a) Landscaping is a major contributor to a vibrant streetscape. A high quality of landscape design shall be required to enhance the visual aesthetics of development and to enhance the site and land use compatibility.

b) Landscaping within private lands shall be complementary to streetscape design and materials within the public realm.

c) Where appropriate, planted landscaping strips and fencing shall be used to buffer development from adjacent uses and mitigate on-site operational activities such as loading and waste storage facilities;

d) Landscape materials shall be selected for their aesthetic, ecological, disease-tolerance and maintenance characteristics.

e) Hard and soft landscaping shall be used for the spaces between the street line and buildings to enhance the streetscape, as well as provide a buffering function when on-site parking is placed close to the street or in side yards. To help create a strong landscape presence that also screens surface parking, a generously sized area and/or low decorative fencing should be provided along the street line to allow for an aesthetically pleasing view from the street into the site.

f) Vehicular entrances often present opportunities for landscaping that highlight entry points into the site. Therefore, appropriate landscaping shall be provided on either side of driveway entrances, particularly at the main entrances.

g) The use of berms along public street frontages shall generally be avoided due to their tendency to isolate buildings from the street.

h) Landscaping can play an important role in delineating a site's side yards and often provides a visual break in large asphalted areas. In this regard, landscape strips planted with trees and/or shrubs and flowers shall be used to separate each development and the associated parking areas. The presence of significant trees on a development site shall be determined through a tree survey and, where appropriate, preserved, maintained and integrated into the new landscape design.

i) Native, non-invasive species shall be used on lands that are adjacent to the Greenlands System.

Landscaping throughout the development is designed to complement the streetscape and enhance the overall visual appeal of the site. Public and private spaces are distinguished by landscaped strips, tree-lined pathways, and native vegetation, contributing to ecological sustainability while providing effective buffering for parking areas and operational elements. These landscaping features not only improve aesthetics but also promote compatibility with adjacent uses.

Key entrances are highlighted with well-integrated landscaping, enhancing the sense of arrival and reinforcing the community's identity. Additionally, landscaping along pedestrian pathways creates a pleasant, walkable environment and adds visual interest, helping to establish a cohesive and inviting community atmosphere

F2.2.2.5 Parking

a) The location of parking is a major determinant for the layout of a development that is pedestrian friendly and transit supportive. Where appropriate, the Town shall encourage the provision of surface parking areas in locations not visible from the public street, such as in rear yards and/or well-landscaped side yards.

b) Where surface parking areas are situated adjacent to a public street in the front yard, their layout should be subdivided into smaller areas to avoid large monotonous asphalt surfaces. In these cases, a certain percentage of the frontage should be reserved for landscaping between the buildings and the street line. The parking areas may be partially buffered and/or screened from the street through the use of landscaping, tree planting, pedestrian facilities, lighting, fencing and/or other landscape elements in order to enhance the visual aesthetics of, and pedestrian activity within, such parking areas.

c) Clearly defined pedestrian accesses between parking and adjacent buildings and entrances should be provided with well-delineated walkways using decorative paving surfaces.

d) Surface parking lots shall be linked to the streets and other public areas with well-delineated walkways, utilizing decorative paving treatments.

In the proposed commercial/mixed use block, parking will be strategically placed to minimize visibility from streets. Where parking areas are adjacent to public roads, they shall be subdivided and screened with landscaping, enhancing visual appeal. Walkways with decorative paving connect parking areas to adjacent buildings, ensuring pedestrian safety and accessibility. A more detailed design will be available at Site Plan stage.

F2.2.2.6 Signage, Display Areas and Lighting

c) All lighting shall be internally oriented so as not to cause glare on adjacent properties or public roads. Outdoor lighting fixtures that reduce energy consumption and direct light away from the night sky shall be encouraged.

Signage shall be designed to be cohesive and complementary to the overall streetscape aesthetic, maintaining visual clarity without clutter. Lighting fixtures are to be energy-efficient, minimizing light pollution and ensuring safety for pedestrians and vehicles alike.

