



16 18 MILL STREET (GEORGETOWN), HALTON HILLS, ONTARIO

Design Brief Revised

(2) Four-Storey
Apartment Buildings
16-18 Mill Street
Halton Hills, Ontario

Revised May 10, 2022

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Part 1- Introduction

1.1 Intent of Document

This Urban Design Brief has been prepared by Urban in Mind on behalf of AGK Multi - Res GP Ltd. to support the development of a residential complex, being (1) 4-storey apartment building with 16 units and (1) 4-storey apartment building with 14 units at 16-18 Mill Street (Georgetown) in Halton Hills, Ontario. The proposed development is a modified version of a previous proposal involving one (1) 6-storey apartment building with 52 units that has now been discontinued.

As indicated in the pre-consultation meeting notes, the submission of an Urban Design Brief is a required document for the Official Plan and Zoning By-Law Amendment applications that are associated with the proposed development.

This Urban Design Brief outlines how site development, building design and landscape elements within the proposal are informed by good urban design principles and practices. It also describes how the proposal fits within the neighbourhood context and how it adheres to the Town's urban design standards.

Part 2 – Site and Surrounding Area

2.1 Subject Property

The subject property is municipally known as 16-18 Mill Street and consists of one rectangular shaped lot with a frontage of 40.24m along Mill Street, a depth of 56.48m and an approximate total site area of 2,271.69m² (**Figure 1**).

The lot is currently occupied by a 2-storey 9-unit townhouse building and a separate 1.5-storey 3-unit (triplex) building (total of 12-units) that are both visible from the street (**Figure 2**). The site also contains an accessory structure in the rear yard (**Figure 1**). The buildings and structures on the site are in a state of disrepair, and have not been identified by the Town as having any cultural heritage significance. Furthermore, all buildings are planned to be demolished to make room for the proposed new residential development. Due to the large existing asphalt parking lot that wraps around the site, there is currently almost no landscaping on the property, with the exception of a few trees and shrubs. The entire frontage of the property is paved connecting the private parking area with the public sidewalk and roadway. There is no discernable driveway on the site, as nearly the entire frontage is currently paved.

Google Maps



Figure 2: Street View of Subject Property (16-18 Mill St)

Town of Halton Hills



Figure 1: Aerial View of Subject Property (16-18 Mill St)

2.2 Surrounding Area

The subject property is located on the north side of Mill Street, roughly mid-block between McNabb Street and Dayfoot Drive. The site is considered to be in a prominent location because it is the first developable lot on the eastern segment of Mill Street, and abuts the greenspace area that forms part of the Silver Creek Natural Heritage Corridor. This greenspace is planned to serve as a future trail connection and parkette (West Side Park) (**Figure 5**).

The surrounding residential neighbourhood is in a state of transition. It consists of a mix of older single detached homes, apartment buildings (**Figure 3**) and a future redevelopment site that is subject to an active application for a 6-storey apartment development with 155 units (**Figure 4**).

Google Maps



Figure 3 – Multi-Dwelling Residential Infill
4-storey apartment at 24 Chapel Street



Figure 4 – Active Development Applications in the Area (Yellow)
Site '163' – Subject Property
Site '202' – Proposed 6 Storey Apartment Building

Immediate Surrounding Land Uses



Figure 8 – NORTH of Subject Property
22 Dayfoot Dr. – Single Detached Home.



Figure 5 – EAST of Subject Property
14 Mill St. – Silver Creek Green Space.



Town of Halton Hills



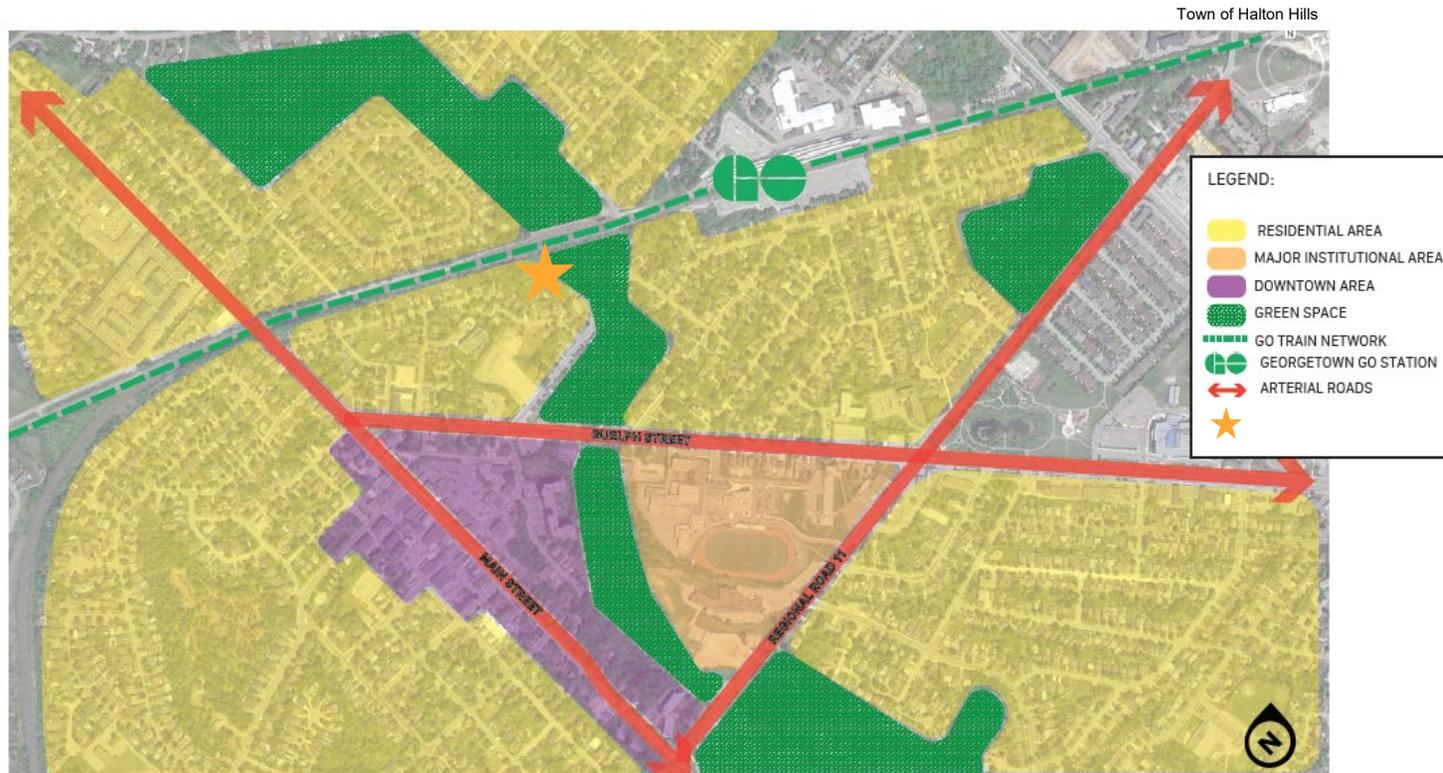
Figure 7 – WEST of Subject Property
22 Mill St. – Single Detached Home.

Google Maps



Figure 6 – SOUTH of Subject Property
21 Mill St. – Kiyo's Car Service.

Google Maps



The subject property is centrally located just northeast of the Downtown Georgetown Commercial Main St (**Figure 10**) and southwest of the Georgetown Go Station (**Figure 11**). This gives the area a very ‘urban feel’ as local shops, community facilities and regional transit amenities can be reached within a 10 min walk.

Figure 9 – Surrounding Land Uses



Figure 10 – Downtown Main Street Shopping District



Figure 11 – Georgetown Go Station

2.3 Transportation Network

Mill Street is classified as a 'Local Road' with an approximate Right-of-Way of 10.0m. The right-of-way width along Mill Street is expected to increase to 20.0m as future development in the area continues. As apart of the subject property's redevelopment, the Town is requiring a 5-metre Right-of-Way dedication from the site for this 'future' road widening. Further, the proposed development will effectively reduce the number of existing vehicular access points to the site from Mill Street as a new delineated driveway will be established.

With the exception of specialized transit services for seniors and people with disabilities, the Town of Halton Hills relies entirely on Go Transit to serve the public transit needs of the Town. With that being said, public transit is an important means of travel for getting to other urban areas in the Greater Toronto Area (GTA). The subject property is located within a 5-10-minute walk from the Georgetown 'Go Station' and within a 5-minute walk from a 'Go Transit Feeder Bus Stop' located near the intersection of Mill Street and Guelph Street. On any given day, residents living on the subject property can easily get to Downtown Acton in approximately 15 to 30 minutes or to Downtown Brampton/Guelph in approximately 45 minutes to 1 hour (**Figure 12**) using Go Transit buses or train. Within Georgetown, there are feeder buses that run along Guelph Street which provide convenient access to the majority of the Town's schools, community facilities, destination parks & trails, employment areas and commercial centres (**Figure 13**).



Figure 12 – GO Transit Kitchener Line

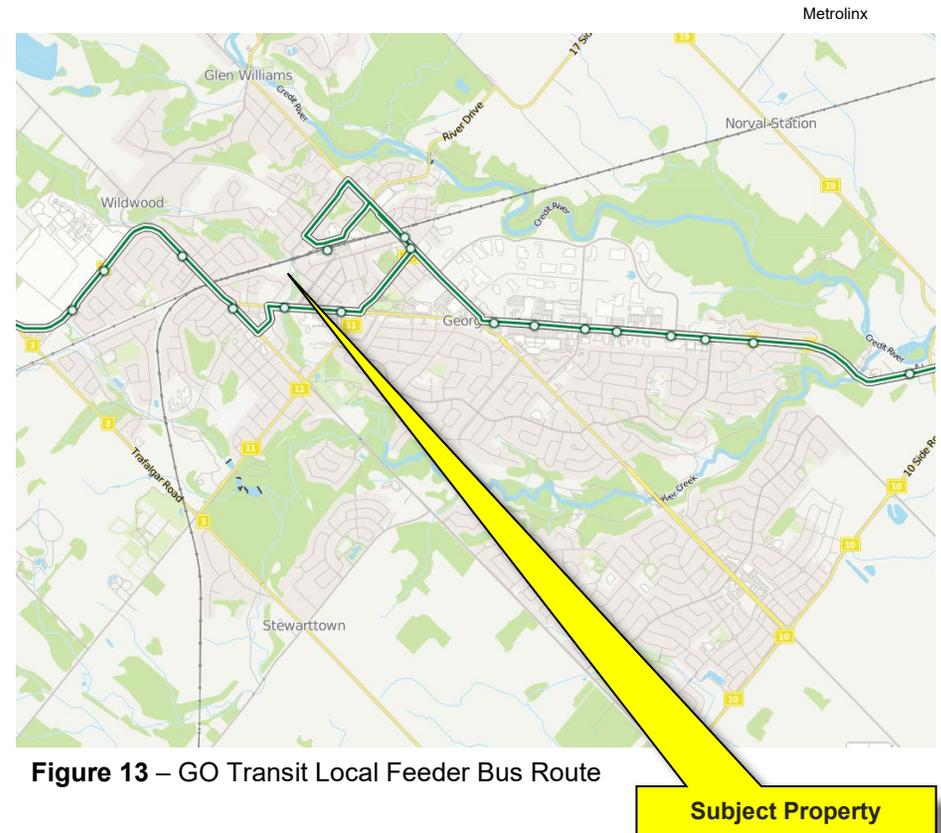


Figure 13 – GO Transit Local Feeder Bus Route

Part 3 – Development Concept

3.1 Site Plan and Site Data

SRM Architects

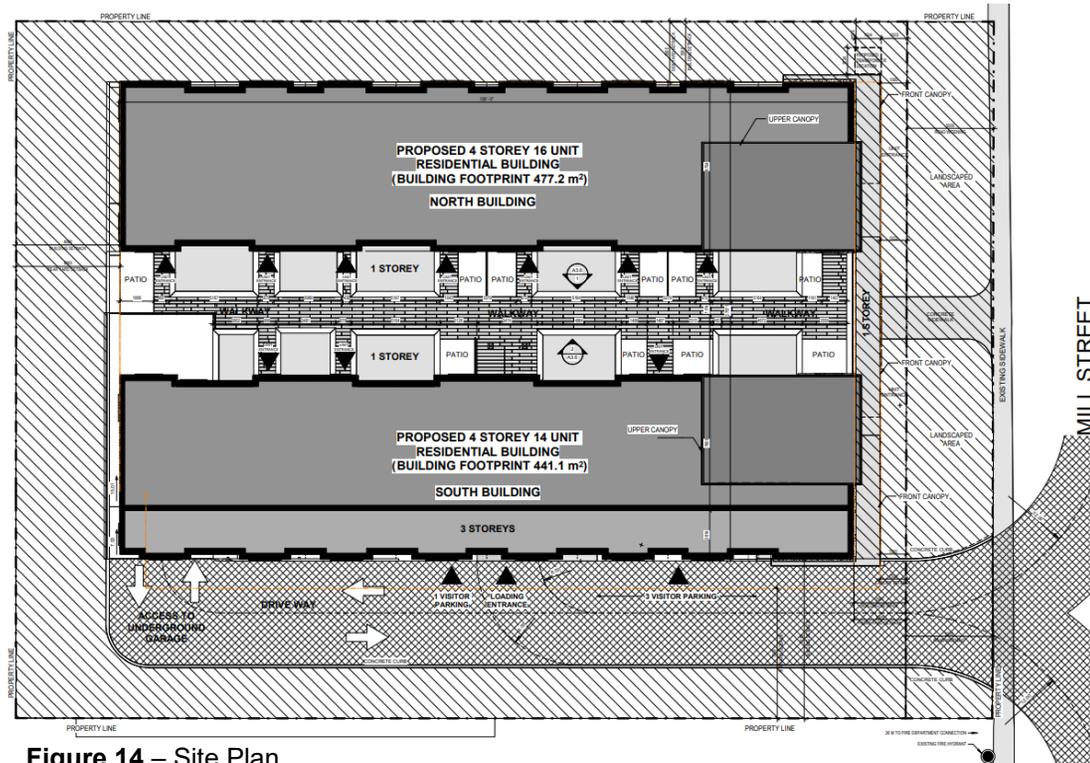


Figure 14 – Site Plan

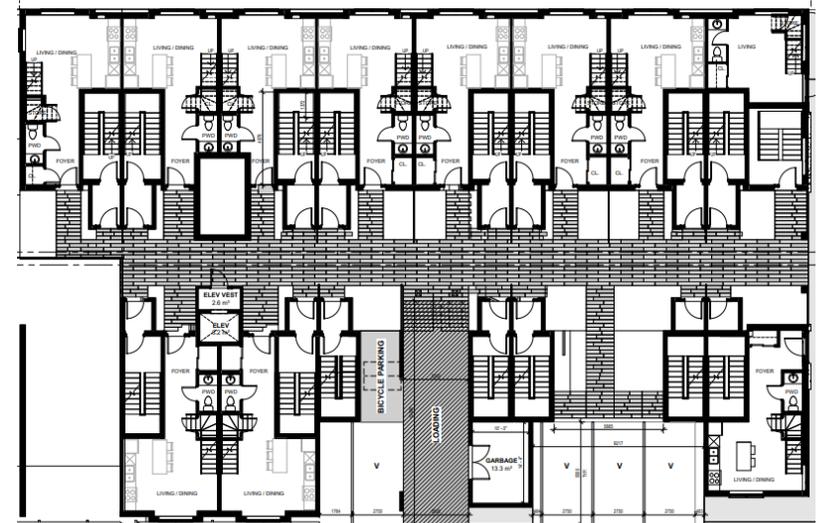


Figure 15 – Floor Plan: Outdoor Common Walkway

3.2 Requested Planning Approvals

To facilitate the proposed residential development, Urban in Mind will need to obtain an Official Plan Amendment, Site Specific Zoning By-Law Amendment, and Site Plan Approval from the Town of Halton Hills, as well as a Conservation Development Permit from the CVC. The Site-Specific Conditions associated with the Official Plan and Zoning By-Law Amendments have been included in Tables 1 & 2 as follows:

Table 1 – Proposed Official Plan Amendment Details

Official Plan Amendment Georgetown GO Secondary Plan	Existing 'Medium Density Residential' and 'Greenlands'	Required 'Medium Density Residential'	Proposed 'Medium Density Residential Special'
Density Range (D.1.3.2)	Max. 4 storeys 21 to 50 units/hectare	Max. 4 storeys 21 to 50 units/hectare	4 storeys 133 units/hectare (pre-road widening) 146 units/hectare (post-road widening)

Table 2 – Proposed Zoning By-Law Amendment Details

Zoning By-Law Amendment By-Law 210-0050	Existing 'LDR1-2' and 'EP1'	Required 'MDR2' (Apartment Dwellings)	Proposed 'MDR2 Special' (Apartment Dwellings)
Min. Lot Frontage		11.0 m	40.23 m
Min. Front Yard		4.5 m	8.0 m (pre-road widening) 3.0 m (post-road widening)
Min. Rear Yard		7.5 m	6.0 m
Min. Int. Side Yard		7.5 m	3.5 m (north) 9.18 m (south)
Max. Building Height		25.0 m	13.2 m
Min. Parking Spaces		30 units x 1.5 parking spaces = 45 30 units x 0.25 visitor parking spaces = 8 Total Parking Required = 53	56 spaces (includes 8 visitor spaces)
Barrier-Free Parking Spaces		--	4 (included in total parking count)
Parking Area Location in Relation to Buildings and Structures (5.2.19)		1.2 m setback required in between parking areas and building	0 m (for the ground level visitor parking spaces)
Table 6.1 – Special Provision 8.		Only the number of units and related floor area that existing on the effective date of this By-law are permitted	Special provision deleted for apartment dwellings
Table 6.4 – Special Provision 5.		The maximum number of dwelling units permitted shall not exceed 50 units per 1.0 hectare of lot area	133 units/hectare (pre-road widening) 146 units/hectare (post-road widening)
USE:	(Limited) Apartment	Apartment	Apartment which must have a minimum of 12 rental units

3.3 Design Considerations

3.3.1 Architectural Design

- The blend of compatible materials and colours creates visual interest while also adding variety to the streetscape.
- The prominent entrance reinforces the street-to-building relationship.
- The balconies and large windows promote natural surveillance (CPTED) and ensures solar efficiency.
- The varied step-back and massing of the building promotes façade variety and a visually ordered design.
- The two buildings are tied together by the front canopy and the common internal walkway with consistent architectural elements that provide for a cohesive design.
- Attractive overhangs provide for weather protection.
- The proposed driveway access will allow for logical access, and new landscaping to transition to the greenspace and surrounding neighbourhood.

The Design renderings shown below are for discussion purposes only. Please refer to the Site Plan and Elevations for a more accurate depiction of the proposal

SRM Architects



Figure 16 – Design Renderings

3.3.2 Site Design

- The landscape strips fronting the property help to beautify the streetscape and reinforces entrances to the building and parking lot.
- Landscaping surrounds the perimeter of the site to provide for an appropriate buffer to the abutting Silver Creek Natural Heritage Corridor and the low-density residential uses.
- A concrete walkway provides pedestrian connections to and from building entrances, the parking lot and the public realm.
- To reduce views of extensive parking from the public realm, the parking lot is situated below grade and visitor parking is screened by the building.
- The loading area and underground parking ramp have been integrated into the side wall of the building so that they are not visible from the public realm.