F2.2.2.7 Services, Utilities, Outside Processing and Storage

a) Site and building services and utilities such as waste storage facilities, loading, air handling equipment, hydro and telephone transformers and switching gears and metering equipment, shall be located and/or screened from public streets and adjacent residential areas or other sensitive land uses, in order to buffer their visual and operational effects. Waste storage areas should be integrated into the main building on the lot. Waste storage areas external to the main building shall be enclosed and shall not face a public street.

b) Site access, service areas and loading areas shall be located away from streets so as to minimize disruption or conflicts with adjacent land uses, sidewalks and both on-site, and off-site, pedestrian routes and shall be visually screened as necessary from public views. Screening should be designed to use landscaping and/or solid fencing. Loading and service areas should be buffered for noise impacts, particularly when located against residential areas. Buffering strategies include berms, tree and shrub planting and opaque noise walls and fences.

d) Storage areas should be paved with hard surfaces such as asphalt, concrete or interlocking pavers where possible to reduce dust.

Service and utility areas will be strategically located to minimize visibility and operational disruption to adjacent uses. Waste storage and loading facilities are screened or enclosed to maintain the community's aesthetic integrity. Landscaping and fencing provide additional buffering where necessary.

F2.2.2.8 Access and Circulation

a) The number of vehicular access points into a site and their width could potentially have detrimental effects on street frontages by reducing available areas for landscaping and by creating large expanses of asphalt. On this basis, joint access driveways shall be considered on adjacent sites.

b) To ensure safety and promote their priority over vehicular traffic, major pedestrian routes on the site should be identified and delineated with paving materials that differentiate them from the driving surfaces. Pedestrian walkways should be made continuous across driving aisles as well as across driveway entrances at the street. The use of soft landscaping is also encouraged along major pedestrian routes.

c) Generously sized walkways shall be provided along buildings, particularly in areas with large pedestrian traffic. These walkways should be connected to other pedestrian routes on the site and linked to major pedestrian entry points at the street, and where appropriate to adjacent developments.

Access points into the development are limited and strategically placed to maintain traffic flow and minimize disruptions. Clearly defined pedestrian routes are provided along roads, connecting key areas such as parks, mixed-use blocks, and residential neighborhoods. Walkways are to be integrated with decorative paving and soft landscaping, promoting safety and continuity for pedestrians while facilitating connectivity to adjacent communities.

Section F3 deals specifically with Design For New Communities. The following policies within section F3 applies to the proposed development:

The following community design principles apply to development in greenfield areas:

- a) Residential development shall include a combination of housing types, with a range of densities that implement the housing objectives and policies of this Plan;*
- c) New development areas shall be integrated with existing Built-up Areas;*
- d) New subdivision streets should align in a grid pattern to create appropriately sized development blocks and to promote traffic permeability and street connectivity;*
- e) The development of reverse-frontage residential lots shall be minimized through techniques such as window streets and where reverse frontage lots are provided, shall incorporate a substantial landscape buffer to improve the visual amenity of such areas;*
- f) Open space and parkland areas shall integrate with adjacent development areas and provide a range of active and passive recreational opportunities;*
- h) New buildings shall be designed and oriented to the street and to street corners to encourage a pedestrian-oriented streetscape;*
- j) Non-residential uses shall address the policies in section F2.2.2;*
- k) Above ground utilities shall be located to minimize visual and environmental impacts; and,*
- l) Collector roads shall be provided approximately mid-block between arterial roads to promote traffic connectivity, and ultimately the establishment of public transit, when feasible.*

The proposed development thoughtfully incorporates the applicable policies by creating a community design that integrates seamlessly with the surrounding context. A variety of low-rise housing types, including single-

detached homes and dual-frontage townhouse units, are provided, along with a cohesive grid-like street pattern that enhances traffic permeability and promotes connectivity.