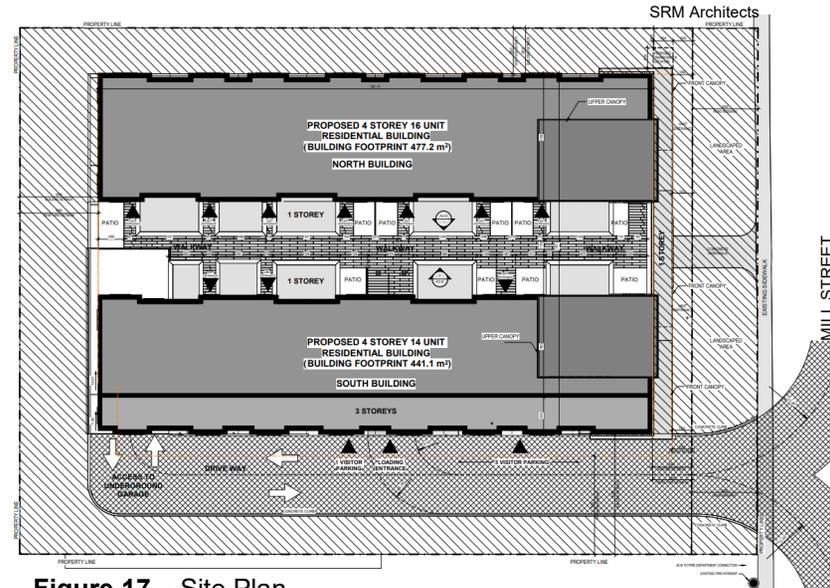


Figure 17 – Site Plan

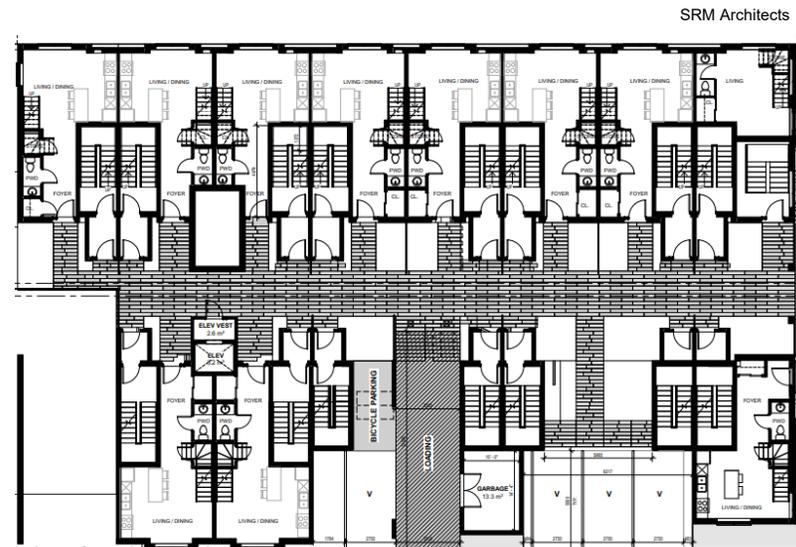


Figure 18 – Floor Plan: Outdoor Common Walkway

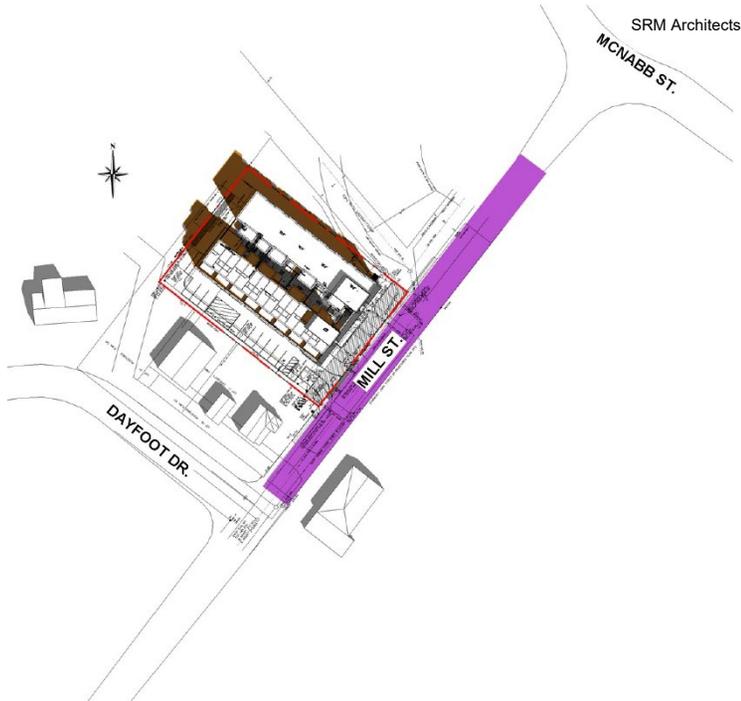


Figure 19 – Shadow Study

2 March 21st at 11:00am
1:1000

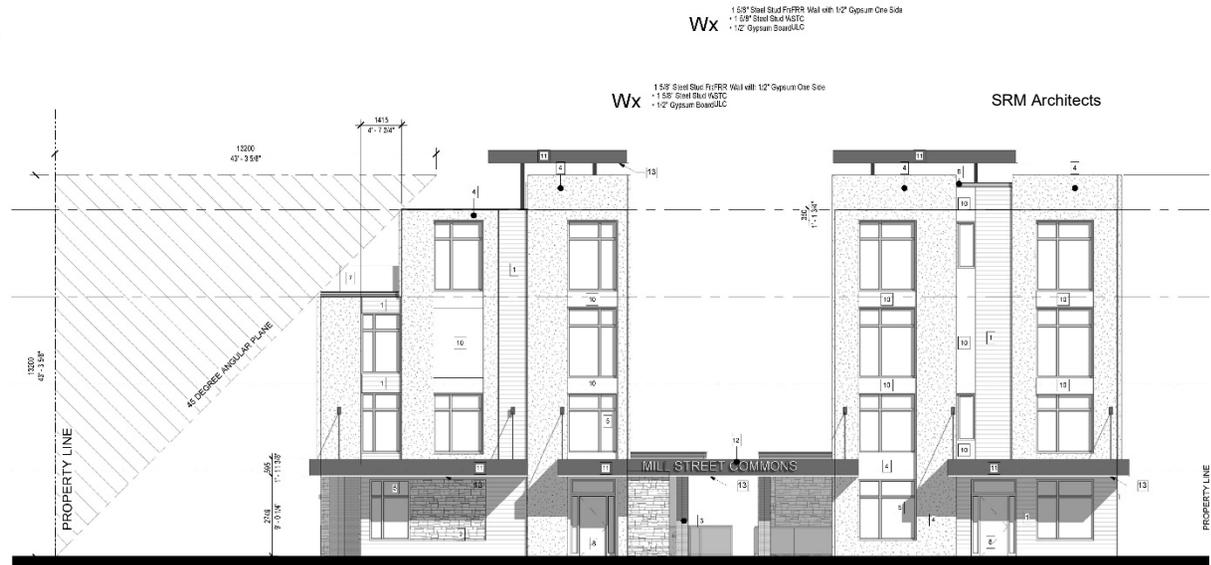


Figure 20 – Project East Elevation

SRM Architects
1 EAST ELEVATION (MILL ST.)
1:100

- The building has been sited towards the eastern boundaries of the property and the upper floors on the west building have been stepped-back to ensure appropriate setbacks from the abutting low-density residential uses.
- Shadow impacts of the proposed development will not impact the neighboring homes.
- The required 45-degree angular plane has been met to ensure appropriate building separation.

Part 4 – Policy Review

4.1 Overarching Policy Framework

The subject property is located within the Georgetown Go Station Secondary Plan which shares the boundaries of the Georgetown Major Transit Station Area (MTSA). The Provincial Policy Statement envisions the planning and development of complete communities, characterized by a well-designed and compact urban form in Designated Growth Areas such as the Georgetown MTSA. The Growth Plan for the Greater Golden Horseshoe seeks to align growth and transit by directing intensification to Major Transit Station Areas that can support the creation of complete communities that offer a variety of housing options. According to the Halton Region Official Plan, Major Transit Station Areas should contribute to a vibrant, diverse and pedestrian-oriented urban environment and provide opportunities for innovative urban design. In general, planning policy in Ontario have shifted towards promoting more compact development patterns in areas that can support growth. The proposed development responds to this objective by providing new energy, new residents and an invigoration of new life into an established low-density residential neighbourhood that is transitioning into a more urban and transit-supportive community.

As per the Georgetown Go Secondary Plan, the subject property is located within the Mill Street Corridor Precinct Area. The majority of the property falls within the 'Medium Density Residential' land use designation. A small corner sliver of the subject property abutting the 'Silver Creek Natural Heritage Corridor' falls within the 'Greenlands' land use designation. The proposed Official Plan Amendment would see the entirety of the subject property re-designated to 'Medium Density Residential', thus allowing for optimal site redevelopment, and as generally supported by the CVC. One of the key objectives of the Georgetown Go Station Secondary Plan is to ensure that new development is compatible, context sensitive and respectful of the character of Mature Neighbourhood Areas.

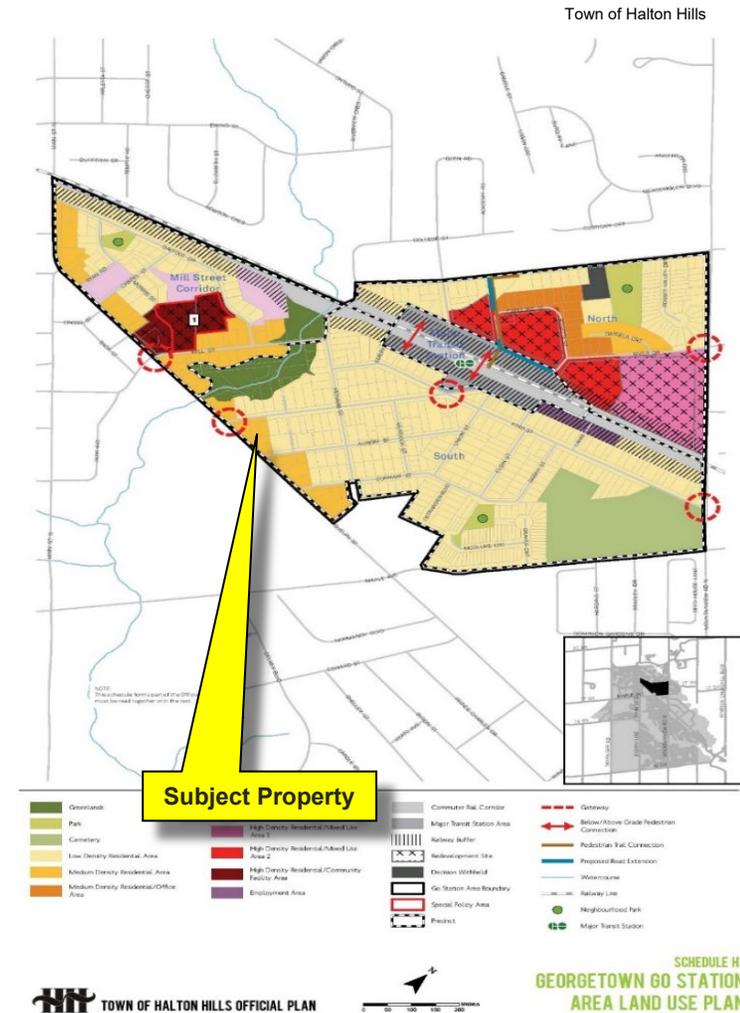


Figure 21 – Georgetown Go Station Area Land Use Plan

4.2 Response to Town's Urban Design Policies

As indicated in the Pre-Consultation Meeting Notes, the Town would like to see conformity with the urban design policies listed in **Sections F2, H3.3.6 and X4.2 of the Town of Halton Hills Official Plan**. The following includes responses that demonstrates adherence to those policies.