Reverse-frontage lots have been minimized through the strategic use of dual-frontage townhouse typologies, ensuring that all road edges feature high-quality architecture and enhanced streetscape treatments. Buildings are carefully oriented toward streets and corners to foster a pedestrian-friendly streetscape and maintain visual interest throughout the neighbourhood. Open spaces and parkland are integrated across the community, offering a range of active and passive recreational opportunities. Collectively, these elements contribute to a well-connected, vibrant, and sustainable community.

Section F7 deals specifically with Public Parkland. The following policies within section F7 applies to the proposed development:

F7.3.1 Parkland Siting And Design

All public parkland shall:

- a) have as much street frontage as possible and be open to view on as many sides as possible to maximize visibility from adjacent streets and promote safety;*
- b) maximize public safety through park block size, visibility, configuration and location of park fixtures and facilities;*
- c) have direct and safe pedestrian access from adjacent residential areas or adjacent greenlands where appropriate;*
- d) be designed to minimize any potential negative impacts on adjacent residential areas through the use of such measures as planting, fencing and the provision of appropriate access, parking and buffers to active recreational facilities;*

e) incorporate natural heritage features wherever possible into the design of the parkland;

f) be integrated into the fabric of the adjacent neighbourhood by promoting open space or walkway linkages to adjacent facilities, neighbourhoods and natural features;

g) be connected, wherever possible, to trail systems, cycling routes, walkways, natural heritage corridors, utility corridors and drainage systems; and,

F7.3.4.1 Parkettes

Parkettes shall:

b) generally range from 0.2 to 0.6 hectares in size;

e) be centrally located within the neighbourhood it is intended to serve; and,

f) provide a range of opportunities for active and passive activities, particularly for young children and older adults seeking close to home activities.

F7.3.4.2 Neighbourhood Parks

Neighbourhood Parks shall:

b) generally range from 1.5 to 2.5 hectares in size;

d) be centrally located within the neighbourhood or neighbourhoods it is intended to serve;

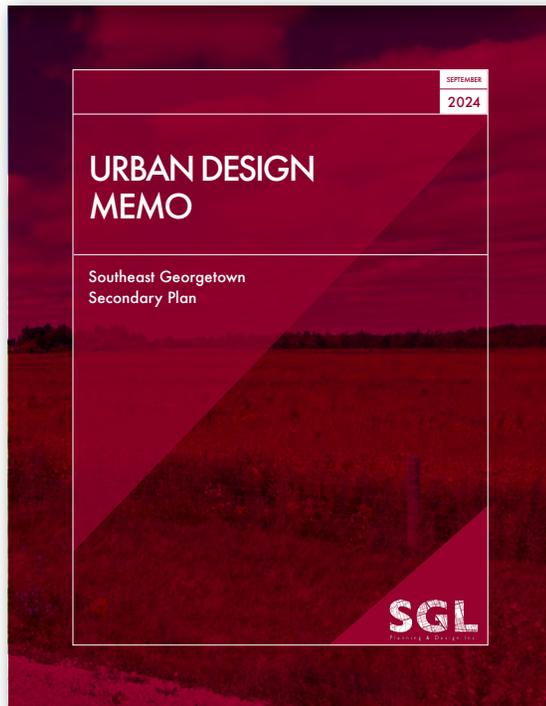
e) have frontage on a Collector Road that is adequate for the provision of on-street parking and site visibility;

The proposed neighbourhood park, parkette, and vista block parkette align with the policies by maximizing street frontage and visibility, promoting safety, and ensuring direct pedestrian access from adjacent residential areas. The centrally located neighbourhood park provides adequate frontage on a collector road for on-street parking and site visibility, ensuring it serves the entire community effectively.

The park system provides important connections to the trail system within the open space, enhancing the overall walkability and accessibility of the development. Together, these public spaces offer a range of active and passive recreational opportunities for various age groups, including children and older adults. Landscaping, fencing, and buffers are incorporated as needed to minimize potential impacts on nearby residential areas and ensure compatibility with adjacent uses.