- **GENERAL DEVELOPMENT POLICIES: SECTION F2 URBAN DESIGN**

"F2.2.2.1 Site Design"

- Site design incorporates the built form of structures, landscaping, services and the layout of all amenities. Site design shall promote appropriate design relationship between the public realm, adjacent land uses, on-site operations and visual aesthetics, in order to promote an environment that is pleasant and attractive to the community.*
- Site design shall address compatibility between differing adjacent land uses in context of density, height and massing through appropriate site layout, building locations and landscape treatments.*
- The design of sites adjacent to parks, woodlots and watercourses shall be sensitive to these features. In these instances, appropriate setbacks shall be maintained between buildings and sensitive*

natural areas, while on-site landscaping shall be well integrated with natural areas.

- Continuous, highly visible, well-articulated and landscaped connections between building(s) and the street should be provided to establish appropriate pedestrian linkages between the sidewalk and building entrances, and generally improve access to public transit users.*
- On large sites, efforts shall be made to encourage pedestrian linkages between uses."*

Response to 'Site Design' Policies:

In terms of site design, the buildings will be sited close to the street frontage (post road widening dedication) to aid in the creation of a pedestrian-scaled and overall inviting environment. 52 parking spaces will be situated below grade while 4 visitor parking will be located (at grade). Centrally located between the two buildings, a common walkway will provide internal movement on the site, creating opportunities for community connections while shielding pedestrians from the movement of vehicles within the development.

A bench and bicycle rack in addition to landscape strips consisting of trees, shrubs, ornamental grasses and perennials is planned to be sited along the frontage of the property to help animate the streetscape and to frame the primary entrance to the building and parking lot. Appropriate setbacks that

incorporate sod/plantings will be maintained around the perimeter of the property, creating an appropriate buffer from the abutting uses including the Silver Creek Natural Heritage Corridor to the east and the low-rise residential uses to the north and west.

"F2.2.2.2 Building Design and Siting"

- The design of new buildings should achieve a complementary design relationship to existing buildings, while accommodating a diversity of architectural styles, building materials and colours, energy conservation techniques and innovative built forms.*
- The design of all buildings shall have regard to pedestrian safety and direct street access. Buildings should be massed to recognize pedestrian scale and provide an appropriate street wall height and street line and be architecturally articulated to provide visual variety and interest, yet be sensitive to high wind speeds, and long periods of shadowing. Generally, building articulation features such as canopies, cornice lines and varying façade materials should be used to reinforce pedestrian scale.*
- Buildings located at major vista terminations in their urban setting, or on view corridors within the development site, shall be given special treatment through the use of massing and building articulation strategies, such as added height, special roof treatments, and use of special cladding materials.*

- f) *Building functions that do not directly serve the public, such as loading areas, shall not face a public street and should be located away from noise sensitive land uses, such as residential areas, and buffered as necessary.*
- g) *Building should employ devices such as awnings, canopies, building cantilevers / overhangs to minimize uncomfortable high winds which may be associated with the height or placement of buildings, and generally improve the level of pedestrian comfort. Sheltered building entrances should be provided at primary building entrances to high-density residential, public, recreational, industrial, office and commercial buildings, where necessary.*
- h) *When development is located adjacent to existing, or planned residential areas, sufficient building setbacks should be provided to minimize potential height and massing impacts such as overlook, shadowing and high wind speeds. Massing strategies such as stepping down towards buildings of lower height should be employed to minimize potential impacts. Site Plan applications may be required to submit wind and/or shadow studies suites to address such potential conditions.*
- i) *Roofscape shall be an integral part of the design of a building and harmonize with the design of the rest of the building. On the basis, roof top mechanical units shall be organized and screened with complementary materials, colours, and finishes as necessary to provide a skyline with desirable visual attributes.*

- j) *Freestanding buildings for retail, restaurants and services, as well as office and residential buildings, shall be located at the street edge where possible, to encourage their use by pedestrians.*
- l) *Building entrances shall be located to be visible from the adjoining street(s) and, where possible, directly linked to the sidewalks through appropriately articulated walkways.”*

Response to ‘Building Design and Siting’ Policies:

The new development will bring about a more compact form of land use to reinvigorate the existing residential neighborhood which is currently transitioning to a more urban and transit-supportive community. The increased density and modern style of the new buildings is reflective of this change which will work to complement the evolving character of the neighborhood, and meet Town policies.

The development site abuts greenspace that forms part of the Silver Creek Natural Heritage Corridor. This greenspace will be home to a future trail connection and parkette (West Side Park). The property is also the first developable lot on the north side of Mill St when coming west from McNabb St. To address the site’s increased visibility, a diversity of building materials, colours and architectural treatments have been incorporated into the building’s design to promote an attractive up-scale development.

In order to aid in the creation of a pedestrian friendly environment, the building has been brought close to the street’s edge (post road widening dedication). Additionally, the entrance to the walkway which provides access to individual units is both visible from the street (*CPTED principle*) and directly linked to the future public sidewalk. Large windows and balconies wrap around the perimeter of the both buildings to promote eyes on the street. Overhangs have also been incorporated into the building to provide for weather shelter. Furthermore, the building’s varied massing configuration also helps to create an articulated roofscape and to break up façades through the use of step-backs and appropriate gaps. The upper levels of the building on the eastern edge of the property have been stepped back to eliminate shadow impact on the adjacent low-density residential uses and to provide for an attractive 45 degree angular plane.

“F2.2.2.4 Landscaping

- a) *Landscaping is a major contributor to a vibrant streetscape. A high quality of landscape design shall be required to enhance the visual aesthetics of development and to enhance the site and land use compatibility.*
- b) *Landscaping within private lands shall be complementary to streetscape design and materials within the public realm.*

- c) *Where appropriate, planted landscaping strips and fencing shall be used to buffer development from adjacent uses and mitigate on-site operational activities such as loading and waste facilities.*
- d) *Landscape materials shall be selected for their aesthetic, ecological, disease-tolerance and maintenance characteristics.*
- i) *Native, non-invasive species shall be used on lands that are adjacent to the Greenlands System.”*

Response to ‘Landscape’ Policies:

A landscape strip consisting of ornamental grasses, perennials, shrubs and deciduous trees will be sited along the frontage of the property. The landscape strip will contribute to the visual aesthetics of the Mill St corridor and will help to interface with the abutting Silver Creek Natural Heritage Corridor to the east. A landscape strip will also be provided next to abutting greenspace which forms part of the Natural Heritage Corridor and Greenlands System to act as a vegetative buffer.

“F2.2.2.5 Parking

- a) *The location of parking is a major determinant for the layout of a development that is pedestrian friendly and transit supportive. Were appropriate, the Town shall encourage the provision of surface parking areas in locations not visible from the public street, such as in rear yards and/or well-landscaped side yards.*

- b) *Clearly defined pedestrian accesses between parking and adjacent buildings and entrances should be provided with well-delineated walkways using decorative paving surfaces.*
- c) *Surface parking lots shall be linked to the streets and other public areas with well-delineated walkways, utilizing decorative paving treatments.”*

Response to ‘Parking’ Policies:

The majority of parking will be located underground with the exception of 4 surface visitor parking spaces on the ground level that will be hidden from the public realm because of their strategic placement into the building wall. The limited surface parking has been located with convenient ground level access to the internal private sidewalk which provides connections to the entrances of both buildings and the public sidewalk. The private walkway also shields pedestrians from on-site traffic movement.

“F2.2.2.6 Signage, Display Areas and Lighting

- c) *All lighting shall be internally oriented so as not to cause glare on adjacent properties or public roads. Outdoor lighting fixtures that reduce energy consumption and direct light away from the night sky shall be encouraged.*

Response to ‘Signage, Display Areas and Lighting’ Policies:

The lighting fixtures will contribute to the aesthetics of the streetscape and the proposed buildings while also enhancing feelings of safety and security (*CPTED Principle*). Glaring effects will be minimized by light shields to reduce adverse impacts on adjacent properties and the night sky as demonstrated in the supporting Photometric Plan.

“F2.2.2.7 Services, Utilities, Outside Processing and Storage

- a) *Site and building services and utilities such as waste storage facilities, loading, air handling equipment, shall be located and/or screened from public streets and adjacent residential areas or other sensitive land uses, in order to buffer their visual and operational effects. Waste storage areas should be integrated into the main building on the lot. Waste storage areas external to the main building shall be enclosed and shall not face a public street.*
- b) *Site access, service areas and loading areas shall be located away from the streets so as to minimize disruption or conflicts with adjacent land uses, sidewalks and both on-site, and off-site, pedestrian routes and shall be visually screened as necessary from public views. Screening areas should be buffered from noise impacts, particularly when located against residential areas. Buffering strategies include berms, tree and shrub planting and opaque noise walls and fences.”*

Response to 'Services, Utilities, Outside Processing and Storage' Policies:

Building services and waste storage will be located inside the building. The loading area and underground parking ramp will be integrated into the exterior side wall of the building so that they are not visible from the public realm.

"F2.2.2.8 Access and Circulation

- a) *The number of vehicular access points into their site and their width could potentially have detrimental effects on street frontages by reducing available areas for landscaping and by creating large expanses of asphalt. On this basis, joint access driveways shall be considered on adjacent sites.*
- b) *To ensure safety and promote their priority over vehicular traffic, major pedestrian routes on the site should be identified and delineated with paving materials that differentiate them from the driving surfaces. Pedestrian walkways should be made continuous across driving aisles as well as across driveway entrances at the street. The use of soft landscaping is also encouraged along major pedestrian routes.*
- c) *Generously sized walkways shall be provided along buildings, particularly in areas with large pedestrian traffic. These walkways should be connected to other pedestrian routes on the site and linked to major pedestrian entry points at the street, and where appropriate to adjacent developments."*

Response to 'Access and Circulation' Policies:

To maintain a pedestrian friendly frontage, only one driveway access has been proposed in the new site design. The driveway will connect to the underground parking area and it will be wide enough to allow for larger vehicles to pull in and out of the loading space area. A continuous walkway will be situated in the interior of the site to provide connections between the public sidewalk and individual unit entrances.

• GEORGETOWN GO STATION AREA SECONDARY PLAN: SECTION H3.3.6 URBAN DESIGN

"d) Building Elements

- i) *materials and colours of new buildings and additions or renovations to existing structures should complement and be compatible with adjacent buildings;*
- ii) *The Facades of buildings should have windows which overlook streets, lanes and parking areas in order to maintain a safe pedestrian environment;*
- iii) *The main access to buildings or a group of buildings should face a street;*
- iv) *pedestrian entrances should be spatially and architecturally prominent and welcoming;*
- vi) *Blank walls are discouraged in situations which are exposed to public view. Where*

such walls are essential upgraded design and material standards are required; and,

- vii) *Fine architectural detailing in building facades should be part of any new building or major addition to complete the streetscape."*

Response to 'Building Elements' Policies:

The proposed development will contribute to the improvement of Mill St by introducing new building materials, colours, architectural elements and an elevated built form that seeks to complement the traditional architectural styles found in the area. The proposed development will also create a varied streetscape that will result in a more diverse and attractive community. Crime Prevention Through Environmental Design (CEPTED) Principles have been incorporated throughout the building design, highlighting the use of a publicly visible and prominent front entrance, large windows and balconies with a focus on promoting 'eyes on the street' and 'natural surveillance' for common areas of the development. Varying level of architectural treatments will be used to promote visual interest while reducing monotony within the streetscape.

"e) Landscape, Parking and Service Areas

- i) *Surface parking lots should be screened from abutting streets through the use of low walls, decorative fences, planters, low shrubs or other landscaping, without*

compromising sightlines or maintenance flexibility.

- ii) *Surface parking areas should be visually softened by introducing landscape islands, planters, low shrubs, or other landscaping, without compromising sightlines or maintenance flexibility.*
- iii) *All parking areas should be well lit to ensure pedestrian safety, but lighting should be designed to avoid light spillage into adjacent areas.*
- iv) *Ramps to underground parking areas should not detract from the façade or landscaping of the building.*
- v) *Parking shall generally not be permitted between buildings, other than a driveway for low and medium density residential uses, and the street. Landscape planting shall be provided between the street and the building to frame the building.*
- vi) *Planting of shade trees shall be encouraged.*
- vii) *Service and loading areas should be located away from the primary face and public view and integrated with buildings.*
- viii) *Appropriate screening of service and loading areas should be incorporated.”*

Response to ‘Landscape, Parking and Service Areas’ Policies:

With the exception of the singular driveway on the site, very little on-site parking or services will be visible from the public realm as they have been located below grade or

strategically shielded through by the built form of the proposed development. Appropriate lighting levels and ceiling paint colour will be utilized to ensure safe movement and access to surface parking and underground parking areas while minimizing the potential for light pollution to neighbouring properties. All of which has been tastefully landscaped offering an additional level of screening along the frontage of the property to help block views from the streetscape.

- **TOWN OF HALTON HILLS URBAN DESIGN GUIDELINES: SECTION X4.2 GO STATION DISTRICT**

X.4.2.1 BUILT FORM

“X4.2.1.2 Building Height and Massing

- *In the North and South Precincts, building height and massing should have regard to the scale, shadow impact, and privacy impact of adjacent properties.”*

Response to ‘Building Height and Massing’ Policies:

To ensure compatibility with the abutting and transitioning residential uses, the proposed building will be setback from the eastern edge of the property and the upper levels of the building will be stepped back to reduce shadow impact. In addition, the required 45-degree angular plain stepback has also been met.

“X4.2.1.3 Building Elements

- *Materials and colours for new buildings, and for additions or renovations to existing structures, should complement and be compatible with adjacent buildings.*
- *The facades of buildings should have windows, which overlook streets, lanes and parking areas, in order to maintain the character if a safe pedestrian environment.*
- *The main entrance of the residential buildings should have direct access facing the street.*
- *Pedestrian entrances to both single and multiple residential buildings should be spatially and architecturally prominent and welcoming.*
- *Blank walls demonstrating no specific architectural design are discouraged. Where such facades exist, the use of murals or landscaping to screen walls and provide visual interest will be encouraged.*
- *The use of reflective (mirror) glass should be discouraged.*

Response to ‘Building Elements’ Policies:

The proposed buildings will add visual diversity to the streetscape by introducing new compatible colours, materials and architectural elements that complement the existing streetscape character. Although the residents will enter the buildings through private entranceways located internally

within the site, the primary courtyard entrance will be orientated towards the street and be complemented by pillars, large windows and landscape elements to reinforce the primary entrance's prominence and the overall pedestrian-friendly character of the site/community.

"X.4.2.3 LANDSCAPE"

- *Planting Strips with minimum widths of 4.5 metres should be provided between the street line and parking lots.*
- *Low fencing, combined with low shrubs, may be used along property lines to screen/protect parked vehicles, and also to provide visual interest.*
- *Decorative fences higher than 1.2 metres, or continuous planting of tall shrubs and coniferous trees, which obscure pedestrian views, should be discouraged.*
- *On sites where buildings are to be located closed to the front lot line and no parking in front of the building is proposed, landscape planting will be required in order to frame the building. Alternatively, the construction of low metal and masonry fences to define the site will be considered and it should be coordinated with the overall streetscape design.*
- *Shrubs should cover a minimum 50% of the planting strip.*
- *Screen planting, where provided, should cover a minimum of 50% of the planting strip area and should form a continuous visual screen between properties."*

Response to 'Landscape' Policies:

Landscape strips have been provided at the street frontage to help frame both pedestrian and vehicular entrances. Landscaping elements will serve to promote a continuous landscaped street frontage that interfaces with the Silver Creek Natural Heritage Corridor.

"X.4.2.5 ACCESS AND CIRCULATION"

X.4.2.5.1 Vehicular Access and Circulation

- *Access into, and circulation within, an individual site should provide safe and well-defined routes.*
- *Property light, landscaping, and pedestrian amenities along the circulation routes will enhance the overall site appearance, promote public safety, and encourage the use of GO transit. Reference should be made to the relevant Regional and municipal documents in regard to safety"*

Response to 'Access and Circulation' Policies:

In terms of pedestrian access and circulation, there will be a continuous concrete walkway that will connect the public sidewalk to building entrances, the surface parking, loading area and the underground parking. Lighting and landscape features such as a bench and plantings will help to improve the walkway's appearance and promote feelings of public safety for

pedestrians. The primary pedestrian walkway will be centrally located between the two buildings. By centrally locating the principal walkway and pedestrian entrance, vehicular movement will be separated offering an additional element safety.

In terms of vehicular access and circulation, there is only one access point to the site and it's from Mill St. Access to the underground parking ramp and loading area are located closer to the rear end of the property for screening and turning movement purposes.

"X.4.2.6 PARKING AND SERVICE AREAS"

- *Parking areas that abut streets should be screened through the use of attractive low walls, decorative fences, planters, low shrubs, or rows of smaller street trees.*
- *Large surface parking areas should be visually softened by introducing street trees, planters and clearly defined pedestrian routes.*
- *Deciduous tree planting should provide definition to the street, shade opportunities, wind control or become part of a visually improving strip.*
- *Coniferous trees should provide wind control and become part of a visually improve planting strip.*
- *Shrub plants shall be used for wind control, visual screening and become part of a visually improve d planting strop. Caution is made that any planting should not*

encourage places for hiding near pedestrian routes.

- *Designated barrier-free parking spaces should be located as close as possible to barrier-free access routes and building entrances.*
- *A clearly defined pedestrian access route shall continue through all*
- *driveways into parking areas and facilities.*
- *Surface parking areas should be well lit to ensue pedestrian safety.*
- *Surface parking areas for the Go station or high density residential uses should avoid light spillage to the adjacent properties.*
- *Service (including exterior garbage storage and recycling containers) and loading areas should be located away from the primary building face and the public view, preferable in the rear or side yard of the building and integrated within the building, where possible.*
- *Appropriate screening of service and loading areas should be incorporated using landscaping, built screens, or a combination of both. Any building materials used to create screens should relate to those of the primary building exterior."*

Response to 'Parking and Service Areas' Policies:

The surface parking area (limited to 4 visitor spaces) will be well lit and physically connected to the internal walkway to provide pedestrian access to the building entrances

and the public sidewalk. An adequate amount of Type 'A' and Type 'B' accessible parking spaces have been provided. A landscape strip will be situated along the frontage of the property to reinforce the entrance to the driveway while also helping screen views from the public realm.