2.3.5 Southeast Georgetown Secondary Plan Urban Design Memo (September 2024)

The Southeast Georgetown Secondary Plan Urban Design Memo (September 2024) serves as the urban design guidelines for the secondary plan area. The memo provides specific design guidance in areas where existing policies are limited or absent, focusing on three key components: Sustainable Design, Public Realm Design, and Local Commercial and Mixed-Use Areas. The objective of the memo is to promote a cohesive and well-connected community that prioritizes environmental sustainability, high-quality public spaces, and vibrant mixed-use areas.



The following sections outline key recommendations and guidance from each of these components that are relevant to the proposed development.

1. Sustainable Design Guidance:

The sustainable design policies prioritize Halton Hills' "environment first" approach, emphasizing the incorporation of green infrastructure and environmentally conscious features. Key recommendations include:

- Implementation of Low Impact Development (LID) measures like bioswales, rain gardens, and permeable paving.
- Preservation of natural features through no-mow zones and native planting.
- Use of sustainable building materials, energy-efficient designs, and incorporation of solar panels or green roofs.
- Design strategies to reduce heat island effects, such as shaded boulevards and tree planting in parking areas.

2. Public Realm Design Guidance

Focused on the design of streetscapes, parks, and trails, this guidance ensures a cohesive and walkable community:

- **Streetscapes:** Accessible sidewalks, street trees on both sides, varied street furniture, and clustering utilities where possible.
- **Streets and Blocks:** Encouraging complete streets for multimodal use, avoiding window streets, and providing mid-block pedestrian connectors to promote walkability.
- **Gateways:** Establishing hierarchy with enhanced landscaping, taller buildings, and vibrant entryways.
- **Views and Vistas:** Protecting significant views of landmarks, such as Russell Farm, and integrating these into the community layout.

- **Parks and Open Space:** Designing parks with recreational programming, crime prevention principles, and heritage-reflective features.
- **Walking and Cycling:** Developing multi-use paths and trails to connect with the Greenbelt and improve accessibility across the community.

3. Local Commercial and Mixed-Use Areas

Private developments, such as residential and commercial buildings, should align with sustainable design goals while allowing for unique architectural identity:

- Mixed-use areas may include residential and commercial uses, with active ground-floor spaces.
- Outdoor patios are encouraged, and energy-efficient building materials and green roofs are recommended.
- Surface parking, where necessary, should incorporate sustainable features such as landscaping and permeable paving.
- Transitions between land-use densities should incorporate appropriate stepbacks to create a harmonious urban form.

The proposed development will align with the key recommendations and guidance outlined in the Southeast Georgetown Secondary Plan Urban Design Memo (September 2024) by prioritizing sustainable design, enhancing the public realm, and integrating local commercial and mixed-use areas into the community fabric.

In keeping with the sustainable design guidance, the development will incorporate sustainability principles aimed at environmental stewardship. These will include strategies for stormwater management, energy-efficient design, and landscaping that enhances ecological health and reduces environmental impact. Detailed measures will be explored in section 6.7 of this brief.

The public realm design is enhanced through the creation of accessible, pedestrian-friendly streetscapes, with sidewalks lined by street trees and varied street furniture. A cohesive street and block pattern is established, avoiding window streets and promoting walkability through mid-block pedestrian connectors. Key intersections and entry points are emphasized with enhanced landscaping, gateway features reinforcing the identity of the neighbourhood. Additionally, the design will aim to integrate views and vistas into the community layout and connects parks, open spaces, and trails to the adjacent Greenbelt, ensuring recreational opportunities and accessibility.

Within the local commercial and mixed-use areas, the development will promote a vibrant street environment by incorporating a mix of residential and commercial uses with active ground-floor spaces. Outdoor patios are encouraged, and appropriate measures will be taken to enhance parking areas through landscaping and other sustainable elements. Transitions between varying land-use densities are carefully managed through appropriate massing and stepbacks to create a harmonious and well-integrated community form.

Together, these design elements will reflect the intent of the memo's guidance, contributing to a cohesive, vibrant, and sustainable community that meets the Town's long-term urban design objectives.