Part 5 – Conclusion

The subject property is located within a Major Transit Station Area. The proposed development would result in a more compact built form on an underutilized lot that is of sufficient size to accommodate a 4-storey 30-unit residential development. The new development is well-aligned with other redevelopment initiatives occurring along the Mill Street Corridor that seek to transform the evolving neighbourhood into a more urban and transit-supportive community. The proposed development also meets Regional and Town policies for the area. Design considerations have been made to ensure that the project is compatible with and complements both the historic character of the neighbourhood and the natural features found in the surrounding area. These matters have been addressed in our response to the Town's urban design policies. To conclude, it is the author's professional opinion that the proposed development is a worthwhile project that will provide positive community outflows, improved neighbourhood character (over the

existing site use), strengthen the social community, increase the local consumer base, better integrate vehicular movements, and an increase in municipal tax base, all within an appropriate and policy mindful development.



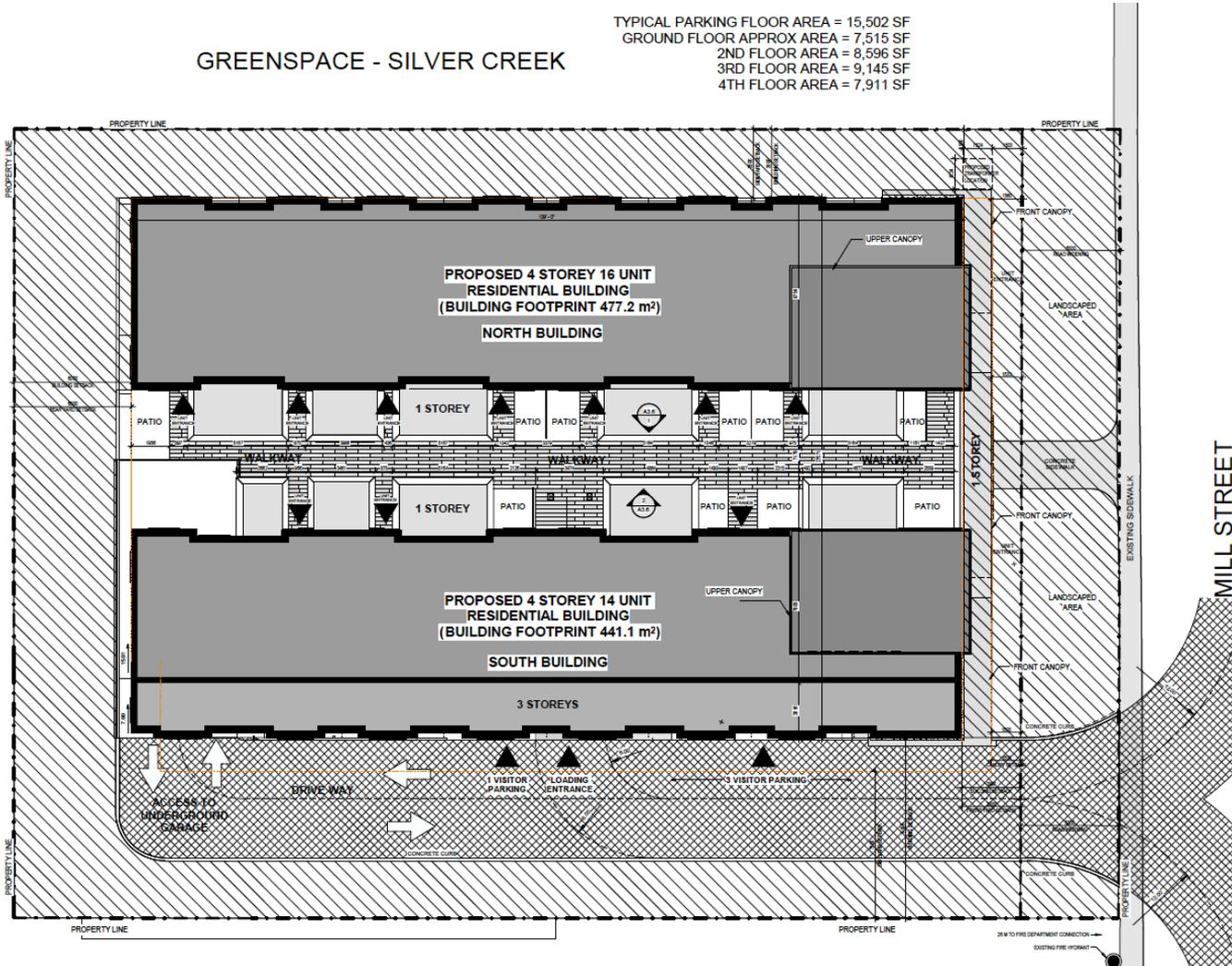

Terrance Wm. Glover, RPP, CPT
Registered Professional Planner

Principal
Urban in Mind Planning Consultants

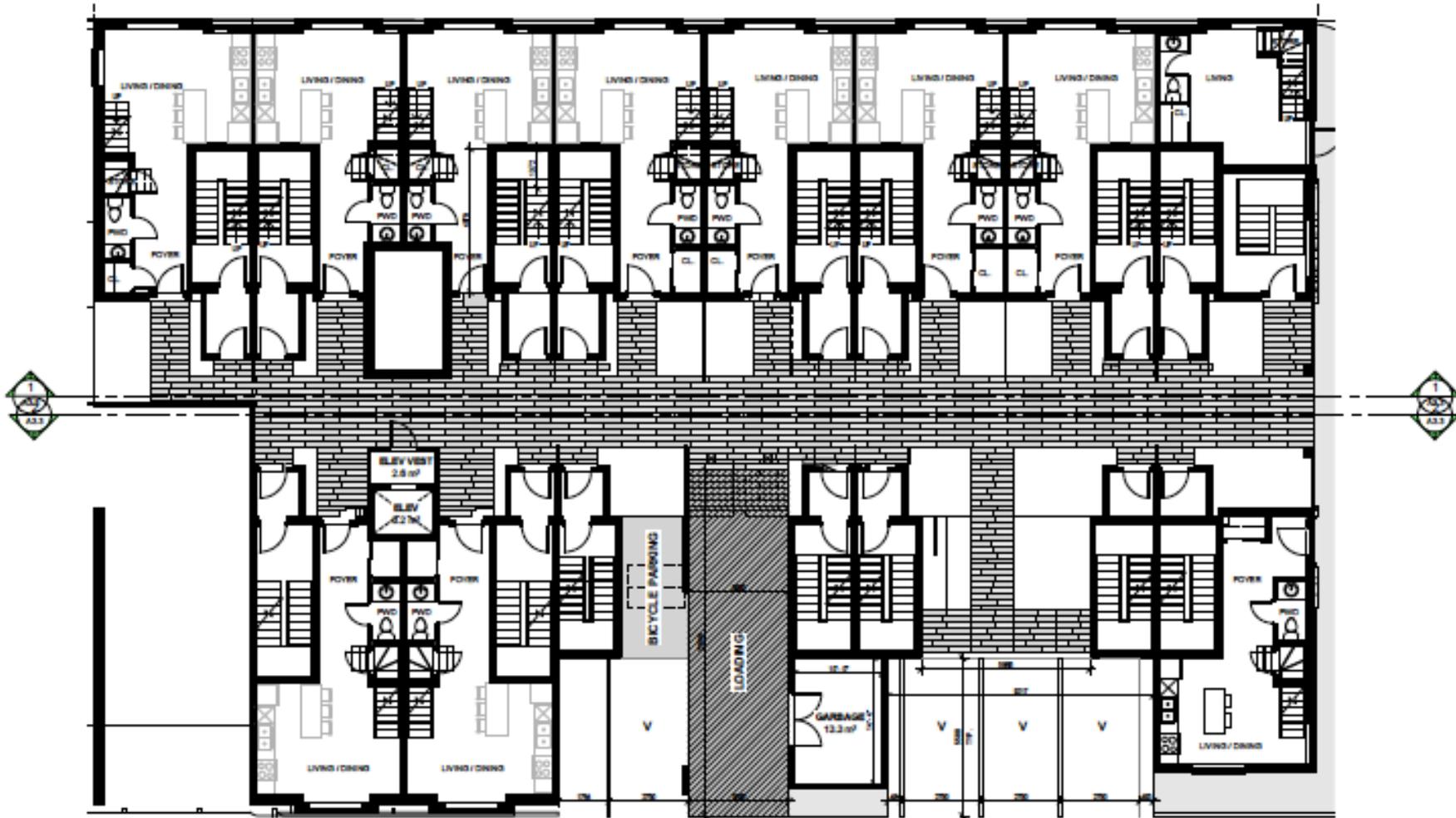
Part 6 – Appendices Appendix A: Site Plan

GREENSPACE - SILVER CREEK

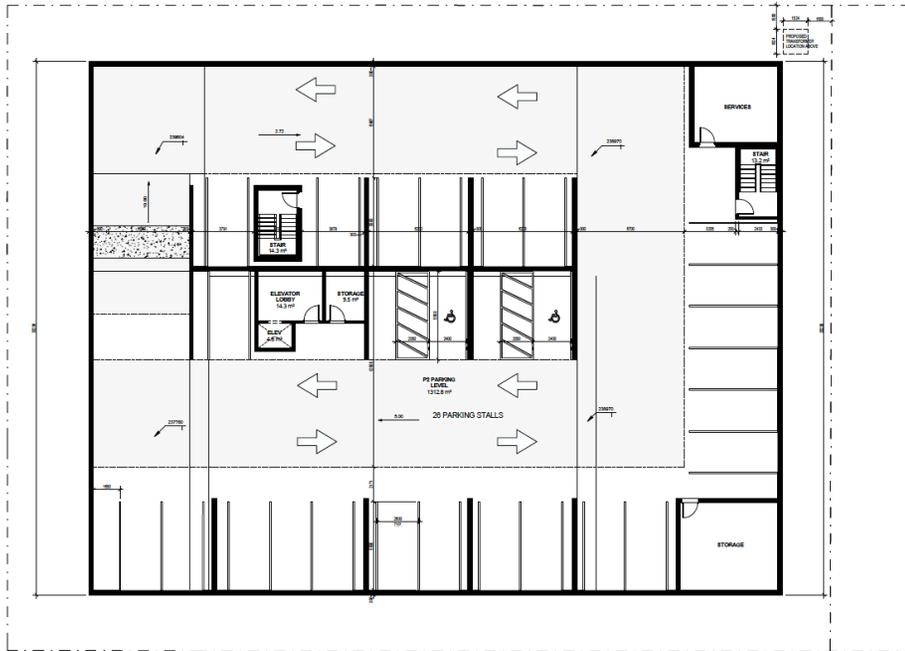
TYPICAL PARKING FLOOR AREA = 15,502 SF
 GROUND FLOOR APPROX AREA = 7,515 SF
 2ND FLOOR AREA = 8,596 SF
 3RD FLOOR AREA = 9,145 SF
 4TH FLOOR AREA = 7,911 SF



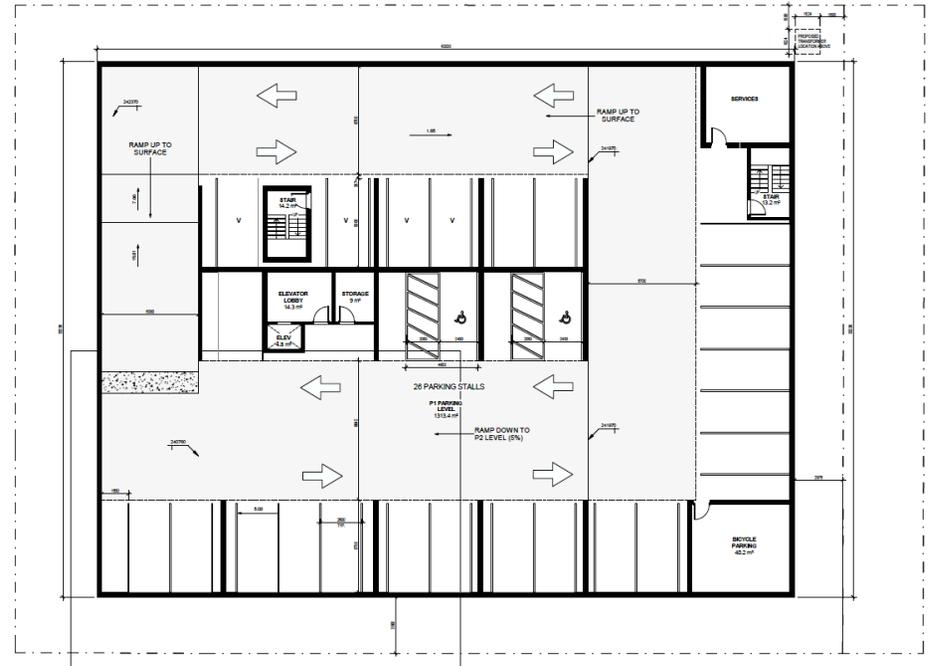
Appendix B: Ground Floor Plan



Appendix C: Underground Parking

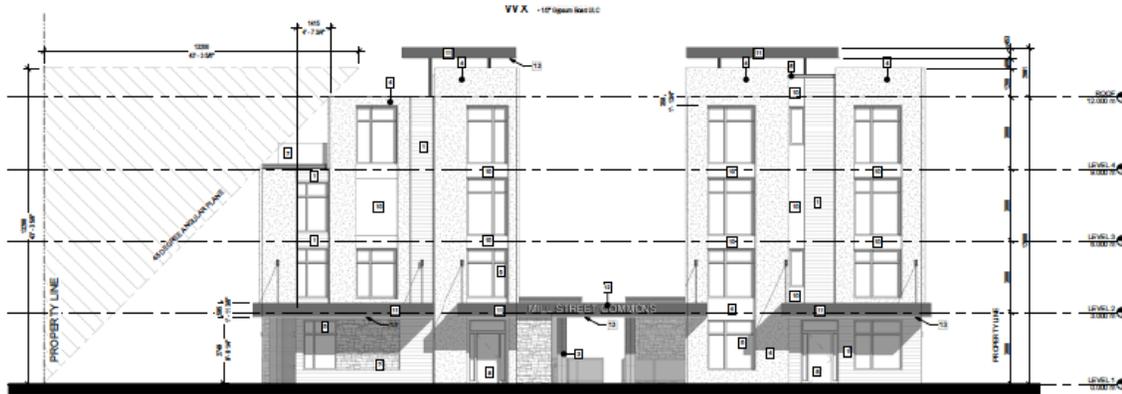


① P2 PARKING LEVEL
1/100

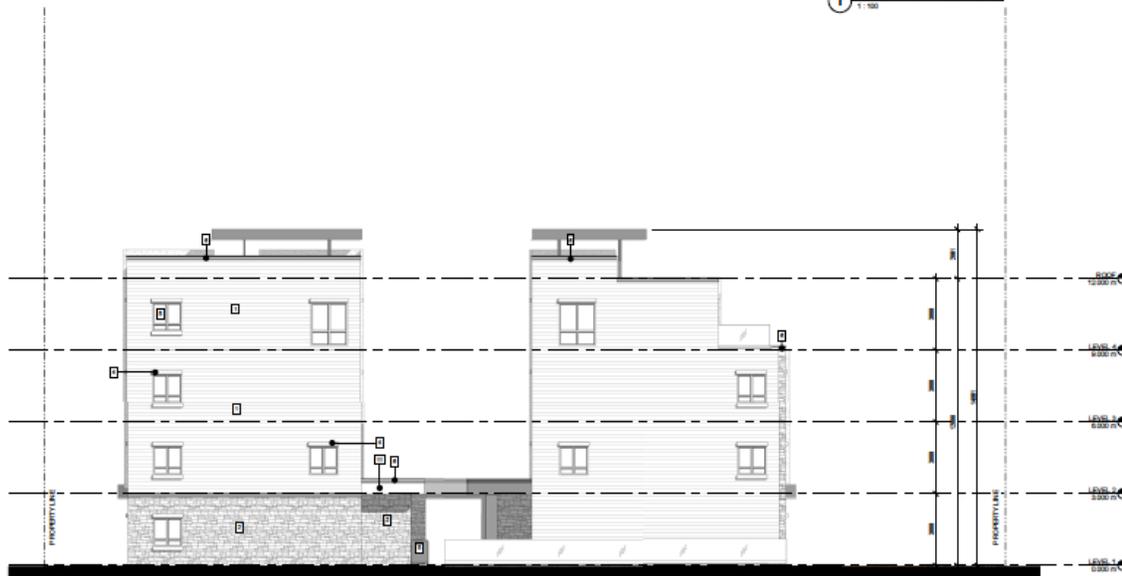


① P1 PARKING LEVEL
1/100

Appendix D: Elevations



1 EAST ELEVATION (MILL ST.)
1/100



2 WEST ELEVATION
1/100



2 SOUTH ELEVATION
1:50

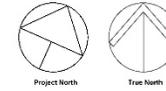
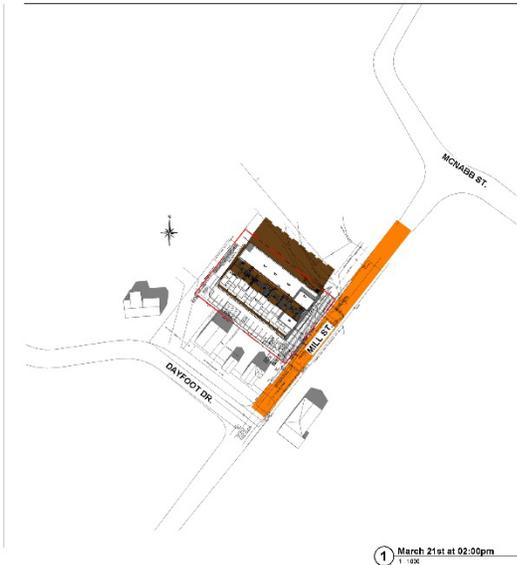


1 NORTH ELEVATION
1:50

Appendix E: Conceptual Design Renderings



Appendix F: Shadow Study



GENERAL NOTES

- DO NOT SCALE DRAWINGS. WRITTEN DIMENSIONS SHALL HAVE PRECEDENCE OVER SCALE DIMENSIONS.
- ALL WORK SHALL COMPLY WITH THE 2012 ONTARIO BUILDING CODE AND REGULATIONS.
- THE USER HEREBY AGREES TO BE RESPONSIBLE FOR THE DIMENSIONS AND RECORDS AND REPORT ANY DISCREPANCIES TO US IMMEDIATELY BEFORE PROCEEDING WITH THE WORK.
- ALL DIMENSIONS AND RECORDS MUST BE CHECKED AND HAVE A SET OF APPROVED CONTRACT DOCUMENTS ON SITE AT ALL TIMES.
- ALL DIMENSIONS SHALL BE IN ACCORDANCE WITH THE ARCHITECT'S REQUIREMENTS AND RECORDS FOR THE DIMENSIONS AND RECORDS OF THESE DOCUMENTS'S REQUIREMENTS. THE USER HEREBY AGREES TO BE RESPONSIBLE FOR THE DIMENSIONS AND RECORDS OF THESE DOCUMENTS'S REQUIREMENTS PREPARED BY THE CONSULTANT FOR THE ACCOUNT OF THE USER.
- THE USER SHALL BE RESPONSIBLE FOR THE LOCAL JURISDICTION'S REGULATIONS. THE USER SHALL BE RESPONSIBLE FOR THE INFORMATION AVAILABLE TO HIM/HER IN THE FIELD OF PROFESSIONAL DESIGN SERVICES. THE USER SHALL BE RESPONSIBLE FOR THE INFORMATION AVAILABLE TO HIM/HER IN THE FIELD OF PROFESSIONAL DESIGN SERVICES. THE USER SHALL BE RESPONSIBLE FOR THE INFORMATION AVAILABLE TO HIM/HER IN THE FIELD OF PROFESSIONAL DESIGN SERVICES.
- THE CONSULTANT ACCEPTS NO RESPONSIBILITY FOR DAMAGES, IN ANY FORM, RESULTING FROM ANY ACTION BASED ON THE CONTRACT DOCUMENTS.

- 10:00 AM Proposed Development Daylight
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- 12:00 PM Proposed Development Daylight
- 01:00 PM Proposed Development Daylight
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1 2022-04-11 ISSUED FOR PERMITS

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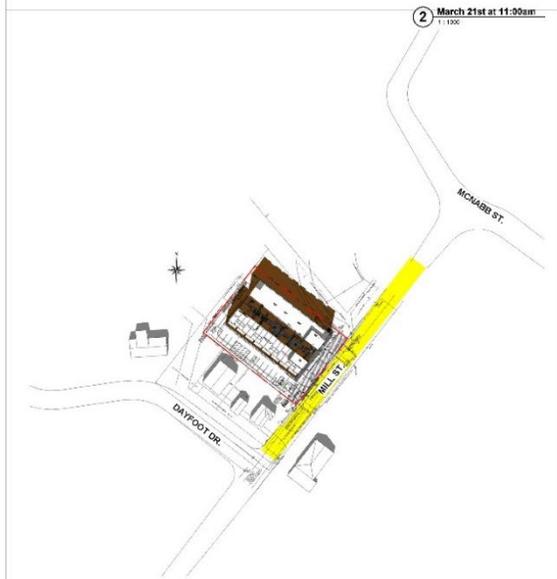
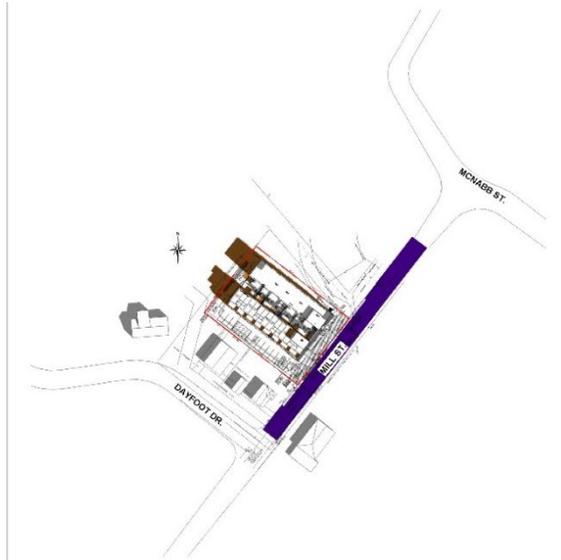
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Project Status	MEP
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16 & 18 MILL ST.
GEORGETOWN

SHADOW STUDY -
MARCH 21st



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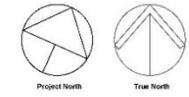


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3 March 21st at 12:00pm
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4 March 21st at 01:00pm
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GENERAL NOTES

- DO NOT SCALE DRAWINGS, DO NOTLY ANALYZING OR ALL THIS INCLUDING VISUAL ANALYSIS.
- ALL WORK SHALL COMPLY WITH THE 2012 ONTARIO BUILDING CODE AND REGULATIONS.
- CONTRACTOR SHALL OBTAIN ALL NECESSARY PERMITS AND INSURANCE AND OPERATIONAL BEFORE ANY DISBURSEMENT TO THE ARCHITECT BEFORE PROCEEDING WITH THE WORK.
- ALL CONTRACTORS AND SUB CONTRACTORS SHALL OBTAIN ALL NECESSARY PERMITS AND INSURANCE BEFORE COMMENCING WORK.
- ALL DOCUMENTS BEING THE PROPERTY OF "URBAN IN MIND" ARCHITECTS INC. ANY REPRODUCTION, DISTRIBUTION OR USE WITHOUT THE WRITTEN PERMISSION OF THE ARCHITECT IS STRICTLY PROHIBITED BY THE CONTRACT DOCUMENTS AND SHALL BE AT THE SOLE RESPONSIBILITY OF THE CONTRACTOR.
- THE MATERIAL CONTAINED HEREIN REFLECTS THE CONSULTANT'S BEST JUDGEMENT IN LIGHT OF THE INFORMATION AVAILABLE TO HER AT THE TIME OF PREPARATION AND SHALL BE SUBJECT TO THE REVIEW OF THE CONSULTANT'S QUALITY CONTROL AND SHALL BE THE SOLE RESPONSIBILITY OF SUCH FIELD OFFICES.
- THE CONSULTANT ACCEPTS NO RESPONSIBILITY FOR DAMAGE OR LOSS SUFFERED BY ANY THIRD PARTY AS A RESULT OF THE INFORMATION OR ACTION BASED ON THE CONTRACT DOCUMENTS.

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2022-04-11 REVISION FOR 02/25/24		
No.	Date	Revision


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 PROJECT Start 2022-04-11
 ARCHITECT DM
 CONSULTANT MEG
 REVISION 02/25/24 10:19:42 AM

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GEORGETOWN

SHADOW STUDY -
MARCH 21st

ONTARIO ASSOCIATION OF ARCHITECTS
 An Incorporated Society
 REG. NO. 10000
 REG. NO. 10000
 REG. NO. 10000
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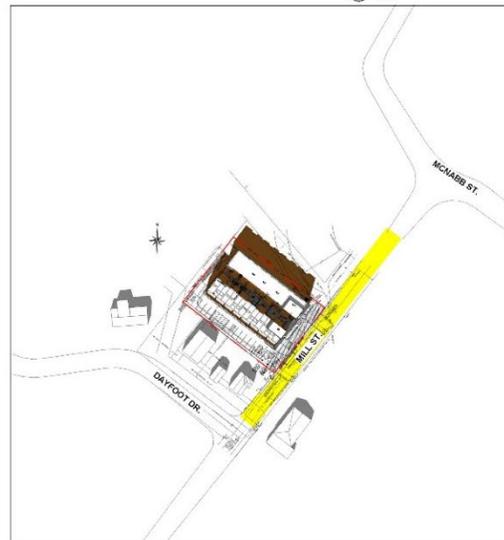
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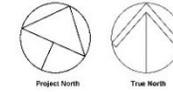
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- GENERAL NOTES**
- DO NOT SCALE DIMENSIONS. ANY TYPED DIMENSIONS SHALL HAVE PRIORITY OVER SCALE DIMENSIONS.
 - ALL WORK SHALL COMPLY WITH THE 2022 ONTARIO BUILDING CODE AND REGULATIONS.
 - CONTRACTOR SHALL OBTAIN ALL NECESSARY PERMITS AND APPROVALS FROM ALL APPLICABLE AGENCIES. ANY MODIFICATIONS TO THE PROJECT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR.
 - ALL DIMENSIONS ARE TO FACE UNLESS OTHERWISE NOTED. ALL DIMENSIONS SHALL BE TO THE CENTERLINE OF THE ROAD.
 - ALL DOCUMENTS BEING THE PROPERTY OF THE ARCHITECT. LEND, LOAN, REPRODUCE, COPY, REPRODUCTION OR DISTRIBUTION OF THESE DOCUMENTS WITHOUT THE WRITTEN PERMISSION OF THE ARCHITECT IS STRICTLY PROHIBITED. ANY CONTRACT DOCUMENTS PREPARED BY THE CONTRACTOR FOR THE ACCOUNT OF THE OWNER.
 - THE MATERIAL CONTAINED HEREIN REFLECTS THE CONSULTANTS BEST AND SOBEREST JUDGMENT IN LIGHT OF THE INFORMATION AVAILABLE TO THEM AT THE TIME OF PREPARATION. THE CONSULTANTS SHALL NOT BE RESPONSIBLE FOR THE CONSTRUCTION OF THE PROJECT OR THE PERFORMANCE OF THE PROJECT OR THE RESULTS THEREOF. THE CONSULTANTS SHALL NOT BE RESPONSIBLE FOR THE CONSTRUCTION OF THE PROJECT OR THE PERFORMANCE OF THE PROJECT OR THE RESULTS THEREOF.
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No.	Date	Revision
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Project No.	20002
Project Name	2022-04-11
Client	EM
Contractor	MES
Issue Date	2022-04-11 10:23:01 AM

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GEORGETOWN

SHADOW STUDY -
SEPTEMBER 21st



Member No.	As indicated
Member Name	SFA
Member Address	1000

SD3.5 - r1



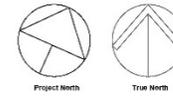
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GENERAL NOTES

1. DO NOT SCALE DRAWINGS. WRITTEN DIMENSIONS SHALL HAVE PRIORITY OVER DATA TO DIMENSIONS.
2. ALL WORK SHALL COMPLY WITH THE 2019 CANADIAN BUILDING CODE AND AMENDMENTS.
3. CONTRACTORS MUST CHECK AND VERIFY ALL DIMENSIONS AND SPECIFICATIONS AND NOTIFY ANY DISCREPANCIES TO THE ARCHITECT IMMEDIATELY UPON RECEIVING THEM IN WRITING.
4. ALL CONTRACTORS AND SUB-CONTRACTORS SHALL BE A PART OF ANY APPROVED CONSTRUCTION DOCUMENTS WITH ONE (1) AT A TIME.
5. ALL DOCUMENTS REMAIN THE PROPERTY OF THE ARCHITECT. UNAUTHORIZED USE, REPRODUCTION, AND/OR REPRODUCTION OF THESE DOCUMENTS IS PROHIBITED WITHOUT THE WRITTEN PERMISSION OF THE ARCHITECT. ANY REPRODUCTION OF THESE DOCUMENTS WITHOUT THE WRITTEN PERMISSION OF THE ARCHITECT SHALL BE AT THE USER'S SOLE RISK AND LIABILITY.
6. THE ARCHITECT'S CONTRACTORS SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND APPROVALS FROM ALL APPLICABLE AGENCIES. ANY USE OF A THIRD PARTY IMAGES OF THE CONSTRUCTION DOCUMENTS OR ANY REPRODUCTION OF THESE DOCUMENTS SHALL BE AT THE USER'S SOLE RISK AND LIABILITY.
7. THE OWNER AND ALL CONTRACTORS SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND APPROVALS FROM ALL APPLICABLE AGENCIES. ANY USE OF A THIRD PARTY IMAGES OF THE CONSTRUCTION DOCUMENTS OR ANY REPRODUCTION OF THESE DOCUMENTS SHALL BE AT THE USER'S SOLE RISK AND LIABILITY.

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1. 2022-04-11 ISSUED FOR DESIGN

No.	Date	Revision

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	Year: 2022-04-11
	Year: DM
	Year: MES
	Year: 2022-04-11 10:32:15 AM

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SEPTEMBER 21st

